GENERAL NOTES

- 1. ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE CURRENT STANDARDS, SPECIFICATIONS AND GENERAL CONDITIONS OF THE CITY OF DETROIT, AND ANY/OR OTHER AGENCIES HAVING JURISDICTION.
- 2. UTILITY INFORMATION SHOWN ON THESE PLANS WAS OBTAINED FROM UTILITY OWNERS AND THEREFORE MAY NOT BE ACCURATE OR COMPLETE. THE CONTRACTOR SHALL VERIFY AND OBTAIN ANY INFORMATION NECESSARY REGARDING THE PRESENCE OF UNDERGROUND UTILITIES WHICH MIGHT HAVE AN IMPACT ON THIS PROJECT, AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES WHETHER THEY ARE SHOWN OR NOT ON THE PLANS.
- 3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AT PROPOSED CONNECTIONS AND/OR CROSSINGS, AND TO NOTIFY THE ENGINEER OF ANY DISCREPANCIES TO THESE PLANS.
- 4. 72 HOURS PRIOR TO EXCAVATION, THE CONTRACTOR SHALL CONTACT MISS DIG AT (800) 482-7171 FOR THE LOCATION OF UNDERGROUND GAS AND CABLE FACILITIES, AND SHALL ALSO NOTIFY REPRESENTATIVES OF OTHER UTILITIES LOCATED IN THE VICINITY OF THE WORK.
- 5. ALL PERMITS REQUIRED SHALL BE OBTAINED BY THE CONTRACTOR. ALL PERMIT FEES, BONDS, AND INSURANCE REQUIRED BY THE ISSUING AGENCIES SHALL BE PROVIDED BY THE CONTRACTOR, AND MUST BE KEPT CURRENT. THE CONTRACTOR IS RESPONSIBLE FOR ALL OTHER FEES, INSPECTION COSTS, ETC., AND SHALL
- ADHERE TO ALL REQUIREMENTS SET FORTH IN SAID PERMITS. 6. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SECURING ALL WORK AREAS TO ENSURE THE SAFETY OF ALL OCCUPANTS, VISITORS, PEDESTRIANS, WORKERS, ETC. THE CONTRACTOR SHALL REPAIR AND MAINTAIN ALL CONSTRUCTION FENCING AS NECESSARY.
- 7. THE CONTRACTOR SHALL PROVIDE FOR CONTROLLED ACCESS TO THE SITE FOR USE BY THE VARIOUS WORK FORCES, EMERGENCY VEHICLES, OCCUPANTS, VISITORS, ETC. THROUGHOUT CONSTRUCTION. THIS ACCESS MUST PROVIDE FOR THE REMOVAL OF MUD FROM VEHICLES TIRES. ROADWAYS AND DRIVEWAYS SHALL BE MAINTAINED OPEN FOR EMERGENCY VEHICLES AT ALL TIMES. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE RESIDENTS AND BUSINESSES WHOSE DRIVEWAYS ARE AFFECTED BY THEIR SCHEDULE 24 HOURS IN ADVANCE. CONTRACTOR SHALL SCHEDULE CONSTRUCTION AT NON-PEAK USE HOURS AND
- SHALL MINIMIZE DRIVEWAY CLOSURE BY EXPEDITING CONSTRUCTION. 8. THE CONTRACTOR SHALL PROVIDE NECESSARY SIGNS, BARRICADES, AND LIGHTS TO PROTECT THE TRAFFIC AND THE WORK AS DIRECTED BY THE PLANS OR BY THE AGENCY WITH JURISDICTION. ALL TRAFFIC CONTROLS SHALL BE IN ACCORDANCE WITH THE MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD). 9. THE CONTRACTOR IS REQUIRED TO CONFINE CONSTRUCTION ACTIVITIES TO THE LIMITS OF THE SITE AS SHOWN ON THE CONSTRUCTION PLANS. ANY DAMAGE OR
- DISRUPTION TO ADJACENT SITES IS THE RESPONSIBILITY OF THE CONTRACTOR TO CORRECT IMMEDIATELY. NO OFF-SITE WORK SHALL BE PERFORMED OUTSIDE OF PUBLIC RIGHTS-OF-WAY OR DEDICATED EASEMENTS WITHOUT PRIOR WRITTEN APPROVAL OF THE PROPERTY OWNER. 10. GREAT CARE SHALL BE TAKEN TO AVOID DAMAGE TO VEGETATION OUTSIDE THE CLEARING AND GRUBBING LIMITS. NO DRIVING OR PARKING OF VEHICLES AND/OR
- 11. ALL ELEVATIONS ON THESE PLANS ARE ON THE CITY OF DETROIT DATUM. 12. THE PROTECTION OF EXISTING TREES, AS REQUIRED, SHALL BE SOLELY THE CONTRACTOR'S RESPONSIBILITY.

STORAGE OF MATERIALS AND SUPPLIES SHALL BE PERMITTED OUTSIDE THE LIMITS OF CONSTRUCTION.

- 13. ALL CONSTRUCTION SHALL HAVE INSPECTION PROVIDED BY THE CITY OF DETROIT. THE CONTRACTOR SHALL CONTACT THE CITY OF DETROIT 48 HOURS BEFORE
- THE START OF CONSTRUCTION. 14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DUST CONTROL, AND SHALL PROVIDE ALL NECESSARY MATERIAL AND EQUIPMENT TO KEEP DUST IN CHECK AT ALL
- TIMES. THE CONTRACTOR SHALL RESPOND IMMEDIATELY TO ANY AND ALL COMPLAINTS. DUST CONTROL SHALL BE INCIDENTAL TO THE PROJECT. 15. DURING CONSTRUCTION, THE CONTRACTOR MAY ENCOUNTER SPRINKLER HEADS, PIPING, LIGHTING AND BURIED ELECTRICAL CABLE, MAILBOXES, FENCES, SIGNS, ETC., THAT MAY OR MAY NOT BE INDICATED ON THESE PLANS. THE CONTRACTOR SHALL REPLACE AND/OR RESTORE ALL COMPONENTS OF SUCH SYSTEMS. ALL DISTURBED AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION, MINIMUM STANDARD REQUIREMENTS, OR AS SPECIFIED HEREIN; WHICHEVER IS MORE
- STRINGENT. 16. ROADWAY, DRIVEWAY, AND PARKING AREA FINAL RESTORATION SHALL BE PERFORMED WITH SURFACE AND BASE MATERIALS MATCHING EITHER THE EXISTING
- MATERIALS IN QUALITY AND THICKNESS, PER MINIMUM REQUIREMENTS, OR PER THE FOLLOWING; WHICHEVER IS MORE STRINGENT:
- A. ASPHALT ROADWAYS 4" ASPHALT MDOT 1100T-20 AA B. ASPHALT DRIVEWAYS - 3" ASPHALT MDOT 1100T-36 A
- C. GRAVEL ROAD AND DRIVEWAYS 8" MDOT 22A GRAVEL
- D. CONCRETE ROADS 8" 4000 PSI CONCRETE
- E. CONCRETE DRIVEWAYS 6" 4000 PSI CONCRETE
- 17. ALL LOT MARKERS AND MONUMENT POINTS DISTURBED DURING CONSTRUCTION SHALL BE REPLACED BY A REGISTERED LAND SURVEYOR AT THE EXPENSE OF THE

18. FINAL CLEANUP AND RESTORATION SHALL CONSIST OF FINE GRADING OF CONSTRUCTION AREAS, REMOVAL OF CONSTRUCTION SIGNS, ETC. TOPSOIL SHALL BE SPREAD OVER ALL DISTURBED AREAS, FOLLOWED BY SEED, FERTILIZER AND EROSION MAT OR STRAW MULCH, OR AS FURTHER REQUIRED BY THE LANDSCAPING PLANS AND SPECIFICATIONS. ALL REQUIRED RESTORATION ITEMS NOT SPECIFICALLY IDENTIFIED AS A PAY ITEM SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. 19. THE UTILITY POLES SHOWN ON THESE DRAWINGS ARE INTENDED TO SHOW ONLY THE LOCATION OF EXISTING POLES. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE QUANTITY AND DIRECTION OF OVERHEAD LINES. THE COST FOR SUPPORTING AND RELOCATING POLES SHALL BE INCIDENTAL TO THE PROJECT. 20. THE MEANS AND METHODS OF CONTROLLING GROUNDWATER AND DEWATERING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL COST ASSOCIATED WITH DEWATERING SHALL BE INCIDENTAL TO THE CONTRACT.

