

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE.

(R) = UTILITY SHOWN FROM RECORDS OR PLANS, & FIELD LOCATED WHERE POSSIBLE.

PRIOR TO THE PLANNED BUILDING IMPROVEMENTS, AND/OR CONSTRUCTION, THE RESPECTIVE UTILITY COMPANIES MUST BE NOTIFIED TO STAKE THE PRECISE LOCATION OF THEIR UTILITIES.

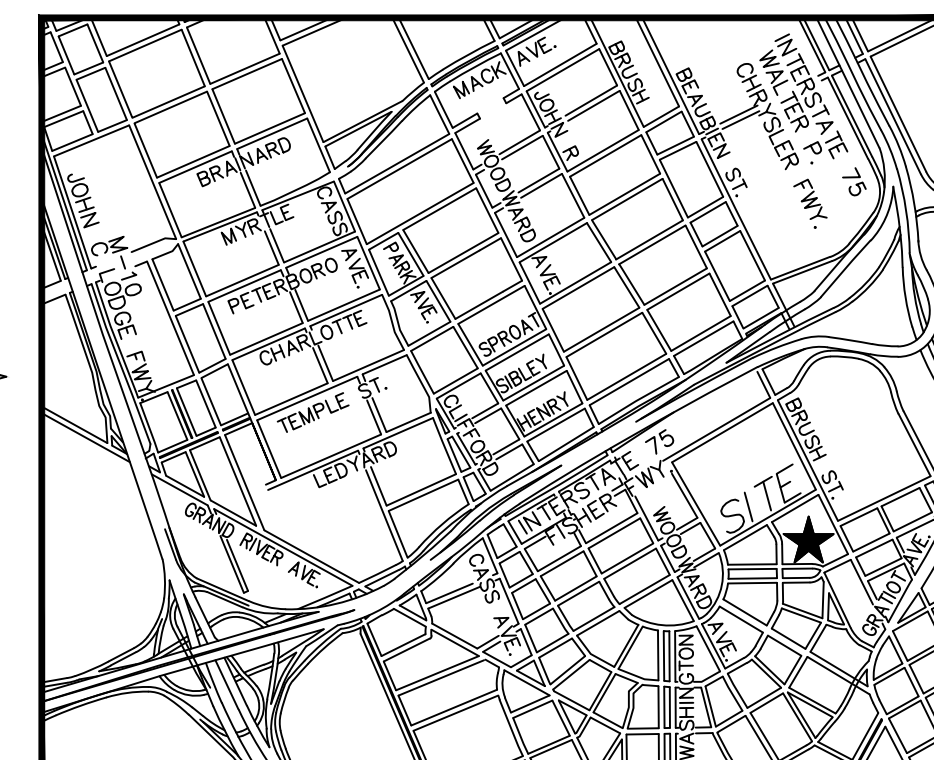
NOTES

- CURRENT CABLE TV, PUBLIC LIGHTING, WATER, AND SEWER SEWER UTILITY MAPS WERE NOT AVAILABLE AT TIME OF SURVEY.
- THIS PARCEL IS LOCATED WITHIN A FEDERALLY DESIGNATED FLOOD HAZARD AREA ZONE "X" (AREA DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN) IN ACCORDANCE WITH THE FEDERAL INSURANCE RATE MAP, PANEL NUMBER 285 OF 575, MAP NUMBER 26163C0285E, EFFECTIVE DATE FEBRUARY 2, 2012.
- NO BOUNDARY SURVEY WAS PERFORMED BY GIFFELS-WEBSTER ENGINEERS, INC. AT TIME OF SURVEY.
- NO TITLE COMMITMENT POLICY WAS PROVIDED, THEREFORE EASEMENTS MAY EXIST THAT ARE NOT SHOWN.
- NO CERTIFICATION CAN BE MADE AS TO LIMITS OF OWNERSHIP.

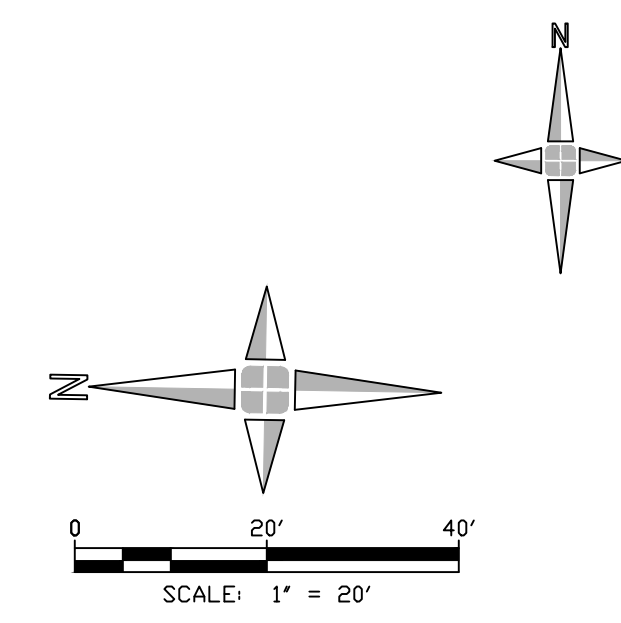
BENCH MARK DATA

(CITY OF DETROIT) DATUM

SITE BENCH MARK NO. 1
ARROW ON HYDRANT LOCATED AT THE SOUTHWEST CORNER OF JOHN R STREET AND ADAMS AVENUE.
ELEVATION=122.65'



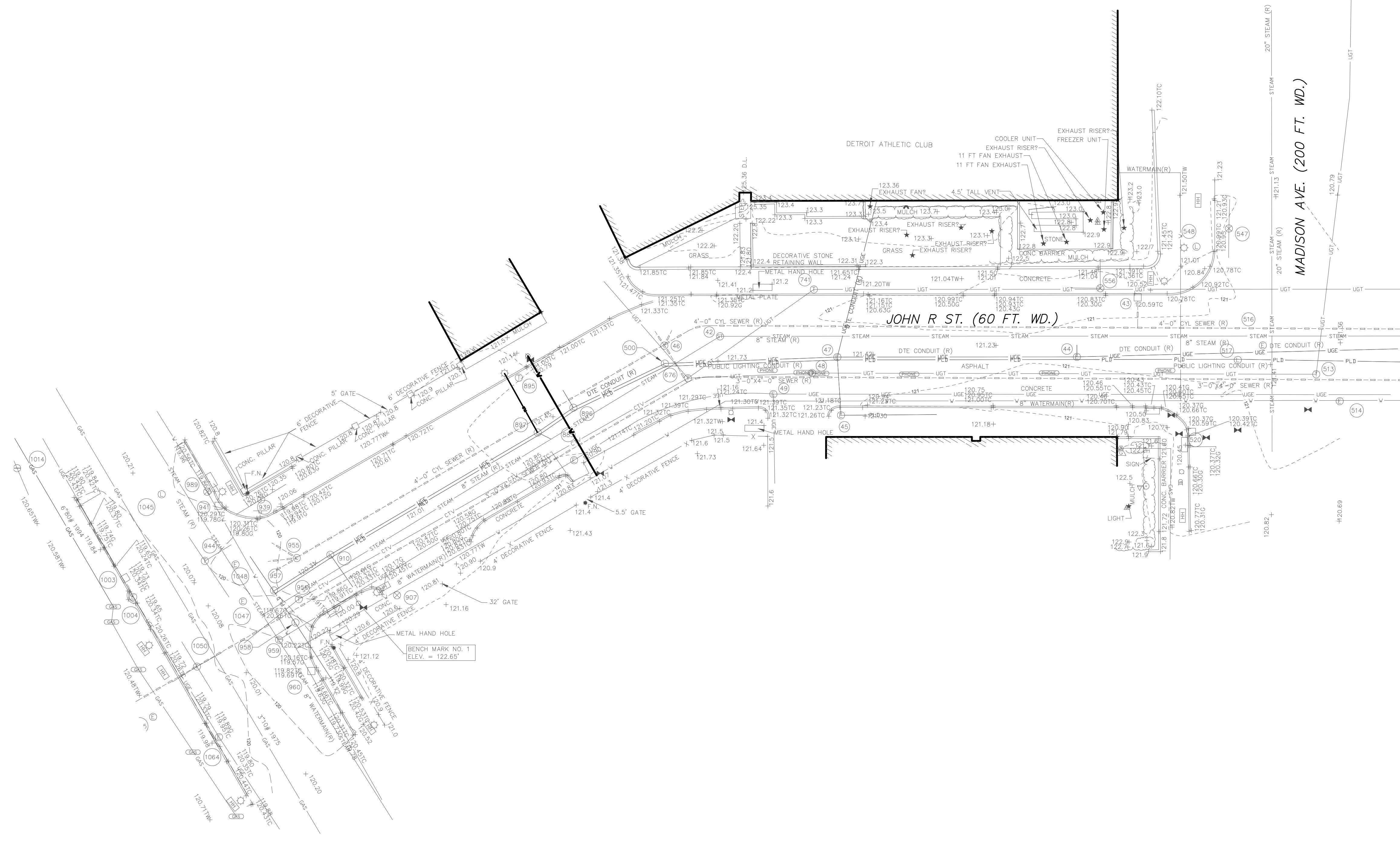
LOCATION MAP
(NOT TO SCALE)



