

CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS

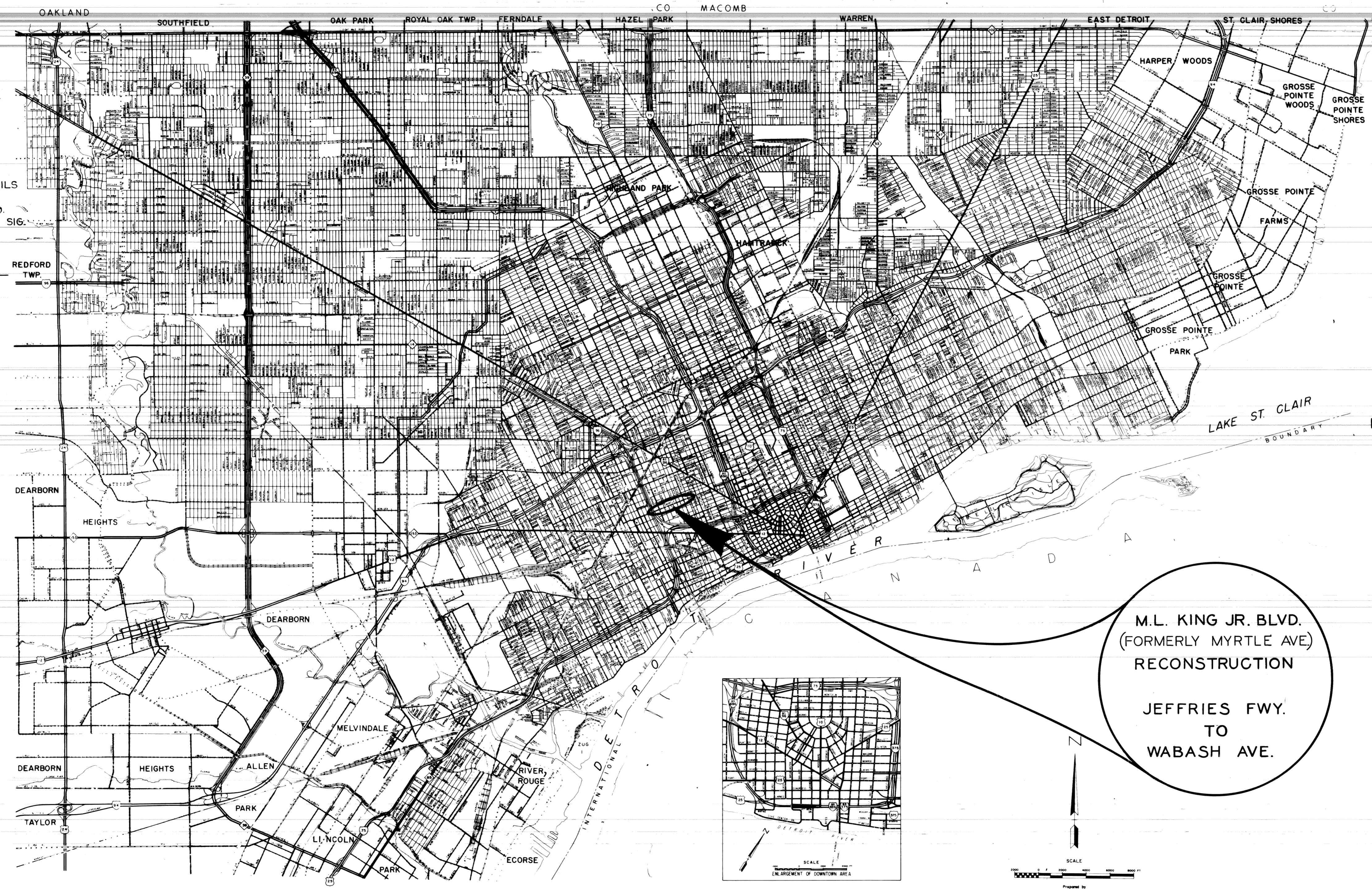
THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF TRANSPORTATION CURRENT STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS.

IN CO-OPERATION WITH MICHIGAN DEPARTMENT OF TRANSPORTATION AND FEDERAL HIGHWAY ADMINISTRATION FEDERAL AID URBAN PROJECT NO. MICHIGAN M 2000 (197) CONTROL SECTION MU 82400 JOB NO. 19151A & 20280(LANDSCAPING)

B.P.R. DIV. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	MICH.				
STREET	CITY	COUNTY	TWP.	SHEET NO.	TOTAL SHEETS

TRAFFIC VOLUME	CURRENT	1995
A.D.T.	5,790	15,400
DESIGN SPEED..... 35 MPH		

- INDEX OF SHEETS**
- 1 TITLE SHEET
 - 2 TYPICAL CROSS-SECTIONS
 - 3 - 5 ALIGNMENT
 - 6 CONSTRUCTION SEQUENCE
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 - 31 - 32 QUANTITY SHEETS
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 - 41 - 50 WIRING DIAGRAMS & TRAFFIC SIG.
 - 51 - 75 PLD DETAIL SHEETS
 - 76 - 77 PLD QUANTITY SHEETS
- STANDARD PLANS**
- | | | |
|--------|--------|---------|
| II 28F | II 41D | IV 84W |
| II 29C | II 42C | V 100A |
| II 39G | II 43C | VI 125E |
| II 40D | II 45C | |



AS BUILT

**M.L. KING JR. BLVD.
(FORMERLY MYRTLE AVE)
RECONSTRUCTION**

**JEFFRIES FWY.
TO
WABASH AVE.**

FED ITEM NO. UFO 838

CONTRACT FOR G, DS, P, & UTILITIES

LOCAL AUTHORITY APPROVAL
CITY OF DETROIT
CITY ENGINEERING DIVISION
DEPARTMENT OF PUBLIC WORKS

APPROVED BY: [Signature] DATE: 4/30/81
ASSISTANT CITY ENGINEER

APPROVED BY: [Signature] DATE: 9/30/81
CITY ENGINEER

PREPARED UNDER SUPERVISION OF

[Signature] 9261
REGISTERED PROFESSIONAL ENGINEER REGISTRATION NO.

CITY OF DETROIT
ORGANIZATION

DETROIT, MICHIGAN
ADDRESS

(SEAL)

FILE NO.	STATE PROJECT	FEDERAL PROJECT	SHEET NO.
NO. 350 BYENCL			1

M 2000 (197) - UFO 838, CONTROL SECTION MU 82400 JOB No's 19151A & 20280 A

Original

LEGEND SHEET

(USE THIS SHEET WITH DRWG. NO. 2)

UNDERGROUND

- EXISTING P.L.C. MANHOLE
- EXISTING P.L.C. HANDHOLE
- EXISTING SINGLE P.L.C. DUCT RUN
-
-
-
-
-
- BUILD NEW MANHOLE (2-WAY)
- BUILD NEW MANHOLE (3-WAY)
- BUILD NEW MANHOLE (CORNER)
- BUILD NEW MANHOLE (4-WAY)
- INSTALL TRAFFIC SIGNAL CONTROLLER FOUNDATION
- INSTALL TRAFFIC SIGNAL MAST ARM FOUNDATION
- INSTALL TRAFFIC SIGNAL PEDESTAL FOUNDATION
- BUILD ROUND HANDHOLE
- BUILD TYPE "D" HANHOLE
- INSTALL MULT. ST. LTG. CONTROL CABINET ON NEW FDN.
- EXISTING DIRECT BURIAL OR PARKWAY CABLE
- ABANDON DIRECT BURIAL OR PARKWAY CABLE
- INSTALL DIRECT BURIAL CABLE (NO. & SIZE AS INDICATED)
-
- EXISTING TRAFFIC SIGNAL CONTROLLER
- EXISTING MAST ARM STANDARD
- EXISTING PEDESTAL
- EXISTING STEEL SHAFT POLE
- EXISTING FIRE OR POLICE CALL BOX
- INSTALL NEW FIRE OR POLICE CALL BOX (EXCEPT AS OTHERWISE INDICATED)
- INSTALL SALVAGED FIRE OR POLICE CALL BOX (EXCEPT AS OTHERWISE INDICATED)
-
- EXISTING U.G.-FED ST. LTG. UNIT
- EXISTING U.G.-FED ST. LTG. UNIT WITH DBL. ARM
- FUTURE U.G.-FED ST. LTG. UNIT
- REMOVE U.G.-FED ST. LTG. UNIT (EXCEPT AS OTHERWISE NOTED)
- INSTALL SALVAGED U.G.-FED ST. LTG. UNIT ON NEW FDN.
-
- INSTALL 400W. TYPE III MERCURY VAPOR LUMINAIRE (240V.)
-
- INSTALL 400W. TYPE III MERCURY VAPOR LUMINAIRE (240V.)
- INDICATES WITHOUT LUMINAIRE
- INDICATES TRAFFIC SIGNAL CONTACT ON ST. LTG. STD.
- INSTALL CODE 117 ST. LTG. UNIT ON NEW FDN.
- INSTALL (2) 400W. TYPE III RECTANGULAR SODIUM VAPOR LUMINAIRE (240V.)
-
- INSTALL 400W. TYPE III MERCURY VAPOR LUMINAIRE (480V.)

OVERHEAD

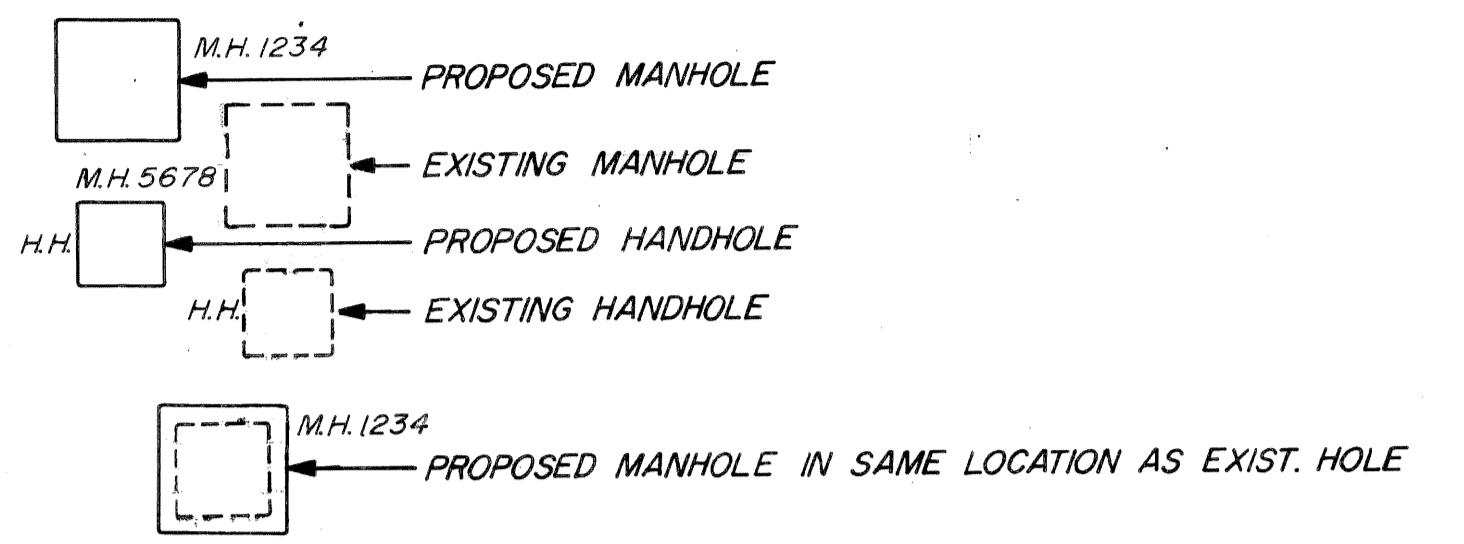
- EXISTING WOOD POLE (D.E. CO. POLE SHOWN)
- REMOVE WOOD POLE (P.L.C. POLE SHOWN)
- REPLACE WOOD POLE (HEIGHT & CLASS AS INDICATED)
- INSTALL 35 FT. CLASS 4 WOOD POLE (EXCEPT AS OTHERWISE INDICATED)
- EXISTING OVERHEAD ST. LTG. UNIT
- REMOVE OVERHEAD ST. LTG. UNIT
- INSTALL OVERHEAD ST. LTG. UNIT WITH 8 FT. ARM (400W. MERCURY VAPOR, 240V. MULTIPLE, EXCEPT AS OTHERWISE NOTED.)
- INSTALL NEW 100W. MERCURY VAPOR LUMINAIRE FOR ALLEY LTG. WITH NEW 6 FT. BRACKET ARM USE SALVAGED LUMINAIRE & ARM WHERE INDICATED.
- INSTALL NEW 250 W. MERCURY VAPOR LUMINAIRE, TYPE I, (2-WAY, OR 4-WAY AS SHOWN) FOR RESIDENTIAL LTG. WITH NEW SERIES COIL & NEW BRACKET ARM OF LENGTH AS SHOWN. (USE SALVAGED LUMINAIRE & ARM WHERE INDICATED.)
- EXISTING OVERHEAD LINE (2-#6 OF STANTON-24 SHOWN)
- REMOVE OVERHEAD LINE (3-#2 OF STANTON-662 SHOWN)
- INSTALL OVERHEAD LINE (2-#6 OF STANTON-24 SHOWN)
- INSTALL & LATER REMOVE OVERHEAD LINE (3-#6 MULT. ST. LTG. SHOWN)
-
- REMOVE GUY & ANCHOR ROD
-
-
- REMOVE GUY (TYPE AS INDICATED)
- MATERIAL TO BE INSTALLED
- MATERIAL TO BE REMOVED
- MAKE WOOD POLE SELF-SUPPORTING IN CRUSHED STONE
- PHASES OF P.L.C. DISTRIBUTION WIRES OR EQUIPMENT
- D.E. CO. DISTRIBUTION WIRE
- D.E. CO. SECONDARY WIRE
- CABLE POLE
- INSTALL SUSPENSION INSULATOR
- P.L.C. DISTRIBUTION WIRE
- P.L.C. SECONDARY WIRE
- P.L.C. STREET LIGHTING WIRE
- P.L.C. MULTIPLE STREET LIGHTING WIRE

GENERAL

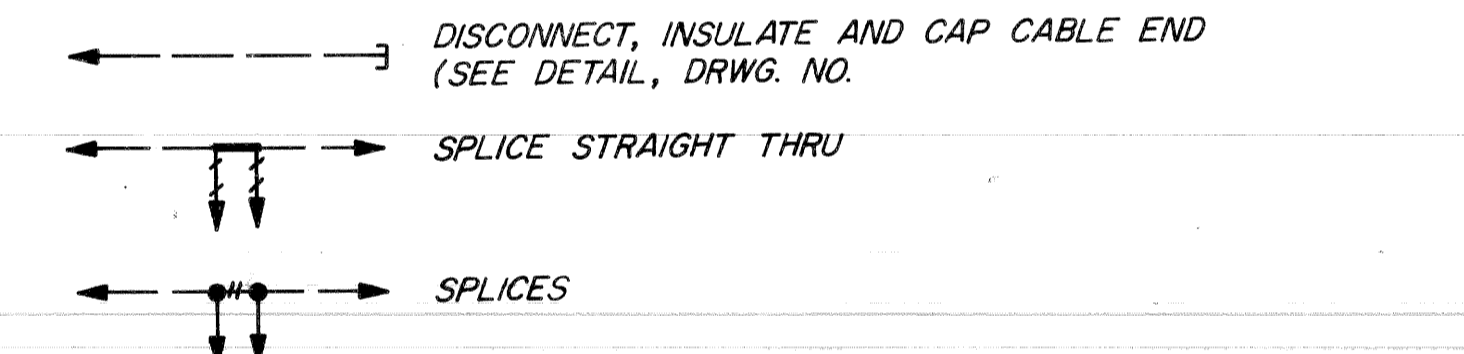
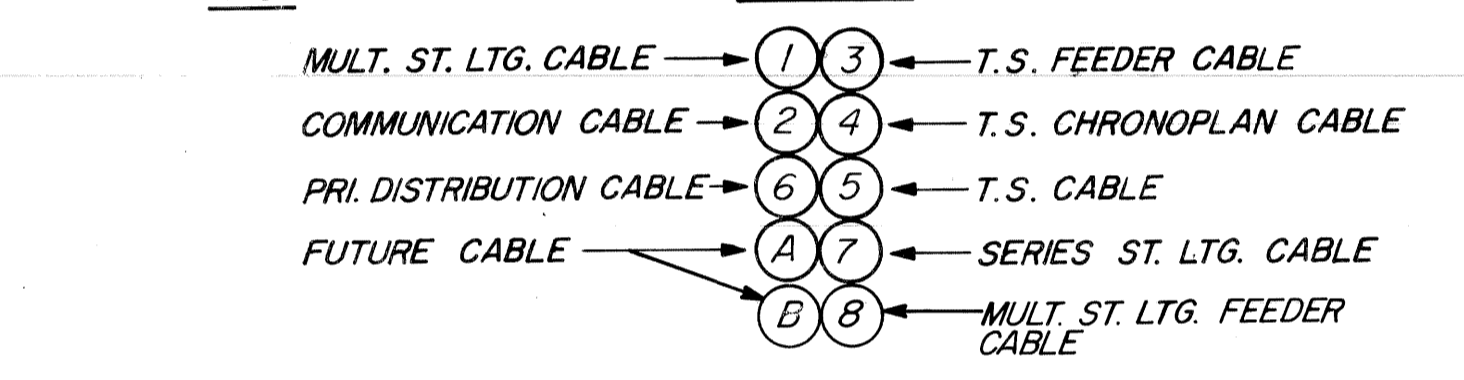
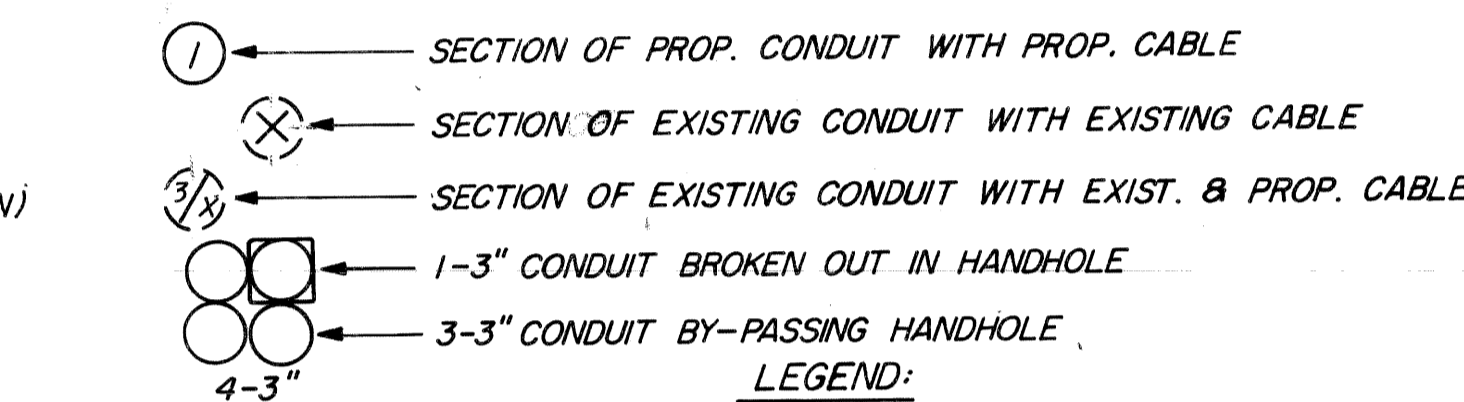
- PROPERTY LINE
- EXISTING PAVEMENT LIMIT
- PROPOSED PAVEMENT JOINT LINE, LIMIT OR FACE OF CURB
- FUTURE PAVEMENT LIMIT
- SEWER LINE, MANHOLE & CATCH BASIN
- DETROIT EDISON COMPANY U.G. LINE & MANHOLE
- MICH. BELL TEL. COMPANY U.G. LINE & MANHOLE
- WATERMAIN & GATEWELL (OTHER UTILITIES SHOWN SIMILAR.)

DIAGRAMS & CONDUIT ALLOCATION

(U.G.-FED ST. LTG. STD. SYMBOLS SAME AS UNDERGROUND LEGEND OF THIS SHEET.)



- INSTALL U.G. OR O.H. CABLE (NO. & SIZE AS INDICATED)
- EXISTING U.G. OR O.H. CABLE
- U.G. CABLE TO BE ABANDONED
- U.G. OR O.H. CABLE TO BE REMOVE
- LEAD SHEATH OF EXIST. SERIES ST. LTG. CABLE USE FOR NEUTRAL OF MULT. ST. LTG. CIRCUITS.