UTILITY NOTES

- 1. REFER TO THE GENERAL NOTES FOR ADDITIONAL REQUIREMENTS AND RESPONSIBILITIES.
- 2. REFER TO ARCHITECTURAL PLANS TO COORDINATE ALL:
- A. WATER SUPPLY, METERING, SPRINKLER AND FDC PIPING, DESIGN AND COORDINATION B. BUILDING SEWER, BUILDING DRAIN DESIGN AND CONNECTIONS TO CLEAN OUTS AND ROOF CONNECTORS
- C. GAS, ELECTRIC AND COMMUNICATION SERVICES, AND LIGHTING DETAILS
- D. ALL BUILDING ACCESS WALKS AND ENTRY DETAILS, INCLUDING SUPPORTED SLABS
- E. ALL WORK TO CONSTRUCT THE BUILDING AND ALL ITEMS CONNECTED TO IT 3. ALL TRENCHES WITHIN A ONE ON ONE SLOPE OF PAVEMENT SHALL BE BACKFILLED WITH SAND (MDOT CLASS II MINIMUM) AND MECHANICALLY COMPACTED IN NOT MORE
- THAN 9" LAYER TO 95% MAXIMUM DRY DENSITY PER MODIFIED PROCTER COMPACTION TEST ASTM D-1557. COMPACTED SAND BACKFILL SHALL ALSO BE PROVIDED FOR ALL SEWER TRENCHES LOCATED UNDER, OR WITHIN, THREE FEET OF PAVEMENT.
- 4. THE COST OF ALL TREE, STUMP, FOUNDATION AND/OR STRUCTURE REMOVAL AND DISPOSAL NOT INCLUDED IN THE PROPOSAL SHALL BE CONSIDERED INCIDENTAL AND INCLUDED IN THE PRICE BID FOR WATERMAIN, SANITARY SEWER, STORM SEWER, AND PAVING WORK.
- 5. A MINIMUM VERTICAL CLEARANCE OF 18 INCHES IS REQUIRED AT UTILITY CROSSINGS (MEASURED FROM THE OUTSIDE OF PIPE TO THE OUTSIDE OF PIPE). POSITIVE PROVISIONS SHALL BE MADE TO ENSURE THAT ALL UTILITY TRENCHES ARE FREE DRAINING DURING ALL PHASES OF CONSTRUCTION.
- 6. THE REQUIRED BEDDING FOR SEWER PIPE SHALL CONSIST OF A MAXIMUM 3/4 INCH DIAMETER CRUSHED STONE.
- 7. THE MINIMUM SLOPE FOR A BUILDING LEAD IS 1%. LEADS SHALL ONLY BE CONNECTED TO THE MAIN LINE WITH WYES.
- 8. ALL STORM SEWER PIPE SHALL BE CONSTRUCTED WITH RUBBER GASKET (PREMIUM) JOINTS. 9. THE CONTRACTOR SHALL COORDINATE THE REMOVAL OF ALL UTILITY LINES AND STRUCTURES, AS OUTLINED ON THE DEMOLITION PLAN, WITH THE INSTALLATION OF UTILITY IMPROVEMENTS.
- 10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING GRADE MODIFICATIONS INDICATED ON THE FINISHED LANDSCAPE PLAN, AND COORDINATE THE ACTUAL FINISH GRADE OF FIRE HYDRANTS, GATE VALVE CASTINGS, MANHOLES, YARD DRAINS, CLEAN OUTS AND OTHER UTILITY STRUCTURES. ENSURE THAT ALL FINISH GRADING IS PERFORMED IN A MANNER THAT ACCOMPLISHES THE PROJECT DESIGN OBJECTIVES AND PROVIDES FOR POSITIVE DRAINAGE OF ALL AREAS. ANY SUBSTANTIAL GRADE CHANGES WHICH MAY CAUSE FUNCTIONAL PROBLEMS SHALL BE REPORTED PROMPTLY TO THE ENGINEER WHO SHALL EVALUATE THE CONDITIONS AND PROVIDED
- CORRECTIONAL RECOMMENDATIONS TO THE OWNER FOR FINAL DETERMINATION. 11. CONTRACTOR SHALL BE REQUIRED TO COORDINATE THE INSTALLATION OF GAS, ELECTRIC, PHONE, CABLE, SPRINKLERS ETC., IN SUCH A MANNER THAT WILL FACILITATE THEIR PROPER INSTALLATION PRIOR TO PLACING THE PAVEMENT MATERIALS. ENSURE THAT ALL REQUIRED PIPES, CONDUITS, CABLES AND SLEEVES ARE PROPERLY PLACED
- AND THAT THE TRENCHES ARE PROPERLY BACKFILLED AND COMPACTED. 12. THE CONTRACTOR SHALL REMOVE UTILITIES, WHICH HAVE BEEN ABANDONED IN PLACE, AS REQUIRED TO COMPLETE INSTALLATION OF NEW UTILITIES. WHENEVER
- ABANDONED UTILITIES ARE CUT, CONTRACTOR SHALL COMPLETELY CAP BOTH ENDS TO PREVENT THE INFILTRATION OF SOILS. 13. NO CONNECTION MAY BE MADE TO ANY EXISTING WATER MAIN UNTIL THE NEW MAIN HAS PASSED ALL PRESSURE AND BACTERIOLOGICAL TESTING.
- 14. ROADWAY, DRIVEWAY AND PARKING AREA CROSSINGS SHALL BE TEMPORARILY CONDITIONED IMMEDIATELY AFTER CROSSING BY PLACING 8" OF MDOT 22A GRAVEL OR SLAG AGGREGATE, AND SHALL BE MAINTAINED IN GOOD, DUST FREE CONDITION UNTIL PAVEMENT RESTORATION IS MADE.

EROSION CONTROL NOTES

- 1. REFER TO THE GENERAL NOTES FOR ADDITIONAL REQUIREMENTS AND RESPONSIBILITIES.
- 2. ALL EROSION AND SEDIMENTATION CONTROL WORK SHALL CONFORM TO THE CURRENT STANDARDS AND SPECIFICATIONS OF THE CITY OF
- 3. ANY EROSION AND SEDIMENTATION FROM WORK ON THIS SITE SHALL BE CONTAINED WITHIN THE WORK AREA AND NOT ALLOWED TO COLLECT ON ANY OFF-SITE AREAS OR IN WATERWAYS. (WATERWAYS INCLUDE BOTH NATURAL AND MAN-MADE OPEN DITCHES, STREAMS, STORM DRAINS, LAKES, PONDS AND WETLANDS)
- 4. THE CONTRACTOR SHALL APPLY TEMPORARY EROSION AND SEDIMENTATION CONTROL MEASURES AS DIRECTED ON THESE PLANS AND WHENEVER OTHERWISE REQUIRED BY THE WORK. THE CONTRACTOR SHALL REMOVE TEMPORARY MEASURES AS SOON AS PERMANENT STABILIZATION OF SLOPES, DITCHES, AND OTHER CHANGES HAVE BEEN ACCOMPLISHED.
- 5. SOIL EROSION CONTROL PRACTICES WILL BE ESTABLISHED IN EARLY STAGES OF CONSTRUCTION BY THE CONTRACTOR. SEDIMENTATION CONTROL PRACTICES WILL BE APPLIED AS A PERIMETER DEFENSE AGAINST ANY TRANSPORTING OF DIRT OUT OF THE WORK AREA.
- THE CONTRACTOR SHALL PRESERVE NATURAL VEGETATION AS MUCH AS POSSIBLE. 7. PROTECT ALL EXISTING TREES. INCLUDING THEIR BRANCHES AND ROOTS. FROM DAMAGE DUE TO THIS WORK UNLESS SPECIFICALLY IDENTIFIED
- 8. VEGETATION STABILIZATION OF ALL DISTURBED AREAS SHALL BE ESTABLISHED WITHIN 15 DAYS OF COMPLETION OF FINAL GRADING.

CONTRACTOR SHALL BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE REQUIRED TO CONTAIN SEDIMENT.

- 9. THE CONTRACTOR SHALL SWEEP THE EXISTING STREETS SURROUNDING THE PROJECT SITE ONCE A WEEK, OR AS DIRECTED BY THE ENGINEER OR INSPECTOR. STREET SCRAPING SHALL BE PERFORMED IN CONJUNCTION WITH THIS SWEEPING ON AN AS NEEDED BASIS. 10. THE SEDIMENT CONTROL FENCING INDICATED ON THIS PLAN IS NOT INTENDED TO SHOW THE EXACT LOCATION OF THE FENCE. THE
- 11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING COMPLIANCE WITH ALL APPLICABLE NPDES REGULATIONS, INCLUDING: INSPECTION, RESTORATION, AND RECORD KEEPING REQUIREMENTS.
- 12. THE CONTRACTOR IS RESPONSIBLE FOR ON-GOING MAINTENANCE OF ALL SOIL EROSION CONTROLS AS INDICATED BY THESE PLANS. 13. CONSTRUCTION ACTIVITIES (INCLUDING INSTALLATION OF PIPE AND ASSOCIATED VALVES, STRUCTURES, BACK FILLING, SURFACE RESTORATION,
- AND REMOVAL OF EXCESS EXCAVATED MATERIAL) SHALL BE ACCOMPLISHED IN ONE CONTINUOUS OPERATION. 14. PAVEMENT AND/OR VEGETATION SHALL NOT BE STRIPPED FROM AN AREA UNLESS CONSTRUCTION ACTIVITIES ARE TO COMMENCE IN THAT
- AREA WITHIN THE NEXT THREE DAYS. 15. IF FOR ANY REASON PERMANENT STABILIZATION CAN NOT BE PROVIDED WITHIN 15 DAYS OF THE COMPLETION OF PIPE LAYING OPERATIONS,
- TEMPORARY STABILIZATION SHALL BE PROVIDED AT ALL DISTURBED AREAS. TEMPORARY STABILIZATION SHALL FURTHERMORE BE PROVIDED DURING THE NON-GROWING SEASON (OCTOBER 1 THROUGH APRIL 20) FOR ALL AREAS TO BE SEEDED. 16. TEMPORARY STABILIZATION SHALL CONSIST OF EITHER SMALL GRAIN STRAW OR GRASS HAY SPREAD AT THE RATE OF 1.5 TO 2 TONS PER ACRE
- OR MULCH BLANKETS, WHICH SHALL BE ANCHORED IN PLACE TO PREVENT DISPLACEMENT FROM WIND AND RAIN. TEMPORARY STABILIZATION SHALL BE REPAIRED AS OFTEN AS NECESSARY, AS DETERMINED BY THE AGENCY WITH JURISDICTION. 17. ALL DEWATERING SHALL BE ACCOMPLISHED IN A MANNER THAT WILL NOT CONTRIBUTE TO DEPOSITION OF SEDIMENT IN ROAD DITCHES OR
- 18. THIS PROJECT SHALL BE CONSTRUCTED IN COMPLIANCE WITH PART 91 OF ACT 451 OF 1994, AS AMENDED. 19. SEDIMENT CONTROL FENCING SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND SEVERAL TIMES DURING PROLONGED STORM EVENTS. IF THE FENCE IS SAGGING, OR SOIL HAS REACHED ONE HALF OF THE HEIGHT OF THE FABRIC, THE SOIL BEHIND THE FABRIC SHALL BE REMOVED AND DISPOSED OF IN A STABLE AREA OF THE SITE. IF WATER IS SEEPING UNDER THE FENCE, OR THE FABRIC IS DECOMPOSED OR
- OTHERWISE INEFFECTIVE, THE FENCE SHALL BE REMOVED AND PROPERLY REINSTALLED AS INDICATED ON THESE PLANS. 20. MUD MAT ENTRANCES SHALL BE INSPECTED IMMEDIATELY AFTER EACH STORM RAINFALL. THE SURROUNDING ROADS SHALL ALSO BE INSPECTED AT THIS TIME FOR EVIDENCE THAT MUD IS BEING TRACKED OFF OF THE SITE. MAINTENANCE SHALL INCLUDE THE INSTALLATION OF ADDITIONAL LAYERS OF STONE WHEN THE ORIGINAL STONE BECOMES COVERED WITH MUD. ALL SEDIMENT DROPPED OR TRACKED ONTO PUBLIC RIGHT-OF-WAYS SHALL BE REMOVED IMMEDIATELY BY SWEEPING AND SCRAPING (AS MAY BE REQUIRED BY THE ENGINEER).
- 21. SEDIMENT INLET FILTERS SHALL BE INSPECTED IMMEDIATELY AFTER EACH RAINFALL AND SEVERAL TIMES DURING PROLONGED STORM EVENTS. THE FILTERS SHALL BE CLEANED PERIODICALLY THROUGHOUT CONSTRUCTION TO AVOID CLOGGING. FILTERS THAT CANNOT BE MAINTAINED BY CLEANING SHALL BE COMPLETELY REPLACED.