EXISTING CONDITIONS - LEGEND

	E/ SITE BOUNDARY
	E/ PAVEMENT
	CURB
	E/ GRAVEL
	CL ROAD
	E/ WALK
	RAILROAD
	E/ BRICK
	MISC. LINE
	FENCE
	GUARD RAIL
	WALL
	BLDG. LINE
	OVERHEAD WIRES
	RAILING
	OVERHANG LINE
	T/ BANK
	B/ BANK
	CL DITCH/STREAM
	SHRUB LINE
	TREE LINE
	WATER EDGE
	WETLAND LINE
	STORM LINE
	SAN. LINE
	WATER LINE
	GAS LINE
	UG ELEC. LINE
	UG TELE. LINE
	UG CABLE TV LINE
	PAINT STRIPE
	COMBINED SEWER
	PLD LINE
	STEAM LINE
	SECTION LINE
	ELECTRIC OVERHEAD
	TELEPHONE OVERHEAD
	RIGHT-OF-WAY

	SAN. MH		PROT. POST/GUARD POST
	SAN. CLEAN OUT		GUY
	SAN. RISER		DECIDUOUS TREE
	SAN. PUMP STATION		CONIFEROUS TREE
	COMB. MH		DEAD TREE
	GATE VALVE		UTILITY FLAG
	HYDRANT		BLDG. CORNER (FIELD LOCATED)
	WATER VALVE		HANDICAP PARKING
	WATER METER		WETLAND FLAG
	POST INDICATOR VALVE		BUSH/SHRUB
	WELL HEAD		PARKING METER
	FDC CONNECTION		RESIDENTIAL MAILBOX
	IRRIGATION CONTROL BOX		U.S. MAILBOX
	STORM MH		EXISTING ELEVATION
	CATCH BASIN		SOIL BORING
	BEEHIVE C.B.		MONITORING WELL
	CULVERT E.S.		LAWN IRRIG. HEAD
	ROOF/DOWN SPOUT		CENTERLINE R.R. TRACK
	OVERFLOW/OUTLET STRUCTURE		MISC. TOPO. SHOT
	STORM CLEAN OUT		SURVEY CONTROL POINT
	LIGHT POLE		FOUND IRON
	UTILITY POLE		FOUND NAIL
	ELEC. TRANS.		F. CUT CROSS
	ELEC. MH		SECTION COR.
	ELEC. METER		FENCE POST
	ELEC. RISER		BENCHMARK
	TRAFFIC CONTROL BOX		FOUND PIPE
	STEAM MH		FOUND MON.
	PUBLIC LIGHTING MH		ASPH.
	GAS METER		CONC.
	GAS RISER		A.C.
	GAS VALVE		G.P.
	GAS MH		C.L.F.
	TELE. RISER		F.F.
	TELE. MH		O.H.
	TELE. CROSS BOX		F.I.
	CABLE RISER		S.I.
	MANHOLE		F.I.P.
	SIGN		M.
			R.
			S.N.
			CMP
			RCP



giffels webster

Engineers
Surveyors
Planners
Landscape Architects

28 West Adams Road
Suite 1200
Detroit, MI 48226
p (313) 962-4442
f (313) 962-5088
www.giffelswebster.com

Executive:	MM
Manager:	LE
Designer:	FG
Quality Control:	LE
Section:	

Professional Seal:

811

Know what's below.
Call before you dig.

DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

P: 313.442.1048

EXISTING CONDITIONS

DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date:	06.09.2023
Scale:	1"=20'
Sheet:	C-200
Project:	19566.01D

Copyright © 2023 Giffels Webster. No reproduction or use is permitted without the prior written consent of Giffels Webster.

Executive: MM
Manager: LE
Designer: FG
Quality Control: LE
Section:

Professional Seal:



Know what's below.
Call before you dig.

DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

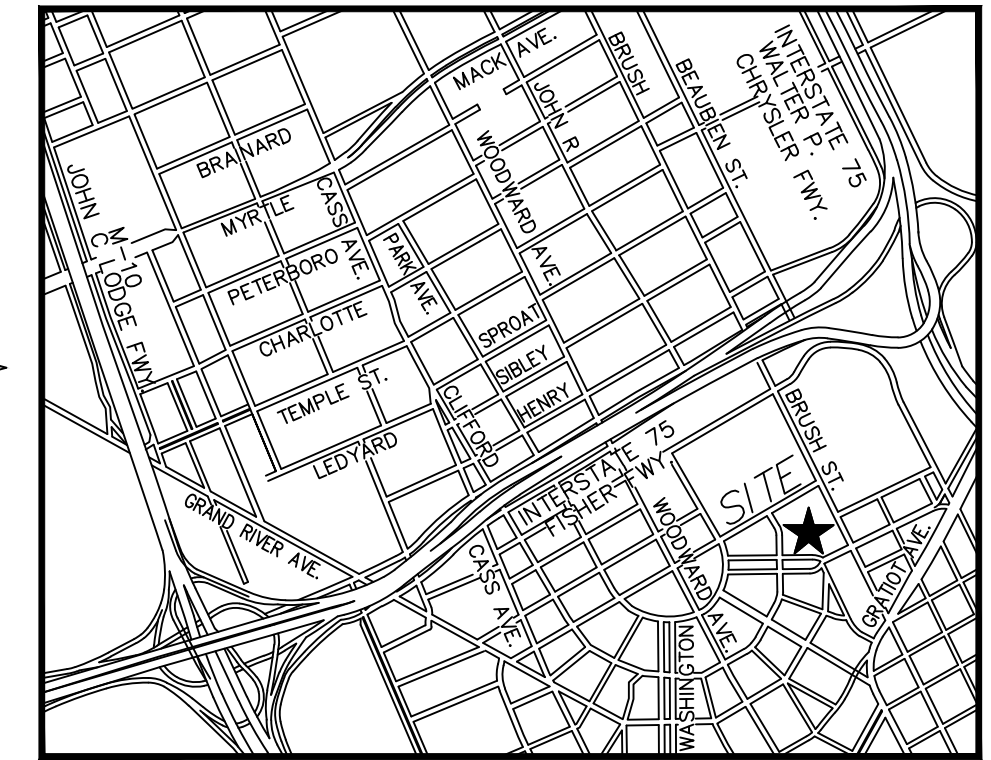
P: 313.442.1048

DEMOLITION PLAN

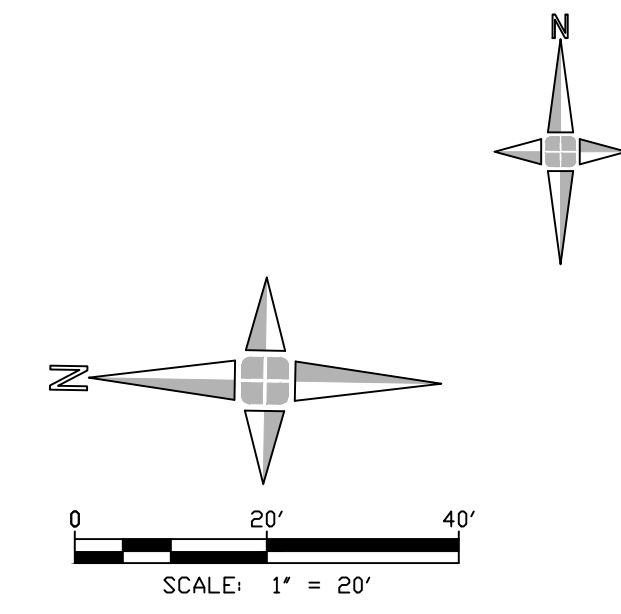
DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date: 06.09.2023
Scale: 1"=20'
Sheet: C-300
Project: 19566.01D



