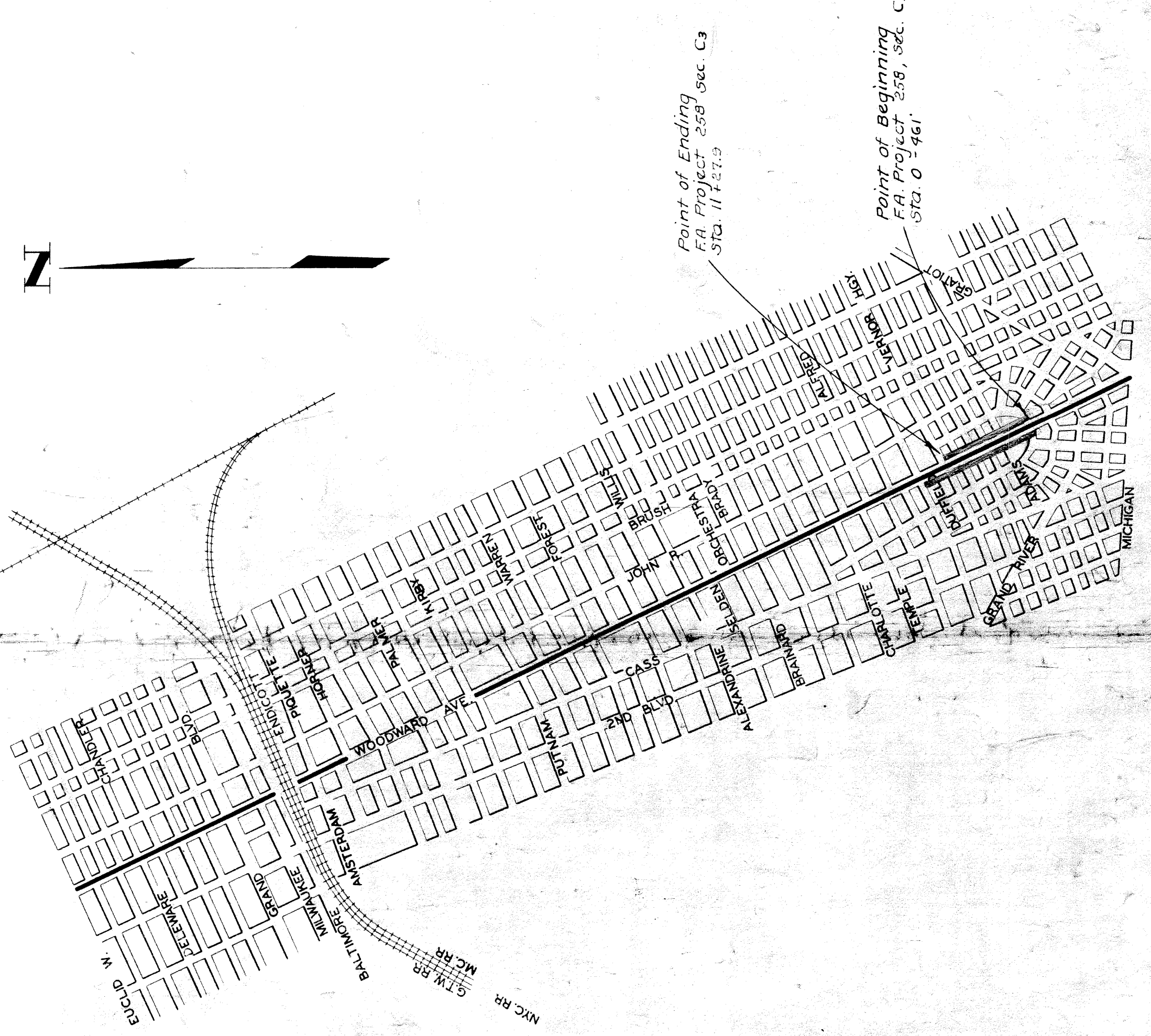


MICHIGAN
STATE HIGHWAY DEPARTMENT
MURRAY D. VAN WAGONER
COMMISSIONER

PLAN AND PROFILE OF PROPOSED
FEDERAL AID PROJECT NO. 258 SEC. C 3
DETROIT - PONTIAC ROAD
WAYNE COUNTY
CITY OF DETROIT

SCALES -
TITLE SHEET 1 IN. = 0.3 MI.
PLAN 1 IN. = 40 FT.
PROFILE 1 IN. = 4 FT.



Point of Ending
FA Project 258, Sec. C3
Sta. 11+27.9

Point of Beginning
FA Project 258, Sec. C3
Sta. 0+00.0

INDEX OF SHEETS

SHEET NO. 1	TITLE SHEET
2	TYPICAL CROSS-SECTIONS
3, 4	SPECIAL PROVISIONS
5	WITNESSES
6, 7	PLAN AND PROFILE
8	SPECIAL DETAILS
9	PUBLIC UTILITIES PLANS
10-12	QUANTITIES

STANDARD PLANS

E-4-A-9A	NO STANDARDS
E-4-A-21C	
E-4-A-27	
E-4-A-27A	
E-4-A-27B	
E-4-A-27T	
E-4-A-29	
E-4-A-33	
E-4-A-27D	
E-6-A-35B	

PLAN AND PROFILE LEGEND

SEWER M.H.	PROPOSED
SEWER C.B.	STORM SEWER M.H.
D.E.	CATCH BASIN
P.L.C.	STORM SEWER
C.G.	
W.U.	
WATER CAP	
MICH. BELL	
POSTAL TEL.	
FIRE DEPT.	
WATER GATE M.H.	
POLICE CALL	
FIRE CALL	
C.B. IN D.S.R.	
FIRE PLUG	
LIGHT POST	
D.S.R. POLES	
MAIL BOX	
COAL CHUTE	
D.E. POLE	
SIGNAL LIGHT	
WATER METER	
GAS CAP	
GAS STOP	
GAS DRIP	

TITLE SHEET LEGEND

PROPOSED PROJECT	---
T.L. OR F.A. PAVED	=====
T.L. OR F.A. IMPROVED NOT PAVED	-----
T.L. OR F.A. UNIMPROVED	-----
NON T.L. OR F.A. PAVED	-----
NON T.L. OR F.A. IMPROVED NOT PAVED	-----
NON T.L. OR F.A. UNIMPROVED	-----
COUNTY LINE	-----
CITY OR VILLAGE	-----
TOWNSHIP LINE	-----
SECTION LINE	-----
RAILROADS	-----

FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	FISCAL YEAR	TOTAL SHEET SHEETS
7	MICH.	258-C3		1
U.S. 10	22-35	WAYNE	DETROIT	1

CITY OF DETROIT
WOODWARD AVE.
F 82-35 C3 Covers 90' and 54' Conc. Base
Sta. 0+46.1 to Sta. 11+27.9
F 82-35 C9 covers 13' Brick and 77' Sheet
Asphalt Sta. 0+46.1 to Sta. 11+27.9

Note: The improvements covered by these plans shall be done in accordance with the Michigan State Highway Department 1934 Standard Specifications.

CONTRACT FOR GRADING - DRAINAGE - SURFACING
FA 258, C3

CHECKED: *[Signature]* DISTRICT ENGR.
CORRECT: *[Signature]* BRIDGE ENGR.
RECOMMENDED FOR APPROVAL: *[Signature]* MUNICIPAL ENGR.
DATE: 4-20-36

APPROVED: MURRAY D. VAN WAGONER, STATE HIGHWAY COMMISSIONER
DATE: 4-20-36

RECOMMENDED FOR APPROVAL: *[Signature]* DISTRICT ENGR.
RECOMMENDED FOR APPROVAL: *[Signature]* CHIEF ENGR.
APPROVED: *[Signature]* CHIEF

Wagoner
U.S. 10
Michigan - Kamin Hoy

FED. ROAD DIST. NO.	STATE	FEDERAL PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	MICH.	258	C3		
US 10	MI.	82-35	WAYNE	DETROIT	9
FED. ROAD DIST. NO.	STATE	FEDERAL PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	MICH.				

Where the following items are called for on plans they are to be constructed according to the Standard Plan given below opposite each item unless otherwise indicated.

- Reverse Curves E-4-A-94, Detail 8
- Pavement Reinforcement E-4-A-91C
- Manholes, Catch Basins and Inlets E-4-A-27
- Cover A E-4-A-27A
- Cover B E-4-A-27B
- Cover D E-4-A-27D
- Catch Basin Trap E-4-A-27T
- Concrete Curb and Concrete Curb and Gutter E-4-A-29
- Curb and Gutter Header E-4-A-29, Detail 9
- Concrete Driveway Returns and Driveway Openings E-4-A-29
- Construction and Contractions Joints E-4-A-33
- Grades for Intersections (Intersections are to be considered as complete units and the grades determined before their construction is started) E-4-A-66
- Barriercases E-6-A-55B

NOTES APPLYING TO STANDARD PLANS

SUPPLEMENTAL SPECIFICATIONS (Continued)
RECONSTRUCTING MANHOLES AND CATCH BASIN

Where called for on plans, existing manholes and catch basins shall be reconstructed in accordance with the Standard Specifications, except that existing covers shall be used if satisfactory for such use in the opinion of the Engineer.

SUPPLEMENTAL SPECIFICATIONS (Continued)
CONSTRUCTION OF NEW SIDEWALK-CONTINUED

old sidewalk as follows:
For building entrance walks, 4 inch Concrete Walkway
For street walks, 6 inch Concrete Walkway, and
For street walks across all driveways, 7 inch Concrete Sidewalk.

POROUS BACKFILL

All trenches excavated deeper than one foot below the subgrade of the new pavement or sidewalk, all abandoned sewer and other utility manholes, catch basins, and similar structures, all basements occurring within the limits of the project, and all old sewer and other utility trenches which may have sunk to levels lower than one foot below the proposed pavement or sidewalk subgrade shall be backfilled with Porous Backfill, thoroughly compacted, to the elevation of the bottom of the proposed pavement or sidewalk. However, pieces of brick pavement, sidewalk, or existing utility structures, not larger than one man size may be retained in place, provided they are not located in areas to be occupied by utility structures, except as directed by the Engineer. The cubical contents of all broken pavement, sidewalk and masonry walls placed in basement fills will be deducted from the yardage of Porous Backfill. The quantities estimated are based on filling all basement areas from which the buildings have been removed and which lie within the proposed right of way lines. In order to properly support the Porous Backfill at right of way lines where there is no building basement wall for a support, it is planned that waste earth from the project shall be placed outside of the right of way lines in such a manner as to properly support such Porous Backfill. Some waste earth may be directed by the Engineer. No backfill shall be placed on top of existing utility structures, and quantities for more or less Porous Backfill being actually required, the quantities to be estimated. Porous Backfill is defined as bank run sand, bank run gravel or coarse aggregate 10A, none of which shall contain over 10% of silt and clay.

DISPOSING OF SURPLUS EXCAVATION

All surplus excavation shall be disposed of by the contractor with no overhaul allowance.

RIGHT OF WAY

The City of Detroit shall be responsible for securing all necessary right of way as shown on plans and for the moving, removing, or altering of all buildings within said right of way.

MAINTAINING TRAFFIC - Grading, Drainage and Base Course Contract.

Detroit Street Railway traffic will be maintained by the Detroit Street Railway Contractor during the time of temporary obstructions. The road contractor shall conduct his operations as to facilitate the maintenance of such traffic.