- INDICATES 7500V. 1-1/C #8 L.C. ST. LTG. CABLE
- INDICATES 7500V. 2-1/C #8 L.C. ST. LTG. CABLES
- INDICATES 2KV. 2-1/C #2/0 ST. LTG. CABLES & 1-#2/0 NEUTRAL
- INDICATES 2KV. 2-1/C #2 ST. LTG. CABLES & 1-#2 NEUTRAL (ALL UNLABELED CABLE GROUPS ON WIRING DIAGRAMS ARE SUCH)
- INDICATES 2KV. 1-1/C #2 ST. LTG. CABLE
- INDICATES 2KV. 1-1/C #2 ST. LTG. CABLE & 1-#2 NEUTRAL
- INDICATES 2KV. 3-1/C #2 ST. LTG. CABLES & 1-#2 NEUTRAL
- INDICATES DRWG. NO. OF AREA (APPLIES TO U.G. CABLE ITEMS ONLY)
- INDICATES NEW ST. LTG. STANDARD NUMBER INDICATES OLD ST. LTG. STANDARD NUMBER
- INDICATES #2 BARE STRANDED GROUND WIRE INCIDENTAL TO INSTALLATION OF ST. LTG. CABLES ON THIS CONTRACT.

DESIGN CRITERIA
 1.75 FOOTCANDLES
 1.4 TO 1 UNIFORMITY RATIO

TRAFFIC SIGNAL

- INSTALL 3-SECTION TRAFFIC SIGNAL (1-WAY SHOWN)
- INSTALL 3-SECTION TRAFFIC SIGNAL WITH SALVAGED HEADS (2-WAY SHOWN)
- REMOVE 3-SECTION TRAFFIC SIGNAL (3-WAY SHOWN)
- EXISTING 3-SECTION TRAFFIC SIGNAL (4-WAY SHOWN)
- INSTALL 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (2-WAY SHOWN)
- INSTALL 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL WITH SALVAGED HEAD (1-WAY SHOWN)
- REMOVE 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (1-WAY SHOWN)
- EXISTING 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (1-WAY SHOWN)
- INSTALL JUNCTION BOX
- INSTALL SALVAGED JUNCTION BOX
- REMOVE JUNCTION BOX
- EXISTING JUNCTION BOX
- INSTALL OVERHEAD PLASTIC JACKETED CABLE
- EXISTING OVERHEAD PLASTIC JACKETED CABLE
- REMOVE OVERHEAD PLASTIC JACKETED CABLE
- INSTALL TRAFFIC SIGNAL CONTROLLER (NEW OR SALVAGED AS INDICATED) ON NEW FDN. (EXCEPT AS OTHERWISE INDICATED)
- INSTALL CANTILEVER TYPE TRAFFIC SIGNAL MAST ARM STANDARD & MAST ARM ON NEW FOUNDATION (EXCEPT AS OTHERWISE INDICATED)
- INSTALL 8 FT. TRAFFIC SIGNAL PEDESTAL ON NEW FOUNDATION (EXCEPT AS OTHERWISE INDICATED)
- INSTALL STEEL SHAFT POLE ON NEW FDN.
- EXISTING TRAFFIC SIGNAL CONTROLLER
- EXISTING MAST ARM STANDARD
- EXISTING PEDESTAL
- EXISTING STEEL SHAFT POLE
- BACK-OUT LAMPS & HOOD SIGNALS (INCIDENTAL TO INSTALLATION OF T.S. ON THIS CONTRACT).
- REMOVE HOOD & INSTALL LAMPS (INCIDENTAL TO INSTALLATION OF T.S. ON THIS CONTRACT).
- POLE CONTACT HEIGHT OF T.S. SPAN WIRE
- LOW CONTACT HEIGHT OF SPAN WIRE T.S. TO SPAN WIRE
- INDICATES TYPE OF SIDEWALK RAMP TO BE CONSTRUCTED.

AS BUILT

PLAN INDEX	
DRWG. NO.	SUB-TITLE
1	LEGEND
2	GENERAL INFORMATION
3-8	GENERAL PLAN
9-12	WIRING DIAGRAMS
13-18	TRAFFIC SIGNAL INSTALLATION
19-40 19-43	DETAIL SHEETS
41-42 44-45	QUANTITY SHEETS

DATE	DESCRIPTION	CHKD BY

ML. KING JR. BLVD. RECONSTR
JEFFRIES FWY. TO WABASH AVE

SHEET **33** OF **77** SHEETS
 JOB NO. 19151 A
 ASSIGNMENT NO.
 DATE **9-81**

CITY OF DETROIT
 CITY ENGINEERING DEPARTMENT

DRAWN **CEA**
 CHECKED **mm**
 APPROVED **mm**
 DATE **7-81**

PLAN PREPARED BY:
 CONSULTING ENGINEERING ASSOCIATES INC.
 ENGINEERING CONSULTANTS
 16580 WYOMING DETROIT MICH., 48221
 DRWG. NO. **1** OF **45** FILE NO. **CEA 1061**

PUBLIC LIGHTING COMMISSION
 CITY OF DETROIT
 FILE NO. **51-0569**
 SHEET NO. **1** OF **45**
 DATE **7-81**

LEGEND

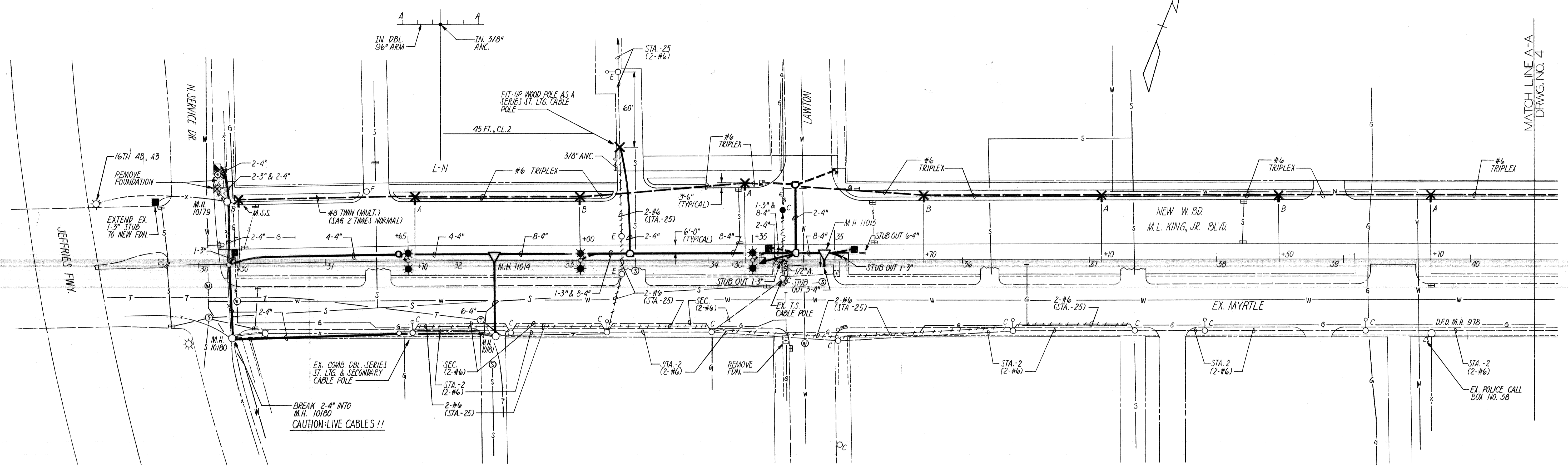
GENERAL INFORMATION SHEET

T-

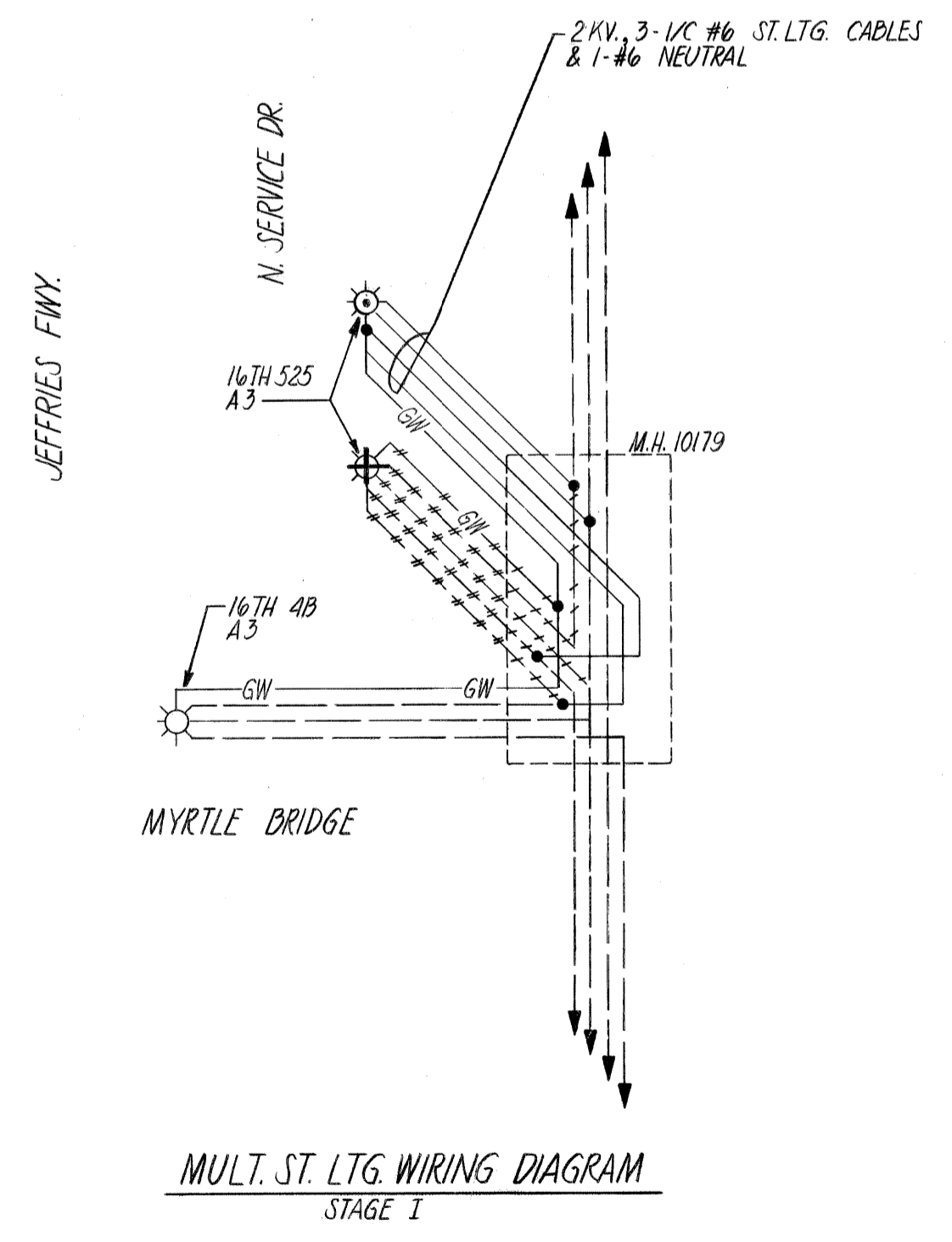
1. CALL MISS DIG (647-7344) 48 HRS. PRIOR TO ANY EXCAVATION FOR THE LOCATIONS OF UNDERGROUND UTILITIES.
2. CONTRACTOR TO NOTIFY MICH. CONS. GAS CO. AT WO-5-8000 IF PROTECTIVE COATED GAS MAIN IS EXPOSED OR DAMAGED.
3. A MINIMUM CLEARANCE OF 3'-6" HORIZONTAL & 1' VERTICAL MUST BE MAINTAINED BETWEEN PROPOSED P.L.D. FACILITIES & EXISTING WATER FACILITIES.
4. CONTRACTOR SHALL NOTIFY SYSTEM OPERATING DIVISION OF THE P.L.D. 48 HRS. IN ADVANCE OF ANY WORK ON UNDERGROUND OR OVERHEAD TRANSMISSION, DISTRIBUTION, & ST. LTG. CIRCUITS. PHONE 961-1360.
5. ALL EXISTING STREET LIGHTING, TRAFFIC SIGNAL, PRIMARY, TRANSMISSION ETC. CIRCUITS SHALL ALWAYS BE MAINTAINED IN AN OPERATIONAL CONDITION (EXCEPT WHERE OTHERWISE NOTED).
6. EXISTING O.H. & T.S. FACILITIES ARE NOT NECESSARILY SHOWN ON PLANS.
7. ALL OVERHEAD WIRES & UNDERGROUND CABLES SHALL CONSIST OF COPPER CONDUCTORS AS PER THE SPECIFICATIONS.
8. WHERE INSTALLATION OF NEW MANHOLES OVER EXISTING CONDUITS (TO ACCOMMODATE NEW & EXIST. CONDUITS) IS CALLED FOR ON PLANS, CONTRACTOR SHALL CAREFULLY & SO AS NOT TO DAMAGE EXIST. CABLES, REMOVE THE EXIST. CONDUITS & ENCASEMENT WITHIN MANHOLES. EXIST. CABLES SHALL BE EXTENDED & PROPERLY TRAINED, RACKED & SUPPORTED.
9. NEW CONDUITS BROKEN INTO EXISTING MANHOLES OR HANDHOLES SHALL NOT INTERFERE WITH RACKING AND/OR TRAINING OF CABLES. EXERCISE CAUTION INSTALLING NEW CABLES INTO EXIST. HOLES.
10. ALL CONDUITS NOT TERMINATING IN STRUCTURES SUCH AS MANHOLES, HANDHOLES OR FOUNDATIONS SHALL EXTEND 2FT. BEYOND PAVEMENT LIMIT (EXCEPT AS OTHERWISE INDICATED). ALL UNOCCUPIED CONDUITS SHALL BE PLUGGED.
11. WHERE ABANDONING OF U.G. CABLES IS CALLED FOR ON PLANS OR DIAGRAMS, CONTRACTOR SHALL CUT & REMOVE CABLES WITH MANHOLES & HANDHOLES.
12. UNDERGROUND CABLE QUANTITIES ARE ITEMIZED ON GENERAL PLANS. ALL CABLES SHALL BE TAGGED IN ALL MANHOLES & HANDHOLES. THIS INCLUDES EXIST. CABLES THAT ARE CONVERTED TO MULTIPLE, RECONNECTED TO OTHER CIRCUITS OR RENDERED DEAD.
13. ALL LUMINAIRES SHALL BE PROVIDED WITH 240V. INTERNAL BALLASTS (EXCEPT AS OTHERWISE NOTED).
14. THE CANDLEPOWER DISTRIBUTION FOR ALL MERCURY VAPOR ST. LTG. LUMINAIRES SHALL BE SEMI-CUTOFF, MEDIUM DISTRIBUTION OF TYPE AS INDICATED ON THE PLANS.
15. ALL NEW, SALVAGED & CONVERTED ST. LTG. STD'S. SHALL BE PAINTED.
16. ALL NEW & SALVAGED ST. LTG. STD'S. INSTALLED ON THIS CONTRACT & EXIST. ST. LTG. STD'S. CONVERTED OR RE-CONNECTED TO OTHER CIRCUITS SHALL BE STENCILLED OR RE-STENCILLED AS SHOWN ON PLANS. (STENCIL-LING & RESTENCILLING OF ST. LTG. STD'S. IS INCIDENTAL TO ST. LTG. STD.)
17. ALL TREE TRIMMING REQUIRED TO CLEAR NEW OR SALVAGED ST. LTG. & TRAFFIC SIGNAL STD'S. & O.H. LINES SHALL BE INCIDENTAL TO THE PAY-ITEM & NO EXTRA PAYMENT SHALL BE MADE.
18. WHERE TRIMMING OF TREES ON CITY PROPERTY IS CALLED FOR ON PLANS THE CONTRACTOR SHALL OBTAIN A PERMIT FROM THE RECREATION DEPT. OF THE CITY OF DETROIT AND SHALL HAVE SUCH WORK DONE BY A LICENSED TREE SERVICE CONTRACTOR. CALL MR. CRAIG GRANT AT 931-3950.
19. WHERE U.G. UTILITIES INTERFERE WITH THE INSTALLATION OF A NEW FOUNDATION, INSTALL THE SPECIAL FOUNDATION OF PARTICULAR DIMENSIONS AS INDICATED ON THE DETAIL DRWG. TO SUIT THE FIELD CONDITION. THERE WILL BE NO EXTRA PAYMENT FOR THE SPECIAL FOUNDATION. IT WILL BE PAID FOR AS A NORMAL FOUNDATION.
20. ALL NEW U.G.-FED ST. LTG. UNITS SHALL BE INSTALLED 2'-6" BACK OF CURB UNLESS OTHERWISE INDICATED ON PLANS.
21. BAND INSULATED CLEVIS TO ST. LTG. STD. SHAFT. BRING U.G. CABLES THRU TOP OF SHAFT WITH WEATHERCAP TO FIT SHAFT OPENING. STORE SHAFT CAP IN BASE OF STANDARD CONNECTION OF O.H. WIRES TO U.G. CABLES SHALL BE MADE OUTSIDE OF SHAFT (INCIDENTAL TO INSTALLATION OF O.H. WIRES).
22. BAND SEC. RACK TO ST. LTG. STD. SHAFT. BRING U.G. CABLES THRU TOP OF SHAFT WITH WEATHERCAP TO FIT SHAFT OPENING. STORE SHAFT CAP IN BASE OF STD. CONNECTION OF O.H. WIRES TO U.G. CABLES SHALL BE MADE OUTSIDE OF SHAFT (INCIDENTAL TO INSTALLATION OF O.H. WIRES).
23. REMOVE WEATHERCAP & INSULATED CLEVIS OR SEC. RACK FROM ST. LTG. STD. SHAFT. MOUNT SHAFT CAP ON ST. LTG. STD. (INCIDENTAL TO REMOVAL OF O.H. WIRES).
24. FOR LOCATIONS OF P.L.D. INSTALLATIONS ON STRUCTURES SUCH AS CONDUITS, HANDHOLES, CONDUIT SLEEVES, GALVANIZED STEEL CONDUITS & ST. LTG. STD. ANCHOR BOLTS SEE STRUCTURE PLANS.
25. INSTALLATION OF ARMS FOR EQUIPMENT, CUTOUTS, POTHEADS, TRANSFORMERS, ETC. ARE NOT SHOWN ON NEW CABLE AND TRANSFORMER POLES. THEY SHALL BE INSTALLED AS PER THE DETAIL DRWG. REQUIREMENT AND SHALL BE INCIDENTAL TO THE FITTING-UP OF THE CABLE AND/OR TRANSFORMER POLE.
26. ALL TRANSFORMER POLES AND CABLE POLES SHALL BE FITTED UP WITH 120" ARMS (EXCEPT WHERE OTHERWISE NOTED).
27. ALL NEW ANCHOR GUYS SHALL BE INSTALLED ON A 1:1 RATIO OR AS NEARLY AS POSSIBLE (EXCEPT WHERE OTHERWISE NOTED. STRUT GUYS ARE EXCEPTED).
28. ALL POTHEADS ON PRIMARY DISTRIBUTION CABLE POLES SHALL BE FLAT DIVERGENT DISCONNECTING TYPE.
29. INSTALL WOOD POLES SO AS NOT TO INTERFERE WITH TRAFFIC OR FUTURE CONSTRUCTION STAGES.
30. CROSSARMS SHALL BE REMOVED AFTER ALL CONTACTS ARE REMOVED (INCIDENTAL TO REMOVAL OF OVERHEAD LINES).
31. WHERE REMOVAL OF LUMINAIRES IS CALLED FOR ON PLANS THE ASSOCIATED O.H. SERIES COIL SHALL BE REMOVED BY THE CONTRACTOR (REMOVE O.H. COIL IS INCIDENTAL TO REMOVE LUMINAIRE).
32. WHERE A P.L.D. WOOD POLE WITH OTHER UTILITY CONTACTS IS TO BE REMOVED THE P.L.D. INSPECTOR WILL INDICATE IF THE POLE IS IN FACT TO BE REMOVED.
33. ALL SALVAGED WOOD POLES SHALL BE POLES PREVIOUSLY INSTALLED NEW ON THIS CONTRACT.
34. ALL TRAFFIC STREET SIGNS SUCH AS "NO PARKING", "NO STANDING" ETC. SHALL BE TRANSFERRED FROM OLD ST. LTG. STD. TO NEW ST. LTG. STD. AT SAME LOCATION OR ON NEW STD. IN CLOSE PROXIMITY BY DEPT. OF TRANSPORTATION (OR WOOD POLES).
35. PROPOSED T.S. SHALL BE PUT INTO OPERATION AT TIME OF REMOVAL OF EXISTING T.S. FACILITIES. CONTRACTOR SHALL NOTIFY THE P.L.D. INSPECTOR IF HE IS UNABLE TO MAINTAIN T.S. IN OPERABLE CONDITION AT ALL TIMES.
36. ALL TRAFFIC SIGNALS SHALL BE MOUNTED WITH NEW STANDARD TRAFFIC SIGNAL BRACKET & FITTINGS.
37. ALL TRAFFIC SIGNAL ITEMS AS CALLED FOR ON PLANS SHALL INCLUDE AS INCIDENTAL TO THE TRAFFIC SIGNAL ALL CABLES FROM THE CONTROLLER TO THE TRAFFIC SIGNALS & FOUNDATIONS AS INDICATED.
38. CONTRACTOR SHALL DELIVER WHERE REQUIRED TO THE PUBLIC LIGHTING DEPARTMENT THE T.S. CONTROLLER FOR TIMING. CONTRACTOR SHALL PICK-UP CONTROLLER FROM THE PUBLIC LIGHTING DEPARTMENT WHEN READY FOR INSTALLATION.
39. ALL SALVAGED TRAFFIC SIGNALS SHALL BE TRAFFIC SIGNALS PREVIOUSLY INSTALLED NEW ON THIS CONTRACT (EXCEPT AS OTHERWISE INDICATED).
40. FOR TRAFFIC SIGNAL SPAN WIRES USE 1/2" SIEMANS-MARTIN GRADE AS SPECIFICATION.
41. ALL TRAFFIC SIGNAL LAMPS SHALL BE FURNISHED BY THE PUBLIC LIGHTING DEPARTMENT.
42. SEAL-END OF CABLE WHERE COILING UP OF CABLE IS CALLED FOR (CONTRACTOR SHALL RECEIVE PAYMENT FOR COILED-UP CABLES).
43. D.S.R. STREETCAR RAILS AND FOUNDATIONS (TRACKS) ARE SHOWN ON THE PLANS IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION. EXACT LOCATIONS WITHIN THE STREETS & INTERSECTIONS ARE NOT KNOWN. SOME RAILS MAY HAVE BEEN REMOVED.
44. SIDEWALK RAMPS OF THE TYPE & LOCATION AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER SHALL BE CONSTRUCTED.
45. "CONDUIT REPAIR - UNDER PAVEMENT" AND "CONDUIT REPAIR UNDER SIDEWALK OR DIRT" SHALL INCLUDE LOCATING EXISTING P.L.D. CONDUIT BREAKDOWN OR RUPTURE AT A POINT LOCATED IN THE EXISTING CONDUIT INSTALLATION WHETHER A SINGLE CONDUIT OR MULTIPLE OF TWO OR MORE. THE WORK SHALL INCLUDE EXCAVATION AT THE POINT OF BREAKDOWN, CHIPPING & BREAKING AWAY THE CONCRETE ENCASEMENT, INSTALLING A CONDUIT SLEEVE TO REPLACE THE CUTAWAY OR REMOVED SECTION OF DEFECTIVE CONDUIT, GROUTING THE CONCRETE ENCASEMENT TO ITS ORIGINAL THICKNESS THEN BACKFILLING THE EARTH AND REPAIRING THE SIDEWALK OR PAVEMENT. EXERCISE CARE NOT TO DAMAGE EXISTING ENERGIZED CABLES. NOTE: NONE OF THIS WORK SHALL BE UNDERTAKEN UNTIL FIRST CONTACTING THE PROJECT ENGINEER.
46. INSTALL 600V, 1-6 PR. #16 I.M. SHIELDED CABLE FOR CHRONOPLAN 1'-0" BELOW D.E. CO. SECONDARY (EXCEPT WHERE OTHERWISE NOTED).
47. COIL-UP 6FT. OF SLACK IN STD. SHAFT FOR CONNECTION TO CALL BOX. CALL BOX CONNECTIONS TO BE MADE BY COMM. DIV.