LOCATION MAP
(NOT TO SCALE)

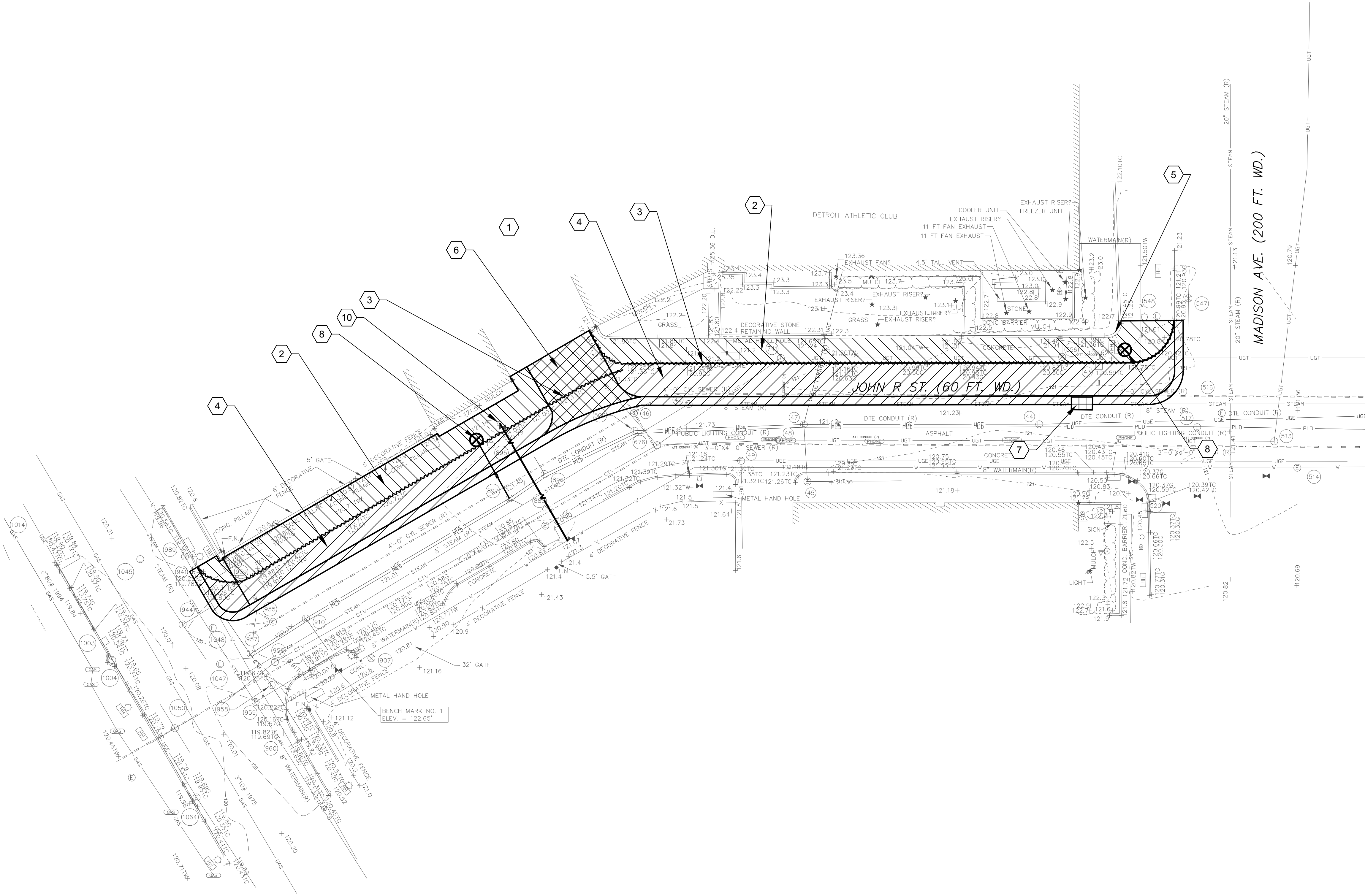


DEMOLITION PLAN - LEGEND

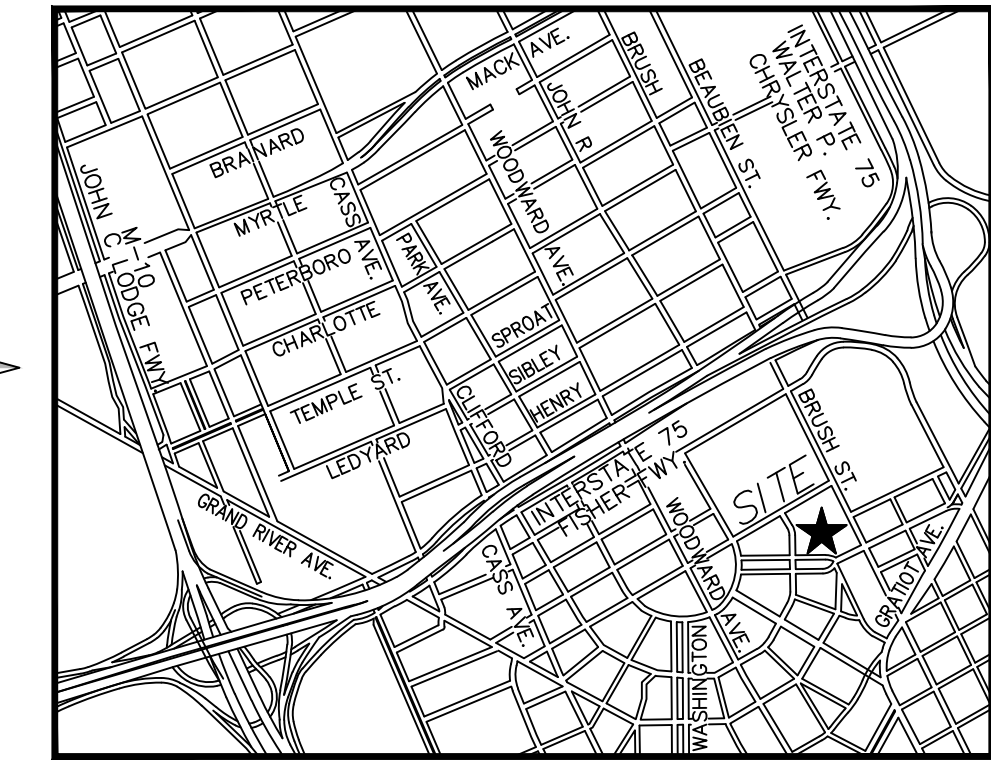
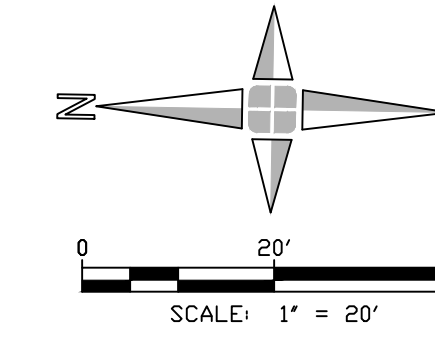
- REMOVE/DEMOLISH UTILITY LINE · · · · ·
- REMOVE/DEMOLISH CURB AND GUTTER ~ ~ ~ ~ ~
- REMOVE FENCE - - - - -
- REMOVE/DEMOLISH UTILITY STRUCTURE ⊗
- REMOVE TREE, SHRUB, OR ITEM X
- REMOVE DRIVEWAY PAVEMENT [Cross-hatched box]
- REMOVE ASPHALT PAVEMENT [Diagonal hatched box]
- REMOVE CONCRETE SIDEWALK [Horizontal hatched box]
- REMOVE GRASS, BRUSH, GRAVEL AND/OR DEBRIS [Dotted box]
- REMOVE FULL DEPTH ASPHALT PAVEMENT [Grid pattern box]