DEMOLITION NOTES

- 1. REFER TO THE GENERAL NOTES FOR ADDITIONAL REQUIREMENTS AND RESPONSIBILITIES.
- 2. WITH THE EXCEPTION OF AN AMOUNT OF EXCAVATED MATERIALS SUFFICIENT FOR BACKFILLING AND CONSTRUCTION OF FILLS AS CALLED FOR ON THE PLANS AND AS INDICATED BELOW, ALL BROKEN CONCRETE, STONE AND EXCESS EXCAVATED MATERIALS SHALL BE DISPOSED OF BY THE CONTRACTOR. THE CONTRACTOR WILL BE REQUIRED TO OBTAIN THEIR OWN DISPOSAL GROUND, AND WILL RECEIVE NO ADDITIONAL COMPENSATION FOR DISPOSING OF ANY OF THE EXCESS MATERIALS. MATERIALS ACCEPTABLE TO THE ENGINEER MAY BE
- DISPOSED OF ON-SITE AT THE CONTRACTORS EXPENSE IN A MANNER APPROVED IN ADVANCE BY THE ENGINEER. 3. THE EDGE OF EXISTING PAVEMENT SHALL BE CLEANED OF EARTH AND OTHER FOREIGN MATERIAL BEFORE ADJACENT POURS ARE
- 4. ALL BULKHEADING AND/OR SEWER PIPE REMOVAL NECESSITATED BY THE REMOVAL OF DRAINAGE STRUCTURES SHALL BE INCLUDED IN
- THE STRUCTURE REMOVAL. 5. STREET SIGNS IN THE WAY OF CONSTRUCTION WILL BE REMOVED AND RESET IMMEDIATELY IN A TEMPORARY LOCATION, AS APPROVED
- BY ENGINEER. 6. THE CONTRACTOR SHALL PROTECT ALL EXISTING SIGNS AND POSTS SCHEDULED TO REMAIN, AS DIRECTED BY THE ENGINEER.
- 7. ALL UNDERGROUND UTILITIES NOT INDICATED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT CONSTRUCTION.
- 8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING ALL PRIVATE PROPERTY (INCLUDING BUILDINGS AND FOUNDATIONS) THROUGHOUT CONSTRUCTION AND SHALL MAINTAIN SAFE PEDESTRIAN ACCESS AT ALL TIMES.
- 9. THE REMOVAL OF PAVEMENT, CURBS AND WALKS SHALL INCLUDE ALL REQUIRED SAWCUTTING. CURB REMOVAL IS INCIDENTAL TO PAVEMENT REMOVAL.

PAVING AND GRADING NOTES

- 1. REFER TO THE GENERAL NOTES FOR ADDITIONAL REQUIREMENTS AND RESPONSIBILITIES. 2. THE PAVING CONTRACTOR SHALL BE REQUIRED TO COORDINATE THE INSTALLATION OF GAS, ELECTRIC, PHONE, CABLE, SPRINKLERS, ETC. IN SUCH A MANNER THAT WILL FACILITATE THEIR PROPER INSTALLATION PRIOR TO PLACING THE PAVEMENT MATERIALS. ENSURE THAT ALL REQUIRED PIPES, CONDUITS, CABLES AND SLEEVES ARE
- PROPERLY PLACED AND THAT THE TRENCHES ARE PROPERLY BACKFILLED AND COMPACTED. 3. BUTT JOINTS SHALL BE PLACED AT ALL LOCATIONS WHERE AN EXISTING ASPHALT PAVEMENT SURFACE IS BEING DISTURBED BY REMOVALS AND/OR THE INSTALLATION OF NEW ASPHALT PAVEMENT.
- 4. ALL PAVEMENT AREAS SHOULD BE CLEARED AND GRUBBED BY REMOVING SURFACE VEGETATION, TOPSOIL, DEBRIS AND OTHER DELETERIOUS MATERIALS
- 5. THE PLACEMENT OF THE FINAL ASPHALT LIFT SHALL BE DELAYED UNTIL THE MAJORITY OF THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED, OR AS APPROVED BY THE OWNER. A BOND COAT OF SS-1H EMULSION SHALL BE APPLIED (AT A RATE OF 0.10 GALLONS/S.Y.D.) BETWEEN THE LEVELING AND WEARING COURSE WHEN 48 HOURS HAVE ELAPSED BETWEEN PLACEMENTS.
- 6. THE FINAL SUB-GRADE SHALL BE THOROUGHLY PROOF-ROLLED UNDER THE OBSERVATION OF THE SOILS ENGINEER. 7. PROPOSED AGGREGATE BASE SHALL EXTEND A MINIMUM OF 1 FOOT BEYOND THE PAVEMENT EDGE/BACK OF CURB.
- 8. ALL TRENCHES WITHIN A ONE ON ONE SLOPE OF PAVEMENT SHALL BE BACKFILLED WITH SAND (MDOT CLASS II MINIMUM) AND MECHANICALLY COMPACTED IN NOT MORE
- THAN 9" LAYER TO 95% MAXIMUM DRY DENSITY PER MODIFIED PROCTER COMPACTION TEST ASTM D-1557. 9. NO FROZEN MATERIAL SHALL BE PERMITTED AS BACKFILL UNDER ANY ROADWAY, DRIVEWAY OR PARKING AREA.
- 10. PRIOR TO THE START OF ANY FILLING, THE CONTRACTOR SHALL REMOVE ALL TOPSOIL AND ALL OTHER UNACCEPTABLE SOIL FROM THE FILL AREAS, AND PROPERLY BACKFILL WITH ACCEPTABLE SOIL.
- 11. BARRIER FREE SIGNAGE SHALL BE PLACED IN FRONT OF EVERY DESIGNATED BARRIER FREE STALL. THE CONTRACTOR SHALL COORDINATE STANDARD AND VAN ACCESSIBILITY SIGNAGE AS INDICATED ON THE PLANS.

16. PRIOR TO THE PLACEMENT OF ANY BASE ASPHALT OR LEVELING COURSE, THE CURBS SHALL BE PARTIALLY BACKFILLED AND THE SUB-GRADE SHALL BE PROOF-ROLLED

- 12. ALL BARRIER FREE RAMPS TO BE A.D.A. COMPLIANT.
- 13. GENERAL GRADING REQUIREMENTS ARE AS FOLLOWS:
- A. FINISH GRADE AT EXISTING BUILDING SHALL MATCH BRICK LEDGES, DOORWAYS OR BASEMENT WINDOWS B. MAINTAIN POSITIVE DRAINAGE AWAY FROM ALL BUILDING (± 2%)
- C. SIDEWALK CROSS SLOPE ±2% UNLESS OTHERWISE NOTED (EXCLUDING RAMPS)
- D. PAVEMENT SLOPES (1.0% MINIMUM, 4.0% MAXIMUM) UNIFORMLY BETWEEN FINISH GRADE ON PLANS
- E. LAWN AREAS ± 1% MINIMUM TO 25% (BERMS) MAXIMUM 14. ALL PROPOSED GRADES ARE AT THE GUTTER UNLESS OTHERWISE NOTED. SEE DETAILS FOR FACE OF CURB, TOP OF CURB AND ASPHALT ADJUSTMENTS.
- 15. REFER TO ARCHITECTURAL PLANS TO COORDINATE ALL: A. WATER SUPPLY, METERING, SPRINKLER AND FDC PIPING, DESIGN AND COORDINATION
- B. BUILDING SEWER, BUILDING DRAIN DESIGN AND CONNECTIONS TO CLEAN OUTS AND ROOF CONNECTORS C. GAS, ELECTRIC AND COMMUNICATION SERVICES AND LIGHTING DETAILS AND COORDINATION.
- D. ALL BUILDING ACCESS WALKS AND ENTRY DETAILS, INCLUDING SUPPORTED SLABS
- E. ALL WORK TO CONSTRUCT THE BUILDING AND ALL ITEMS CONNECTED TO IT
- UNDER THE SUPERVISION OF THE SOILS ENGINEER.
- 17. ALL SIDEWALK AND PATHWAYS IN ANY PUBLIC R.O.W. SHALL BE INSPECTED BY THE AGENCY WITH JURISDICTION.

EROSION CONTROL SEQUENCE

- 1. INSTALL MUD MATS, SILT FENCE AND INLET FILTERS AT ALL EXISTING LOCATIONS AS SHOWN AND AS REQUIRED TO
- ACHIEVE ON-SITE CONTAINMENT. 2. INSTALL STORM SEWER AND ALL ASSOCIATED STORM WATER IMPROVEMENTS AS SHOWN ON PLANS. IMMEDIATELY
- INSTALL INLET FILTERS AT ALL CATCH BASINS.
- 3. ROUGH GRADE THE PROJECT "WORK AREA" AS NEEDED. 4. INSTALL SANITARY SEWER AS SHOWN ON PLANS.
- 5. INSTALL WATER MAIN AS SHOWN ON PLANS. 6. INSTALL OTHER UTILITIES (GAS, ELECTRIC, PHONE, CABLE, ETC.) AND/OR ALL NEEDED CONDUITS AND SLEEVES.
- 7. INSTALL PAVEMENT BACKFILL AND SEED & MULCH ALL DISTURBED AREAS.
- 8. CLEAR ALL ACCUMULATED SILT AND REMOVE ALL EROSION CONTROL DEVICES. 9. INSTALL LANDSCAPING MATERIALS AS INDICATED PER PLANS & RE-SEED, FERTILIZE AND MULCH ALL DISTURBED

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GENERAL NOTES

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City of Detroit Wayne County MICHIGAN

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06.09.2023

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH

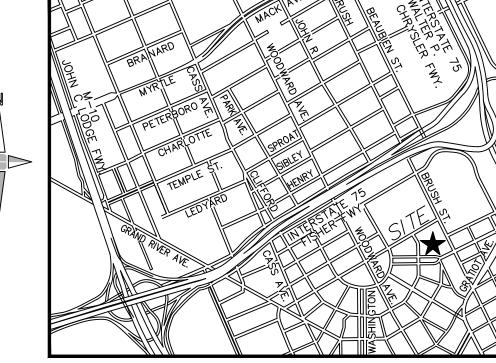
- CURRENT CABLE TV, PUBLIC LIGHTING, WATER, AND SEWER SEWER UTILITY MAPS WERE NOT AVAILABLE AT TIME OF SURVEY.

- THIS PARCEL IS LOCATED WITHIN A FEDERALLY DESIGNATED FLOOD HAZARD AREA ZONE "X" (AREA DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN) IN ACCORDANCE WITH THE FEDERAL INSURANCE RATE MAP, PANEL NUMBER 285 OF 575, MAP

BENCH MARK DATA

(CITY OF DETROIT) DATUM

SITE BENCH MARK NO. 1 ARROW ON HYDRANT LOCATED AT THE SOUTHWEST CORNER OF JOHN R STREET AND ADAMS AVENUE. ELEVATION=122.65'



LOCATION MAP (NOT TO SCALE)

EXISTING CONDITIONS - LEGEND

E/ PAVEMENT _ _ _ _ _ E/ GRAVEL ---- E/ BRICK TENCE GUARD RAIL

.----- SHRUB LINE

TELEPHONE OVERHEAD

○ PROT. POST/GUARD POST

DECIDUOUS TREE

CONIFEROUS TREE

DEAD TREE

CUTFLAG UTILITY FLAG

BLDG. CORNER

BUSH/SHRUB

PARKING METER

U.S. MAILBOX

SOIL BORING

MONITORING WELL

→ LAWN IRRIG. HEAD

▲ CENTERLINE R.R. TRACK

△ SURVEY CONTROL POINT

★ MISC. TOPO. SHOT

FOUND IRON

★ FOUND NAIL

"X" F. CUT CROSS

SECTION COR.