AS BUILT

	<p>M.L. KING JR. BLVD. RECONSTR. JEFFRIES FWY. TO WABASH AVE.</p> <p style="text-align: center;">GENERAL INFORMATION & AREA MAP</p>	<p>SHEET <u>34</u> OF <u>77</u> SHEETS</p> <p>JOB NO. 19151 A</p> <p>ASSIGNMENT NO.</p> <p>DATE 9-81</p>	<p>CITY OF DETROIT</p> <p>CITY ENGINEERING DEPARTMENT</p>	<p>DRAWN CEA</p> <p>CHECKED <i>[Signature]</i></p> <p>APPROVED <i>[Signature]</i></p> <p>DATE 7-81</p>	<p>PLAN PREPARED BY CONSULTING ENGINEERING ASSOCIATES INC. ENGINEERING CONSULTANTS</p> <p>16580 WYOMING DETROIT, MICH. 48221</p> <p>DRWG. NO. 2 OF 45</p> <p>FILE NO. CEA 1061</p>	<p>DRAWN BY</p> <p>CHECKED BY</p> <p>APPROVED BY</p>	<p>PUBLIC LIGHTING COMMISSION</p> <p>CITY OF DETROIT</p>	<p>FILE NO. 51-0569</p> <p>SHEET NO. 2 OF 45</p> <p>DATE 7-81</p>
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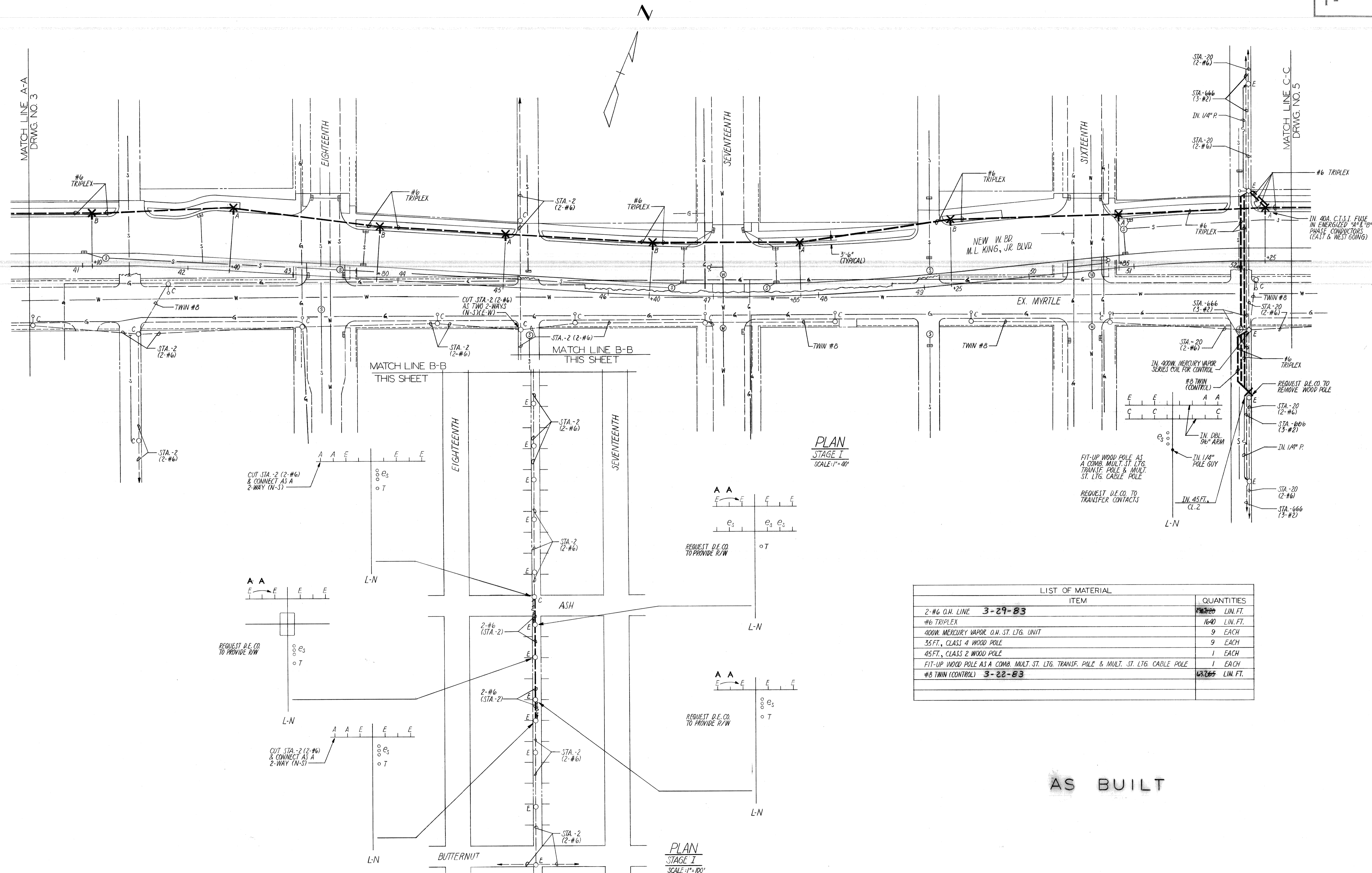
PLAN
STAGE I
SCALE: 1"=40'



LIST OF MATERIAL	
ITEM	QUANTITIES
1-3" ENCASED CONDUIT	137 LIN. FT.
2-3" ENCASED CONDUIT	7 LIN. FT.
2-4" ENCASED CONDUIT	416 LIN. FT.
3-4" ENCASED CONDUIT	10 LIN. FT.
4-4" ENCASED CONDUIT	209 LIN. FT.
6-4" ENCASED CONDUIT	171 LIN. FT.
8-4" ENCASED CONDUIT	209 LIN. FT.
1-3" & 8-4" ENCASED CONDUIT	51 LIN. FT.
2-3" & 2-4" ENCASED CONDUIT	16 LIN. FT.
TYPE "D" HANDHOLE	3 EACH
ROUND HANDHOLE	1 EACH
3-WAY MANHOLE	2 EACH
REMOVE FOUNDATION	3 EACH
REMOVE U.G.-FED ST. LTG. UNIT	1 EACH
400W. RECTANGULAR SODIUM VAPOR LUMINAIRE	6 EACH
400W. MERCURY VAPOR LUMINAIRE	1 EACH
CODE 117 UG-FED ST. LTG. UNIT ON NEW FDN.	3 EACH
CODE 009-00 UG-FED ST. LTG. STD. ON NEW FDN.	1 EACH
6 FT. CLAMP-ON BRACKET ARM (3'-0" RISE)	1 EACH
CHROMPLAN CABLE, 16GA., PLASTIC JACKETED, 6 PR., SHIELDED	557 LIN. FT.
2 KV, 2-1/2 #6 SECONDARY CABLES	443 LIN. FT.
7500V, 2-1/2 #8 L.C. ST. LTG. CABLES	360 LIN. FT.
REMOVE WOOD POLE	1 EACH
REMOVE 2-#6 O.H. LINE	695 LIN. FT.
400W. MERCURY VAPOR O.H. ST. LTG. UNIT	8 EACH
#8 TWIN	139 LIN. FT.
#6 TRIPLEX	283 LIN. FT.
35 FT., CL. 4 WOOD POLE	8 EACH
45 FT., CL. 2 WOOD POLE	1 EACH
FIT-UP WOOD POLE AS A SERIES ST. LTG. CABLE POLE	1 EACH
2 KV, 3-1/2 #6 ST. LTG. CABLES & 1-#6 NEUTRAL	25 LIN. FT.
2 KV, 1-7/8 #14 P.J. CABLE FOR CHROMPLAN	57 LIN. FT.
6" SEMER (LIGHTING)	10 LIN. FT.

AS BUILT

DATE DESCRIPTION CHKD. BY	M.L. KING JR. BLVD. RECONSTR. JEFFRIES FWY. TO WABASH AVE.		SHEET 35 OF 77 SHEETS		CITY OF DETROIT		PLAN PREPARED BY: CONSULTING ENGINEERING ASSOCIATES INC. ENGINEERING CONSULTANTS		PUBLIC LIGHTING COMMISSION		FILE NO. 51-0569
	GENERAL PLAN - STAGE I		JOB NO. 19151 A		CITY ENGINEERING DEPARTMENT		16580 WYOMING DETROIT MICH., 48221		CITY OF DETROIT		SHEET NO. 3 OF 45
	ASSIGNMENT NO.		DATE 9-81		DRAWN BY CEA		DRWG. NO. 3 OF 45		FILE NO. CEA 1061		DATE 7-81
	CHECKED BY		APPROVED BY		DATE 7-81		DRWG. NO.		FILE NO.		DATE 7-81



PLAN
STAGE I
SCALE 1\"/>

PLAN
STAGE I
SCALE 1\"/>

LIST OF MATERIAL		QUANTITIES
ITEM		
2-#6 O.H. LINE	3-29-83	1640 LIN. FT.
#6 TRIPLEX		1640 LIN. FT.
400W. MERCURY VAPOR O.H. ST. LTG. UNIT		9 EACH
35FT., CLASS 4 WOOD POLE		9 EACH
45FT., CLASS 2 WOOD POLE		1 EACH
FIT-UP WOOD POLE AS A COMB. MULT. ST. LTG. TRANSF. POLE & MULT. ST. LTG. CABLE POLE		1 EACH
#8 TWIN (CONTROL)	3-22-83	63265 LIN. FT.

AS BUILT

DATE	DESCRIPTION	CHKD. BY

**M.L. KING JR. BLVD RECONSTR.
JEFFRIES FWY. TO WABASH AVE.**
GENERAL PLAN - STAGE I

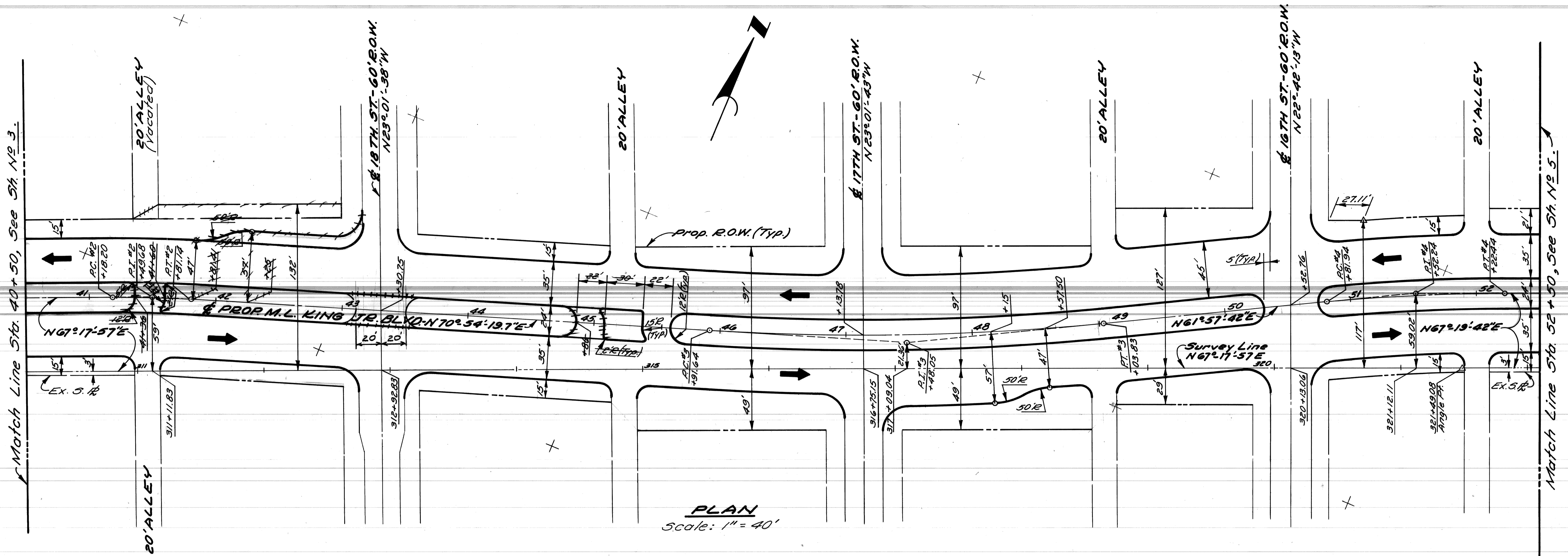
SHEET 36 OF 77 SHEETS
JOB NO. 19151 A
ASSIGNMENT NO.
DATE 9-81

CITY OF DETROIT
CITY ENGINEERING DEPARTMENT

DRAWN CEA
CHECKED
APPROVED
DATE 7-81

PLAN PREPARED BY:
CONSULTING ENGINEERING ASSOCIATES INC.
ENGINEERING CONSULTANTS
16580 WYOMING DETROIT MICH. 48221
DRWG. NO. 4 OF 45
FILE NO. CEA 1061

DRAWN BY
CHECKED BY
APPROVED BY
PUBLIC LIGHTING COMMISSION
CITY OF DETROIT
SHEET NO. 4 OF 45
DATE 7-81



PLAN
Scale: 1" = 40'

M.L. KING JR. BLVD.-CURVE DATA

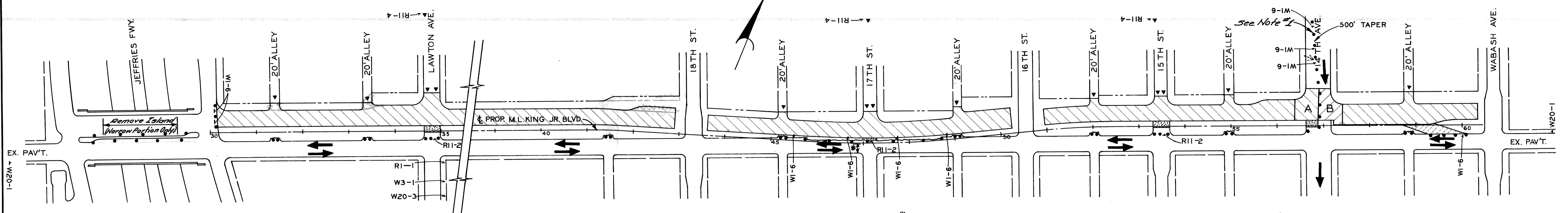
CURVE #2	CURVE #3	CURVE #4
$\Delta = 3^{\circ}36'22.7''$	$\Delta = 8^{\circ}56'37.7''$	$\Delta = 5^{\circ}22'0.0''$
$D = 5^{\circ}43'46.48''$	$D = 2^{\circ}51'53.24''$	$D = 3^{\circ}49'10.99''$
$R = 1000'$	$R = 2000'$	$R = 1500'$
$T = 31.48'$	$T = 156.42'$	$T = 70.30'$
$L = 62.94'$	$L = 312.20'$	$L = 140.50'$
$E = 0.50'$	$E = 6.11'$	$E = 1.65'$
$PC = 41+18.20$	$PC = 45+31.64$	$PC = 50+81.94$
$PI = 41+49.68$	$PI = 47+48.05$	$PI = 51+52.24$
$PT = 41+81.14$	$PT = 49+03.83$	$PT = 52+22.44$

NOTES:

1. All prop. street return radii shall be 20' unless otherwise noted.
2. All prop. alley return radii shall be 10' unless otherwise noted.