DEMOLITION PLAN - KEYNOTES

- 1 PROPERTY LINE, TYP
- 2 REMOVE CONCRETE SIDEWALK PAVEMENT, TYP
- 3 REMOVE AND DISPOSE OF CONCRETE CURB, TYP
- 4 REMOVE ASPHALT PAVEMENT, TYP
- 5 CURB TO REMAIN, TYP
- 6 REMOVE DRIVEWAY PAVEMENT
- 7 REMOVE FULL DEPTH ASPHALT PAVEMENT.
- 8 DEMOLISH AND STOCKPILE TRAFFIC SIGNAL POLE FOR RELOCATION AND INSTALLATION.
- 9 DEMOLISH AND STOCKPILE LIGHT POLES FOR RELOCATION AND INSTALLATION.
- 10 RELOCATE FIRE HYDRANT TO BE 3 FEET OFF CURB FACE



SOIL EROSION QUANTITY TABLE	
DESCRIPTION	QUANTITY
SEDIMENT INLET FILTER	1 - EACH



LOCATION MAP

EROSION AND SEDIMENT CONTROL PLAN - LEGEND

- SEDIMENT INLET FILTER
- SEDIMENT CONTROL FENCE
- LIMITS OF DISTURBANCE (1.45 ACRES)
- MUD MAT ENTRANCE

EROSION AND SEDIMENT CONTROL PLAN - KEY NOTES

- 1 EXISTING PROPERTY LINE, TYP
- 2 PROVIDE, INSTALL AND MAINTAIN SEDIMENT INLET FILTER DURING CONSTRUCTION. SEE DETAILS ON SHEET C-800

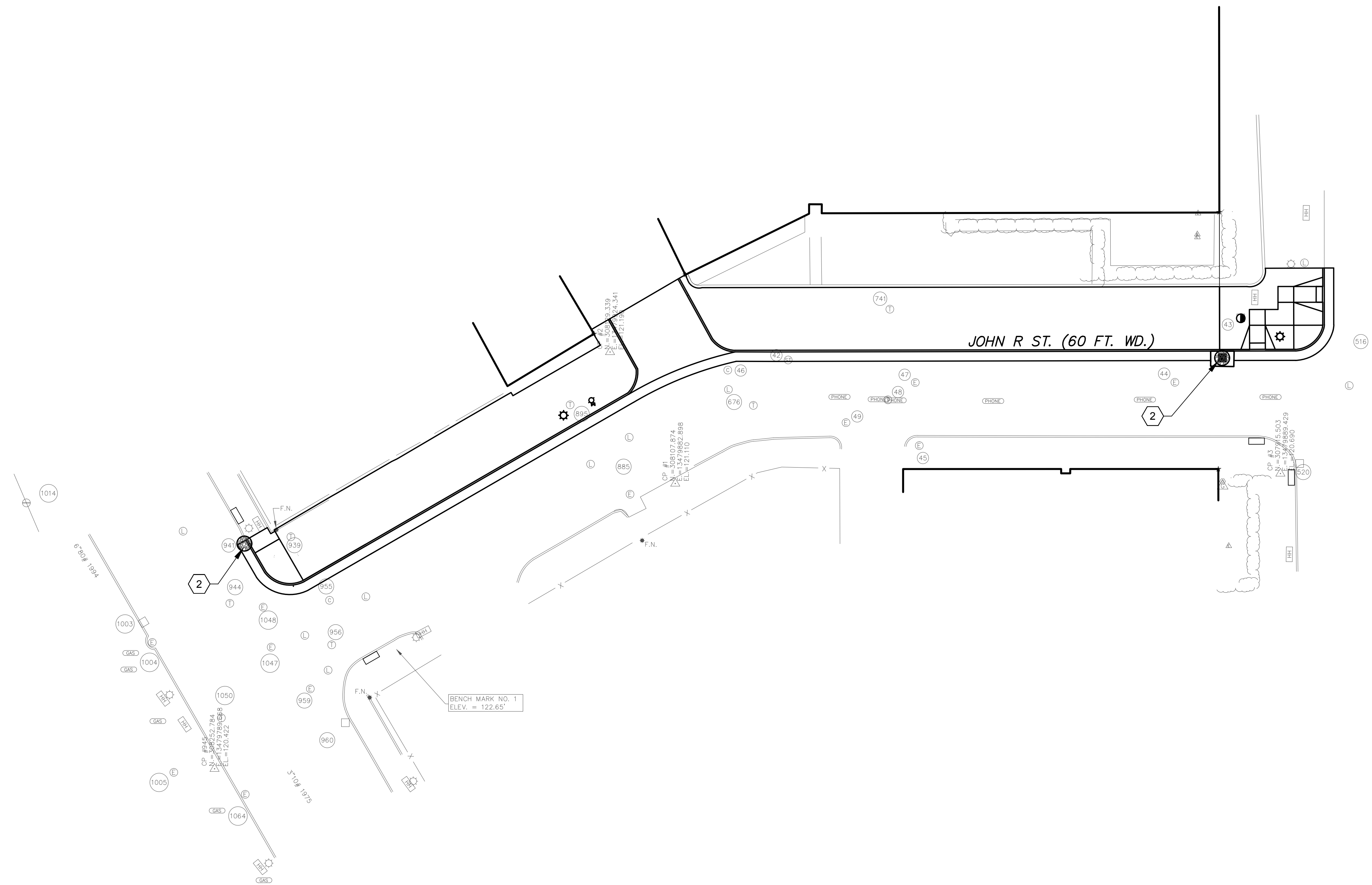
EROSION AND SEDIMENT CONTROL PLAN -NOTES

1. SEE SHEET C100 FOR GENERAL LEGEND AND ADDITIONAL NOTES.
2. SEE SHEET C800 FOR SESC DETAILS.
3. SOIL EROSION CONTROL MEASURES ARE SHOWN FOR THE INITIAL DEMOLITION AND MASS GRADING PHASES OF THE PROJECT. THE CONTRACTOR SHALL MAINTAIN AND ADJUST THESE MEASURES AS OTHER PHASES OF CONSTRUCTION (I.E. BUILDINGS AND PAVEMENTS) ARE UNDERTAKEN TO ENSURE THAT ALL SEDIMENT IS CONTAINED ON-SITE.
4. THE DEVELOPMENT OF THIS SITE WILL NOT RESULT IN ANY INCREASE IN DUST, ODOR, SMOKE, FUMES, NOISE, LIGHTS, OR OTHER OBJECTIONABLE FEATURES.

SOIL EROSION AND SEDIMENTATION CONTROL SEQUENCE

1. INSTALL MUD MATS, SILT FENCE AND INLET FILTERS AT ALL EXISTING LOCATIONS AS SHOWN AND AS REQUIRED TO ACHIEVE ON-SITE CONTAINMENT.
2. INSTALL STORM SEWER AND ALL ASSOCIATED STORM WATER IMPROVEMENTS AS SHOWN ON PLANS. IMMEDIATELY INSTALL INLET FILTERS AT ALL CATCH BASINS.
3. ROUGH GRADE THE PROJECT "WORK AREA" AS NEEDED.
4. INSTALL OTHER UTILITIES (GAS, ELECTRIC, PHONE, CABLE, ETC.) AND/OR ALL NEEDED CONDUITS AND SLEEVES.
5. INSTALL PAVEMENT BACKFILL AND SEED & MULCH ALL DISTURBED AREAS.
6. CLEAR ALL ACCUMULATED SILT AND REMOVE ALL EROSION AND SEDIMENT CONTROL DEVICES.
7. INSTALL LANDSCAPING MATERIALS AS INDICATED PER PLANS & RE-SEED, FERTILIZE AND MULCH ALL DISTURBED AREAS.