(Maintaining Traffic Continued)

Cross Traffic shall be maintained by the contractor over the intersections of Edman Avenue and Vermer Highway with Woodward Avenue. During the construction of the D.S.R. tracks a movable bridge over railway, capable of carrying legal loads and city fire trucks, and conforming essentially to the details shown on sheet 6, shall be provided at each of these crossings. If required for the construction of the tracks, the bridge over railway shall be temporarily moved to the next adjacent street intersection and traffic diverted to the same until feasible to restore cross traffic over the designated crossing.

Satisfactory facilities shall be provided by the contractor for local vehicular traffic along Woodward Avenue over all other interesting streets.

The contractor shall notify the Engineer at least 36 hours in advance of starting any work that would interfere with traffic and the Engineer shall give the contractor advance notice of the location of temporary obstructions. The Engineer shall also keep the fire and police departments properly informed regarding the condition of any portions of the project that may be open to traffic.

Maintaining traffic shall be considered as incidental to the construction of the project, except as follows:

- "Bridges over Railway" will be paid for at the contract unit price each, which price shall include the cost of the structures, including ramps, complete tiebacks, lighting, and traffic signs, as required for proper maintenance of cross traffic, and maintaining traffic over the same.
- MAINTAINING TRAFFIC - Sheet Asphalt Contract
- Reasonable facilities shall be provided by the contractor for local traffic at all times except during the period actually required for placing the asphalt binder course and sheet asphalt wearing course. After the placing of the sheet asphalt wearing course, the project or sections thereof, as designated by the Engineer, shall be opened to through traffic. Maintaining traffic shall be considered as incidental to the construction of the project.
- REMOVING ASPHALT SURFACE - Sheet Asphalt Contract
- At street returns sufficient existing asphalt surface shall be removed, before placing new sheet asphalt, to secure proper connecting grades, as directed by the Engineer.
- This will be paid for at the contract unit price per square yard for "Removing Asphalt Surface".

SUPPLEMENTAL SPECIFICATIONS
DETROIT STREET RAILWAY COMMISSION

TRACK REMOVAL - The Detroit Street Railway Commission shall remove its tracks and move its trolley poles through the project. The road contractor shall remove the remaining portions of the railway track and enclosure, including the removal of 4" and 6" drain tile underneath the track foundation. The trenches from which these tiles have been removed shall be backfilled with porous backfill to the level of the subgrade. The removal of this porous backfill shall be as certified above and will be paid for according to specifications 2.07, Removing Railway Track, with no allowance for overhaul.

CONSTRUCTION - The City of Detroit Commission will furnish and install all the new pipes and rails to sewer bases and alignment, all trolley poles and wires, and all surface track drains between tracks (including tie outlets and manholes for same), also all sewer connections from these manholes to the existing city sewer system. The road contractor shall place all sub-base tile underdrain (including all connections to manholes), all sub-base, concrete encasement (in two courses) and asphalt surface course as shown on the plans. The tile underdrain connections to manholes shall be made at intervals of approximately 400 feet, which shall include all low points in grade and drains where conduits for other underground utilities are provided to such depths as to construct the continuity of the tile underdrain grade.

COOPERATION - The work to be performed by the Detroit Street Railway Commission shall be subject to the approval of the Engineer. The road contractor shall conduct his work at all times so as to expedite the work of new track installation and operation. No claims will be allowed under this contract for any delay or inconvenience caused by failure of any of the above parties to complete any part of the work at any specified time.

TEMPORARY TRACK - The Detroit Street Railway Commission shall make complete installation of all temporary tracks and temporary reverse curves (including any track encasement) that may be necessary.

TREATMENT OF OTHER PUBLIC UTILITIES

With the exception of the Detroit Street Railway Commission, the various public utilities, both privately and publicly owned, which occur within the limits of construction covered by these plans are to be taken care of in the following manner:

REMOVAL - All structures now in place which are to be abandoned, are to be removed by the road contractor as part of Earth Excavation, unless same are removed by the owner during or previous to grading operations.

CONSTRUCTION - All new utility structures are to be furnished and installed by the respective owners. Likewise, any adjusting or re-aligning of existing structures to be left in place is to be done by the owners. The road contractor shall make any necessary final adjustments.

(Construction Continued) F. A. 258 C3 (2)

of the surface portions of the above structures to fit each of the pavement or sidewalk units. The contractor shall be responsible for making such adjustments as may be necessary to the contractor, including construction of concrete base or sidewalk.

COOPERATION - Detail plans shall be submitted to the Engineer by each public utility, whether publicly or privately owned, for all work proposed to be performed by said utility before proceeding with such work. Notice as to the time proposed for starting such work shall also be served sufficiently in advance so that the Engineer may coordinate the same with the work of the contractor and maintenance of traffic. The work to be performed by each utility to expedite the work of the road contractor or the Engineer shall be possible with the work of other utilities or the road contractor, and finish the installation previous to the completion of the grading by the road contractor. EXCEPT that final adjustment of surface structures may be made later when requested by the Engineer. All trenches made by any utility for installation of underground work shall be backfilled by said utility with porous backfill placed in approximately twelve (12) inch layers and thoroughly compacted, subject to approval by the Engineer. "Porous backfill" is defined as bank run sand, bank run gravel or coarse aggregate 10A (Standard Specifications), none of which shall contain over 10% of silt and clay.

The road contractor shall conduct his work at all times so as to expedite the work of installing these utilities. No claims will be allowed under this contract for delays or inconvenience caused by the failure of any of the above parties to complete any part of the work by any specified time.

OLD PAVEMENT AND CURB

The entire old pavement, curb and gutters in place are to be removed to the limits shown on the plans by heavy lines at street returns as Removing Old Pavement. The material removed as above specified may be placed in the porous backfill areas as directed by the Engineer, provided that no pieces shall be larger than one-man size or placed in areas to be occupied by new utility structures. The existing pavement is reported as consisting of 6 inch concrete base with 3 1/2 inch sheet asphalt surface.

SIDEWALK

REMOVING SIDEWALK - The extent of this work shall be as follows: within the Woodward Avenue right of way lines north from Sta. 0+00 all sidewalk is to be removed, except that, on the side of the street where no widening is to be done, the Engineer will determine at the time of construction the amount of sidewalk necessary to be removed. At building entrance walks and on intersecting streets back of Woodward Avenue right of way lines and along Woodward Avenue south of Sta. 0+00 the Engineer will likewise determine the amount of sidewalk to be removed. Approximate limits indicated by hatching on the plans. The amount of sidewalk to be removed shall be as certified above. Portions of the brick pavement and curbs to be placed in porous backfill areas as outlined under "OLD PAVEMENT AND CURB" above.

CONSTRUCTION OF NEW SIDEWALK - New concrete sidewalk is to be placed between back of curb and new right of way lines or as connections to

F. A. 258 C3

F. A. 258 C3 (3)

F. A. 258 C3 (5)

F. A. 258 C3 (4)

NOTES

GRADES FOR INTERSECTIONS
All intersections are to be considered as complete units and the grades determined before construction is started. The central 54 feet of pavement with on Woodward Avenue is to have the same crown throughout the entire project.

TILTING OF PAVEMENT
Where plan gutter grades show the proposed left and right gutters to be at different elevations, the crown for the pavement is to be adjusted as follows: the central 54 feet of pavement width is to have the same crown throughout the entire project and the difference in gutter elevations obtained by increasing or decreasing the normal crown in the outer 18 feet of width to meet the plan grades.

The following public utilities are represented on this project:

- Detroit Edison Co.
- Detroit Public Lighting Commission
- Western Union Telegraph Co.
- Postal Telegraph-Cable Co.
- Michigan Bell Telephone Co.
- Detroit Department of Water Supply
- Detroit Fire Department (Signal Dept.)
- Detroit Police Department
- Detroit Street Railway Commission
- United States Postal Department

(See Sheet 4 for Summary of Quantities)

FED. ROAD DIST. NO.	STATE	FEDERAL PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	MICH.	260	C9		
ROUTE	STATE	COUNTY	CITY	SHEET NO.	TOTAL SHEETS
US 10	WAYNE	DETROIT		4	

F. A. SFS C3
SUMMARY OF QUANTITIES
LENGTH 0.23 MILES

PARTICIPATION - Federal Aid 50% - State 50%

DESCRIPTION	QUANTITY	UNIT	CONTRACT NO.	CONTRACT DATE	CONTRACT VALUE
Contract for Grading, Drainage and Base Course					
Removing Old Pavement	7,662	sq yds			
Removing Sidewalk	3,322	sq yds			
Removing Masonry	400	cu yds			
Removing Railway Tracks (Single Tracks)	2,805	lin ft			
Earth Excavation	3,452	cu yds			
12" Sewer	740	lin ft			
6" Tile Underdrain	1,496	lin ft			
Catch Basins 3A	5	each			
Catch Basins 3B	8	each			
Catch Basin 3D	1	each			
Inlets 2B	4	each			
Inlets 5B	7	each			
Adjusting Catch Basin Covers	4	each			
Crushed Stone or Slag Sub-base	564	cu yds (loose measure)			
Porous Backfill	9,743	cu yds (loose measure)			
Fine Gracing and Cleanup	10,723	stas			
Concrete Base Course 10-14-10"	11,336.9	sq yds			
Pavement Reinforcement	11,336.9	sq yds			
Track Encasement, Grade A Concrete	1,154	cu yds			
Concrete Curb, Det. 2A	23	lin ft			
Concrete Curb, Det. 3	2,485	lin ft			
Concrete Driveway Return, Det. 1B	66	lin ft			
4" Concrete Sidewalk	25	sq ft			
6" Concrete Sidewalk	27,694	sq ft			
7" Concrete Sidewalk	740	sq ft			
Bridge over Railway	2	each			
Adjusting Manhole Covers	2	each			

DESCRIPTION	QUANTITY	UNIT	CONTRACT NO.	CONTRACT DATE	CONTRACT VALUE
Contract for Sheet Asphalt					
Removing Asphalt Surface	670.0	sq yds			
Sheet Asphalt Wearing Course 2"	13,675.7	sq yds			
Asphaltic Binder Course 1 1/2"	13,675.7	sq yds			
Wedge Course	30	tons			
Bituminous Prime Coat	170	gal			
Vitrified Brick Surface Course 3"	969.5	sq yds			