AS BUILT

REVISIONS LOCATED BY COORDINATES ON SHEET 38 Eliminate Bus Bay at 18 th RP WB 4/83 35C Open 18 th St through the median RP WB 3/92 2C5C Eliminate Median Openings at Sta. 41+36 and at Sta. 44+86. RP WB 3/92		REFERENCE DRAWINGS DESIGNED BY DRAWN BY TRACED BY CHECKED BY	APPROVED: ENGINEER OF STREETS HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. ALIGNMENT STA. 40 + 50 TO STA. 52 + 50	SHEET 4 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81
--	--	--	--	--	---	--



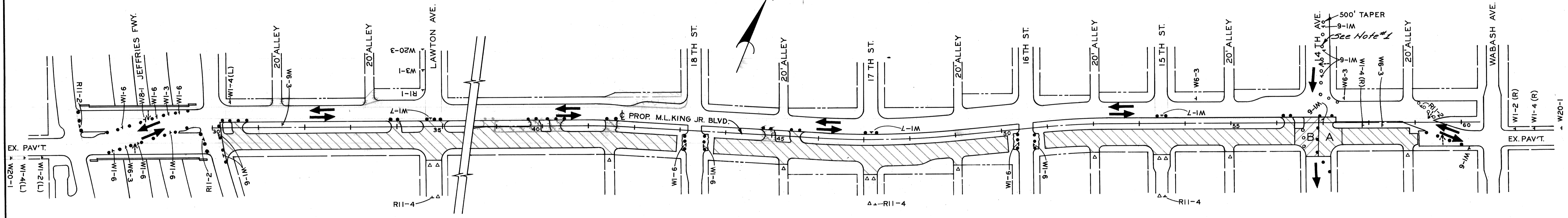
LEGEND (ALL STAGES)

- Permanent Pav't Construction (Sub-Stage A)
- Permanent Pav't Construction (Sub-Stage B)
- Temporary Conc. Pav't - Nonreinf. - 7 in.
- Temporary Pav't Connection (See Sht. # 14.)
- Construct Curb
- Sign, Type B Temporary
- Barricade Type II, Lighted
- Barricade Type II, Lighted (Placed Previously)
- Barricade Type III, Lighted
- Barricade Type III, Lighted (Placed Previously)
- Lighted Arrow, Type A

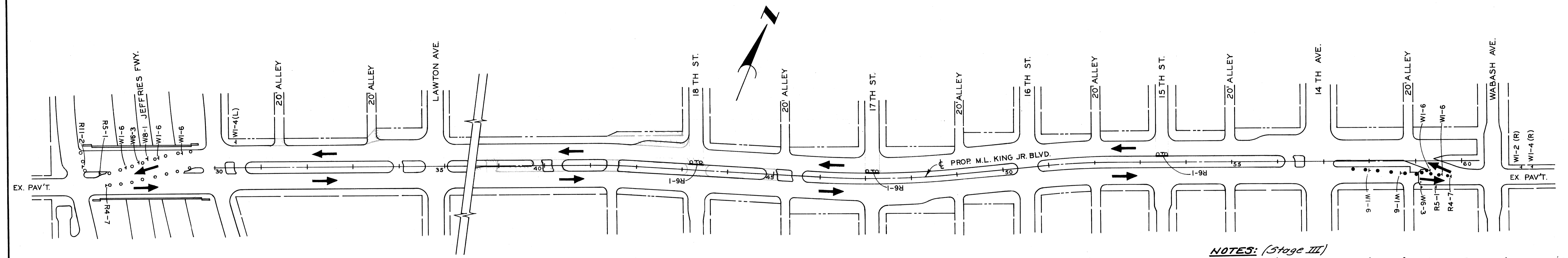
STAGE I

NOTES: (Stages I & II)

- Barricades on 14th Ave are shown for Sub-Stage "A" only. Barricades must be re-positioned for Sub-Stage "B"
- Consecutive cross-streets may not be closed at the same time during construction. Assuming Lawton, 17th, & 15th are closed at one time, 18th & 16th may not be closed until the others are re-opened.



STAGE II



STAGE III

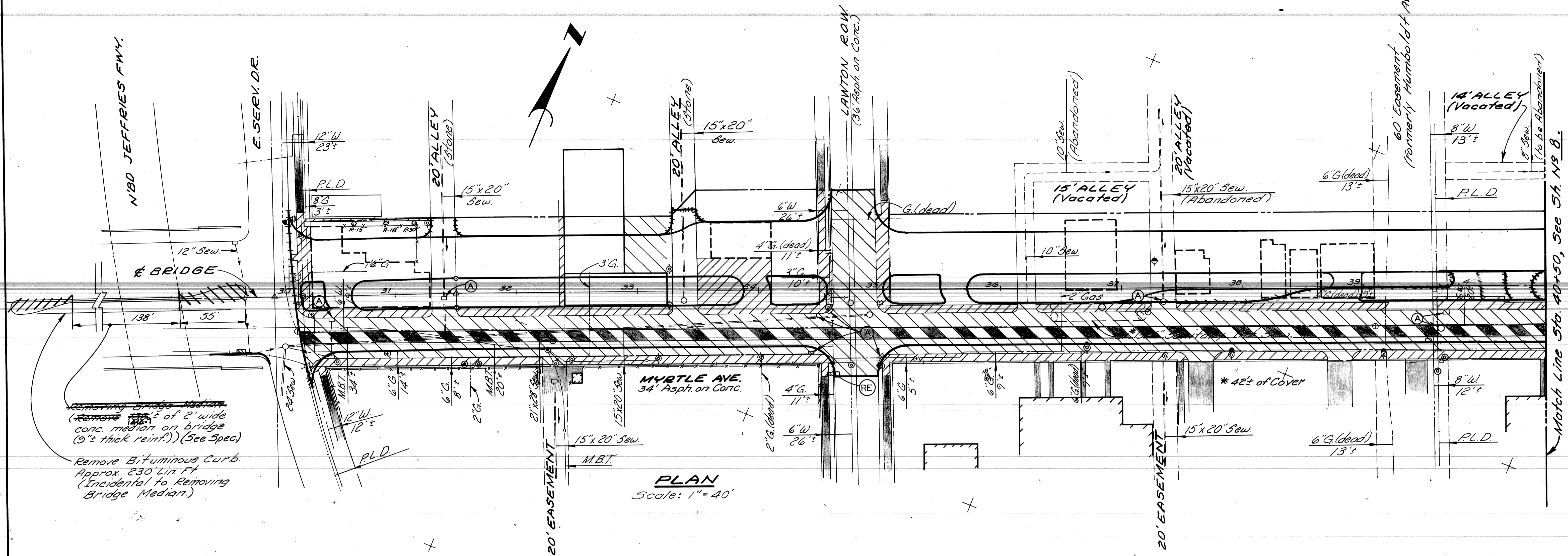
AS BUILT

NOTES: (Stage III)

- Barricades & signing shown in Stage III are to remain in place upon completion of the contract. They shall become the property of the City of Detroit & shall be maintained by City forces. (East of 14th & West of Lawton Only)

SCALE: 1" = 100'

SA-SDSF Eliminate Bus Bay at 18th 2/AADP Eliminate Alley Return 3/AADP Eliminate Alley Return 4D-6D Revise Median 4E-6E Revise Median		DESIGNED BY DRAWN BY TRACED BY CHECKED BY	APPROVED: ENGINEER OF STREETS HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. CONSTRUCTION SEQUENCE JEFFRIES FWY. TO WABASH AVE.	SHEET 6 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81
REVISIONS LOCATED BY COORDINATES ON SHEET						



Removing Bridge Median
(Remove 12' of 2' wide conc. median on bridge (3" thick reinf.)) (See Spec.)
Remove Bituminous Curb. Approx. 230 Lin. Ft. (Incidental to Removing Bridge Median.)

PLAN
Scale: 1"=40'

REMOVAL LEGEND

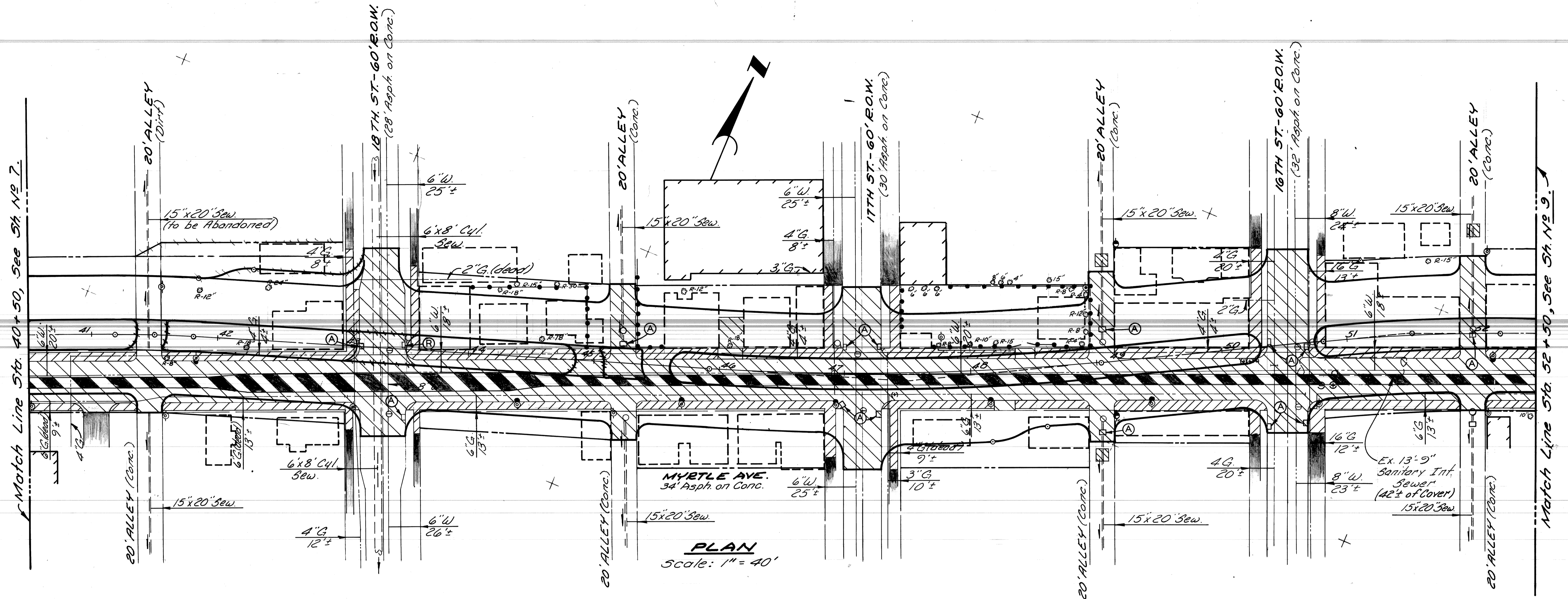
- Removing Pavement
- Removing Sidewalk
- R(size) Removing Tree
- Existing Basements
- Existing Foundation
- Existing Building to Remain
- Abandoning Drainage Structures
- Existing Sidewalk or Driveway to Remain
- Removing Drainage Structures
- Existing Fence to Remain
- Removing Track Encasement
- Removing Curb

AS BUILT

NOTES:
1. For Utility Legend, See Sh. # 29
2. Locations of exist. utility lines are based on the best available records & are not guaranteed for accuracy.

CALL MISS DIG
48 HOURS PRIOR TO
CONSTRUCTION 647-7344

REVISIONS LOCATED BY COORDINATES ON SHEET		REFERENCE DRAWINGS	DESIGNED BY M.S.	APPROVED: <i>Allen E. [Signature]</i> ENGINEER OF STREETS HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAU OF STREETS AND HIGHWAYS	M.L.KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. REMOVALS JEFFRIES FWY. TO STA. 40+50	SHEET 7 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81
82-11C Revise Median 83+85 Eliminate Alley Returns + Revise Bus Bay	RP/WB 3/83	DRN CKD APVD DATE	DRAWN BY M.S.	CHECKED BY W.B.	1 2 3 4 5 6 7 8 9 10 11	11	



PLAN
Scale: 1" = 40'

REMOVAL LEGEND

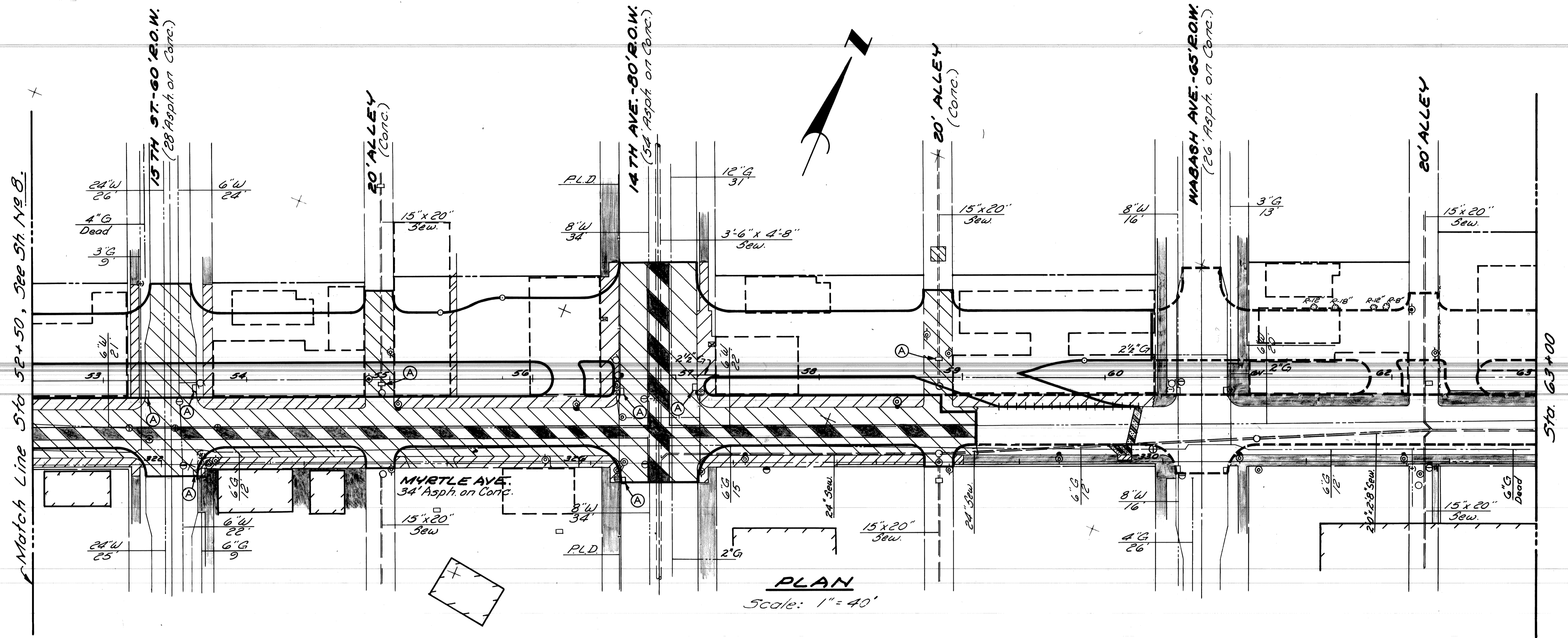
- Removing Pavement
- Removing Sidewalk
- Removing Tree
- Existing Basements
- Existing Foundation
- Existing Building to Remain
- Abandoning Drainage Structures
- Existing Sidewalk or Driveway to Remain
- Removing Drainage Structures
- Removing Fence
- Removing Track Encasement

AS BUILT

NOTES:
 1. For Utility Legend, See Sh. # 29
 2. Locations of exist utility lines are based on the best available records & are not guaranteed for accuracy.

CALL MISS DIG
 48 HOURS PRIOR TO
 CONSTRUCTION 647-7344

<p>REVISIONS LOCATED BY COORDINATES ON SHEET</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th>NO.</th> <th>DESCRIPTION</th> <th>DRN</th> <th>CK'D</th> <th>APVD</th> <th>DATE</th> </tr> <tr> <td>2.5 B</td> <td>Eliminate Bus Bay at 18th</td> <td>EP</td> <td>WB</td> <td>4/83</td> <td></td> </tr> <tr> <td>1.5 C</td> <td>Eliminate Median Open-</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>4.5 C</td> <td>ings</td> <td>EP</td> <td>WB</td> <td>3/83</td> <td></td> </tr> <tr> <td>3 C</td> <td>Add Med. opening @ 18th St.</td> <td></td> <td></td> <td></td> <td></td> </tr> </table>	NO.	DESCRIPTION	DRN	CK'D	APVD	DATE	2.5 B	Eliminate Bus Bay at 18 th	EP	WB	4/83		1.5 C	Eliminate Median Open-					4.5 C	ings	EP	WB	3/83		3 C	Add Med. opening @ 18 th St.					<p>DESIGNED BY</p> <p>DRAWN BY <i>M.S.</i></p> <p>TRACED BY</p> <p>CHECKED BY <i>W.B.</i></p>	<p>APPROVED:</p> <p>ENGINEER OF STREETS <i>Allen E. Hooper</i> HIGHWAY ENGINEER</p>	<p>CITY OF DETROIT CITY ENGINEERING DIVISION - D.R.W. BUREAUS OF STREETS AND HIGHWAYS</p>	<p>M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.</p> <p>REMOVALS STA. 40+50 TO STA. 52+50</p>	<p>SHEET 8 OF 77 SHEETS</p> <p>CONTRACT NO. 19151A</p> <p>ASSIGNMENT NO.</p> <p>DATE 9-81</p>
NO.	DESCRIPTION	DRN	CK'D	APVD	DATE																														
2.5 B	Eliminate Bus Bay at 18 th	EP	WB	4/83																															
1.5 C	Eliminate Median Open-																																		
4.5 C	ings	EP	WB	3/83																															
3 C	Add Med. opening @ 18 th St.																																		



PLAN
Scale: 1" = 40'

REMOVAL LEGEND

- Removing Pavement
- Removing Sidewalk
- Removing Curb
- Existing Basements
- Existing Foundation
- Existing Building to Remain
- Abandoning Drainage Structures
- Existing Sidewalk or Driveway to Remain
- Removing Old Pavement (Patching) and replace with Concrete Pavement Patching, Type F5, 9", Nonreinforced.
- Removing Track Encasement

NOTES:
 1. For Utility Legend, See Sh # 29
 2. Locations of exist. utility lines are based on the best available records & are not guaranteed for accuracy.

AS BUILT

CALL MISS DIG
 48 HOURS PRIOR TO
 CONSTRUCTION 647-7344

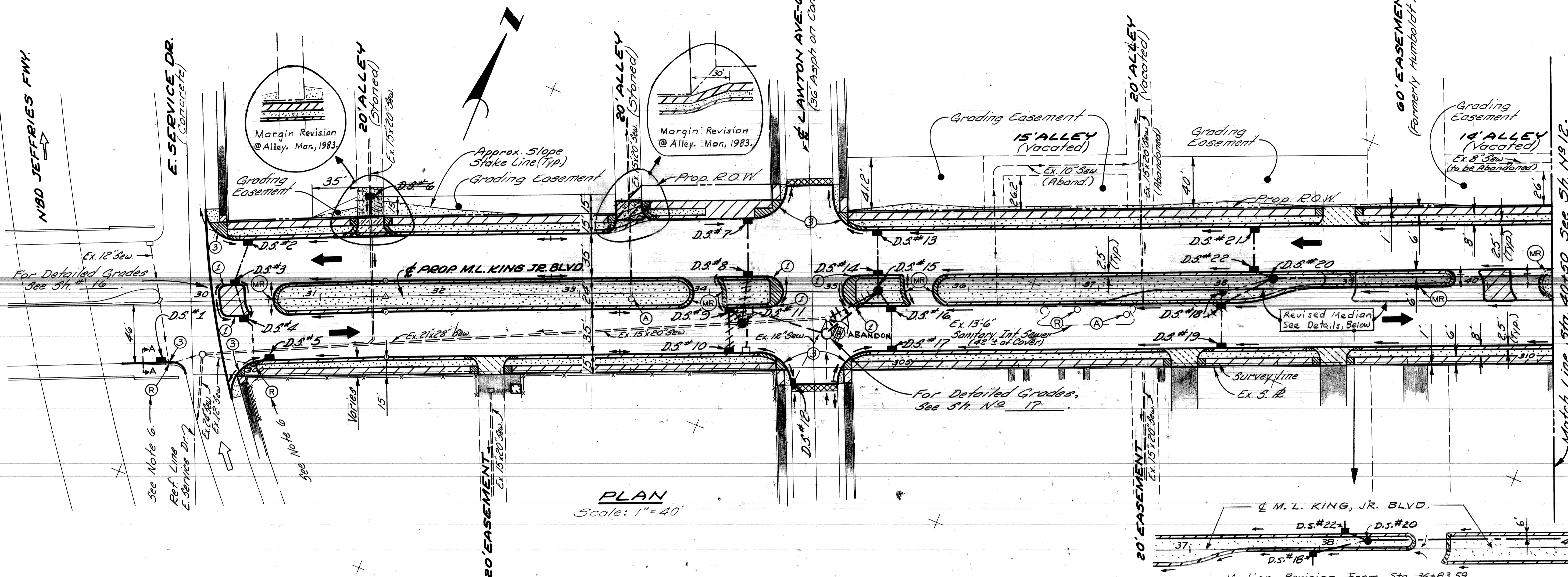
REFERENCE DRAWINGS DESIGNED BY DRAWN BY <i>M.A.O.</i> TRACED BY CHECKED BY <i>W.B.</i>		APPROVED ENGINEER OF STREETS HIGHWAY ENGINEER		CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAU OF STREETS AND HIGHWAYS			M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. REMOVALS STA. 52 + 50 TO WABASH AVE.		SHEET 9 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81	
COORD.	DESCRIPTION	DRN.	CHKD.	APVD.	DATE					
REVISIONS LOCATED BY COORDINATES ON SHEET										

C.B.M. Elev 120.96 Spike in Pole
100' E. of Jeffries Fwy, S.S. M.L. King Jr. Blvd.

C.B.M. Elev. 121.32 Spike in Pole
S.E. Cor. Lawton Ave & M.L. King Jr. Blvd.

C.B.M. Elev. 123.04 Spike in Pole
55 M.L. King Jr. Blvd at Bldg. No. 2685

C.B.M. Elev. 121.23 Brass Cap S.W.
Cor. M.L. King Jr. Blvd. - Jeffries Bridge



PLAN
Scale: 1" = 40'

Median Revision From Sta. 36+83.59
To Sta. 40+30. March, 1983.