FENCE POST

S BENCHMARK

O FOUND PIPE

FOUND MON.

A.C. AIR CONDITIONER

C.L.F. CHAIN-LINK FENCE

F.F. FINISHED FLOOR

F.I.P. FOUND IRON PIPE

F.M. FOUND MONUMENT

CMP CORREGATED METAL PIPE

G.P. GUARD POST

D.L. DOOR LEDGE

O.H. OVERHANG

F.I. FOUND IRON

M. MEASURED

R. RECORD

S.N. SET NAIL

S.I. SET IRON

ASPH. ASPHALT

CONC. CONCRETE

RESIDENTIAL MAILBOX

(FIELD LOCATED)

له HANDICAP PARKING

< GUY

BLDG. LINE ---- OH ------ OVERHEAD WIRES — — — — — OVERHANG LINE ---- T/ BANK ----- B/ BANK --- CL DITCH/STREAM

— UGE — UG ELEC. LINE

— CTV———— UG CABLE TV LINE ---- COMBINED SEWER PLD PLD PLD PLD LINE _____ _ SECTION LINE

S SAN. MH Oc. SAN. CLEAN OUT 🛕 SAN. RISER

______TEL____

P.S. SAN. PUMP STATION © COMB. MH

₩ WATER VALVE ⟨W⟩ WATER METER POST INDICATOR VALVE WELL HEAD FDC CONNECTION W IRRIGATION CONTROL BOX

 \ominus STORM MH CATCH BASIN $000.00 + 0^{\circ}$ EXISTING ELEVATION BEEHIVE CB CULVERT E.S. ® ROOF/DOWN SPOUT O^{C.O.}STORM CLEAN OUT

ROUND CB LIGHT POLE Ø UTILITY POLE E ELEC. TRANS. AC AIR CONDITIONER (E) ELEC. MH

⟨E⟩ ELEC. METER £ ELEC. RISER C TRAFFIC CONTROL BOX ST) STEAM MH (L) PUBLIC LIGHTING MH

GAS GAS VALVE G GAS MH ⚠ TELE. RISER

T TELE. MH TELE. CROSS BOX ⚠ CABLE RISER

S PAY PHONE MANHOLE - SIGN

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EXISTING CONDITIONS

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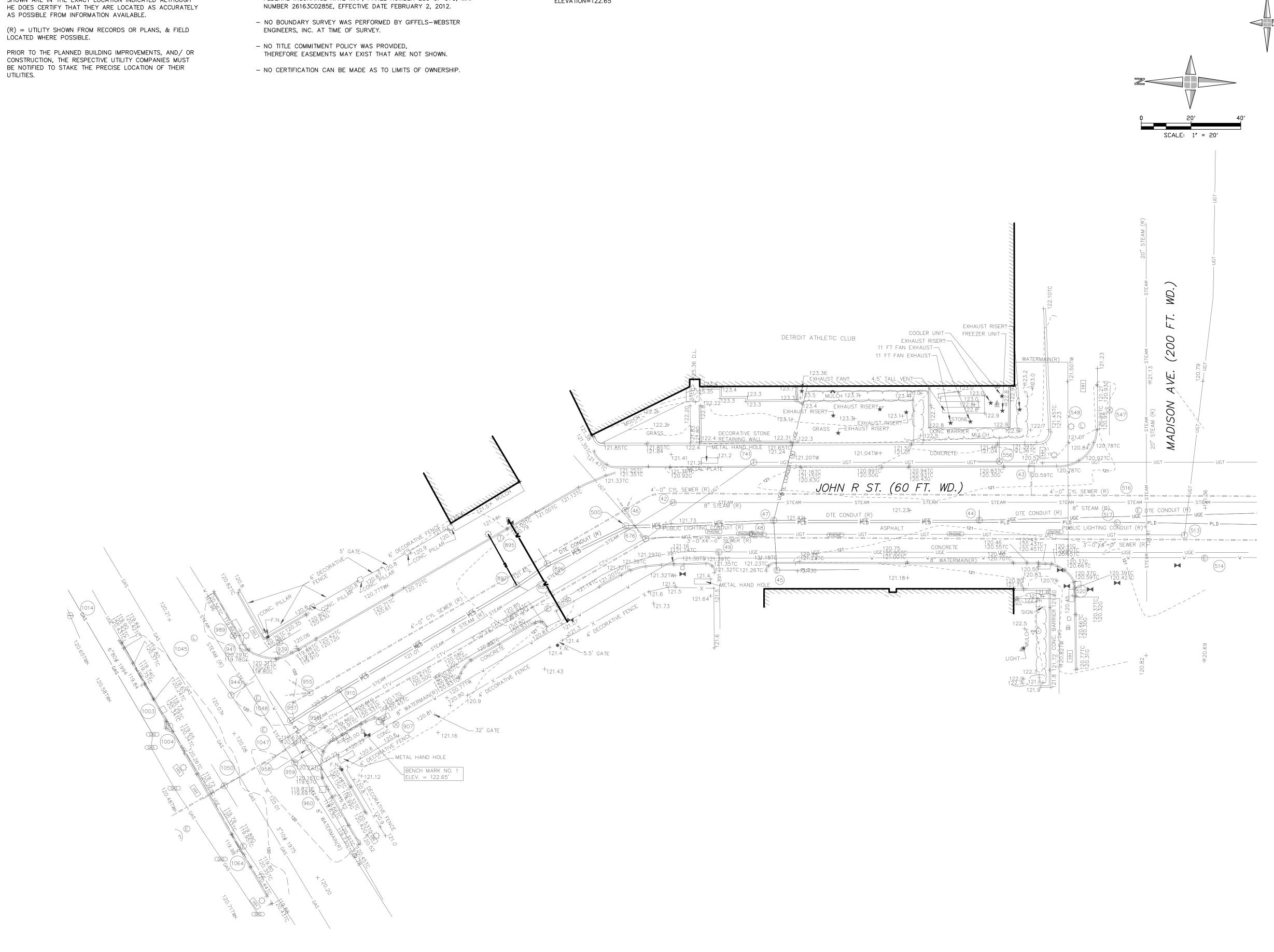
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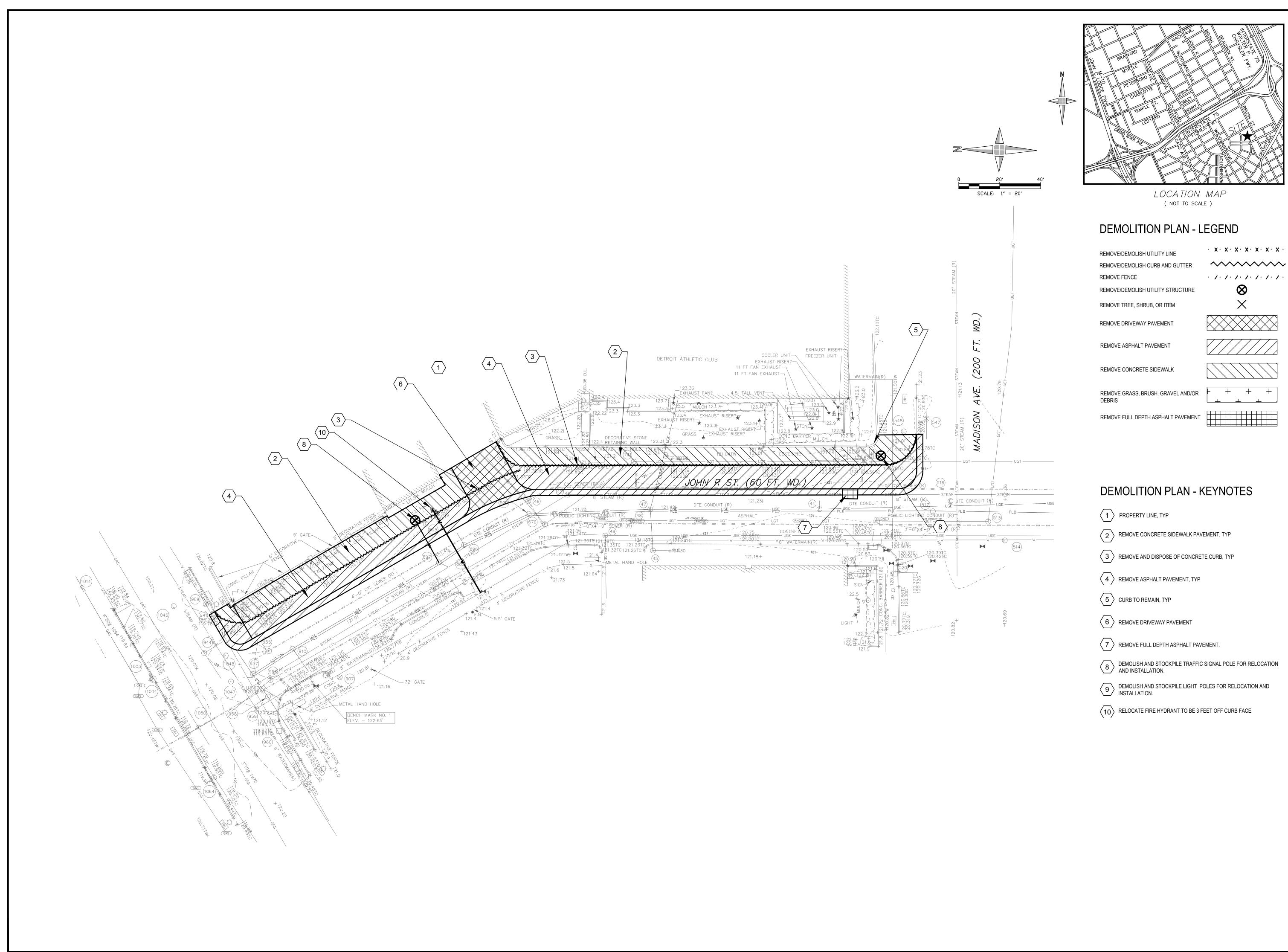
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DEMOLITION PLAN