F. A. SFS C3
SUMMARY OF QUANTITIES
LENGTH 0.23 MILES

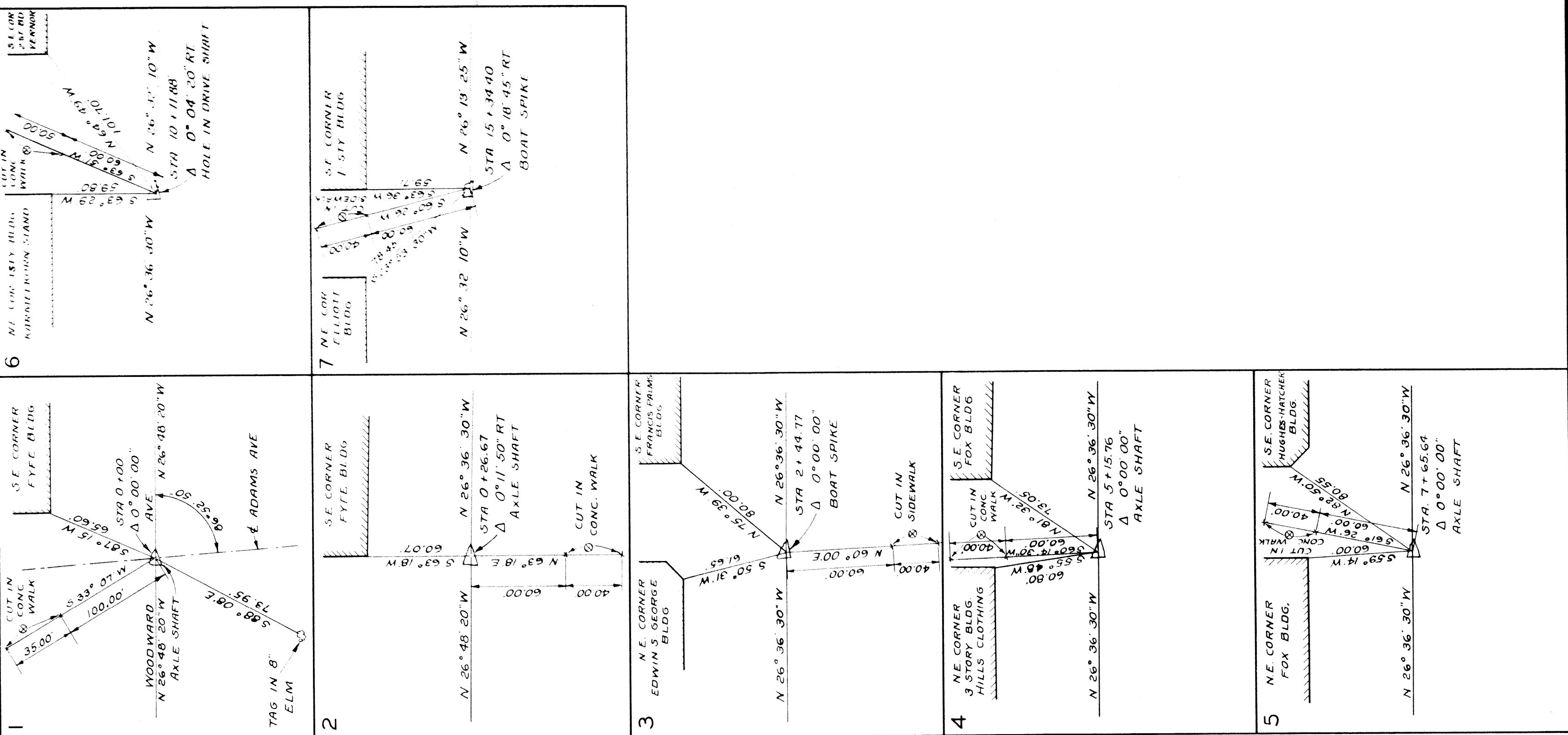
PARTICIPATION - Federal Aid 50% - State 50%

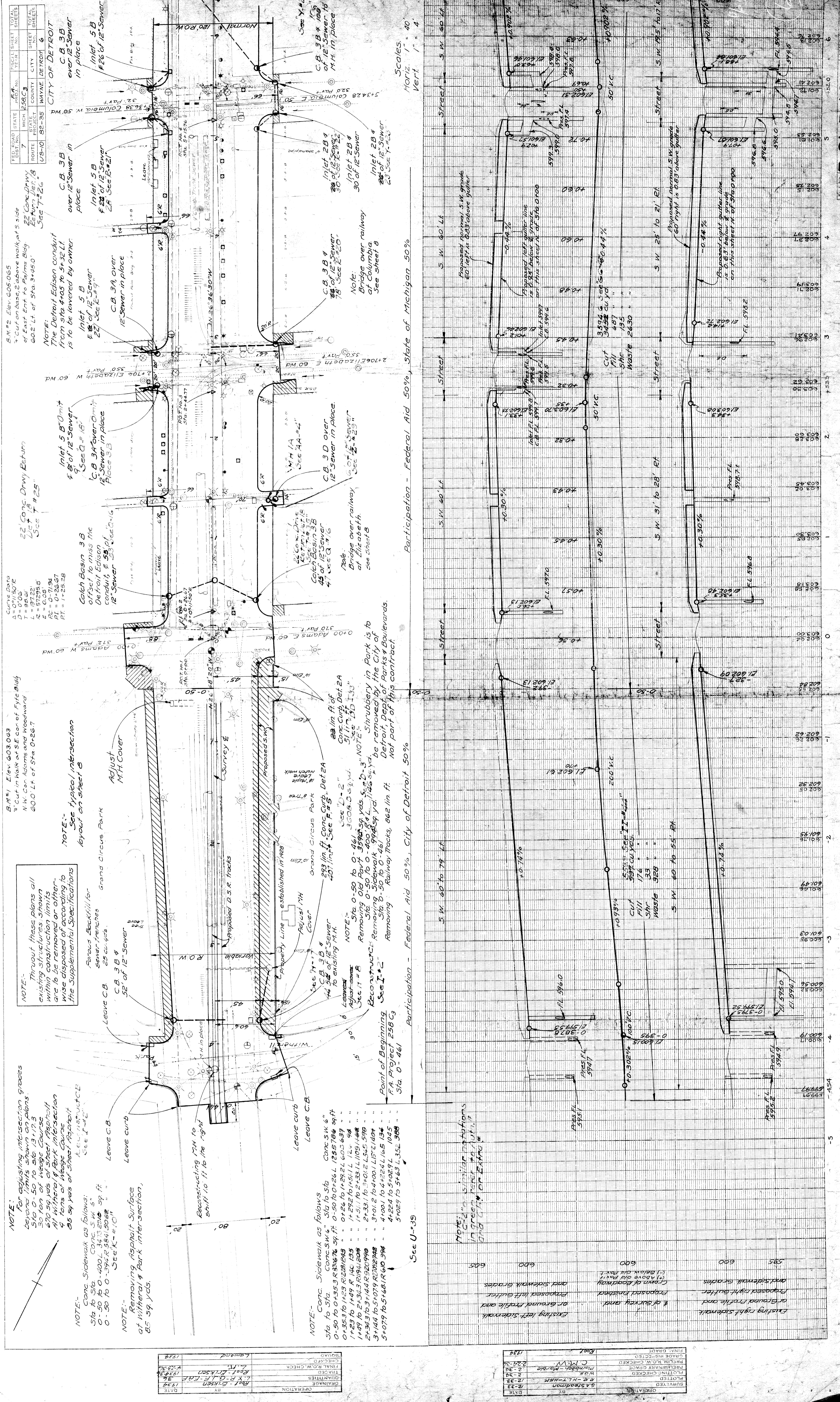
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Bituminous Prime Coat	170	gal			
Vitrified Brick Surface Course 3"	969.5	sq yds			

SURVEY LINE WITNESSES

FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
7	MICH.	258-C-3			
ROUTE	STATE	COUNTY	CITY	SHEET NO.	TOTAL SHEETS
U.S. 10	MI	WAYNE	DETROIT	5	5
FED. ROAD DIST. NO.	STATE	FED. PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	MICH.				
ROUTE	STATE	COUNTY	TWP.	SHEET NO.	TOTAL SHEETS





NO.	DATE	BY	OPERATION
2-33	1934	G. S. Steadman	SURVEY
2-34	1933	E. H. L. THAM	PLOTTED
2-35	1933	C. M. V.	FINAL CHECK
2-36	1934	L. B. G. L. F.	FINAL R.O.W. CHECK
2-37	1934	R. J. E. E.	CHECKED
2-38	1934	R. J. E. E.	SOUND

NOTE: Through these plans all existing structures shown within construction limits are to be removed or otherwise disposed of according to the Supplemental Specifications.

NOTE: For adjusting intersection grades beyond limits shown on plans, 3' 0" to 5' 0" to 5' 0" 1/2" 1/2" 3/4" 1" 1 1/4" 1 1/2" 1 3/4" 2" 2 1/4" 2 1/2" 3" 3 1/4" 3 1/2" 4" 4 1/4" 4 1/2" 5" 5 1/4" 5 1/2" 6" 6 1/4" 6 1/2" 7" 7 1/4" 7 1/2" 8" 8 1/4" 8 1/2" 9" 9 1/4" 9 1/2" 10" 10 1/4" 10 1/2" 11" 11 1/4" 11 1/2" 12" 12 1/4" 12 1/2" 13" 13 1/4" 13 1/2" 14" 14 1/4" 14 1/2" 15" 15 1/4" 15 1/2" 16" 16 1/4" 16 1/2" 17" 17 1/4" 17 1/2" 18" 18 1/4" 18 1/2" 19" 19 1/4" 19 1/2" 20" 20 1/4" 20 1/2" 21" 21 1/4" 21 1/2" 22" 22 1/4" 22 1/2" 23" 23 1/4" 23 1/2" 24" 24 1/4" 24 1/2" 25" 25 1/4" 25 1/2" 26" 26 1/4" 26 1/2" 27" 27 1/4" 27 1/2" 28" 28 1/4" 28 1/2" 29" 29 1/4" 29 1/2" 30" 30 1/4" 30 1/2" 31" 31 1/4" 31 1/2" 32" 32 1/4" 32 1/2" 33" 33 1/4" 33 1/2" 34" 34 1/4" 34 1/2" 35" 35 1/4" 35 1/2" 36" 36 1/4" 36 1/2" 37" 37 1/4" 37 1/2" 38" 38 1/4" 38 1/2" 39" 39 1/4" 39 1/2" 40" 40 1/4" 40 1/2" 41" 41 1/4" 41 1/2" 42" 42 1/4" 42 1/2" 43" 43 1/4" 43 1/2" 44" 44 1/4" 44 1/2" 45" 45 1/4" 45 1/2" 46" 46 1/4" 46 1/2" 47" 47 1/4" 47 1/2" 48" 48 1/4" 48 1/2" 49" 49 1/4" 49 1/2" 50" 50 1/4" 50 1/2" 51" 51 1/4" 51 1/2" 52" 52 1/4" 52 1/2" 53" 53 1/4" 53 1/2" 54" 54 1/4" 54 1/2" 55" 55 1/4" 55 1/2" 56" 56 1/4" 56 1/2" 57" 57 1/4" 57 1/2" 58" 58 1/4" 58 1/2" 59" 59 1/4" 59 1/2" 60" 60 1/4" 60 1/2" 61" 61 1/4" 61 1/2" 62" 62 1/4" 62 1/2" 63" 63 1/4" 63 1/2" 64" 64 1/4" 64 1/2" 65" 65 1/4" 65 1/2" 66" 66 1/4" 66 1/2" 67" 67 1/4" 67 1/2" 68" 68 1/4" 68 1/2" 69" 69 1/4" 69 1/2" 70" 70 1/4" 70 1/2" 71" 71 1/4" 71 1/2" 72" 72 1/4" 72 1/2" 73" 73 1/4" 73 1/2" 74" 74 1/4" 74 1/2" 75" 75 1/4" 75 1/2" 76" 76 1/4" 76 1/2" 77" 77 1/4" 77 1/2" 78" 78 1/4" 78 1/2" 79" 79 1/4" 79 1/2" 80" 80 1/4" 80 1/2" 81" 81 1/4" 81 1/2" 82" 82 1/4" 82 1/2" 83" 83 1/4" 83 1/2" 84" 84 1/4" 84 1/2" 85" 85 1/4" 85 1/2" 86" 86 1/4" 86 1/2" 87" 87 1/4" 87 1/2" 88" 88 1/4" 88 1/2" 89" 89 1/4" 89 1/2" 90" 90 1/4" 90 1/2" 91" 91 1/4" 91 1/2" 92" 92 1/4" 92 1/2" 93" 93 1/4" 93 1/2" 94" 94 1/4" 94 1/2" 95" 95 1/4" 95 1/2" 96" 96 1/4" 96 1/2" 97" 97 1/4" 97 1/2" 98" 98 1/4" 98 1/2" 99" 99 1/4" 99 1/2" 100" 100 1/4" 100 1/2" 101" 101 1/4" 101 1/2" 102" 102 1/4" 102 1/2" 103" 103 1/4" 103 1/2" 104" 104 1/4" 104 1/2" 105" 105 1/4" 105 1/2" 106" 106 1/4" 106 1/2" 107" 107 1/4" 107 1/2" 108" 108 1/4" 108 1/2" 109" 109 1/4" 109 1/2" 110" 110 1/4" 110 1/2" 111" 111 1/4" 111 1/2" 112" 112 1/4" 112 1/2" 113" 113 1/4" 113 1/2" 114" 114 1/4" 114 1/2" 115" 115 1/4" 115 1/2" 116" 116 1/4" 116 1/2" 117" 117 1/4" 117 1/2" 118" 118 1/4" 118 1/2" 119" 119 1/4" 119 1/2" 120" 120 1/4" 120 1/2" 121" 121 1/4" 121 1/2" 122" 122 1/4" 122 1/2" 123" 123 1/4" 123 1/2" 124" 124 1/4" 124 1/2" 125" 125 1/4" 125 1/2" 126" 126 1/4" 126 1/2" 127" 127 1/4" 127 1/2" 128" 128 1/4" 128 1/2" 129" 129 1/4" 129 1/2" 130" 130 1/4" 130 1/2" 131" 131 1/4" 131 1/2" 132" 132 1/4" 132 1/2" 133" 133 1/4" 133 1/2" 134" 134 1/4" 134 1/2" 135" 135 1/4" 135 1/2" 136" 136 1/4" 136 1/2" 137" 137 1/4" 137 1/2" 138" 138 1/4" 138 1/2" 139" 139 1/4" 139 1/2" 140" 140 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FILE NO.	STATE	PROJ. NO.	SHEET NO.	TOTAL SHEETS
7	MICH.	2893	7	7
ROUTE	COUNTY	CITY	CITY OF DETROIT	
US 10	82-35	WAYNE	DETROIT	