DRAINAGE STRUCTURE TABLE

DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT.	WITH TRAP (T)	SEWER LENGTH & TYPE LEAVING (D.S.)			SEWER TAP SIZE (IN.)	TRENCH DETAIL (L.F.)		COMMENTS
			LENGTH (FT.)	SIZE (IN.)	CLASS C-76-		8	9	
1	C.B. "L"		2	12	III	12	2		Taps into Trapped Exist. C.B.
2	C.B. "A"		32	12	III			32	
3	C.B. "B"		22	12	III		22		
4	C.B. "B"		7	12	III			7	Runs to Trapped Exist. C.B.
5	C.B. "A"		9	12	III	12	9		Taps into Trapped Exist. C.B. *
6	C.B. "B"		40	12	III			40	
7	C.B. "B"	T	31	12	III		31		
8	C.B. "B"	T	8	12	III			8	
9	C.B. "A"		30	12	III			30	
10	C.B. "A"		30	12	III			30	
11	M.H. 57d							5	Connect to Ex. C.B. Sewer Line**
12	C.B. "B"	T	5	12	III			30	
13	C.B. "A"		30	12	III			30	
14	C.B. "B"	T	8	12	III		8		
15	M.H. 57d		43	12	IV	12	23	20	
16	C.B. "B"	T	10	12	IV			10	
17	C.B. "A"		30	12	III			30	
18	C.B. "B"	T	35	12	III		35	20	
19	C.B. "A"		40	12	III			38	40
20	M.H. 57d		280	15	IV		235	45	
21	C.B. "A"		30	12	III			30	
22	C.B. "B"	T	9	12	III		9		

* If interference with Ex. 6" Gas is encountered, an "L" type catch basin may be substituted.
** Corbel may have to be offset to clear Ex. 4" Gas.

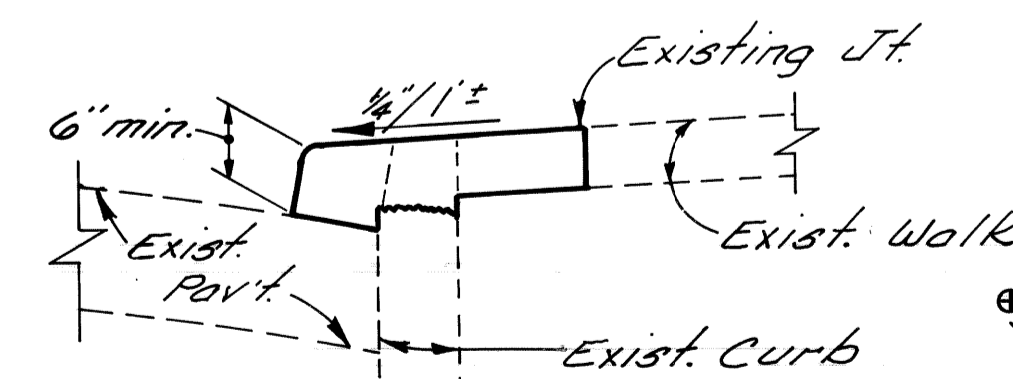
SURFACING LEGEND

- Concrete Pavement (Reinforced) - 9" Uniform with Integral Curb
- Concrete Pavt. - Nonreinforced - 9"
- Concrete Pavt. - Nonreinforced - 8"
- 4" Concrete Sidewalk
- 6" Concrete Sidewalk (Ramp areas are paid for separately)
- Exist. Sid'wk. or Drwy. to Remain.
- Existing Building to Remain.
- Removing Bitum. Surf. & Resurf. (Approaches)
- Class "A" Sodding with 3" Topsoil
- Drainage Flow Arrows
- Adjust Drainage Structure Covers
- Reconstructing Drainage Structs.
- Sidewalk Ramps, Types 1 & 3
- Mower Ramp (Incidental to Contract Cost; For Details See Sh. #28.)
- Bituminous Approaches
- Existing Fence to Remain

NOTES:

1. For Alignment, See Sh. #3.
2. For Typical Cross-Section, See Sh. #2.
3. For Construction Sequence, See Sh. #6.
4. For Driveway Details, See Sh. #29.
5. For Utility Legend & Symbols, See Sh. #29.
6. Reconstruct Ex. drainage structure and replace Catch Basin Cover with Manhole Cover.
7. For Landscaping, See Sh. # 23, thru #25.

AS BUILT



INTEGRAL CURB & SIDEWALK
SECTION A-A
NO SCALE

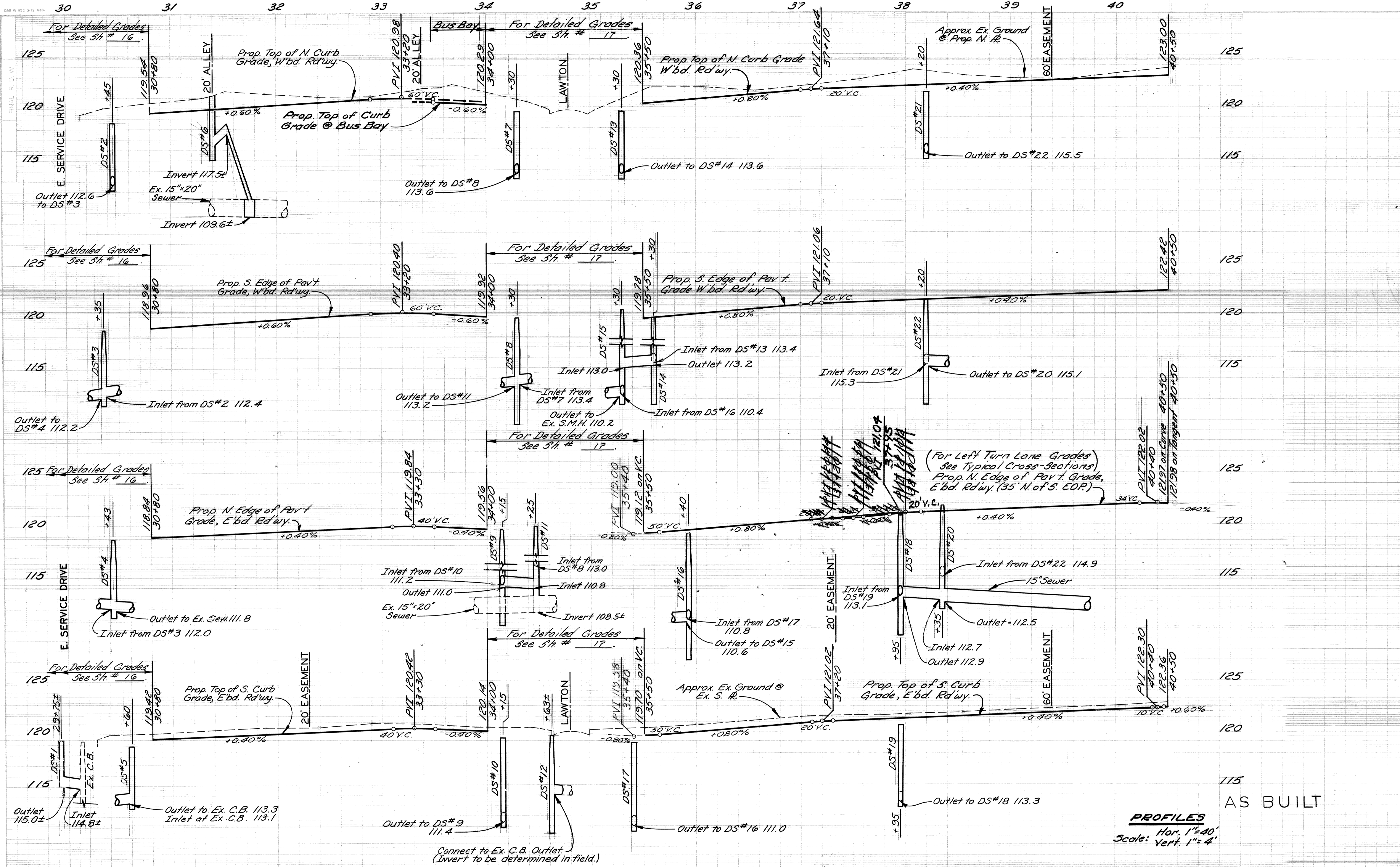
DESIGNED BY	TRACED BY	CHECKED BY	W.B.
DRAWN BY			
APPROVED			
ENGINEER OF STREETS			
HIGHWAY ENGINEER			

CITY OF DETROIT
CITY ENGINEERING DIVISION - DPW
BUREAU OF STREETS AND HIGHWAYS

M.L. KING JR. BLVD. RECONSTRUCTION
WEST GRAND BLVD. TO LINCOLN AVE.

PLAN
JEFFRIES FWY. TO STA. 40+50

SHEET 10 OF 77 SHEETS
CONTRACT NO. 19151A
ASSIGNMENT NO.
DATE 9-81

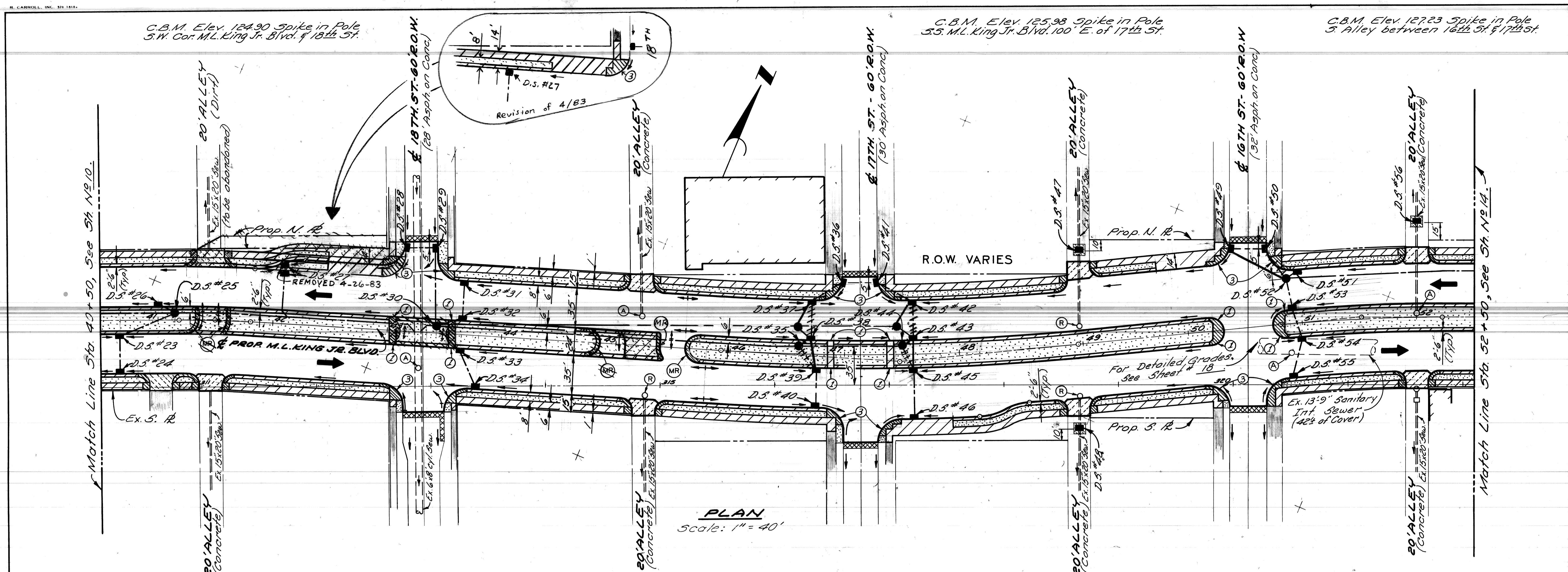


PROFILES
 Hor. 1"=40'
 Scale: Vert. 1"=4'

AS BUILT

30	31	32	33	34	35	36	37	38	39	40	
APPROVED			 ENGINEER OF EXPRESSWAY			CITY OF DETROIT CITY ENGINEERING DIVISION-D.P.W.			M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.		SHEET 11 OF 77 SHEETS
DRWN BY									CONTRACT NO. 19151A		
CK'D BY			W.B.			PROFILES JEFFRIES FWY. TO STA. 40+50			DATE 9-81		

3/83 Revise Grade WB/RBP



PLAN
Scale: 1" = 40'

DRAINAGE STRUCTURE TABLE

DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT	WITH TRAP (T)	SEWER LENGTH & TYPE LEAVING (D.S.)			SEWER TAP SIZE (IN)	TRENCH DETAIL (L.F.)		COMMENTS
			LENGTH (FT)	SIZE (IN)	CLASS		8	9	
23	C.B. "B"	T	45	12	III		45		
24	C.B. "A"		30	12	III			30	
25	M.H. Drop		210	18	IV	18	185	25	Manhole Has 24" Cushion
26	C.B. "B"	T	8	12	III		8		
27	C.B. "B"	T	45	12	IV		5	40	Inlet to Prop. 18" Sew. = 110.3±
28	C.B. "A"		23	12	III			23	
29	C.B. "B"	T	63	12	III		5	58	
30	M.H. Drop		15	18	IV	18	15		Manhole has 24" Cushion
31	C.B. "A"		30	12	III			30	
32	C.B. "B"	T	18	12	III		18		
33	C.B. "B"	T	23	12	III			23	
34	C.B. "A"		33	12	III			33	
35	M.H. Drop		320	18	III		270	50	Manhole Has 12" Cushion
36	C.B. "A"		28	12	III			28	
37	C.B. "A"		29	12	III			29	
38	C.B. "B"	T	6	12	III		6		**
39	C.B. "B"	T	18	12	III		18		**
40	C.B. "A"		30	12	III			30	
41	C.B. "A"		30	12	III			30	
42	C.B. "B"	T	29	12	III		32	29	
43	C.B. "A"		4	12	III		4		**
44	M.H. 3' dia		90	15	III		90		**
45	C.B. "B"	T	18	12	III		18		**
46	C.B. "A"		40	12	III			40	
47	18" x 12"		14	12	III			14	
48	18" x 12"		14	12	III			14	
49	C.B. "B"	T	27	12	III			27	** **
50	C.B. "B"	T	4	12	III		16	10	** **
51	C.B. "B"	T	4	12	III			4	** **
52	M.H. 3' dia		315	18	III			315	
53	C.B. "B"	T	22	12	III			22	
54	C.B. "B"	T	24	12	III		24		
55	C.B. "A"		32	12	III			32	*
56	18" x 12"		13	12	III			13	

SURFACING LEGEND

- Concrete Pavement (Reinforced) 9" Uniform with Integral Curb
- Concrete Pavt. Nonreinforced - 9"
- Concrete Pavt. Nonreinforced - 8"
- 4" Concrete Sidewalk
- 6" Concrete Sidewalk (Ramp areas are paid for separately)
- Exist. Sidw/ or Dr'wy. to Remain
- Existing Building to Remain
- Removing Bitum Surf & Resurf. (Bitum. Approaches)
- Class "A" Sodding with 3" Topsoil
- Drainage Flow Arrows
- Adjust Drainage Structure Covers
- Reconstructing Drainage Structures
- Sidewalk Ramps, Types 1 & 3
- Mower Ramp (Incidental to Contract Cost, For Details, See Sh. #28.)

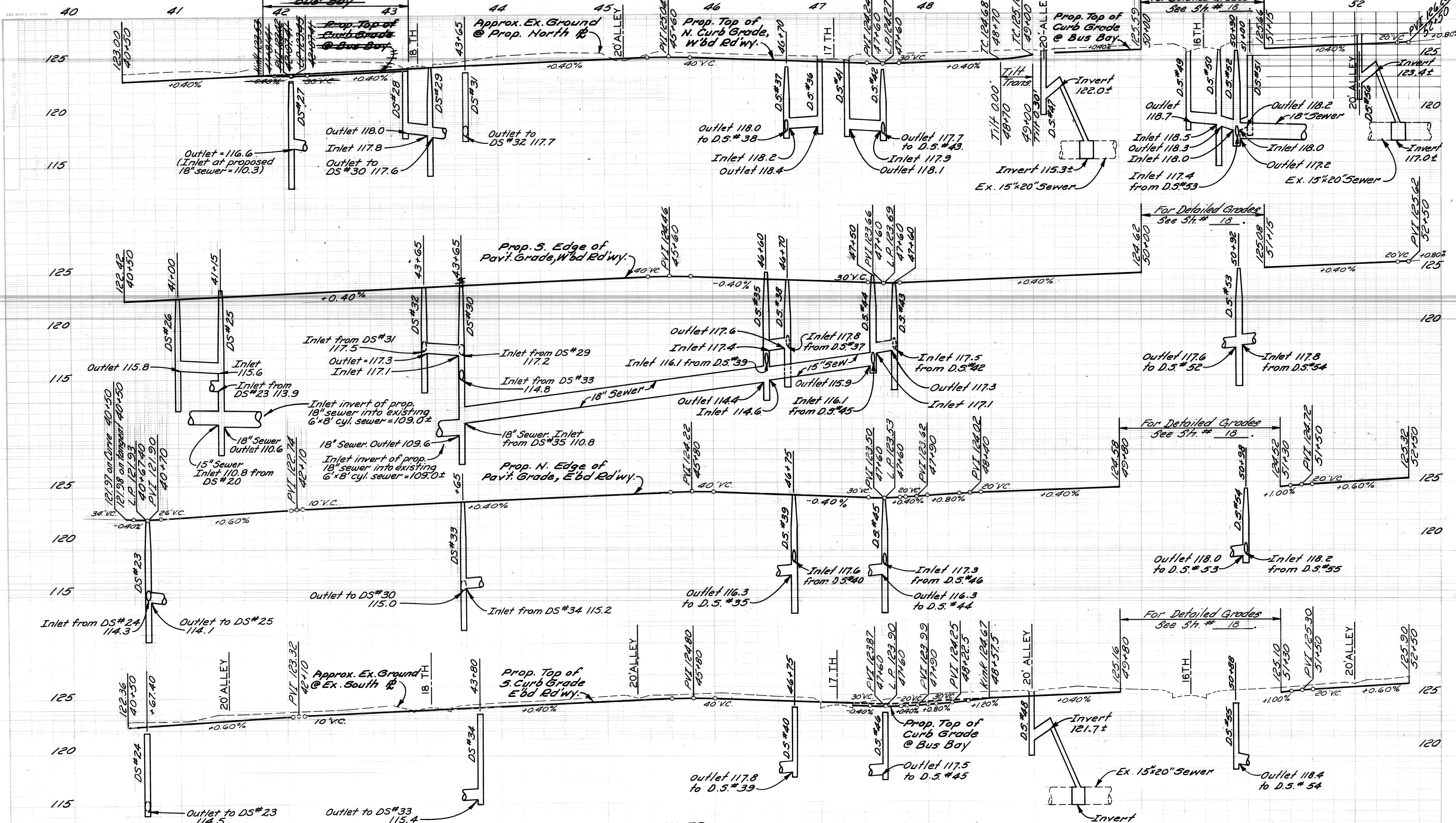
NOTES:

1. For Alignment, See Sh. #4.
2. For Typical Cross Section, See Sh. #2.
3. For Construction Sequence, See Sh. #6.
4. For Driveway Details, See Sh. #29.
5. For Utility Legend & Symbols, See Sh. #29.
6. For Landscaping, See Sh. #23. thru #25.

AS BUILT

- * If interference with exist. 6" gas main is encountered, an "L" type catch basin may be substituted.
- ** Corbel may have to be offset to clear existing gas main.
- ** * If D.S. #49 and #50 cannot be constructed as shown due to interference with existing 16" gas main, construct alternate as directed by the Engineer.