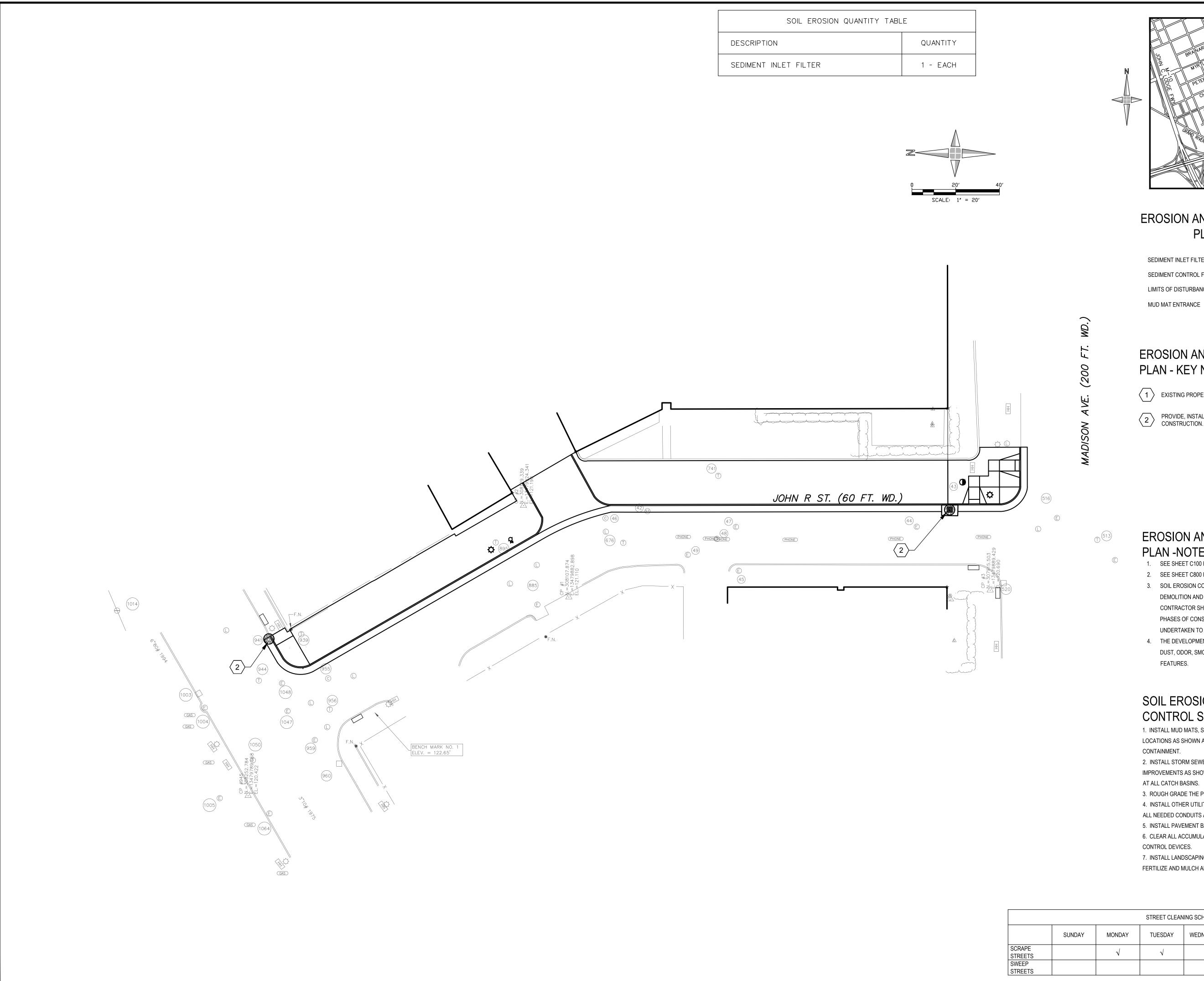
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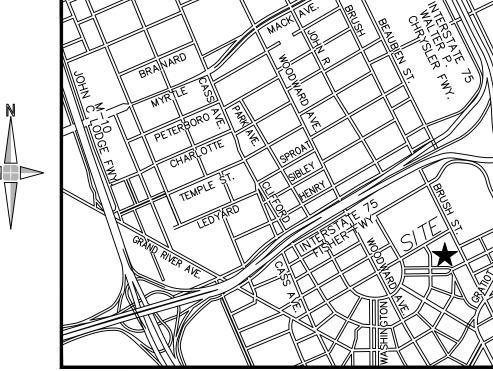
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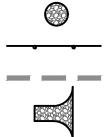




LOCATION MAP

EROSION AND SEDIMENT CONTROL PLAN - LEGEND

SEDIMENT INLET FILTER SEDIMENT CONTROL FENCE LIMITS OF DISTURBANCE (1.45 ACRES)



EROSION AND SEDIMENT CONTROL PLAN - KEY NOTES

1 EXISTING PROPERTY LINE, TYP

PROVIDE, INSTALL AND MAINTAIN SEDIMENT INLET FILTER DURING CONSTRUCTION. SEE DETAILS ON SHEET C-800

EROSION AND SEDIMENT CONTROL PLAN -NOTES

- 1. SEE SHEET C100 FOR GENERAL LEGEND AND ADDITIONAL NOTES.
- 2. SEE SHEET C800 FOR SESC DETAILS..
- 3. SOIL EROSION CONTROL MEASURES ARE SHOWN FOR THE INITIAL DEMOLITION AND MASS GRADING PHASES OF THE PROJECT. THE CONTRACTOR SHALL MAINTAIN AND ADJUST THESE MEASURES AS OTHER PHASES OF CONSTRUCTION (I.E. BUILDINGS AND PAVEMENTS) ARE UNDERTAKEN TO ENSURE THAT ALL SEDIMENT IS CONTAINED ON-SITE.
- 4. THE DEVELOPMENT OF THIS SITE WILL NOT RESULT IN ANY INCREASE IN DUST, ODOR, SMOKE, FUMES, NOISE, LIGHTS, OR OTHER OBJECTIONABLE FEATURES.

SOIL EROSION AND SEDIMENTATION CONTROL SEQUENCE

- 1. INSTALL MUD MATS, SILT FENCE AND INLET FILTERS AT ALL EXISTING LOCATIONS AS SHOWN AND AS REQUIRED TO ACHIEVE ON-SITE
- 2. INSTALL STORM SEWER AND ALL ASSOCIATED STORM WATER IMPROVEMENTS AS SHOWN ON PLANS. IMMEDIATELY INSTALL INLET FILTERS
- AT ALL CATCH BASINS.
- 3. ROUGH GRADE THE PROJECT "WORK AREA" AS NEEDED. 4. INSTALL OTHER UTILITIES (GAS, ELECTRIC, PHONE, CABLE, ETC.) AND/OR ALL NEEDED CONDUITS AND SLEEVES.
- 5. INSTALL PAVEMENT BACKFILL AND SEED & MULCH ALL DISTURBED AREAS. 6. CLEAR ALL ACCUMULATED SILT AND REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
- 7. INSTALL LANDSCAPING MATERIALS AS INDICATED PER PLANS & RE-SEED, FERTILIZE AND MULCH ALL DISTURBED AREAS.

STREET CLEANING SCHEDULE							
	SUNDAY	MONDAY	TUESDAY	WEDNESDAY	THURSDAY	FRIDAY	SATURDAY (IF WORKING)
CRAPE TREETS		V	V	V	V	V	√
WEEP TREETS				V			

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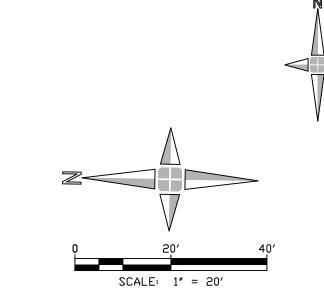
SOIL EROSION & SEDIMENT CONTROL PLAN

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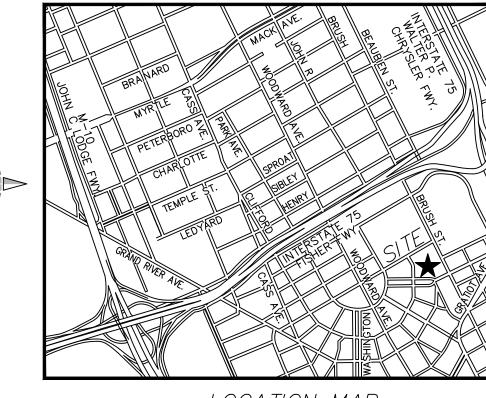
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DETROIT ATHLETIC CLUB



LOCATION MAP

4" CONCRETE SIDEWALK PAVEMENT 8" CONCRETE PAVEMENT PAVEMENT REPAIR PER CITY OF DETROIT + + + + + + + + +

PROPOSED GRASS

- 3 NEW 6" CURB. SEE DETAIL ON SHEET C-800.
- 5 EXISTING CURB TO REMAIN
- 8 NEW DETECTABLE WARNING STRIP PER CITY OF DETROIT STANDARDS, SEE DETAILS ON SHEET C-800.
- ADA COMPLIANT BARRIER FREE CURB RAMP WITH 6" THICK CONCRETE PER CITY OF DETROIT STANDARDS.

PAVING PI	LAN - I	LEGEND

STANDARDS AND SPECIFICATIONS

ASPHALT PAVEMENT REPAIR

6" CONCRETE SIDEWALK PAVEMENT

* * * * * *

* * * * * *



- 1 PROPERTY LINE
- 2 NEW 4" CONCRETE SIDEWALK PAVEMENT, TYP. SEE DETAIL ON SHEET C-800
- NEW 8" CONCRETE DRIVEWAY PAVEMENT. SEE DETAIL ON SHEET C-800
- 6 PROPOSED LANDSCAPE, TYP
- ASPHALT PAVEMENT REPAIR, 3'-0" WIDE (MIN.), PER CITY OF DETROIT STANDARDS AND SPECIFICATIONS. EXISTING STREET ASPHALT
 PAVEMENT SHALL BE MILLED AND REPAIRD TO NEW GUTTER GRADE PER
 GRADING PLAN. ASPHALT PAVEMENT SHALL HAVE UNIFORM SLOPE
 TOWARDS FACE OF CURB.

- NEW 6" CONCRETE SIDEWALK PAVEMENT, TYP. SEE DETAIL ON SHEET C-800
- 8" CONCRETE COLLAR FOR CATCH BASIN. SEE CONCRETE PAVEMENT DETAIL ON SHEET C-800.