Curve Data
 $\Delta = 0^{\circ}18'45''$
 $T = 93.75'$
 $L = 187.50'$
 $E = 0.1875'$
 $PC = 14+40.65$
 $PT = 15+28.15$

BM # 6 Elev. 609.629
 " cut in base 2 above mark 2' 2" of NE
 Blg. of NW cor. of Woodward & Durfield
 60.00 Lt. of Sta. 13+15.5

NOTE: -
 See typical intersection
 layout on sheet 8

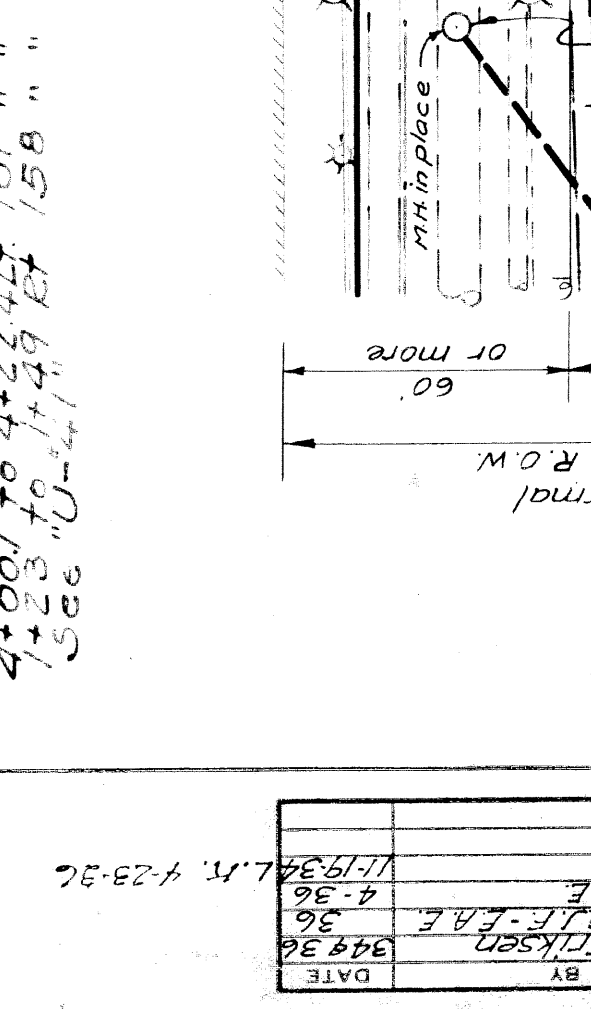
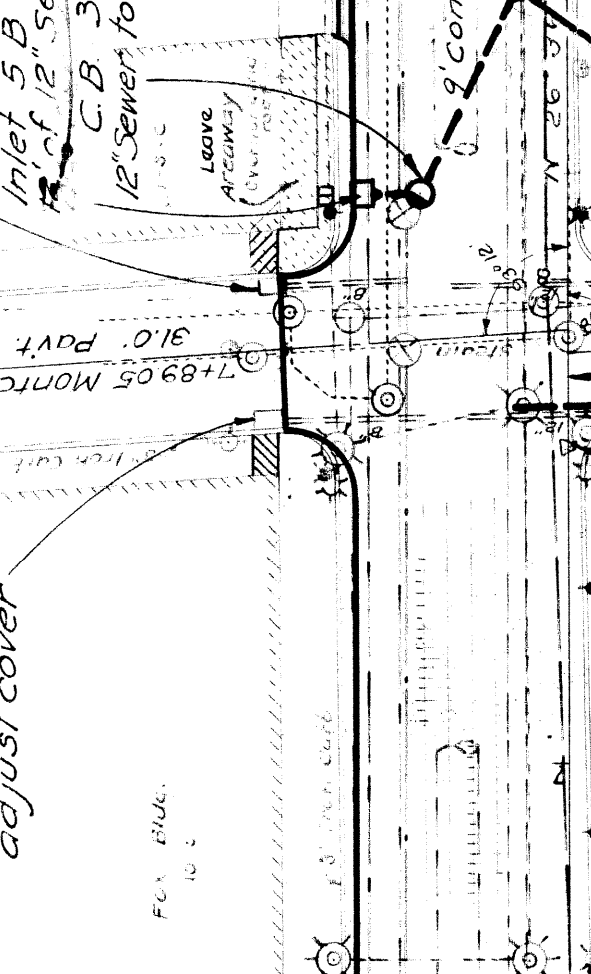
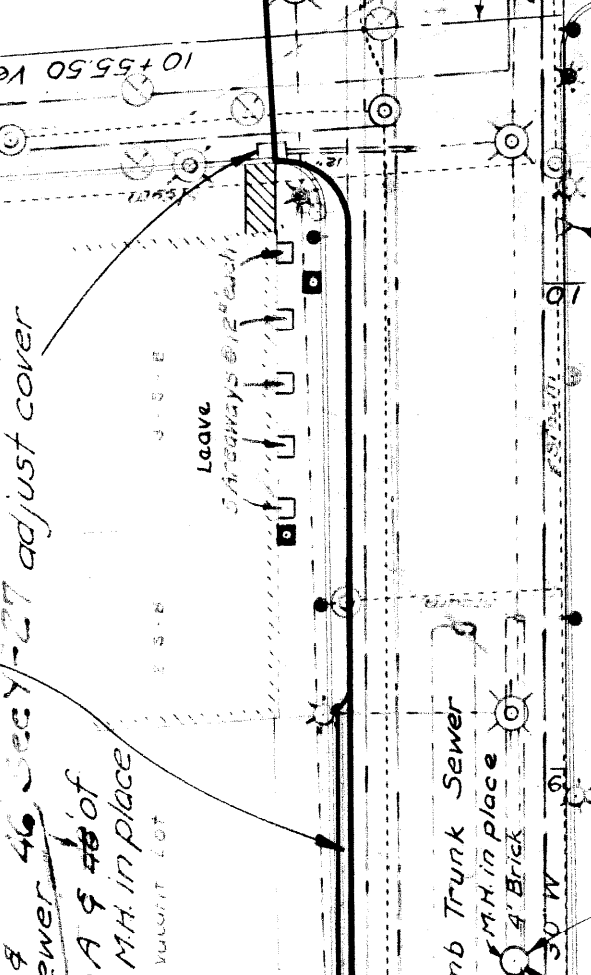
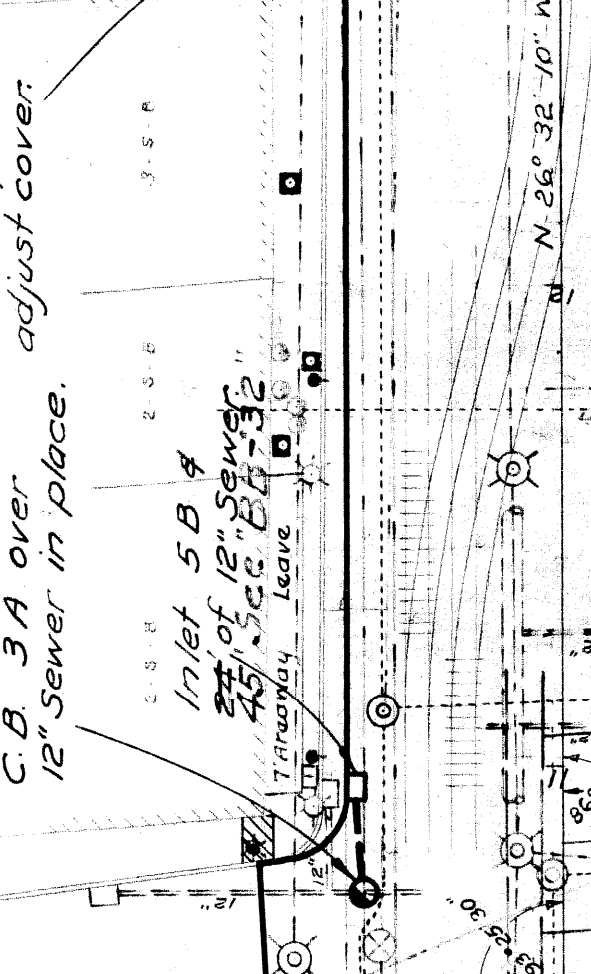
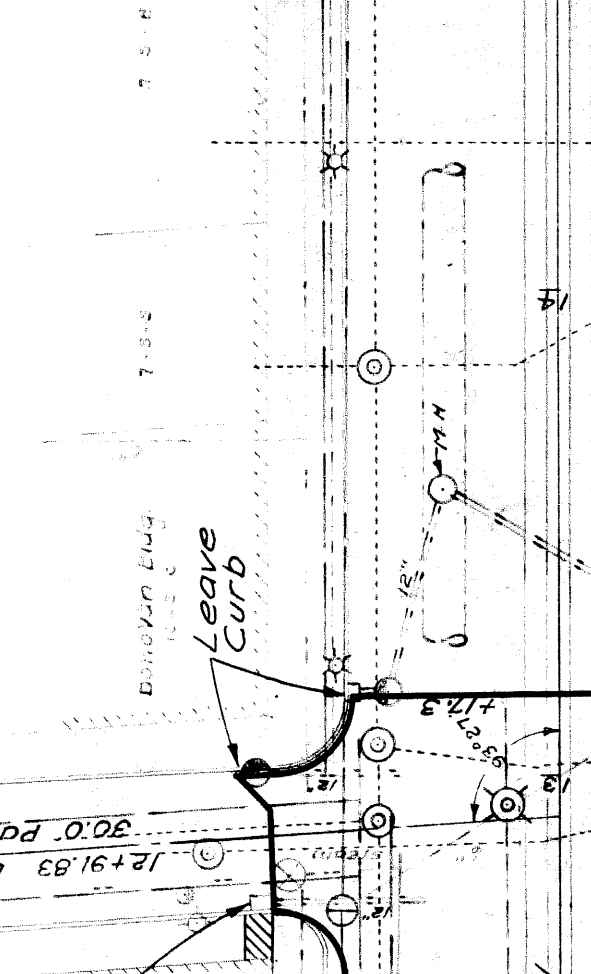
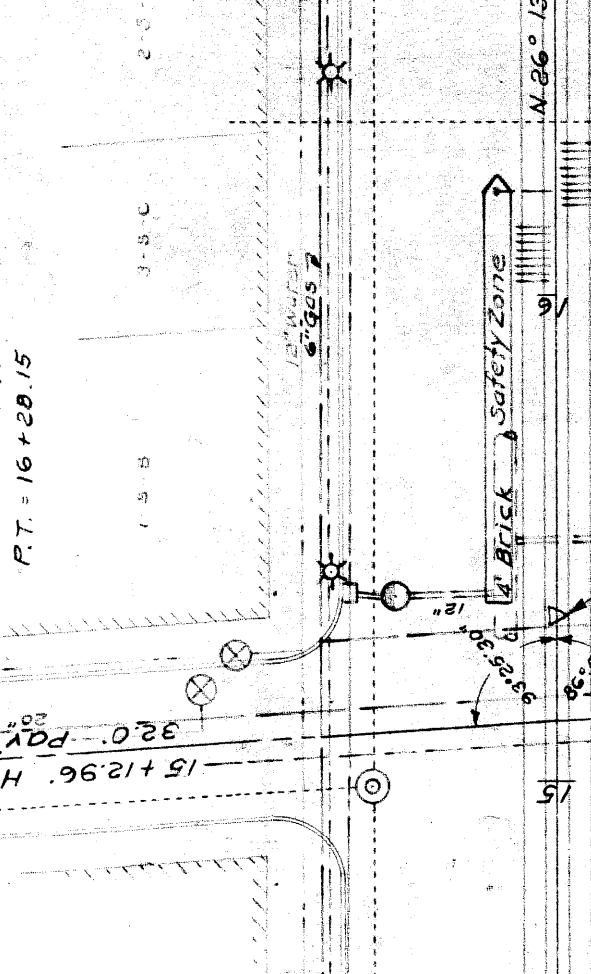
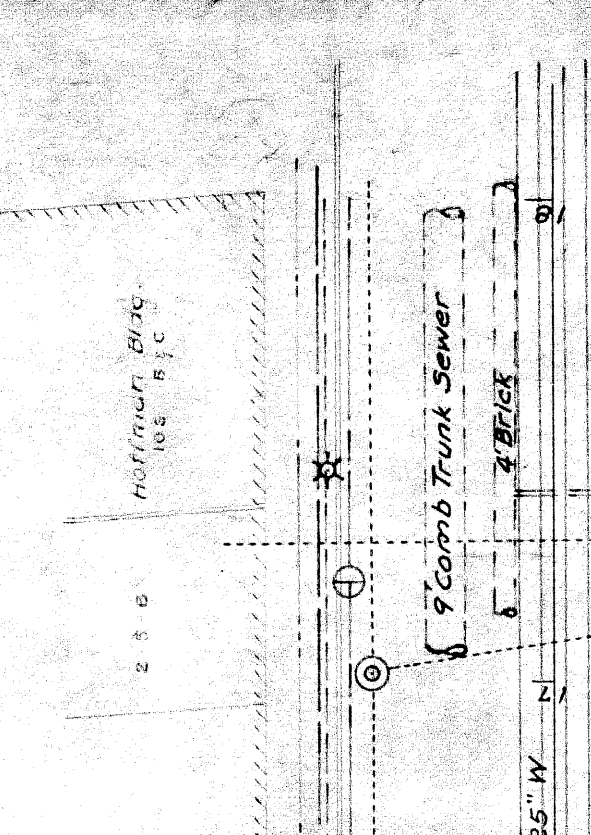
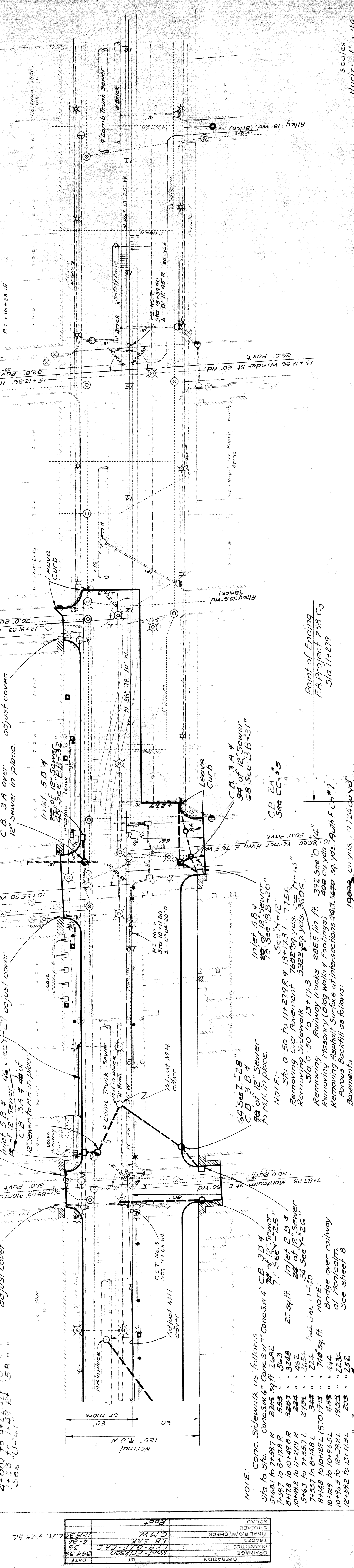
BM # 5 Elev. 610.124
 " cut on base 2 above mark 10' N. of
 S.E. cor. of Blg. of Woodward & Durfield
 61.00 Lt. of Sta. 11+01.0