REVISIONS LOCATED BY COORDINATES ON SHEET 3B Eliminate Bus Bay at 18th 1.5B Eliminate Alley Return. 1.5C9 Eliminate Median Open- 4.5C ing. 3C Add Med. Opening @ 18th St.	REFERENCE DRAWINGS DESIGNED BY DRAWN BY TRACED BY CHECKED BY	APPROVED: ENGINEER OF STREETS HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING DIVISION-DP BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. PLAN STA. 40 + 50 TO STA. 52 + 50	SHEET 12 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81
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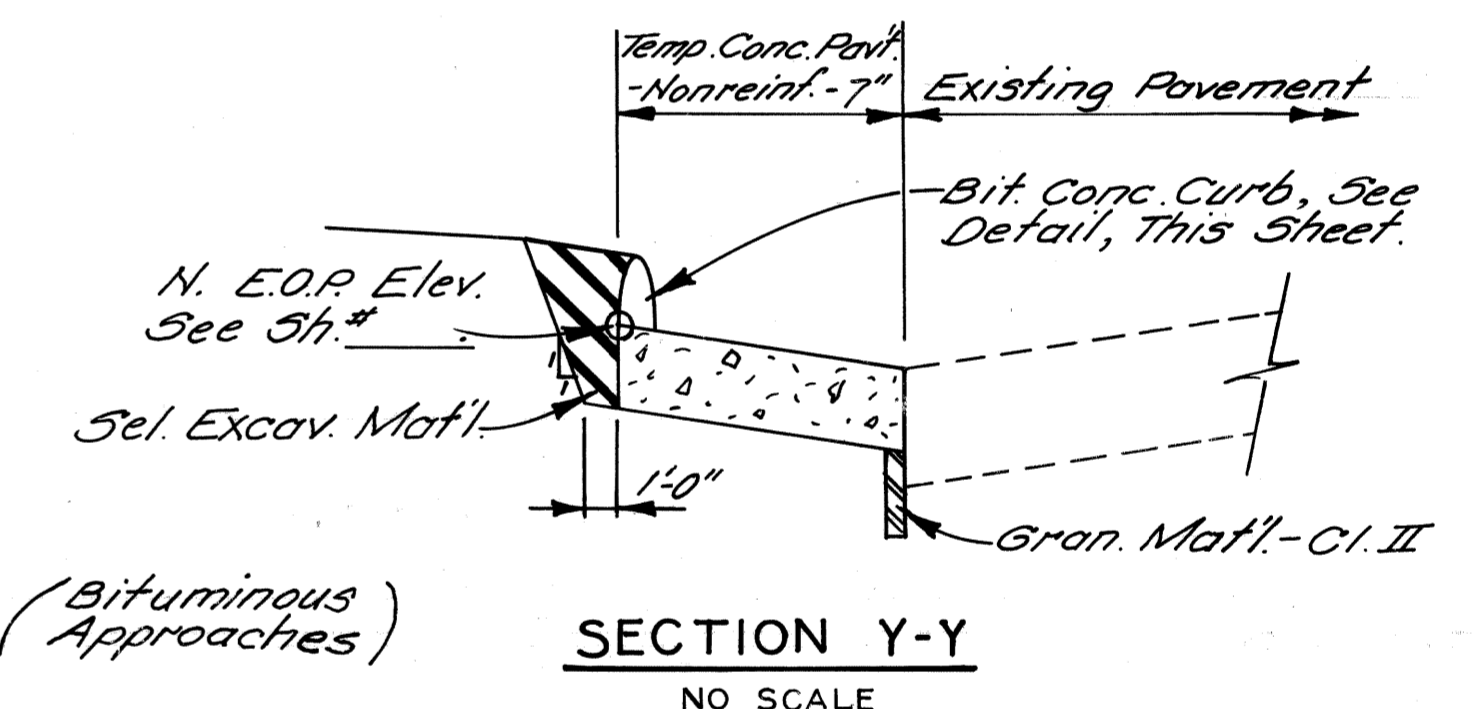
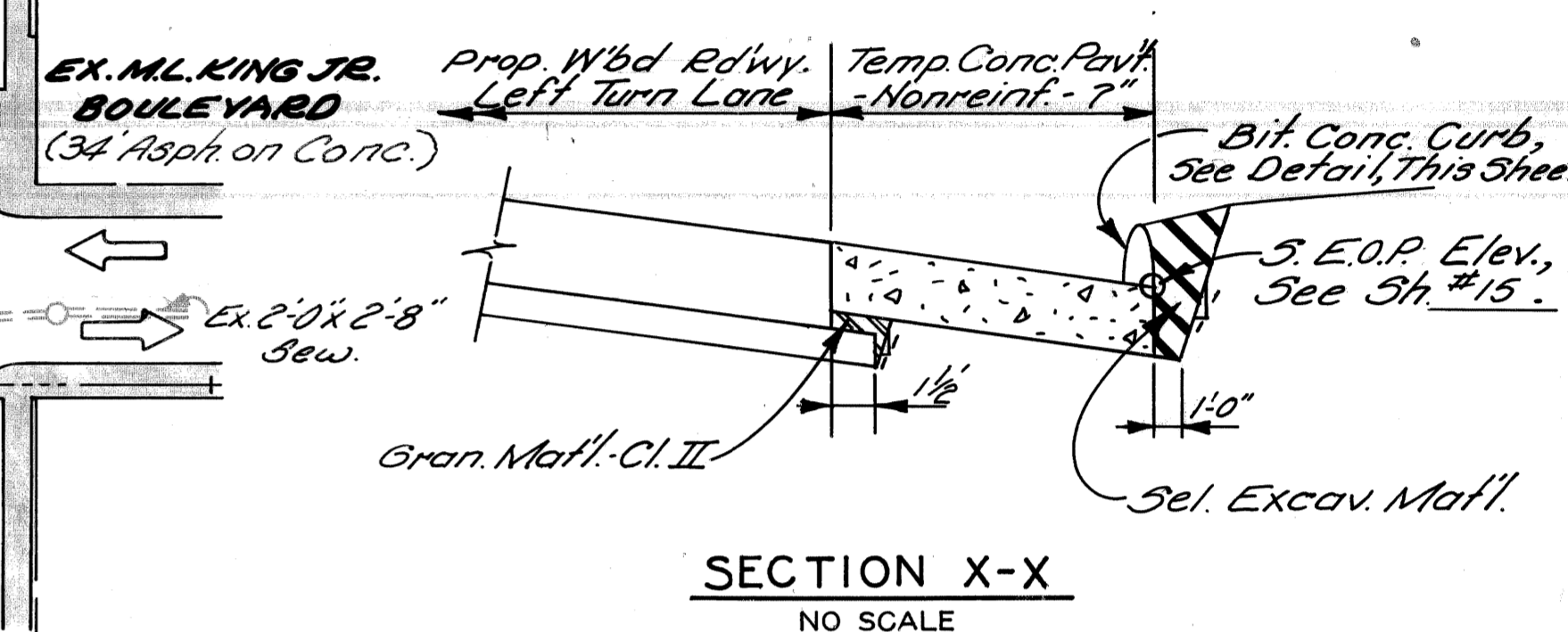
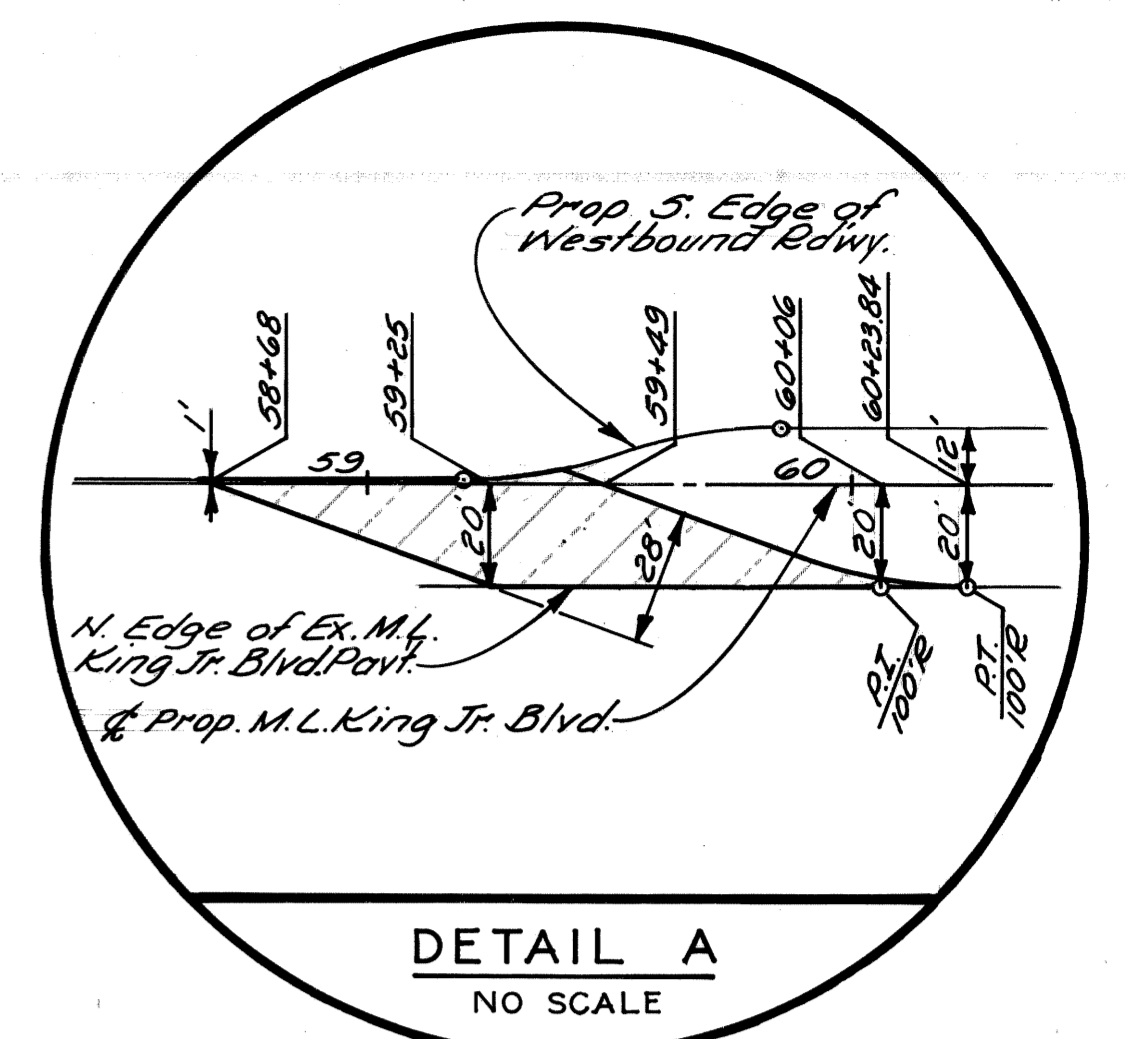
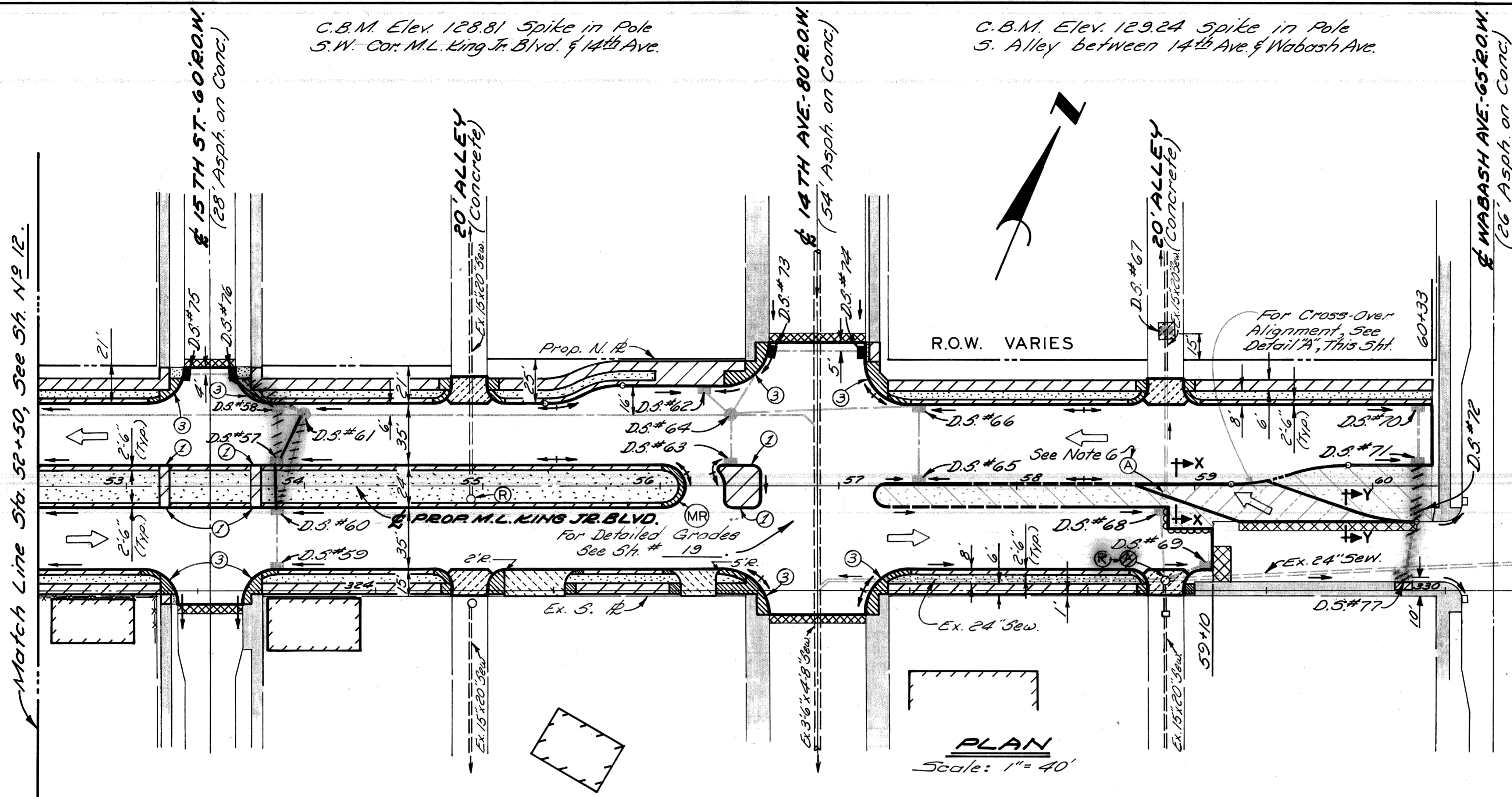


PROFILES
 Hor. 1"=40'
 Scale: Vert. 1"=4'

AS BUILT

40	41	42	43	44	45	46	47	48	49	50	51	52
APPROVED			CITY OF DETROIT			M.L. KING JR. BLVD. RECONSTRUCTION			SHEET 13 OF 77 SHEETS			
DRWN BY: <i>MAP</i> ENGINEER OF EXPRESSWAY: <i>Allen E. Pappas</i>			CITY ENGINEERING DIVISION-D.P.W.			WEST GRAND BLVD. TO LINCOLN AVE.			CONTRACT NO. 19151A			
CK'D BY: W.B.			PROFILES STA. 40+50 TO STA. 52+50			DATE 9-81						

4/83 Eliminate Bus Bay and its Profile W. of 18th R/W
 3/83 Lower Top of DS#30 W.B./ROR



DRAINAGE STRUCTURE TABLE

DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT.	WITH TRAP (T)	SEWER LENGTH & TYPE LEAVING (D.S.)			SEWER TAP SIZE (IN.)	TRENCH DETAIL (L.F.)		COMMENTS
			LENGTH (FT.)	SIZE (IN.)	CLASS C-76-		8	9	
57	C.B. "A"		30	12	III		30		
58	C.B. "B"	T	9	12	III		9		
59	C.B. "A"		30	12	III		30		
60	C.B. "B"	T	50	12	III	22	28		
61	M.H. Drop		240	24	IV		240	Manhole Has 18" Cushion	
62	C.B. "B"	T	15	12	III		15		
63	C.B. "B"	T	22	12	III		22		
64	M.H. Drop		48	24	IV	24	48	Manhole Has 24" Cushion, Inlet to Ex. 3'6" x 4'8" Sew. = 113.95	
65	C.B. "A"		40	12	III		40		
66	C.B. "B"	T	99	12	III		99		
67	18"x12"		12	12	III			See Detail C4399, Sh. N ^o 28.	
68	C.B. "B"	T	36	12	III	12	36		
69	C.B. "B"	T	20	12	III	12	20	*	
70	C.B. "A"		30	12	III		30		
71	C.B. "B"		21	12	III		21		
72	C.B. "B"	T	26	12	III	12	26	Build to Future Pavt. Elev. (126.20) and Fit with 'City of Detroit Standard Flat Catch Basin Grate-Type A Solid Cover' Available From East Jordan Iron works. Cover with Fill and Cereal Rye Seeding as shown on plan.	
73	C.B. "B"	T	37	12	III		37		
74	C.B. "A"		49	12	III		49		
75	C.B. "A"		23	12	III		23		
76	C.B. "B"	T	38	12	III	28	10		
77	M.H. Drop		5	12	III	12	5	Set Top of Structure at future berm elev. of 127.0'. CAUTION to Gas.	