STREET CLEANING SCHEDULE							
	SUNDAY	MONDAY	TUESDAY	WEDNESDAY	THURSDAY	FRIDAY	SATURDAY (IF WORKING)
SCRAPE STREETS		√	√	√	√	√	√
SWEEP STREETS				√			



Executive:	MM
Manager:	LE
Designer:	FG
Quality Control:	LE
Section:	

Professional Seal:



DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

P: 313.442.1048

SOIL EROSION & SEDIMENT CONTROL PLAN

DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date:	06.09.2023
Scale:	1"=20'
Sheet:	C-400
Project:	19566.01D

Executive:	MM
Manager:	LE
Designer:	FG
Quality Control:	LE
Section:	

Professional Seal:



811
Know what's below.
Call before you dig.

DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

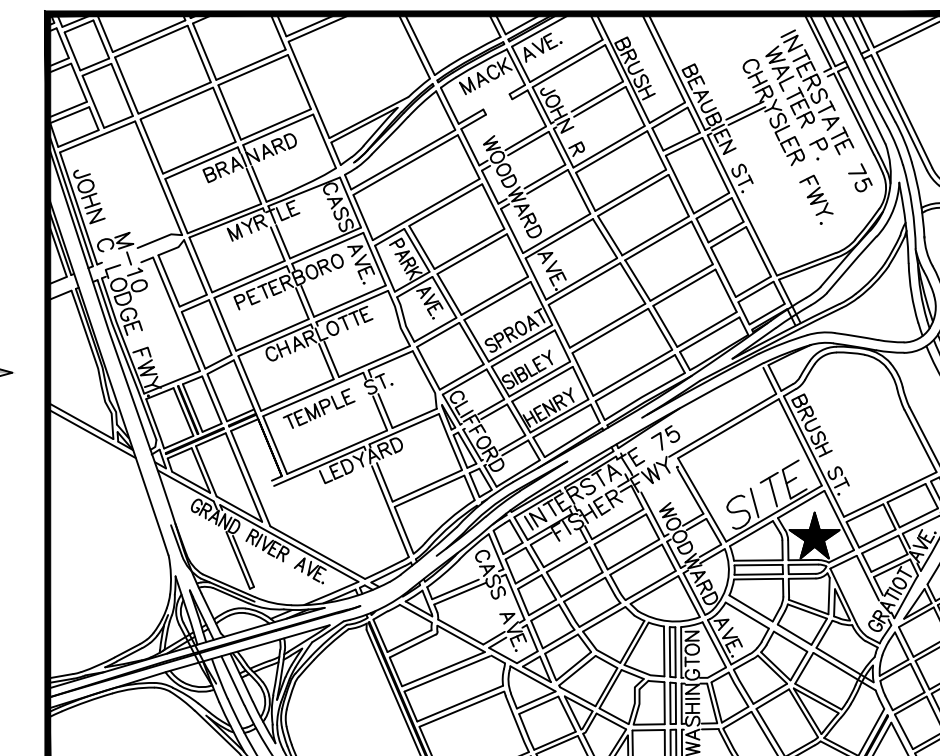
P: 313.442.1048

**GEOMETRIC &
PAVING PLAN**

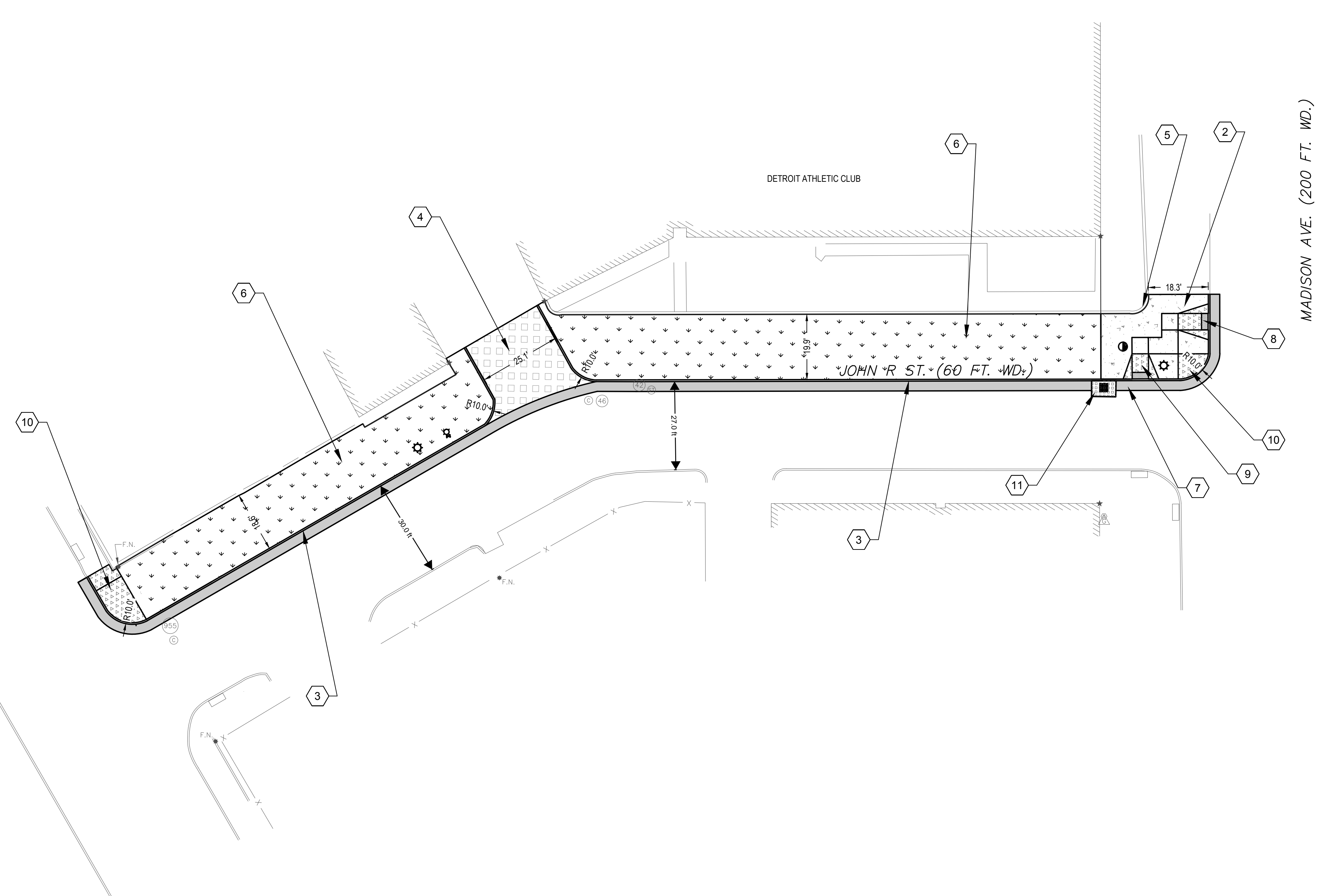
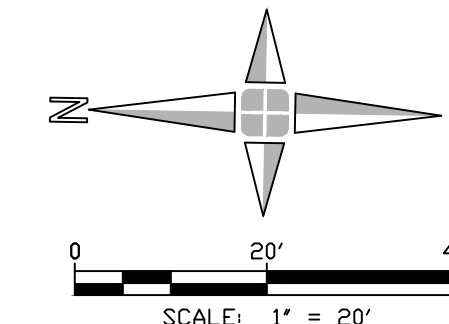
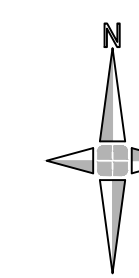
DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date:	06.09.2023
Scale:	1"=20'
Sheet:	C-500
Project:	19566.01D