Curve Data
 $\Delta = 0^{\circ}04'20''$
 $T = 64.71'$
 $L = 129.42'$
 $E = 0.0420'$
 $PC = 9+11.88$
 $PT = 10+16.65$

BM # 4 Elev. 606.930
 " cut on base 2 above mark at S.E. cor.
 of Hughes and Hatcher Bldg.
 59.71 Lt. of Sta. 8+17.0

NOTE: See sheet 5 for
 P.I. & P.O.T. witnesses.

7' Conc. S.W.
 4' 0.07' to 4' 2.31' Lt. = 1.62 sq. ft.
 1.25' 0.07' to 1.41' Lt. = 1.58 "



OPERATION	DATE
DRAINAGE	3/28/36
QUANTITIES	3/30/36
TRACED	4/30/36
FINAL R.O.W. CHECK	1/17/36
CONC. CHECKED	
ROOF	

OPERATION	DATE
SURVEYED	12-33
PLOTTED	12-33
PLOTTING CHECKED	12-33
PRELIMINARY CHECK	2-34
PRELIM. R.O.W. CHECKED	2-34
CONC. CHECKED	2-34
ROOF	

NOTE: Conc. Sidewalk as follows
 Sta to Sta
 Conc. S.W. 6" Conc. S.W. 7" Conc. S.W. 8" Conc. S.W. 9" Conc. S.W. 10"
 5468.1 to 7+97.7 R 2735 sq. ft. 2-52
 7+97.7 to 8+17.8 R 533 " 5-03
 8+17.8 to 10+97.8 R 3287 " 3-24
 10+97.8 to 11+27.9 R 224 " 4-2
 11+27.9 to 12+57.7 L 2734 " 4-57
 12+57.7 to 14+14.8 L 3673 " 2-21
 14+14.8 to 15+17.1 L 740 sq. ft.
 15+17.1 to 16+54.5 L 453 " 4-44
 16+54.5 to 18+57.2 L 1953 " 2-23
 18+57.2 to 18+17.5 L 203 " 2-52

NOTE: -
 See U-39

NOTE: -
 See U-35

NOTE: -
 See U-42

NOTE: -
 See U-42

NOTE: -
 See U-42

NOTE: -
 See U-42

Scale:
 Horiz. 1" = 40'
 Vert. 1" = 4'

Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

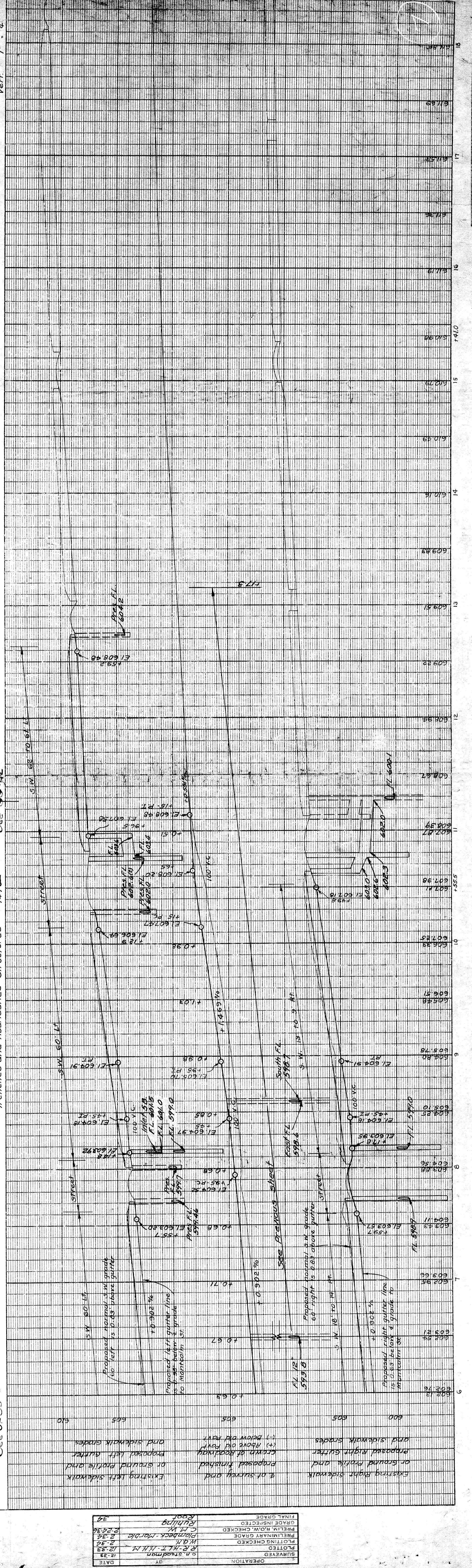
Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