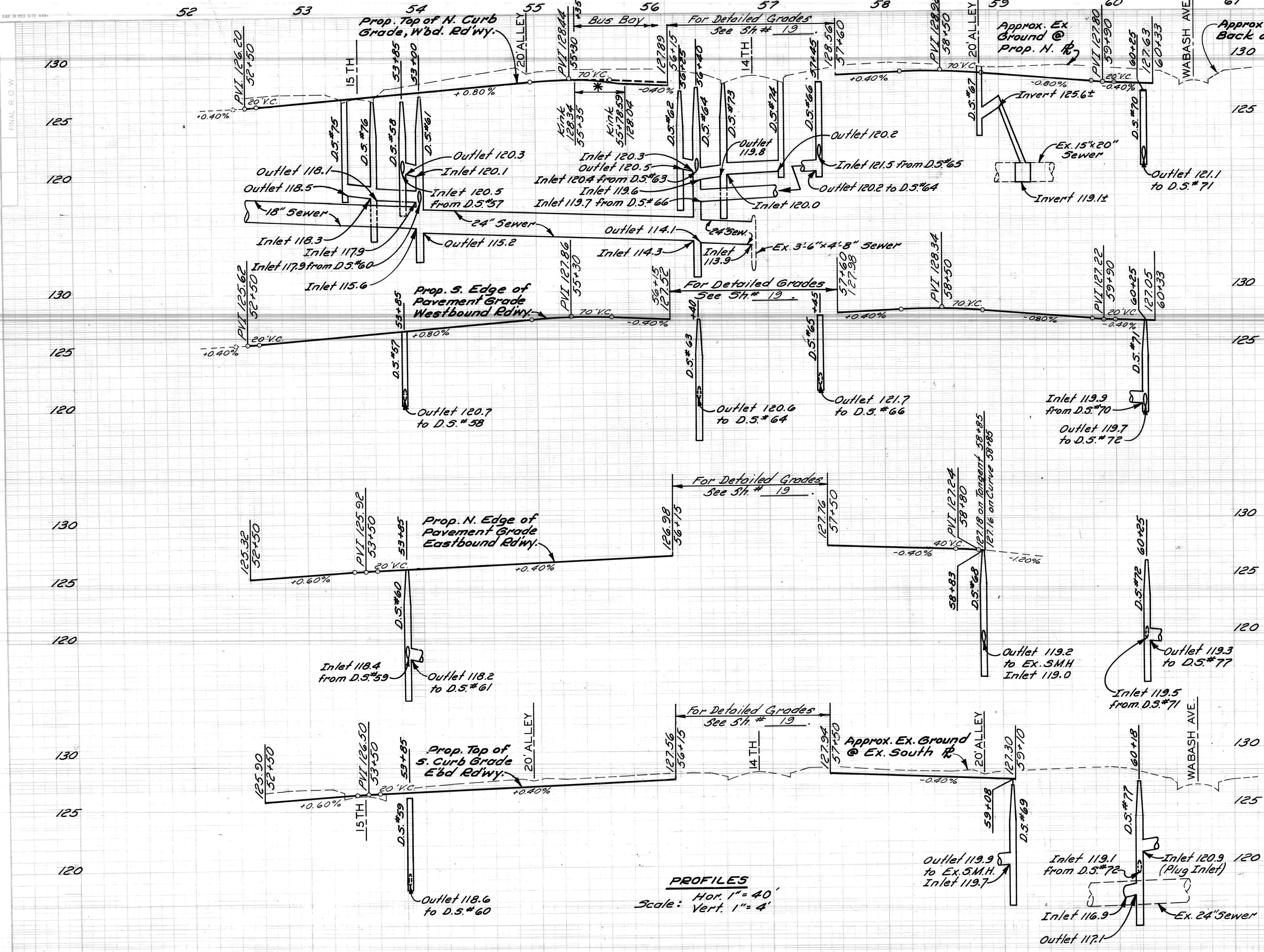
SURFACING LEGEND

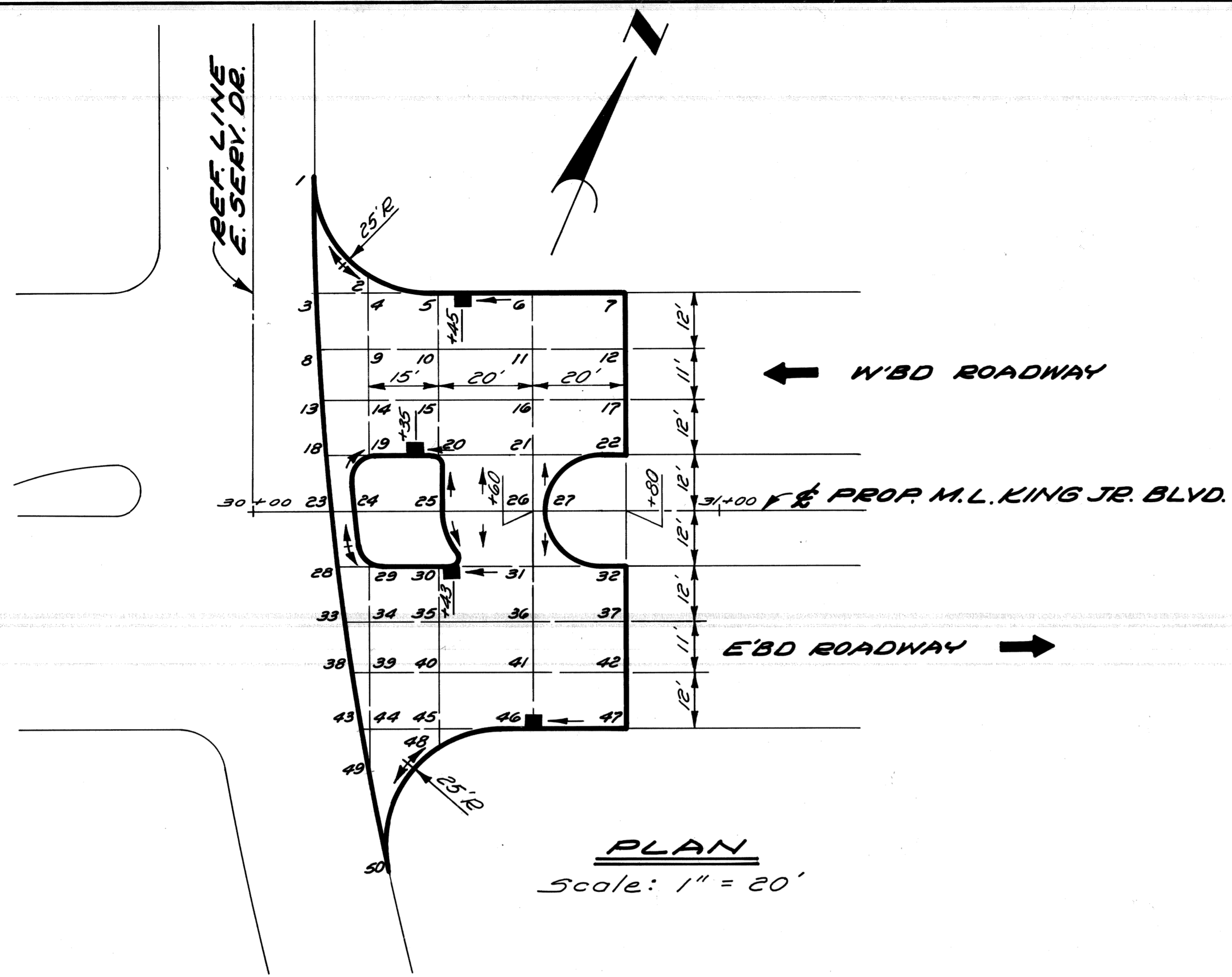
- Concrete Pavement - (Reinforced) - 9" Uniform with Integral Curb
- Concrete Pavt. - Nonreinforced - 9"
- Concrete Pavt. - Nonreinforced - 8"
- Temp. Concrete Pavt. - Nonreinf. - 7" with Bituminous Concrete Curb
- 4" Concrete Sidewalk
- 6" Concrete Sidewalk (Ramp areas are paid for separately)
- Exist. Sidewalk or Dr'wy. to Remain
- Exist. Building to Remain
- Exist. Fence to Remain
- Removing Bitum. Surf. & Resurf. (Bituminous Approaches)
- Class "A" Sodding with 3" Topsoil
- Cereal Rye Seeding
- Drainage Flow Arrows
- Adjust Drainage Structure Covers
- Reconstructing Drainage Structures
- Sidewalk Ramps, Types 1 & 3
- Mower Ramp (Incidental to Contract Cost, For Details See Sh. #28.)
- Conc. Curb - Det. "Y" (Concrete Grade X)
- Bitum. Conc. Curb (See Detail, This Sheet)

* Corbel may have to be offset to clear exist. 6" gas main. AS BUILT

- NOTES:**
- For Alignment, See Sh. #5.
 - For Typical Cross-Section, See Sh. #2.
 - For Construction Sequence, See Sh. #6.
 - For Driveway Details, See Sh. #29.
 - For Utility Legend & Symbols, See Sh. #29.
 - Adjust and fit with catch basin cover to drain detour pavement.
 - For Landscaping, See Sh. #23, thru #25.

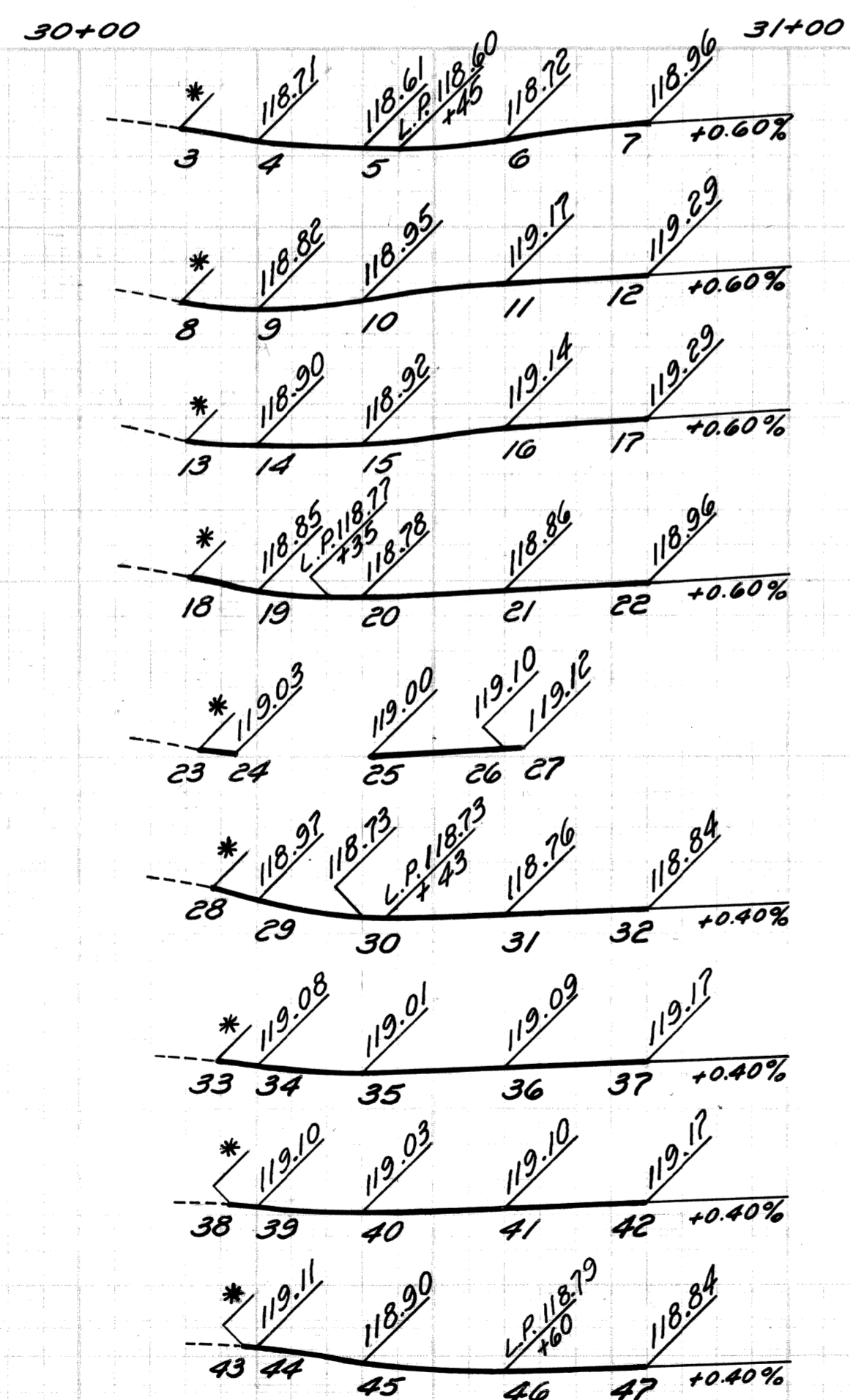
REFERENCE DRAWINGS DESIGNED BY DRAWN BY <i>WAB</i> TRACED BY CHECKED BY <i>W.B.</i>		APPROVED: ENGINEER OF STREETS HIGHWAY ENGINEER		CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAUS OF STREETS AND HIGHWAYS		M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. PLAN STA. 52+50 TO WABASH AVE.		SHEET 14 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81	
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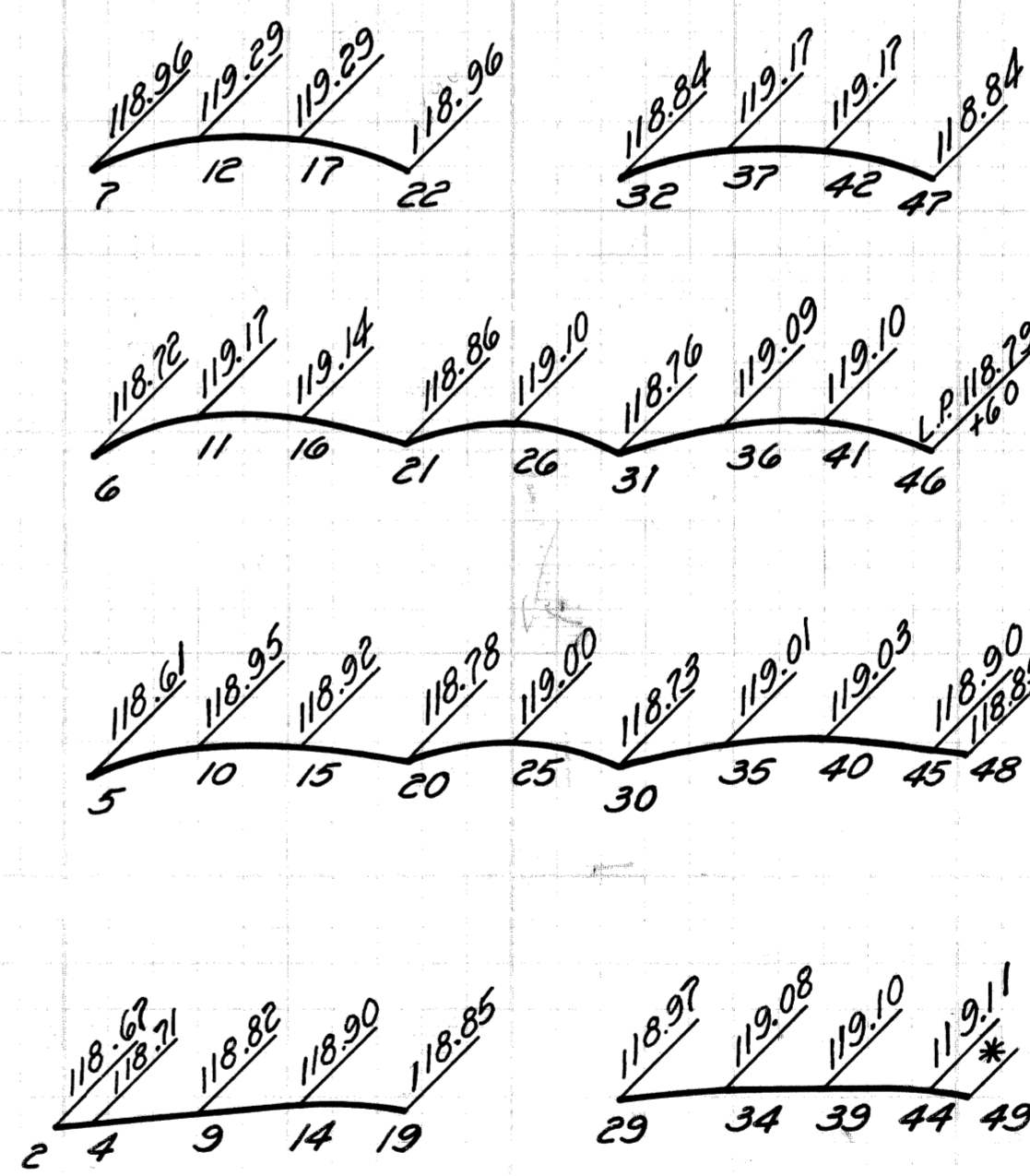


PLAN
Scale: 1" = 20'

AS BUILT

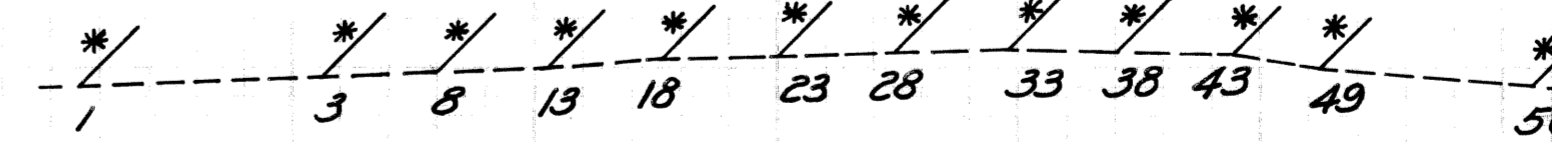


* Meet Existing Pavement



PROFILES & CROSS-SECTIONS

Scale: Hor. 1" = 20'
Vert. 1" = 2'

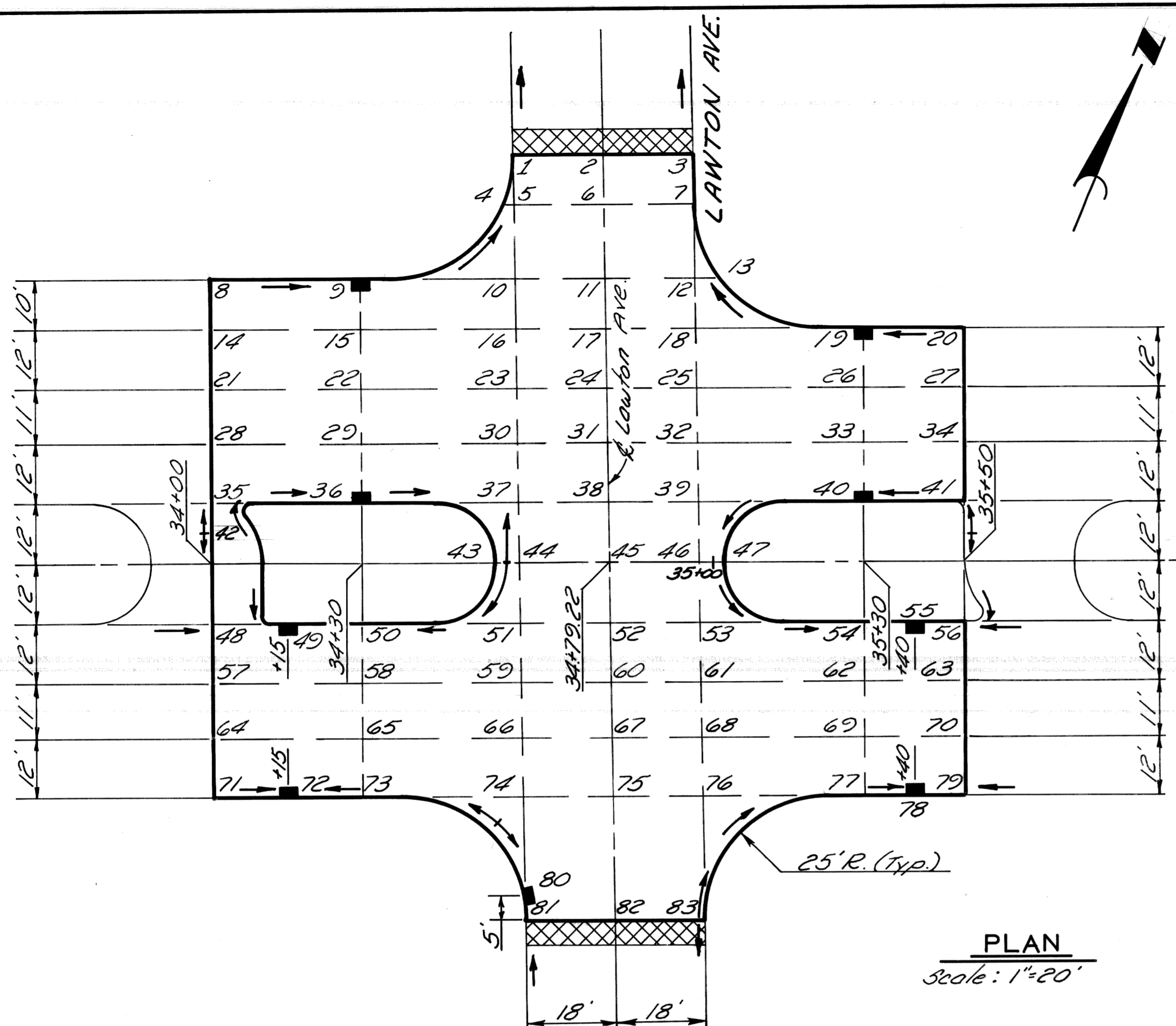


DESCRIPTION	DRN	CK'D	AP'D	DATE	BY	CHECKED BY	APPROVED:
PLAN					MAP		
GRADE					MAP		
ESTIMATE							
FINAL					W.B.		

CITY OF DETROIT
CITY ENGINEERING DIVISION - D.R.W.
BUREAUS OF STREETS AND HIGHWAYS

M.L. KING JR. BLVD. RECONSTRUCTION
WEST GRAND BLVD. TO LINCOLN AVE.
DETAILED GRADES FOR
M.L. KING JR. BLVD. AT JEFFRIES E. SERV. DR.