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GEOMETRIC & PAVING PLAN

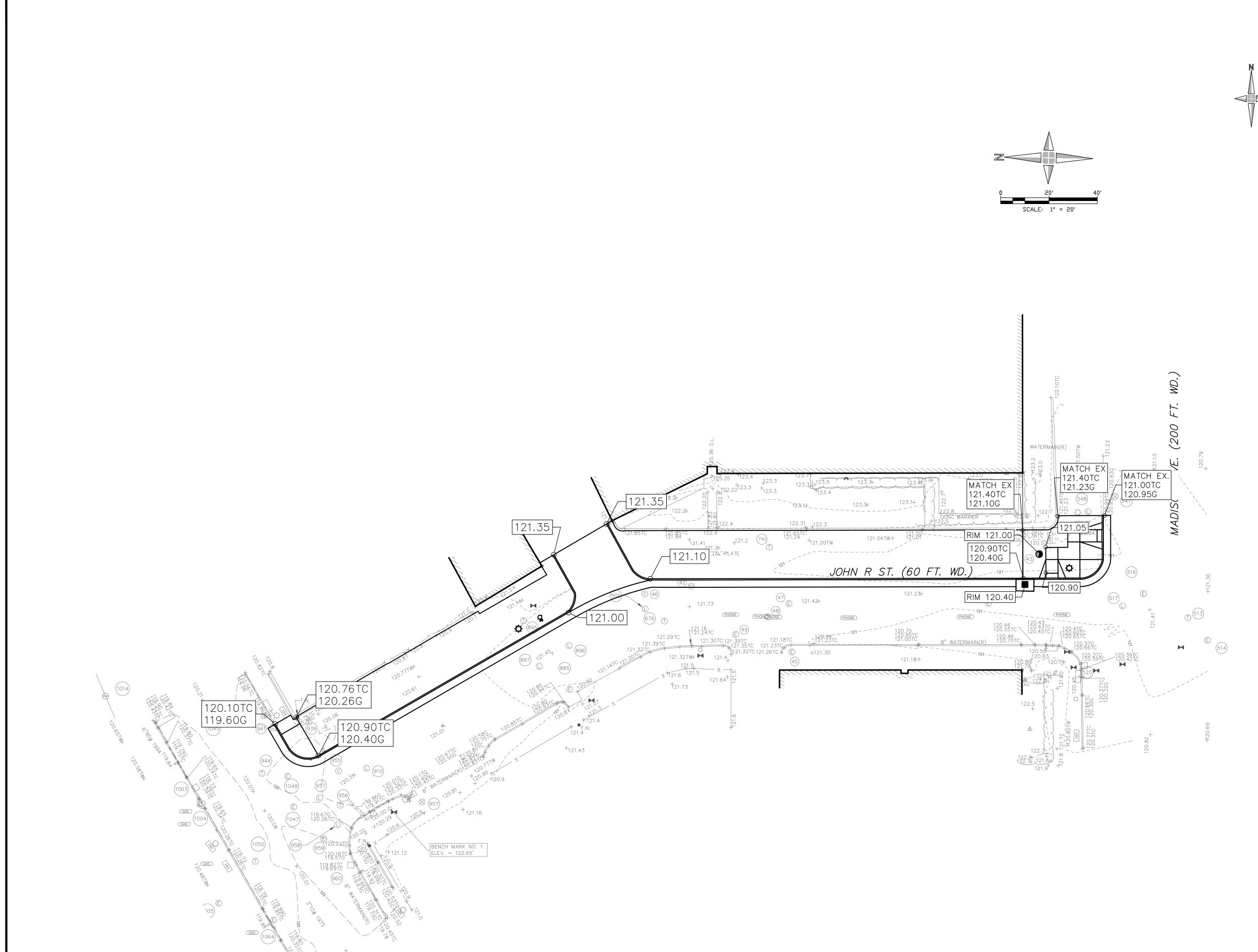
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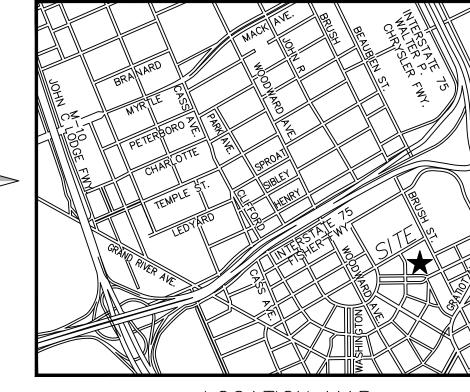
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LOCATION MAP

GRADING PLAN - GRADING LEGEND

	EXISTING	PROPOSED
SPOT ELEVATION	× 150.23	×XXX.XX
MATCH EXISTING GRADE		×[XXX.XX*]
FF		FINISHED FLOOR
FG		FINISHED GRADE
T/WALL		TOP OF WALL
TS		TOP OF STEP
TC		TOP OF CURB
		GUTTER
G		ADA RAMP, SLOPE = 8.33%</td
R		ADA LANDING, SLOPE = 2%</td
L		RADIUS OF A CURVE, MEASURED FROM
RXX.X'		FACE OF CURB

GRADING PLAN - NOTES

- SEE SHEET C-100 FOR ADDITIONAL PROJECT NOTES AND INFORMATION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY
 CONFLICT OR DISCREPANCY IS IDENTIFIED WITH NEW FINISHED
 GRADES
- 3. THE CONTRACTOR SHALL VERIFY ALL EXISTING GRADES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONFLICT OCCURS WHERE NEW FINISHED GRADES MATCH OR TIE-INTO EXISTING GRADES.
- 4. PAVEMENT SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL BE IN ACCORDANCE WITH ADA STANDARDS FOR ACCESSIBLE DESIGN.
- 5. THE CROSS SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL NOT EXCEED 1 IN 50 (2%) AND THE RUNNING SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL NOT EXCEED 1 IN 20 (5%).
- 6. THE CONTRACTOR SHALL PROVIDE A MINIMUM 5' LANDING AT THE TOP AND BOTTOM OF ALL DOORS, STAIRS, ADA RAMPS AND FENCE GATES IN ACCORDANCE WITH ADA STANDARDS FOR ACCESSIBLE DESIGN.
 THE LANDING SLOPE SHALL NOT EXCEED 1 IN 50 (2%) IN ALL DIRECTIONS.
- 7. ALL NEW PAVING AND STREET REPAIRS WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH CITY OF DETROIT STANDARDS.
- 8. ALL PAVEMENT AND GRASSY AREAS SHALL DIRECT RUNOFF TO CATCH
 BASIN OR DRAIN INLET STRUCTURES WITHOUT ANY PONDING OR
 BIRDBATHS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER
 IMMEDIATELY IF ANY PONDING OR BIRDBATHS SHALL OCCUR.
- 9. THE CASTING OF ALL EXISTING STRUCTURES INVOLVED IN THE SCOPE OF CIVIL WORKS SHALL BE FIELD ADJUSTED AND FLUSHED WITH THE FINISHED GRADE OF NEW PAVING.
- 10. THE CONTRACTOR SHALL SUBMIT A JOINTING PLAN DETAILING ALL JOINTS BASED ON ACI GUIDELINES FOR REVIEW AND APPROVAL BY THE ENGINEER PRIOR TO PLACING CONCRETE.



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GRADING PLAN

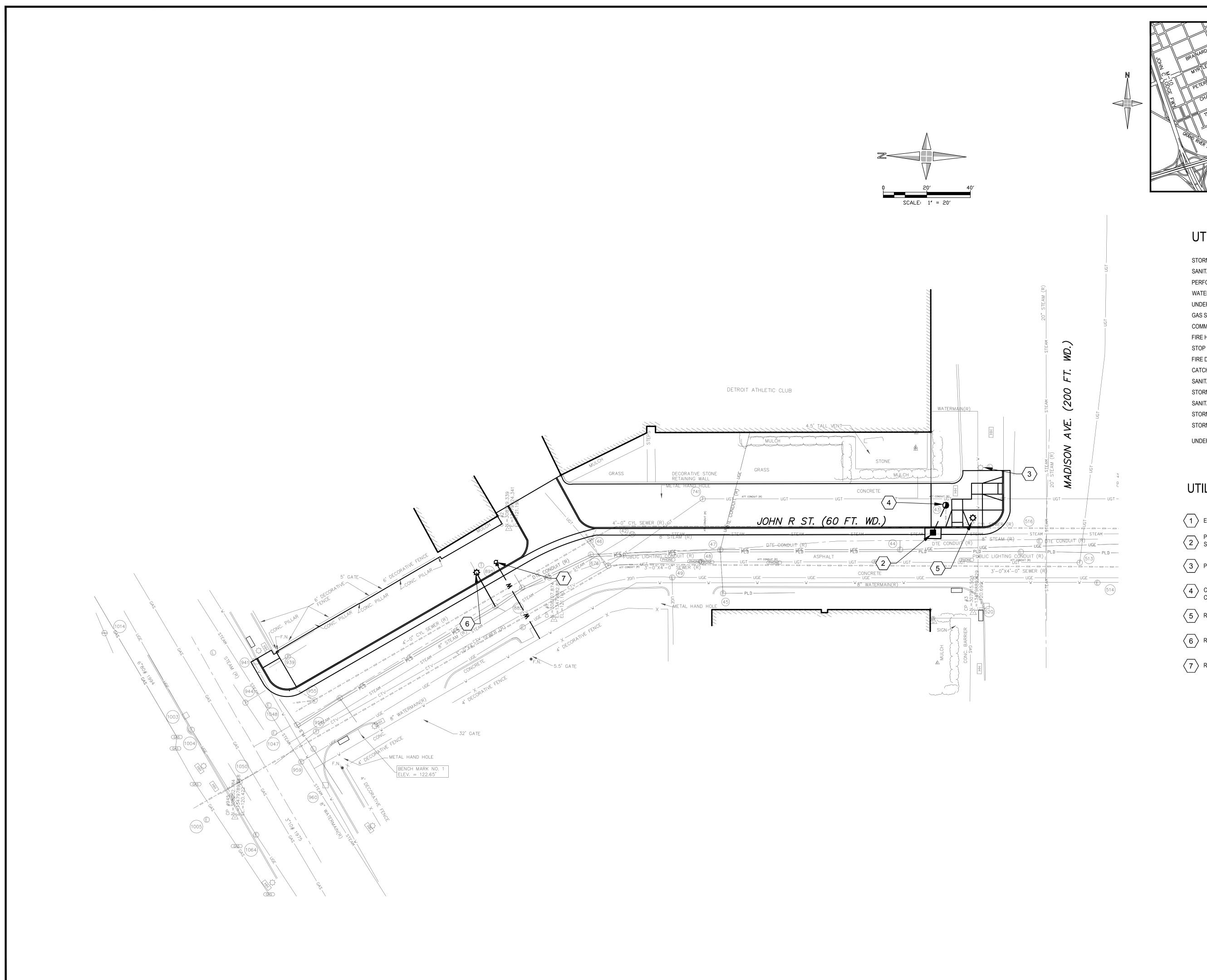
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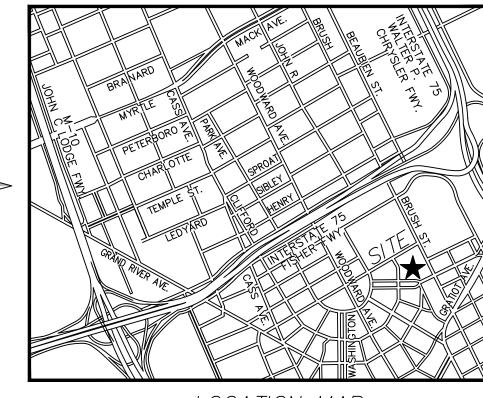
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LOCATION MAP