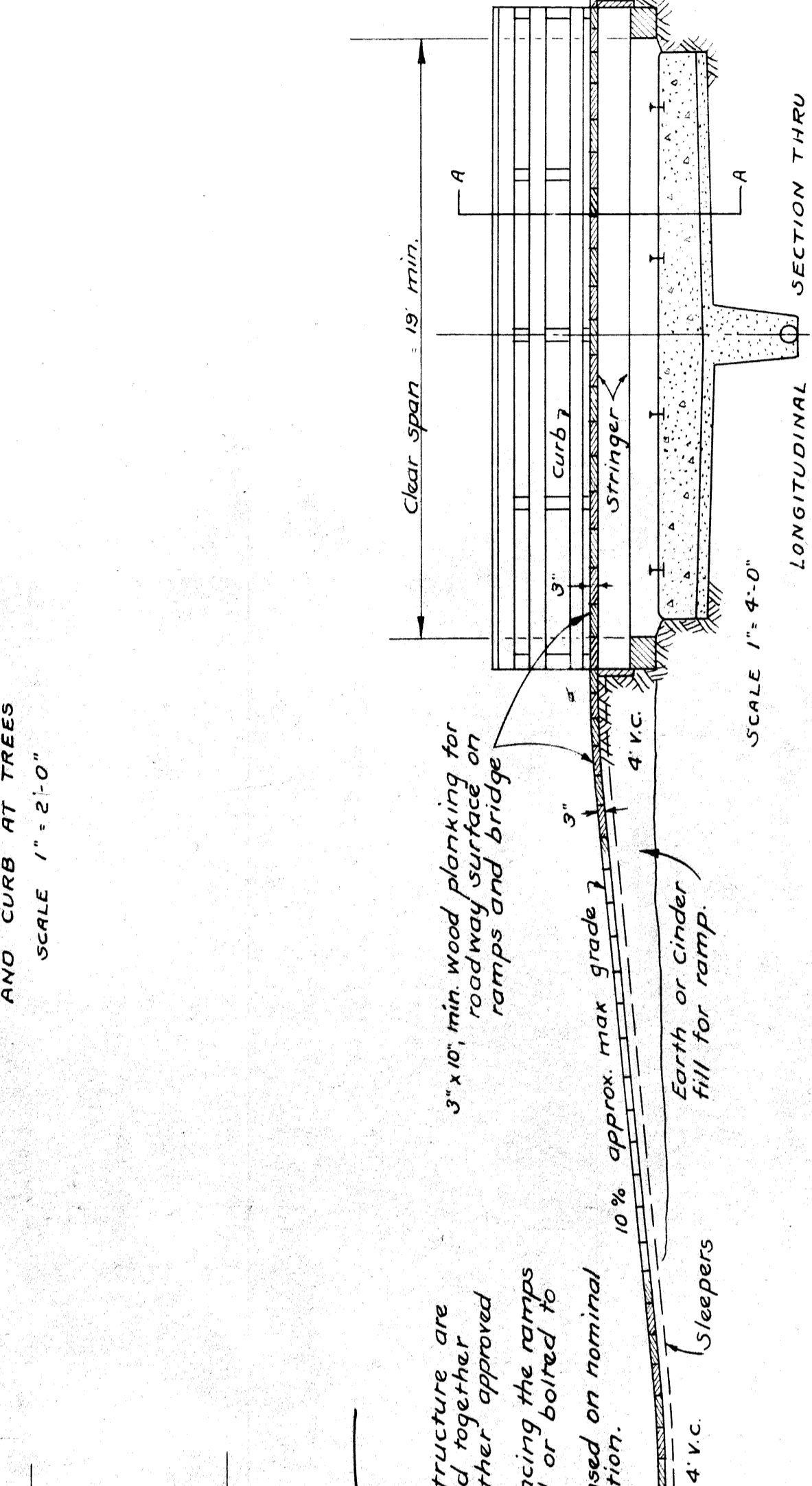
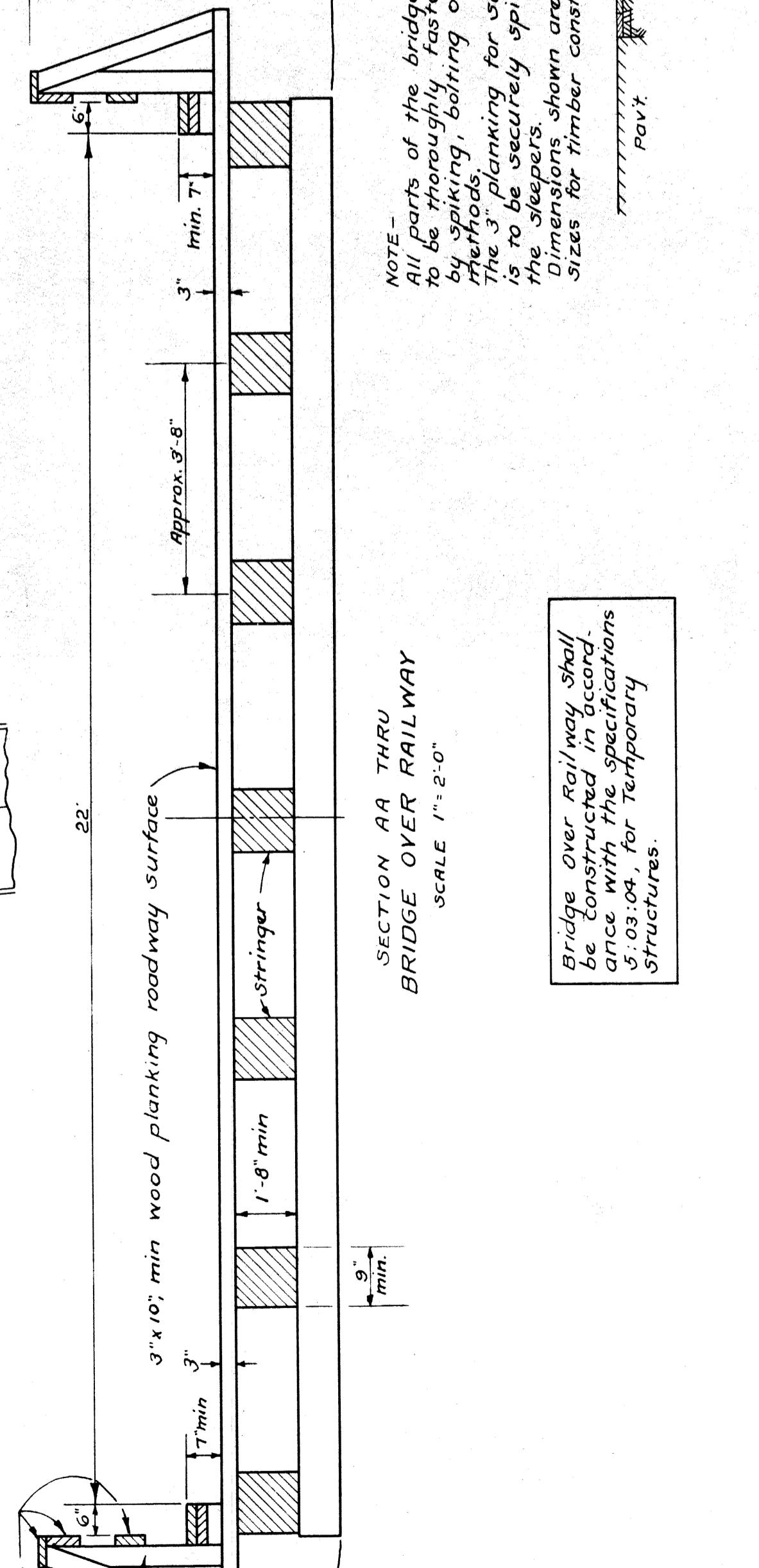
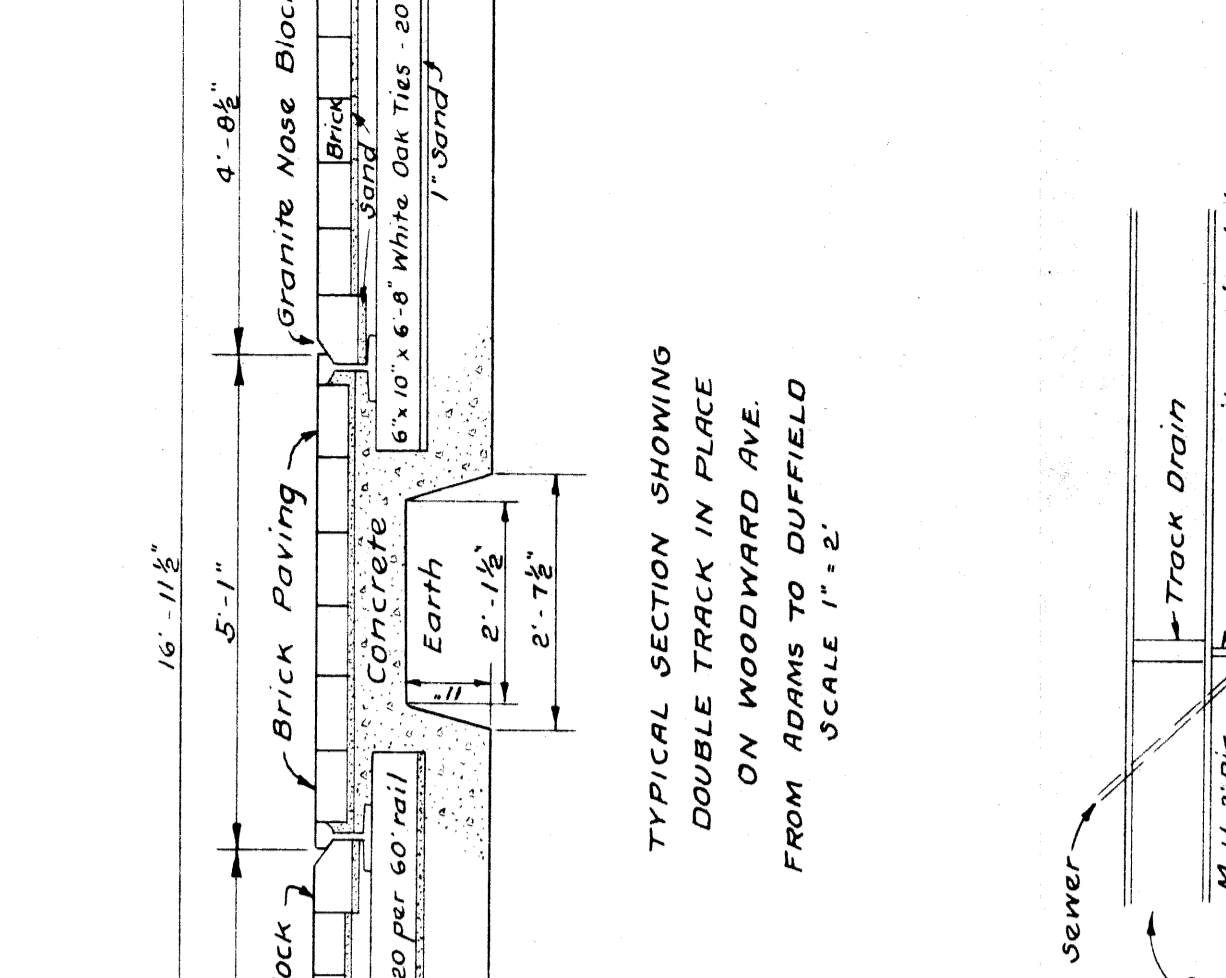
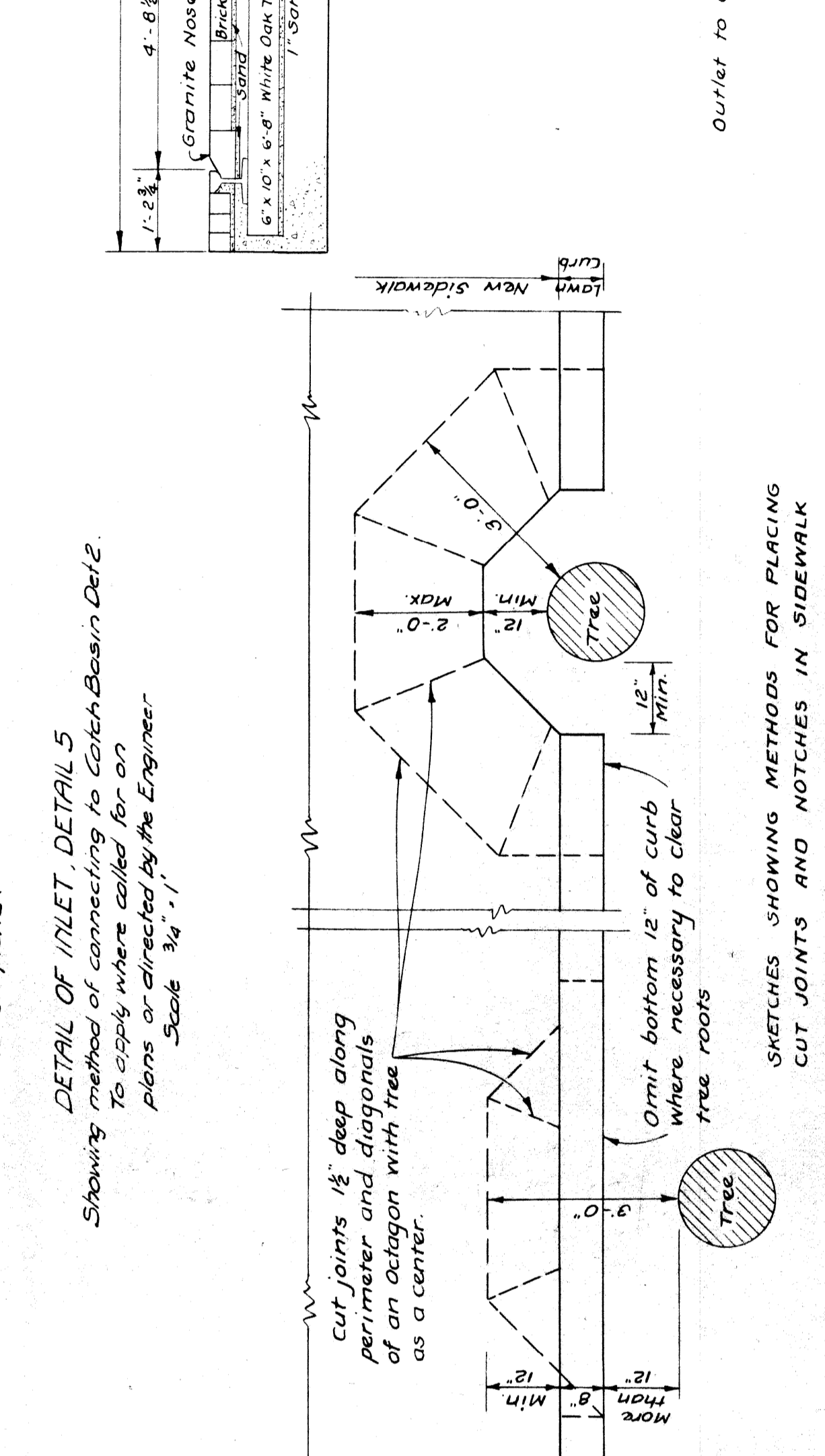