LOCATION MAP

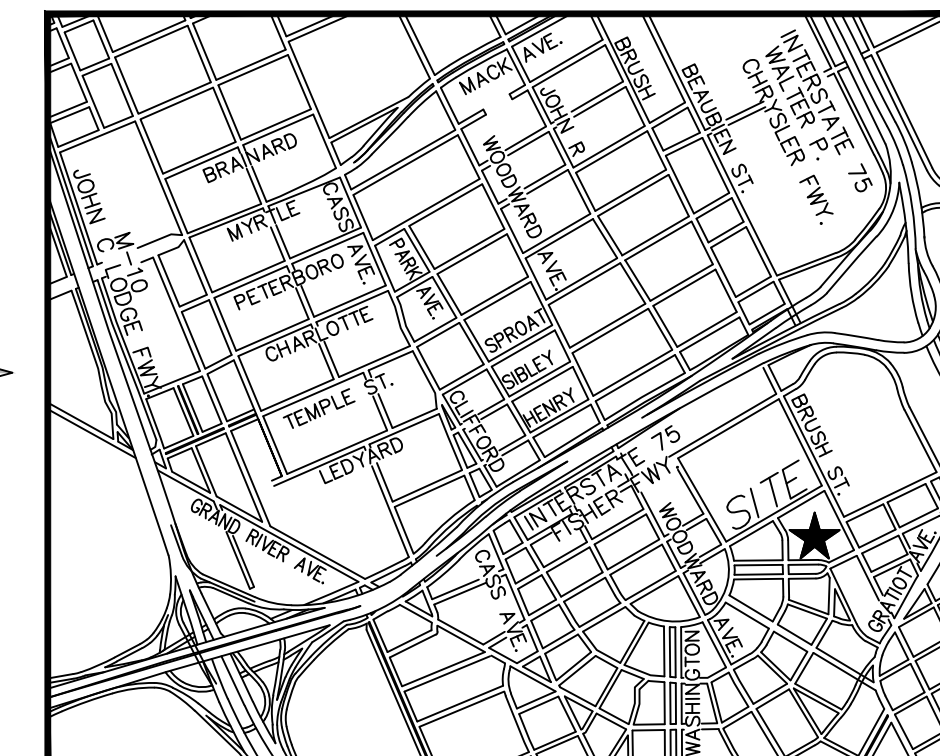


PAVING PLAN - LEGEND

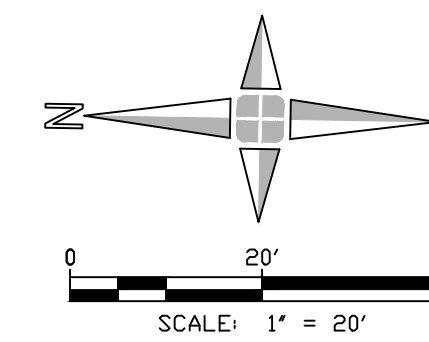
- 4" CONCRETE SIDEWALK PAVEMENT
- 8" CONCRETE PAVEMENT
- PAVEMENT REPAIR PER CITY OF DETROIT STANDARDS AND SPECIFICATIONS
- ASPHALT PAVEMENT REPAIR
- PROPOSED GRASS
- 6" CONCRETE SIDEWALK PAVEMENT

PAVING PLAN - KEY NOTES

- 1 PROPERTY LINE
- 2 NEW 4" CONCRETE SIDEWALK PAVEMENT, TYP. SEE DETAIL ON SHEET C-800
- 3 NEW 6" CURB. SEE DETAIL ON SHEET C-800.
- 4 NEW 8" CONCRETE DRIVEWAY PAVEMENT. SEE DETAIL ON SHEET C-800
- 5 EXISTING CURB TO REMAIN
- 6 PROPOSED LANDSCAPE, TYP
- 7 ASPHALT PAVEMENT REPAIR, 3'-0" WIDE (MIN.), PER CITY OF DETROIT STANDARDS AND SPECIFICATIONS. EXISTING STREET ASPHALT PAVEMENT SHALL BE MILLED AND REPAIRED TO NEW GUTTER GRADE PER GRADING PLAN. ASPHALT PAVEMENT SHALL HAVE UNIFORM SLOPE TOWARDS FACE OF CURB.
- 8 NEW DETECTABLE WARNING STRIP PER CITY OF DETROIT STANDARDS. SEE DETAILS ON SHEET C-800.
- 9 ADA COMPLIANT BARRIER FREE CURB RAMP WITH 6" THICK CONCRETE PER CITY OF DETROIT STANDARDS.
- 10 NEW 6" CONCRETE SIDEWALK PAVEMENT, TYP. SEE DETAIL ON SHEET C-800
- 11 8" CONCRETE COLLAR FOR CATCH BASIN. SEE CONCRETE PAVEMENT DETAIL ON SHEET C-800.



LOCATION MAP



GRADING PLAN - GRADING LEGEND

EXISTING	PROPOSED
SPOT ELEVATION	×[XXXX.XX]
MATCH EXISTING GRADE	×[XXXX.XX]
FF	FINISHED FLOOR
FG	FINISHED GRADE
TWALL	TOP OF WALL
TS	TOP OF STEP
TC	TOP OF CURB
G	GUTTER
R	ADA RAMP, SLOPE <= 8.33%
L	ADA LANDING, SLOPE <= 2%
RXXX'	RADIUS OF A CURVE, MEASURED FROM FACE OF CURB

GRADING PLAN - NOTES

- SEE SHEET C-100 FOR ADDITIONAL PROJECT NOTES AND INFORMATION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONFLICT OR DISCREPANCY IS IDENTIFIED WITH NEW FINISHED GRADES.
- THE CONTRACTOR SHALL VERIFY ALL EXISTING GRADES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY CONFLICT OCCURS WHERE NEW FINISHED GRADES MATCH OR TIE-INTO EXISTING GRADES.
- PAVEMENT SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL BE IN ACCORDANCE WITH ADA STANDARDS FOR ACCESSIBLE DESIGN.
- THE CROSS SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL NOT EXCEED 1 IN 50 (2%) AND THE RUNNING SLOPE OF ALL ADA ACCESSIBLE ROUTES SHALL NOT EXCEED 1 IN 20 (5%).
- THE CONTRACTOR SHALL PROVIDE A MINIMUM 5' LANDING AT THE TOP AND BOTTOM OF ALL DOORS, STAIRS, ADA RAMPS AND FENCE GATES IN ACCORDANCE WITH ADA STANDARDS FOR ACCESSIBLE DESIGN. THE LANDING SLOPE SHALL NOT EXCEED 1 IN 50 (2%) IN ALL DIRECTIONS.
- ALL NEW PAVING AND STREET REPAIRS WITHIN THE RIGHT-OF-WAY SHALL COMPLY WITH CITY OF DETROIT STANDARDS.
- ALL PAVEMENT AND GRASSY AREAS SHALL DIRECT RUNOFF TO CATCH BASIN OR DRAIN INLET STRUCTURES WITHOUT ANY PONDING OR BIRDBATHS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF ANY PONDING OR BIRDBATHS SHALL OCCUR.
- THE CASTING OF ALL EXISTING STRUCTURES INVOLVED IN THE SCOPE OF CIVIL WORKS SHALL BE FIELD ADJUSTED AND FLUSHED WITH THE FINISHED GRADE OF NEW PAVING.
- THE CONTRACTOR SHALL SUBMIT A JOINTING PLAN DETAILING ALL JOINTS BASED ON ACI GUIDELINES FOR REVIEW AND APPROVAL BY THE ENGINEER PRIOR TO PLACING CONCRETE.

Executive: MM
 Manager: LE
 Designer: FG
 Quality Control: LE
 Section:

Professional Seal:



Know what's below. Call before you dig.

DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

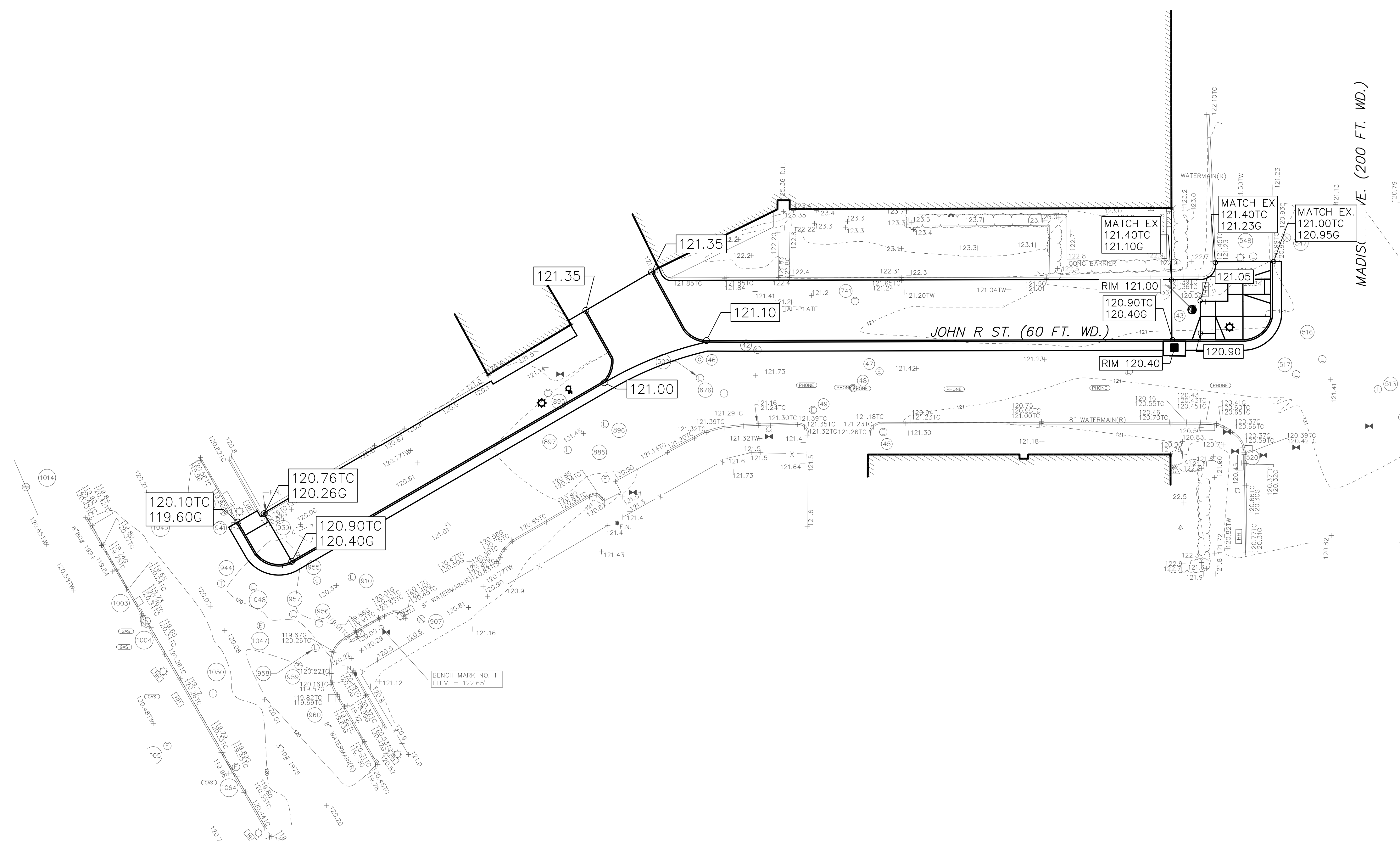
P: 313.442.1048

GRADING PLAN

DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date: 06.09.2023
 Scale: 1"=20'
 Sheet: C-700
 Project: 19566.01D



Executive:	MM
Manager:	LE
Designer:	FG
Quality Control:	LE
Section:	

Professional Seal:



Know what's below.
Call before you dig.

DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club

241 Madison
Detroit, Michigan 48226

P: 313.442.1048

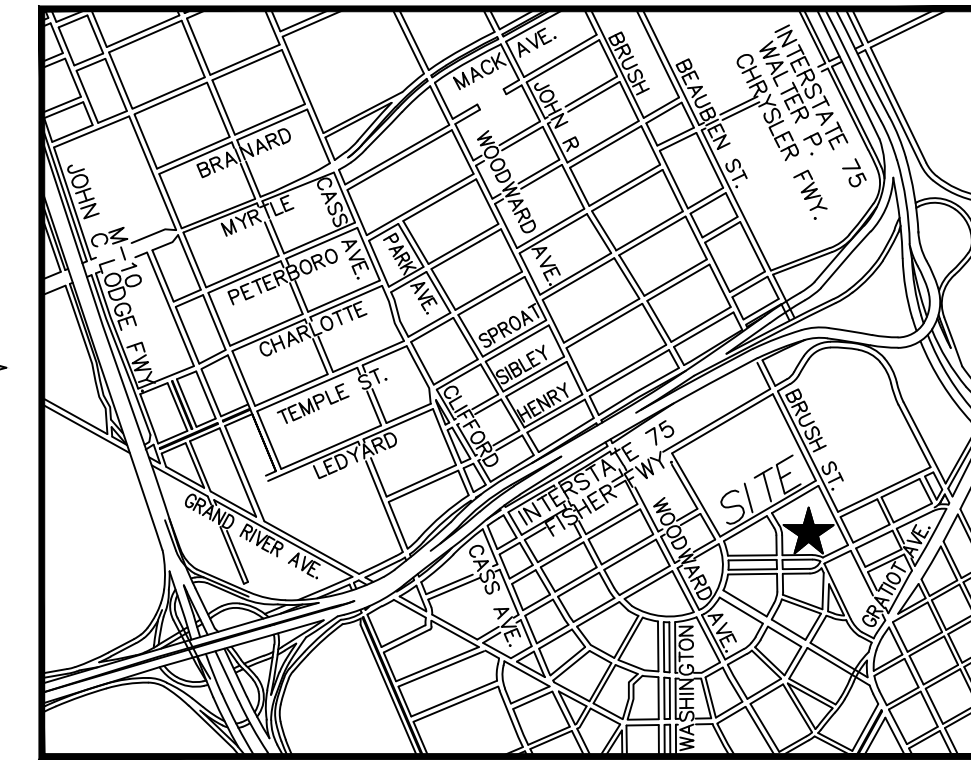
UTILITY PLAN

DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date:	06.09.2023
Scale:	1"=20'
Sheet:	C-700
Project:	19566.01D

Copyright © 2023 Giffels Webster. No reproduction shall be made without the prior written consent of Giffels Webster.