UTILITY PLAN - LEGEND

STORM SEWER		
SANITARY SEWER		
PERFORATED DRAIN		
WATER SERVICE		
UNDERGROUND ELECTRIC		UGE
GAS SERVICE		GAS
COMMUNICATIONS		CTV
FIRE HYDRANT		•
STOP BOX AND VALVE		©
FIRE DEPARTMENT CONNECT	ION	>
CATCH BASIN		
SANITARY SEWER DRAIN		
STORM CLEANOUT		Oc.o.
SANITARY MANHOLE		
STORM MANHOLE		
STORM SEWER CLEANOUT		Ō
UNDERGROUND DETENTION S	SYSTEM	

UTILITY PLAN - KEY NOTES

- 1 EXISTING RIGHT OF WAY
- PROVIDE AND INSTALL NEW CATCH BASIN PER DWSD STANDARDS AND SPECIFICATIONS.
- 3 PROTECT LIGHT POLE THROUGHOUT CONSTRUCTION
- CONVERT CATCH BASIN TO MANHOLE, INSTALL MANHOLE COVER
- 5 RELOCATE TRAFFIC SIGNAL POLE
- 6 RELOCATE LIGHT POLE
- 7 RELOCATE FIRE HYDRANT

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UTILITY PLAN

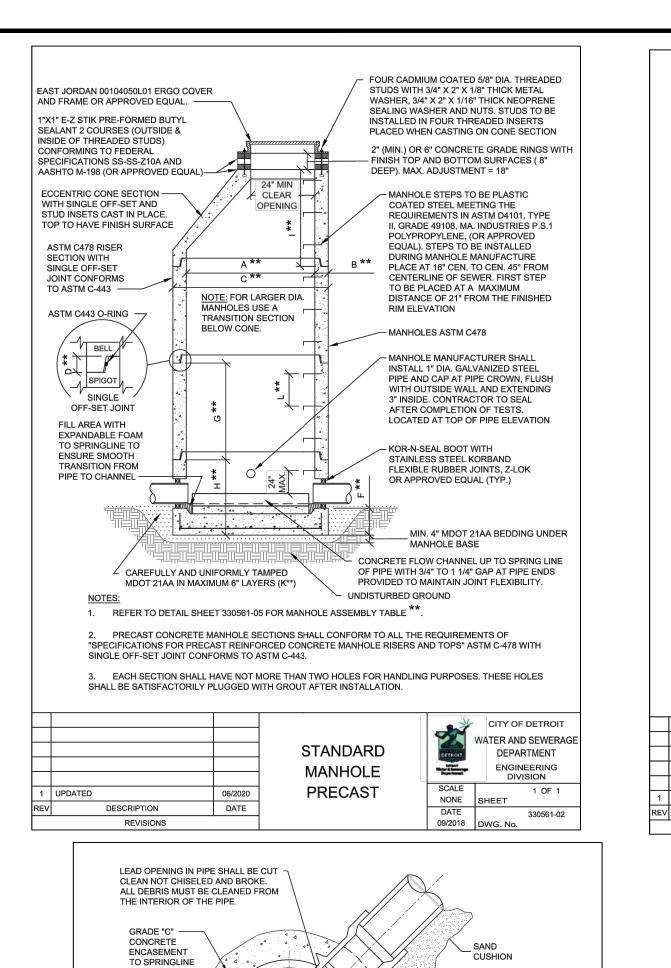
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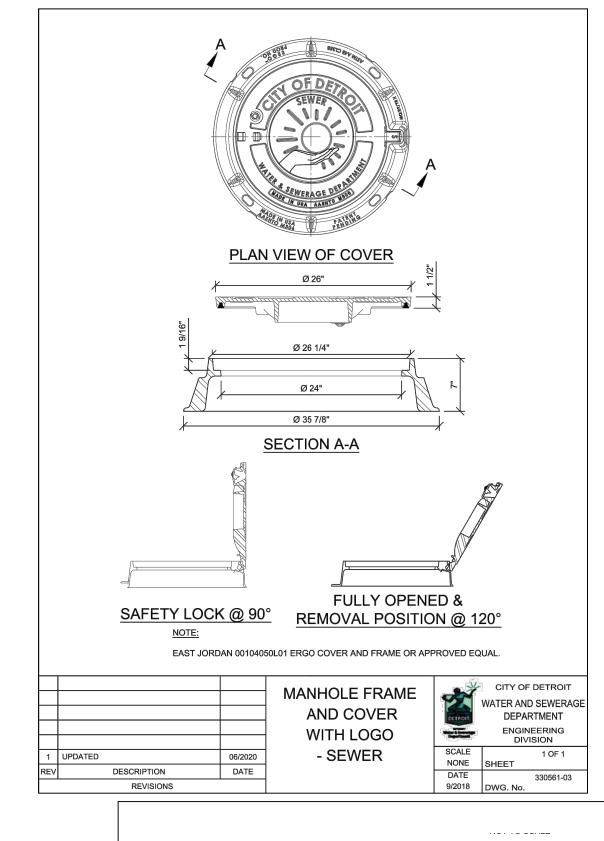
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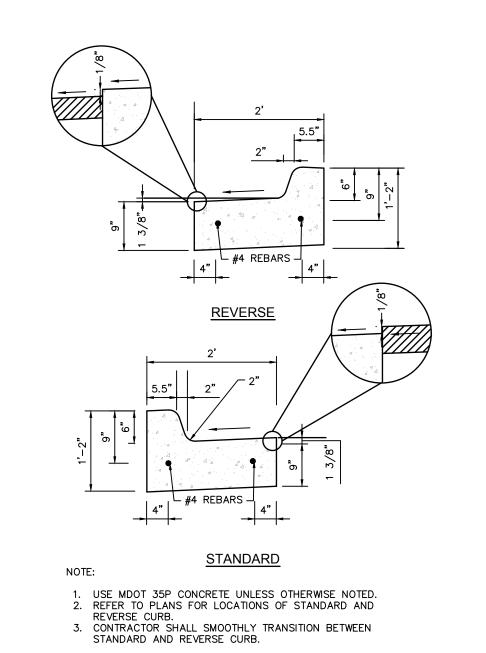
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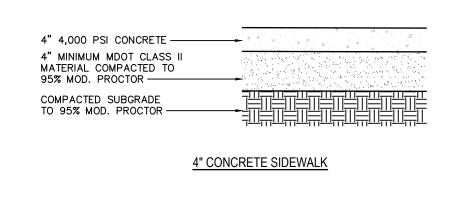




CONCRETE CURB & GUTTER

SIDEWALK RAMP TYPE R

SIDEWALK RAMP AND



6" 4,000 PSI CONCRETE ----6" MINIMUM MDOT CLASS II MATERIAL COMPACTED TO 95% MOD. PROCTOR -COMPACTED SUBGRADE TO 95% MOD. PROCTOR -

6" CONCRETE SIDEWALK

MAXIMUM TURNING SPACE SLOPE IS:2.0% IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5' x 5'. SEE MOTES.

... MAXIMUM RAMP CROSS SLOPE IS 2.0%. RUNNING SLOPE 5% 7% (8.3% MAXIMUM). SEE NOTES.

REINFORCEMENT AS IN ADJACENT CURB & GUTTER

- 1. IF ANY OF THE EXISTING BASE CANNOT BE UTILIZED, IT SHALL BE REMOVED AND REPLACED WITH MOOT CLASS II MATERIAL 2. BROOM FINISH SURFACE. 3. IF NOT INDICATED ON THE PLANS THE CONTRACTOR SHALL SUBMIT A
- JOINTING PLAN PRIOR TO PLACEMENT OF CONCRETE PAVEMENT. 4. SAWCUT JOINTS AS SOON AS PAVEMENT CAN SUPPORT MACHINE 5. PLACE EXPANSION JOINT BETWEEN NEW WALKS AND EXISTING PAVEMENT AND/OR BUILDINGS.



SIDEWALK RAMP TYPE RF

(ROLLED / FLARED SIDES)

PAVEMENT SHALL END FLUSH WITH THE GUTTER PAN

RAMP SLOPE ---

SECTION THROUGH CURB CUT (TYPICAL ALL RAMP TYPES)