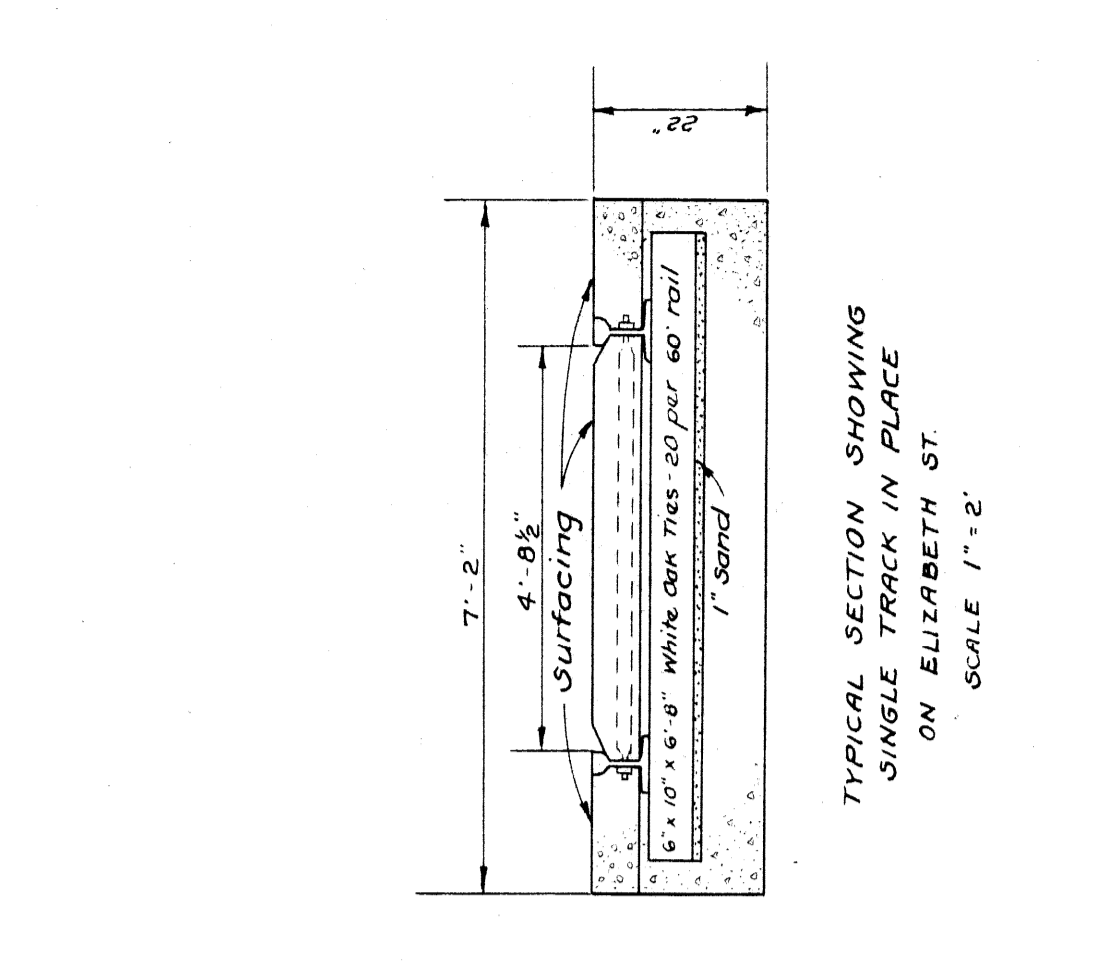
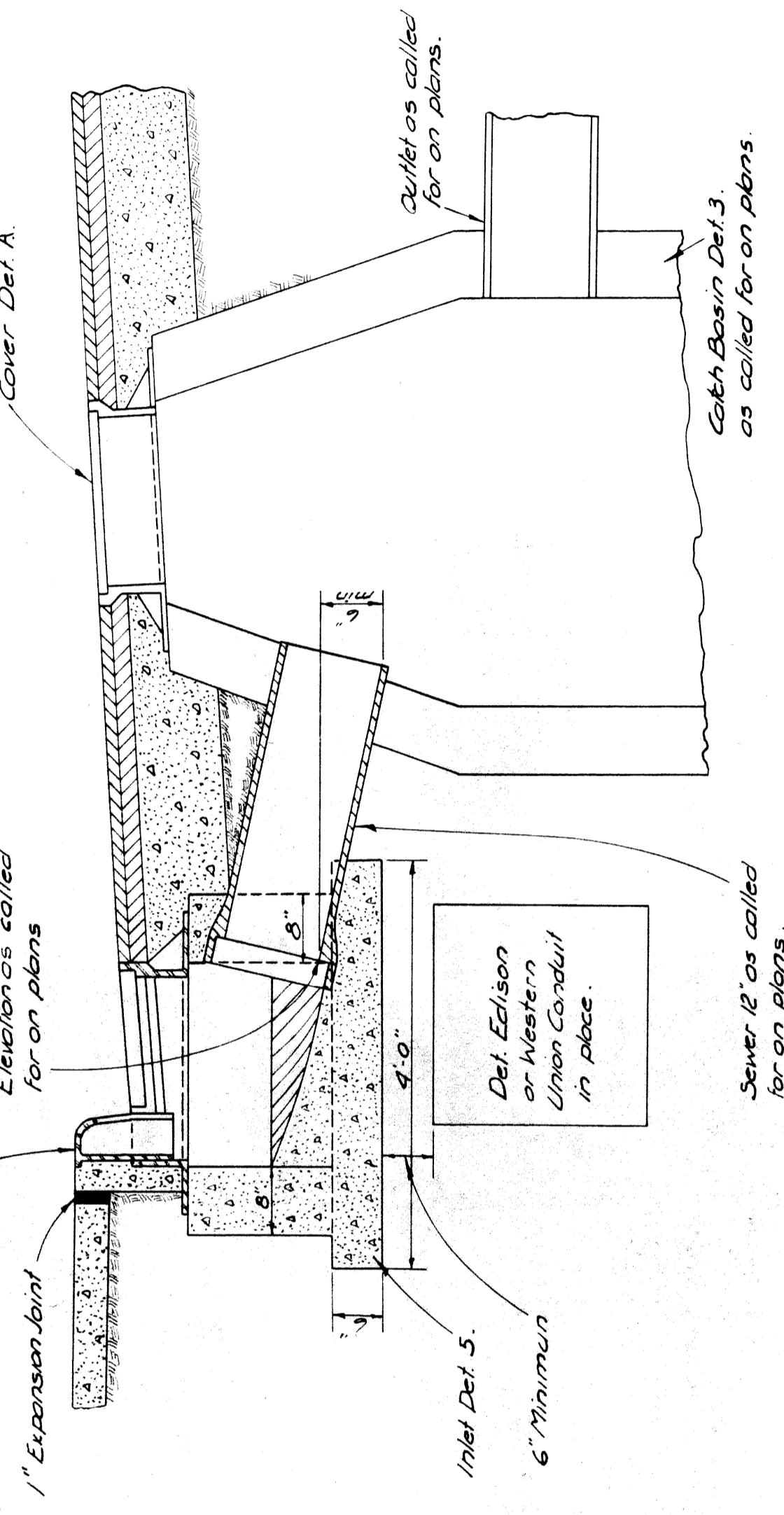
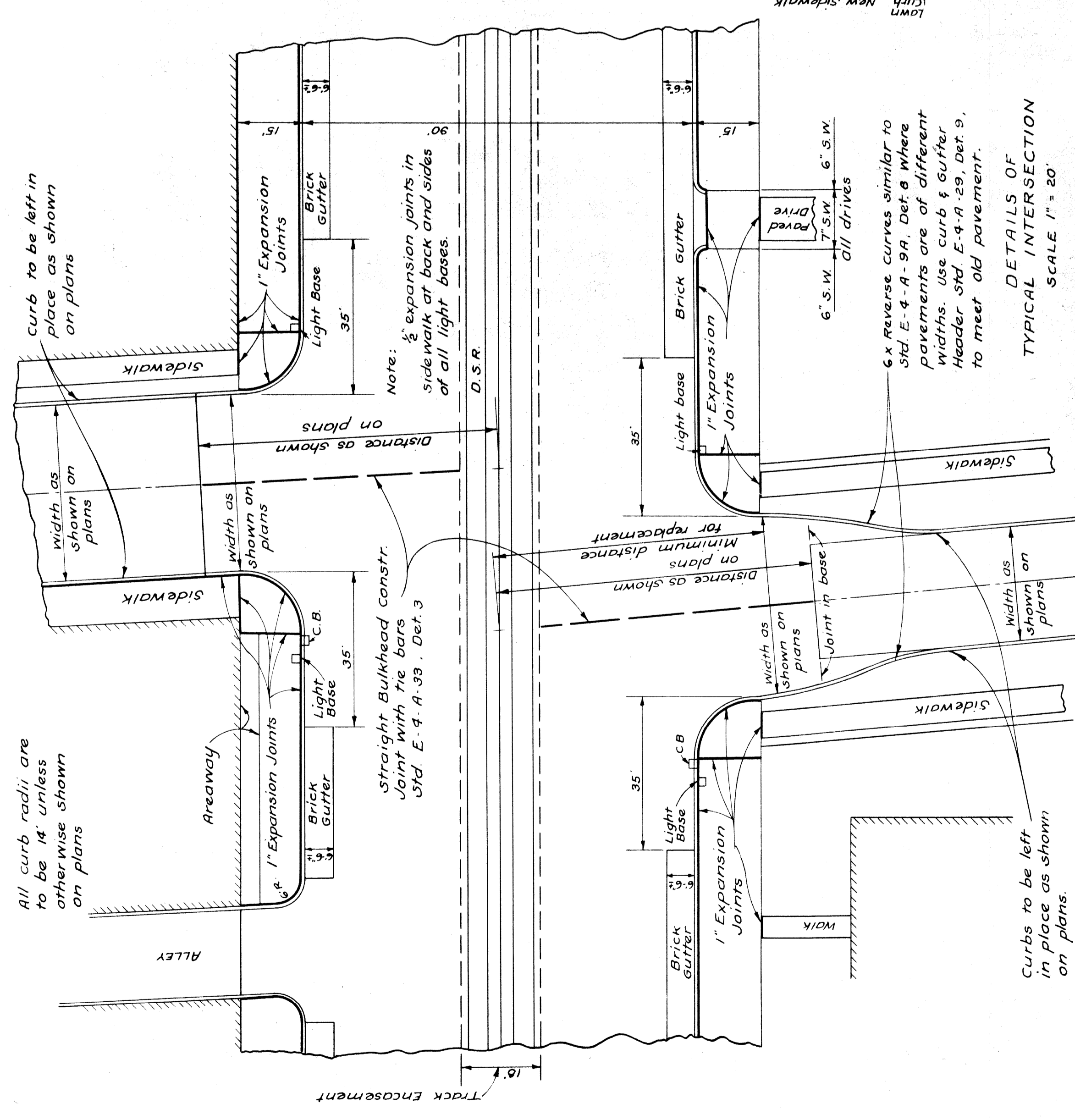
Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