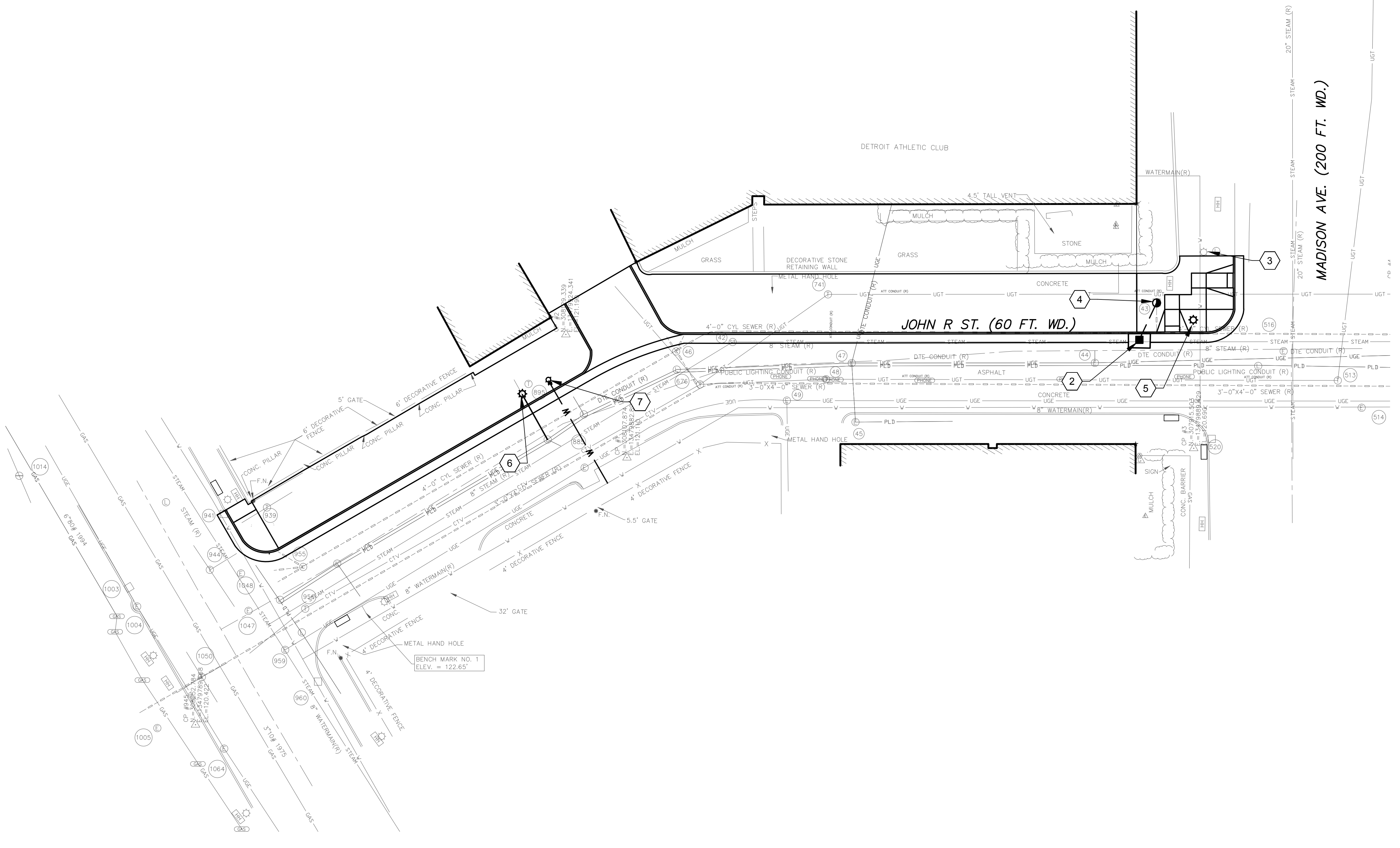
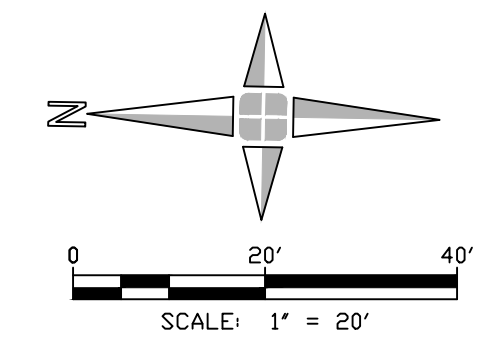
LOCATION MAP

UTILITY PLAN - LEGEND

STORM SEWER	---
SANITARY SEWER	- - - -
PERFORATED DRAIN	- · - · -
WATER SERVICE	- - - -
UNDERGROUND ELECTRIC	UGL
GAS SERVICE	GAS
COMMUNICATIONS	CTV
FIRE HYDRANT	●
STOP BOX AND VALVE	○
FIRE DEPARTMENT CONNECTION	⊙
CATCH BASIN	■
SANITARY SEWER DRAIN	⊠
STORM CLEANOUT	⊙
SANITARY MANHOLE	⊙
STORM MANHOLE	⊙
STORM SEWER CLEANOUT	⊙
UNDERGROUND DETENTION SYSTEM	▭

UTILITY PLAN - KEY NOTES

- ① EXISTING RIGHT OF WAY
- ② PROVIDE AND INSTALL NEW CATCH BASIN PER DWSD STANDARDS AND SPECIFICATIONS.
- ③ PROTECT LIGHT POLE THROUGHOUT CONSTRUCTION
- ④ CONVERT CATCH BASIN TO MANHOLE, INSTALL MANHOLE COVER
- ⑤ RELOCATE TRAFFIC SIGNAL POLE
- ⑥ RELOCATE LIGHT POLE
- ⑦ RELOCATE FIRE HYDRANT



Executive:	MM
Manager:	LE
Designer:	FG
Quality Control:	LE
Section:	

Professional Seal:

STANDARD MANHOLE PRECAST

CITY OF DETROIT WATER AND SEWERAGE DEPARTMENT ENGINEERING DIVISION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. 33561-02

REV	DESCRIPTION	DATE
1	UPDATED	09/20/18

MANHOLE FRAME AND COVER WITH LOGO - SEWER

CITY OF DETROIT WATER AND SEWERAGE DEPARTMENT ENGINEERING DIVISION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. 33561-03

REV	DESCRIPTION	DATE
1	UPDATED	09/20/18

CONCRETE CURB & GUTTER

NOT TO SCALE

NOTE:
1. USE MDO 35P CONCRETE UNLESS OTHERWISE NOTED.
2. REFER TO PLANS FOR LOCATIONS OF STANDARD AND REVERSE CURB.
3. CONTRACTOR SHALL SMOOTHLY TRANSITION BETWEEN STANDARD AND REVERSE CURB.

CONCRETE SIDEWALK

NOT TO SCALE

NOTE:
1. IF ANY OF THE EXISTING BASE CANNOT BE UTILIZED, IT SHALL BE REMOVED AND REPLACED WITH MDO CLASS II MATERIAL.
2. BROOM FINISH SURFACE.
3. IF NOT INDICATED ON THE PLANS THE CONTRACTOR SHALL SUBMIT A JOINTING PLAN PRIOR TO PLACEMENT OF CONCRETE PAVEMENT.
4. SAWCUT JOINTS AS SOON AS PAVEMENT CAN SUPPORT MACHINE.
5. PLACE EXPANSION JOINT BETWEEN NEW WALKS AND EXISTING PAVEMENT AND/OR BUILDINGS.

SEDIMENT INLET FILTER

NOT TO SCALE

NOTE:
1. INSTALL TEMPORARY SEDIMENT INLET FILTER TO BE INSTALLED ON ALL PAVED CATCH BASINS OR STORM INLETS, OR AS SPECIFIED ON THE SOIL EROSION CONTROL PLAN.
2. INLET FILTER TO BE SIMILAR TO "STEAMGUARD" AS MANUFACTURED BY STORMWATER SERVICES CORPORATION OR "SILTSAK" AS MANUFACTURED BY ATLANTIC CONSTRUCTION FABRICS, INC.
3. CLEAN FILTER AS NEEDED, OR AS REQUIRED BY THE SOIL EROSION AND SEDIMENT CONTROL PLAN.

CONNECTION, SADDLE TO LATERAL SEWER

CITY OF DETROIT WATER AND SEWERAGE DEPARTMENT ENGINEERING DIVISION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. 33311-01

REV	DESCRIPTION	DATE
1	UPDATED	09/20/18

SEWER PIPE CONNECTION WITH MANHOLE

CITY OF DETROIT WATER AND SEWERAGE DEPARTMENT ENGINEERING DIVISION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. 33311-05

REV	DESCRIPTION	DATE
1	UPDATED	09/20/18

SIDEWALK RAMP AND DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. R-28-1

SIDEWALK RAMP AND DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 2 OF 1
DATE: 09/20/18 DWG. No. R-28-1

SIDEWALK RAMP AND DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 3 OF 1
DATE: 09/20/18 DWG. No. R-28-1

CONCRETE PAVEMENT

NOT TO SCALE

NOTE:
1. IF ANY OF THE EXISTING BASE CANNOT BE UTILIZED, REMOVE AND REPLACE WITH MDO 21AA MATERIAL.
2. BROOM FINISH SURFACE.
3. IF JOINTING PLAN IS NOT INDICATED ON THE PLANS, SUBMIT ONE PRIOR TO PLACEMENT OF CONCRETE PAVEMENT.

SIDEWALK RAMP AND DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. R-28-1

DETECTABLE WARNING AT RAILROAD CROSSING

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. R-28-1

SIDEWALK RAMP ORIENTATION

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. R-28-1

DETECTABLE WARNING DETAILS

MICHIGAN DEPARTMENT OF TRANSPORTATION

SCALE: NONE SHEET: 1 OF 1
DATE: 09/20/18 DWG. No. R-28-1



DATE:	ISSUE:
2023-07-12	OWNER REVIEW

Developed For:
Detroit Athletic Club
241 Madison
Detroit, Michigan 48226
P: 313.442.1048

DETAILS

DAC John R Street

City of Detroit
Wayne County
MICHIGAN

Date:	06.09.2023
Scale:	N/A
Sheet:	C-800
Project:	19566.01D