" EXPANSION JOINT

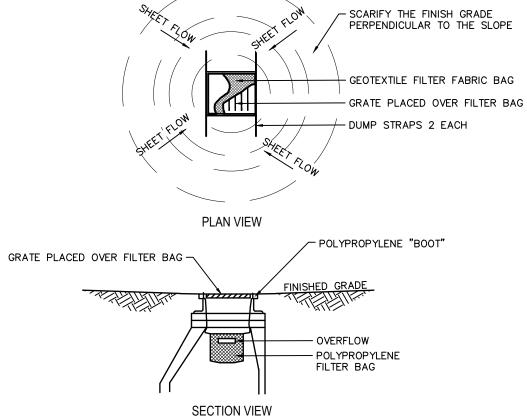
RAMP AND TURNING SPACE SLAB THICKNESSES SHALL BE AS CALLED FOR ON THE PLANS

WITH BACK OF CURB

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR

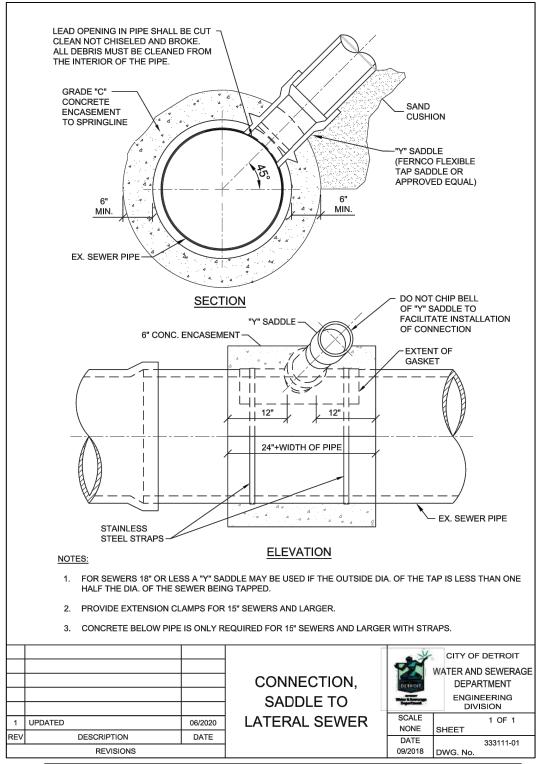
SIDEWALK RAMP AND

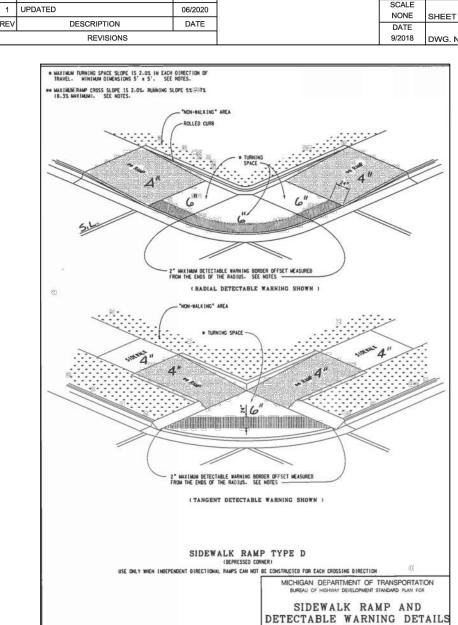
DETECTABLE WARNING DETAILS



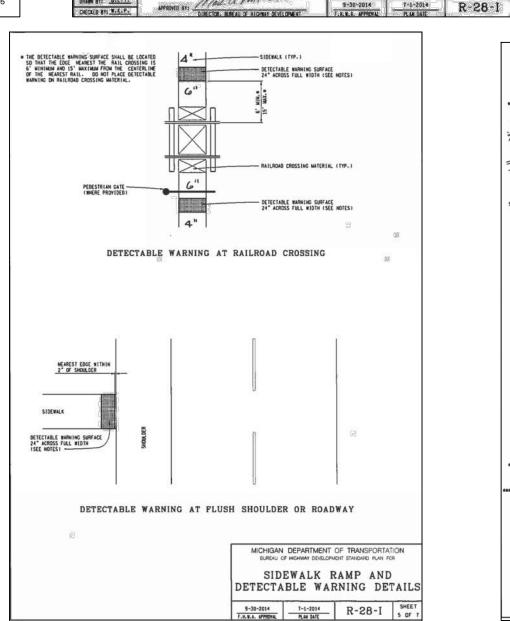
- 1. INSTALL TEMPORARY SEDIMENT INLET FILTER TO BE INSTALLED ON ALL PAVED CATCH BASINS OR STORM INLETS, OR AS SPECIFIED ON THE SOIL EROSION
- 2. INLET FILTER TO BE SIMILAR TO "STEAMGUARD" AS MANUFACTURED BY STORMWATER SERVICES CORPORATION OR "SILTSACK" AS MANUFACTURED BY ATLANTIC CONSTRUCTION FABRICS, INC.
- 3. CLEAN FILTER AS NEEDED, OR AS REQUIRED BY THE SOIL EROSION AND SEDIMENT

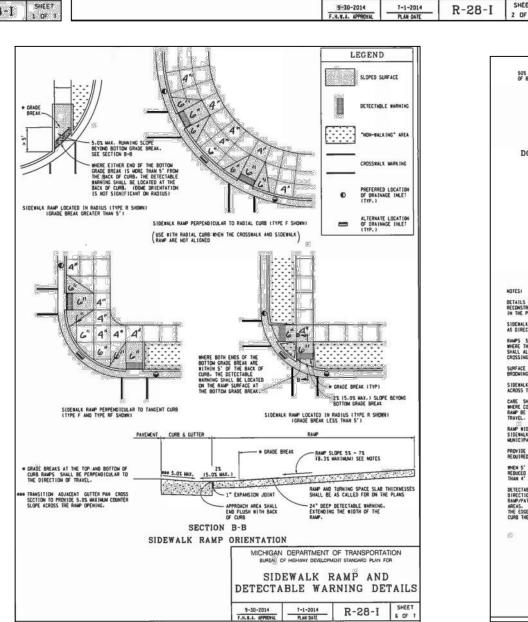


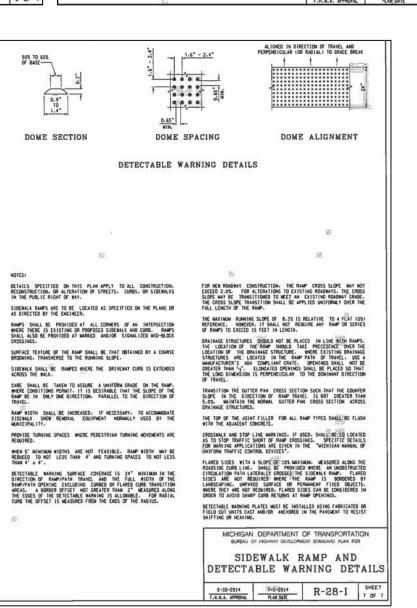


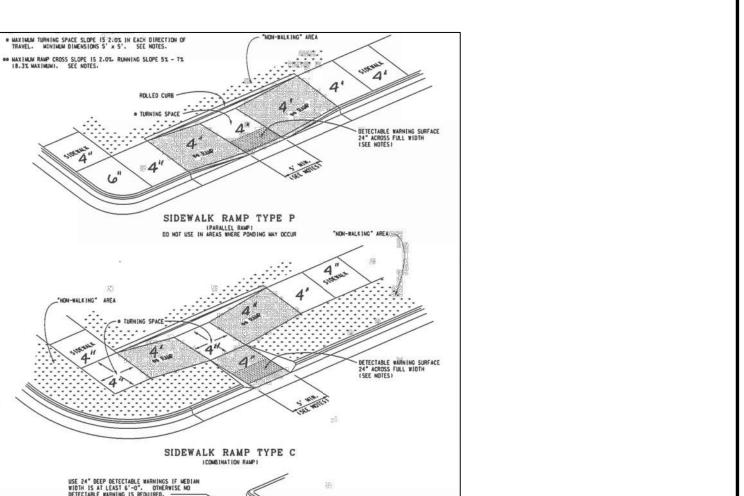


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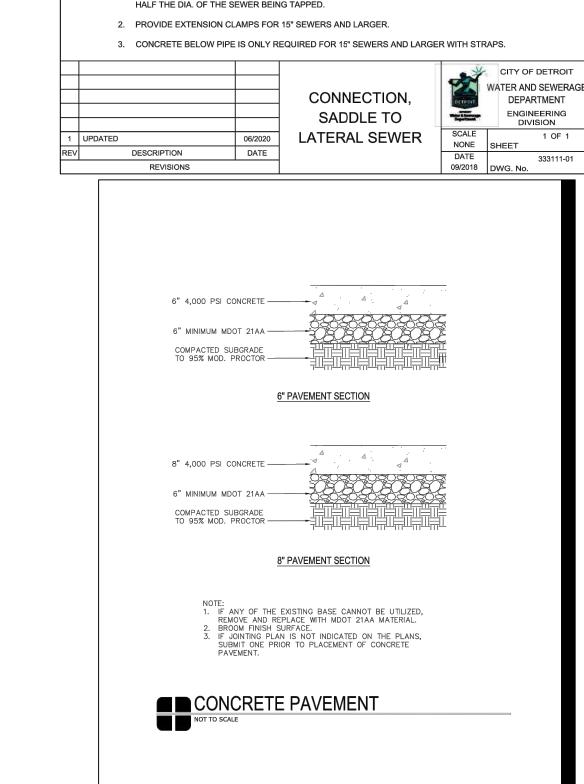
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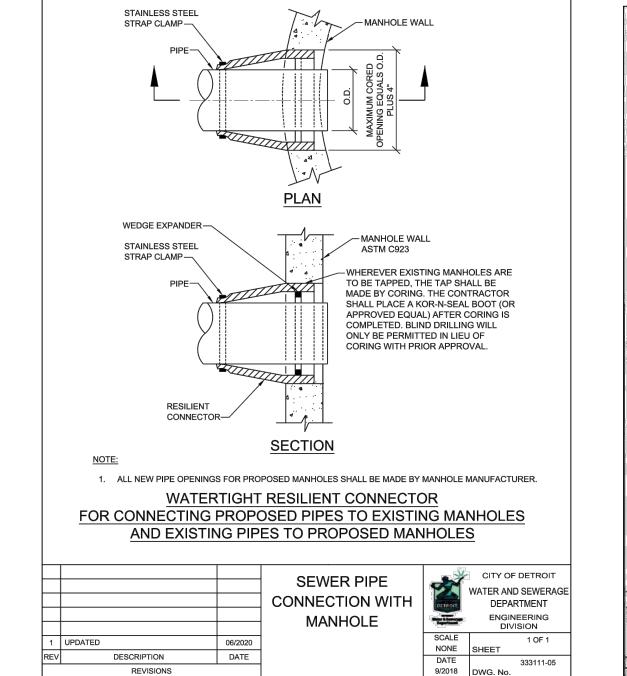
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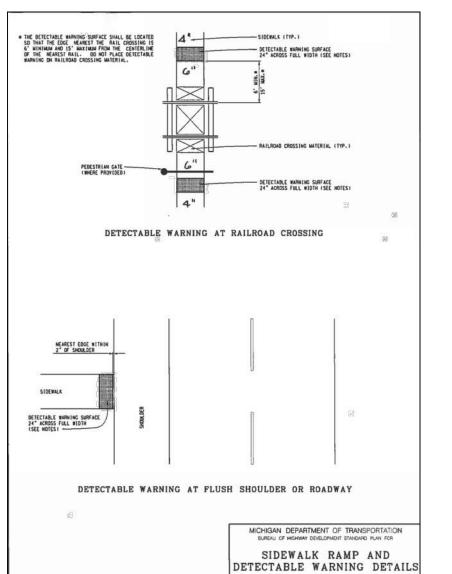
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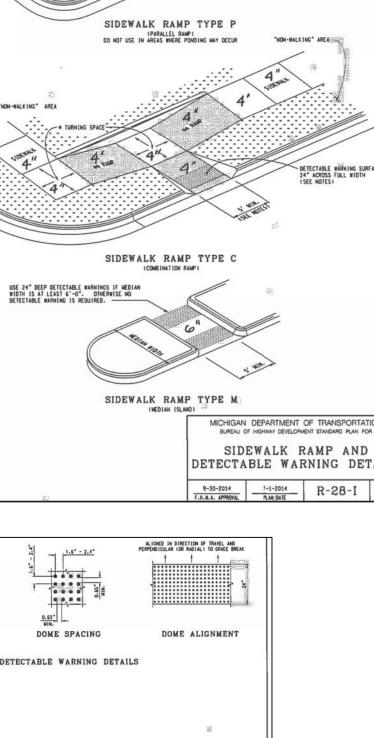
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* MAXIMUM TURNING SPACE SLOPE IS 2.02 IN EACH DIRECTION OF TRAVEL. MINIMUM DIMENSIONS 5" x 5. SEE NOTES.

** MAXIMAN TRAF CROSS SLOPE IS 2.0%, RUNNING SLOPE 5% - 7% (8.3% MAXIMAN), SEE NOTES.

DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAY DEVELOPMENT STANDARD PLAN FOR SIDEWALK RAMP AND DETECTABLE WARNING DETAILS

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