Point of Ending
 FA Project 258 C3
 Sta. 11+27.9

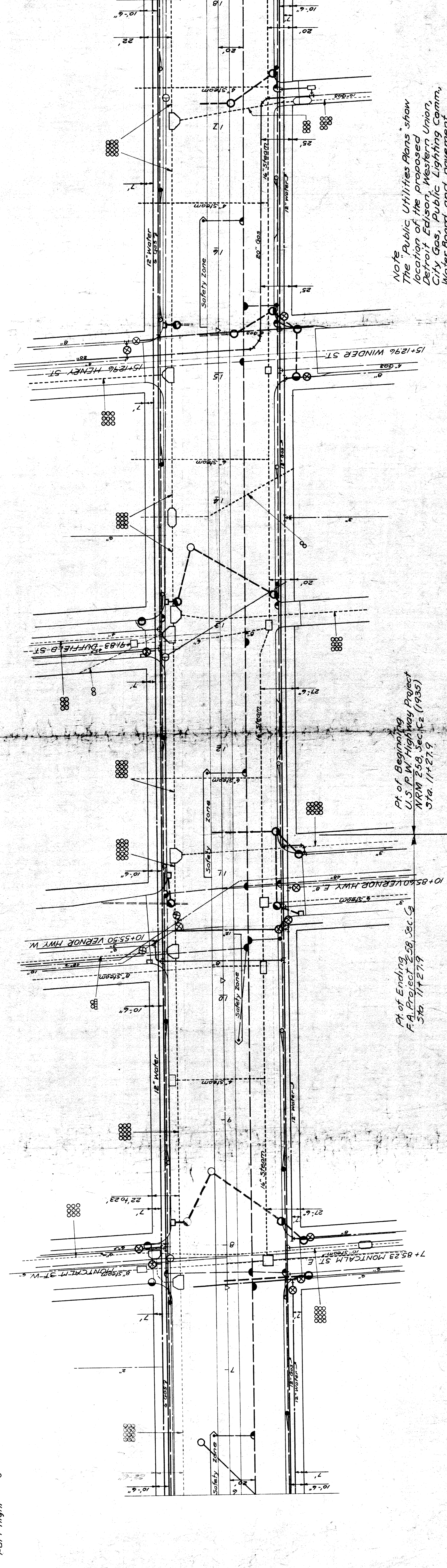
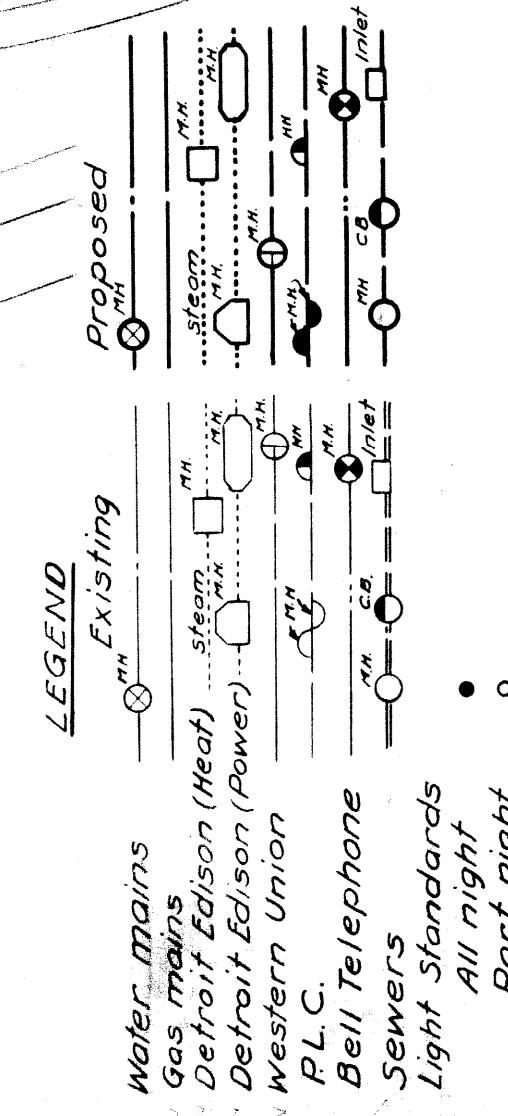
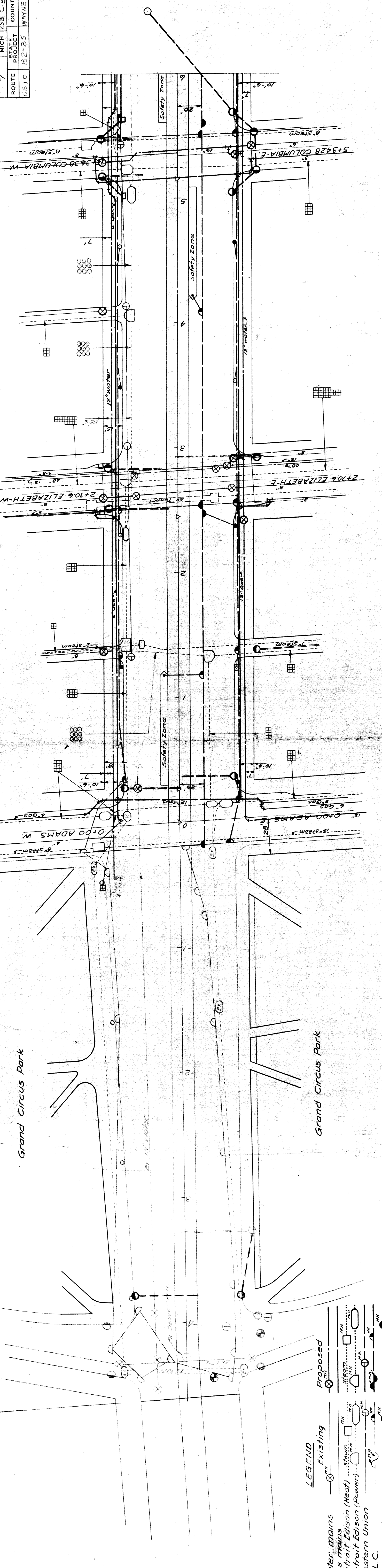


FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	TOTAL SHEETS
7	MICH.	250, C3		
ROUTE NO.	COUNTY	CITY	SHEET NO.	TOTAL SHEETS
82-35	WAYNE	DETROIT	9	
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	TOTAL SHEETS
	MICH.			
ROUTE NO.	COUNTY	TWP.	SHEET NO.	TOTAL SHEETS



FED. ROAD DIST. NO.	STATE	FISCAL YEAR	FISCAL YEAR	TOTAL SHEETS
7	MICH.	250C2 (1933)	1933	7
ROUTE	COUNTY	CITY	TOTAL SHEETS	
U.S. 10	WAYNE	DETROIT	7	
FED. ROAD DIST. NO.	STATE	FISCAL YEAR	FISCAL YEAR	TOTAL SHEETS
7	MICH.	250C2 (1933)	1933	7
ROUTE	COUNTY	CITY	TOTAL SHEETS	
U.S. 10	WAYNE	DETROIT	7	

Pt. of Beginning
FA Project 250, Sec. C3
Sta. 0+461'



Pt. of Ending
FA Project 250, Sec. C3
Sta. 11+27.9

Pt. of Beginning
U.S. 10 Highway Project
M.P.M. 250, Sec. C2 (1933)
Sta. 11+27.9

Note
The Public Utilities Plans show
location of the proposed
Detroit Edison, Western Union,
City Gas, Public Lighting Comm.,
Water Board and Power Dept.
charge systems for Woodward
Ave. (including existing systems
that are to be maintained.)

AS PER PLANS										AS CONSTRUCTED												
STA. TO STA.	LENGTH	WIDTH	CONCRETE		CURB		GUTTER		PAVEMENT		SIDEWALK		STREET		CATCH BASIN		MANHOLE		REMARKS	STA. TO STA.	LENGTH	WIDTH
			AREA	LINEAL	AREA	LINEAL	AREA	LINEAL	AREA	LINEAL	AREA	LINEAL	AREA	LINEAL	AREA	LINEAL	AREA	LINEAL				
0-375 B	81.5	INT	422.2	533.7	2.3	3.3	220.1	47.3	149	39.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0				
0-375 C	329.5	45	1318.0	1427.4	273	330	2201	47.3	149	39.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0				
0-461	73.2	INT	404.4	477.6	3.3	3.3	221.4	47.6	158	39.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0				
0-387 B	337.8	42-60	1576.4	1692.8	338	338	221.4	47.6	158	39.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0				
0-461	0-50		* 85																			
* For adjusting intersection grades beyond limits shown on plans.																						
TOTALS			3720.6	3720.6	4206.5	4206.5	293	724	441.5	798.0	994	349	168	431	862	537	209	328				