INDEX NO.	SHEET 16 OF 77 SHEETS
TOP NO.	CONTRACT NO. 19151A
INDEXED	ASSIGNMENT NO.
	DATE 9-81

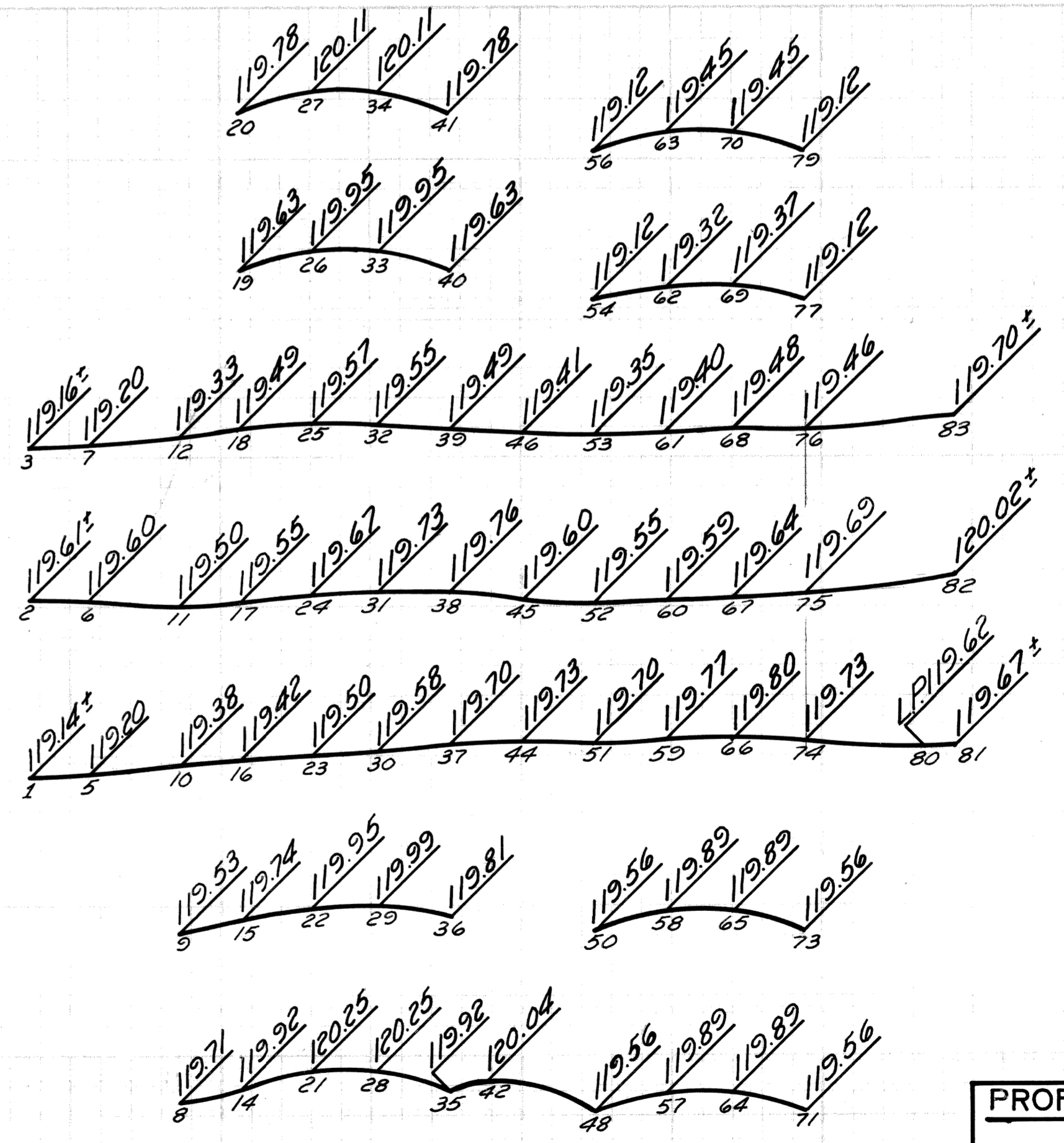
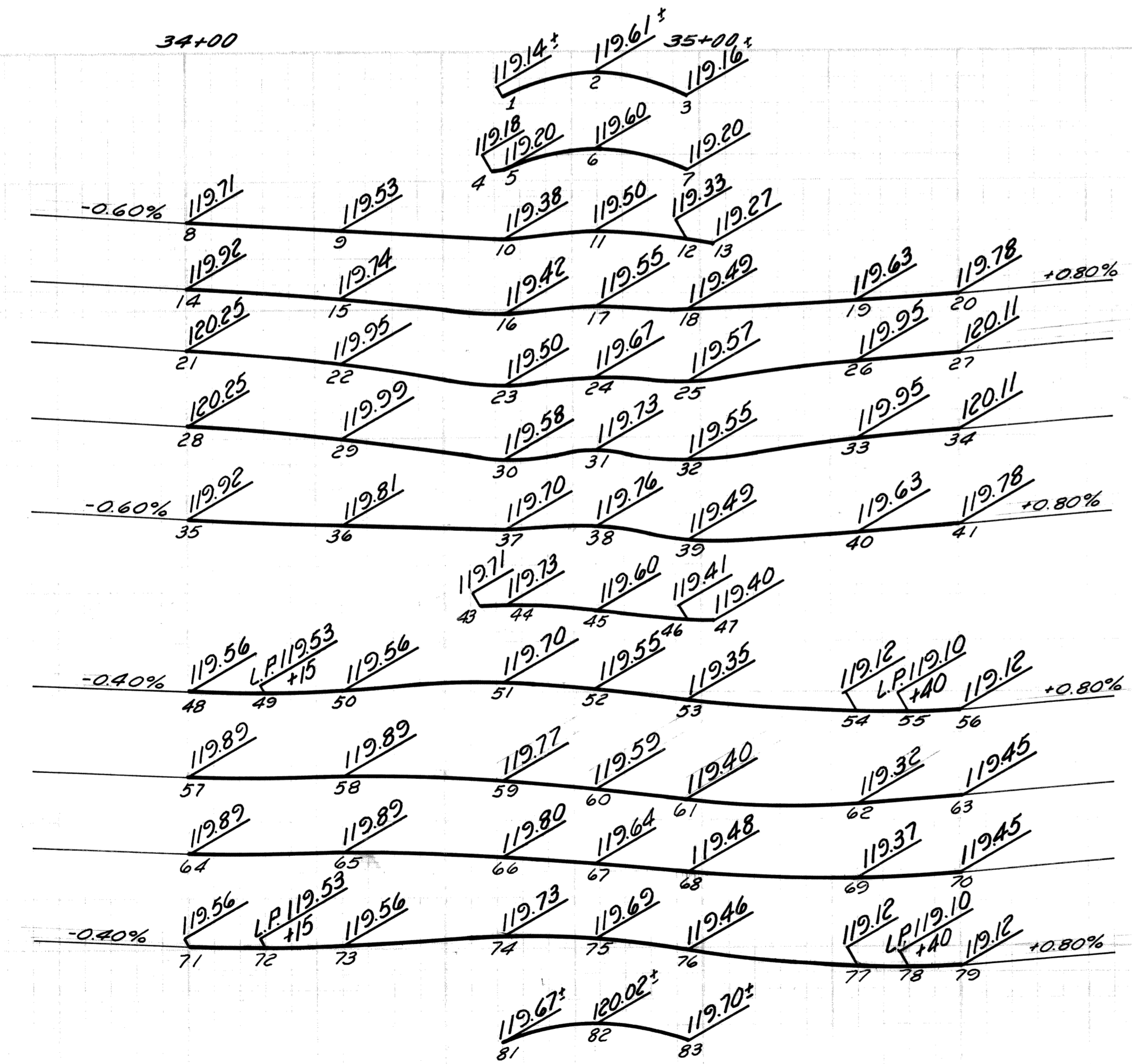


Westbound Roadway
 ← PROP. M.L. KING JR. BLVD.
 Eastbound Roadway
 →

LEGEND

- Strip & Resurface
- Prop. Catch Basin
- Drainage Arrow

AS BUILT



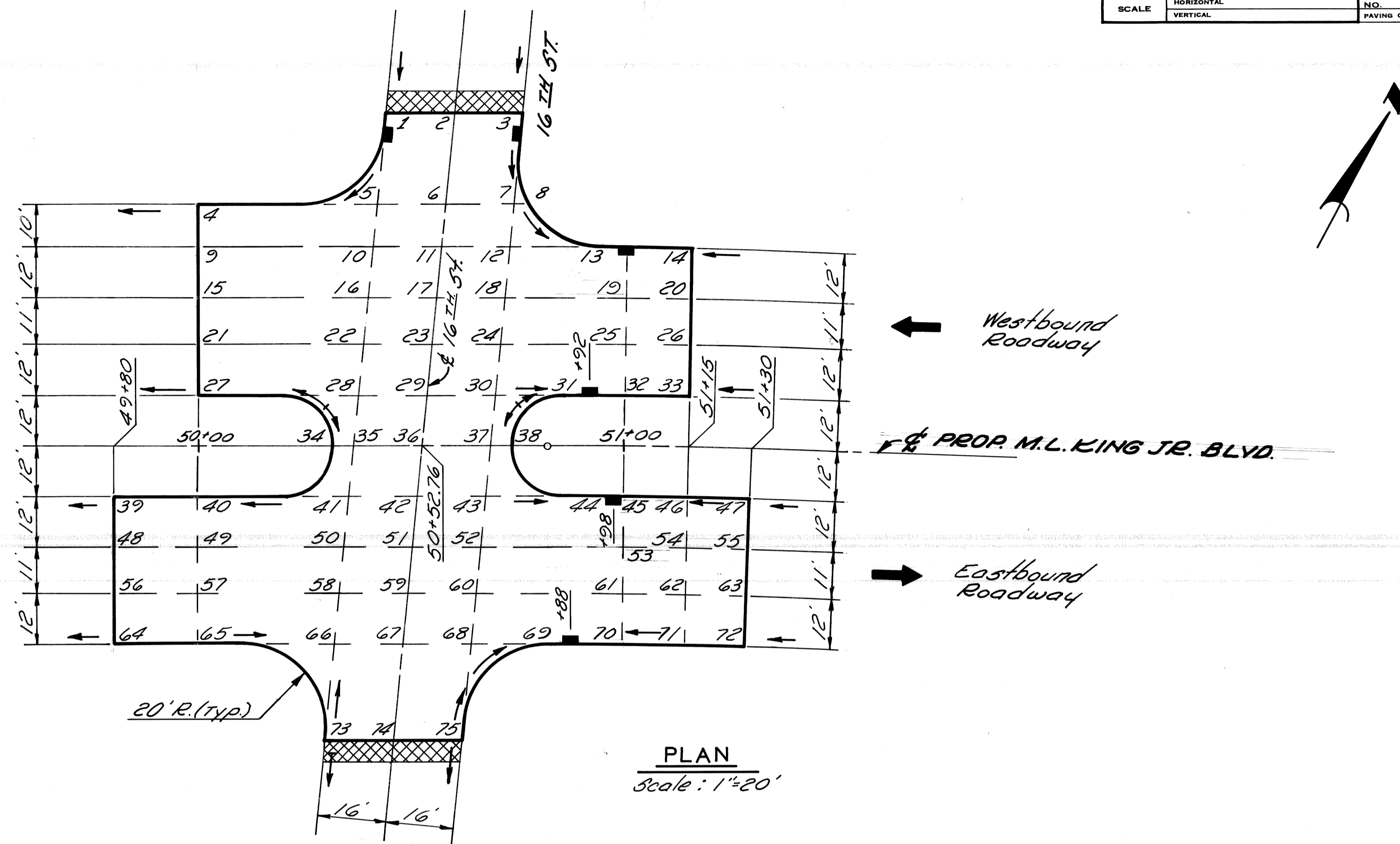
PROFILE & CROSS-SECTIONS
 SCALE: HOR. 1"=20'
 VERT. 1"=2'

DESCRIPTION	DATE	BY	CHECKED BY	APPROVED:
PLAN		M.S.		
GRADE		M.S.		
ESTIMATE				
FINAL		W.B.		

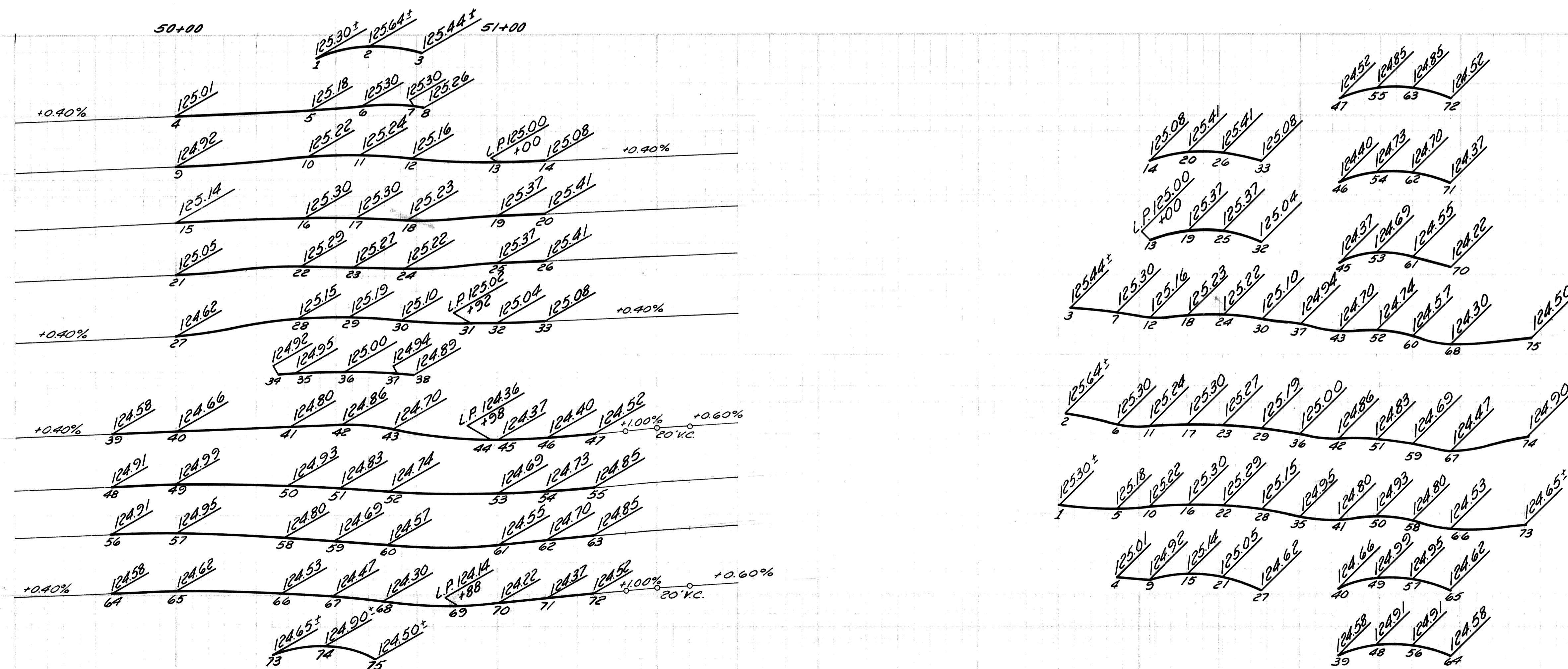
CITY OF DETROIT
 CITY ENGINEERING DIVISION-D.P.W.
 BUREAUS OF STREETS AND HIGHWAYS

M.L. KING JR. BLVD. RECONSTRUCTION
 WEST GRAND BLVD. TO LINCOLN AVE.
 DETAILED GRADES FOR
 M.L. KING JR. BLVD. AT LAWTON AVE.

INDEX NO.	SHEET 17 OF 77 SHEETS
CONTRACT NO.	19151A
ASSIGNMENT NO.	
DATE	9-81



AS BUILT

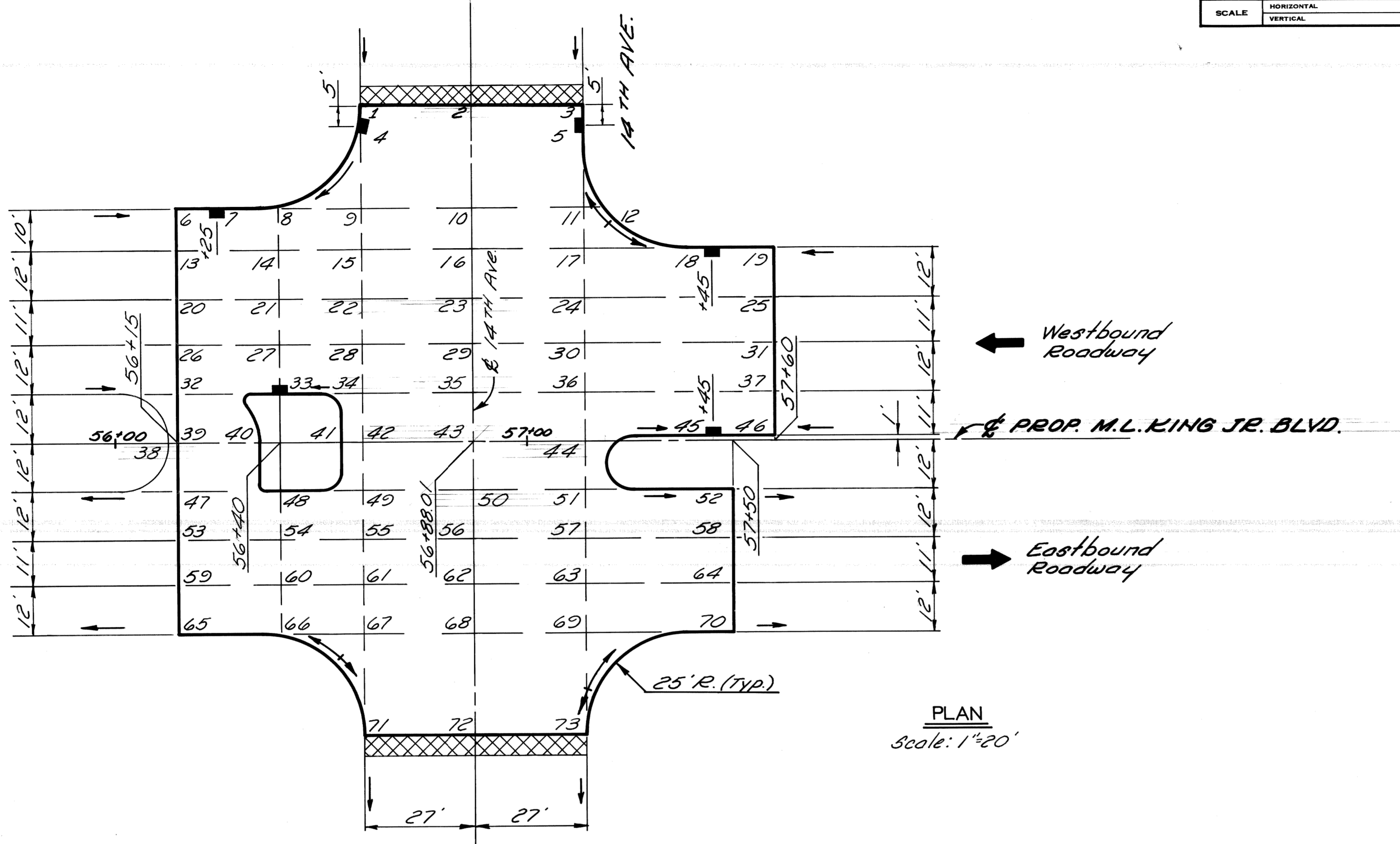


DESCRIPTION	DRN	CK'D	AP'D	DATE	BY	CHECKED BY	APPROVED:
PLAN					M.S.		
GRADE					M.S.		
ESTIMATE							
FINAL					W.B.		

CITY OF DETROIT
 CITY ENGINEERING DIVISION - D.P.W.
 BUREAUS OF STREETS AND HIGHWAYS

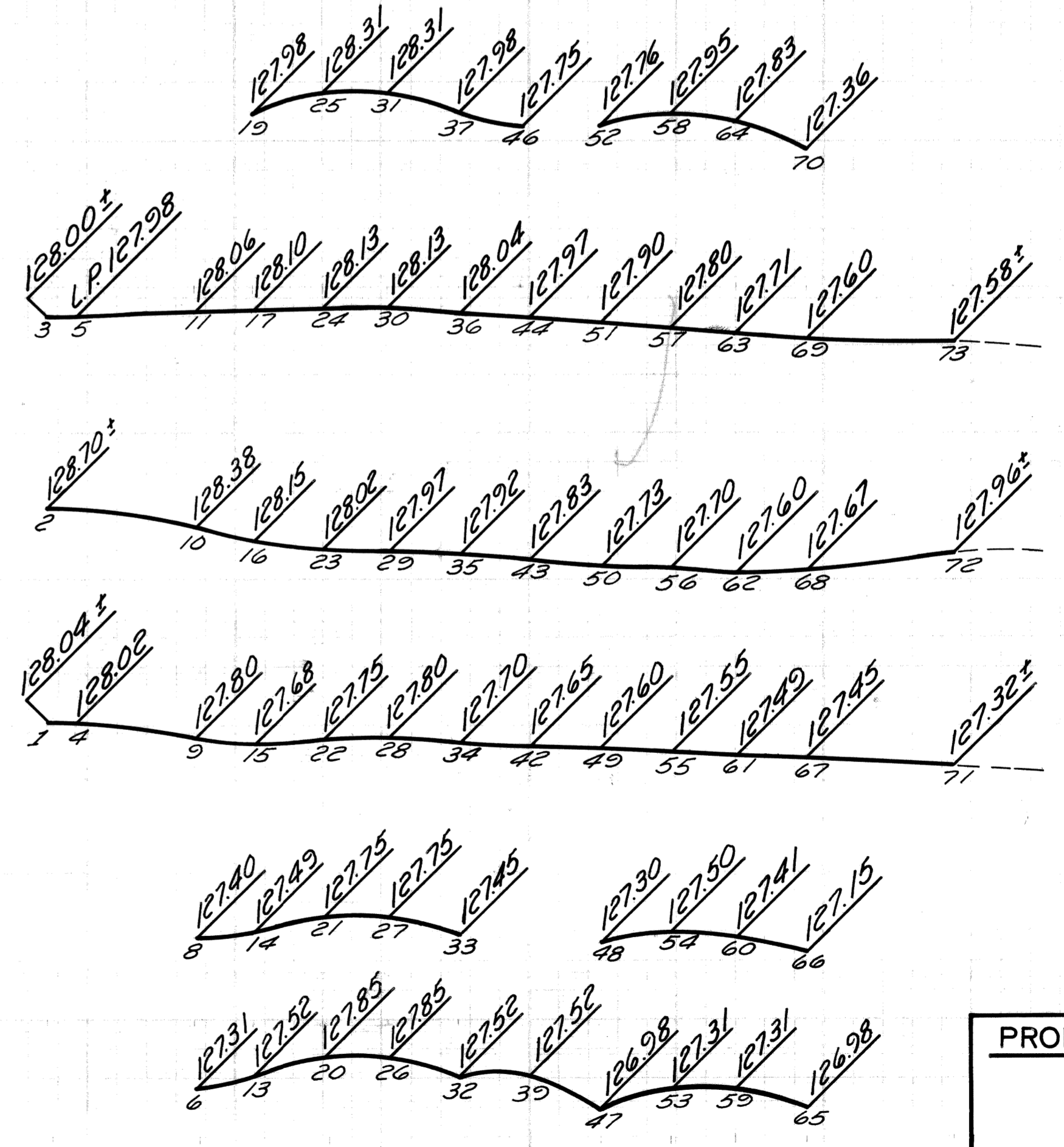
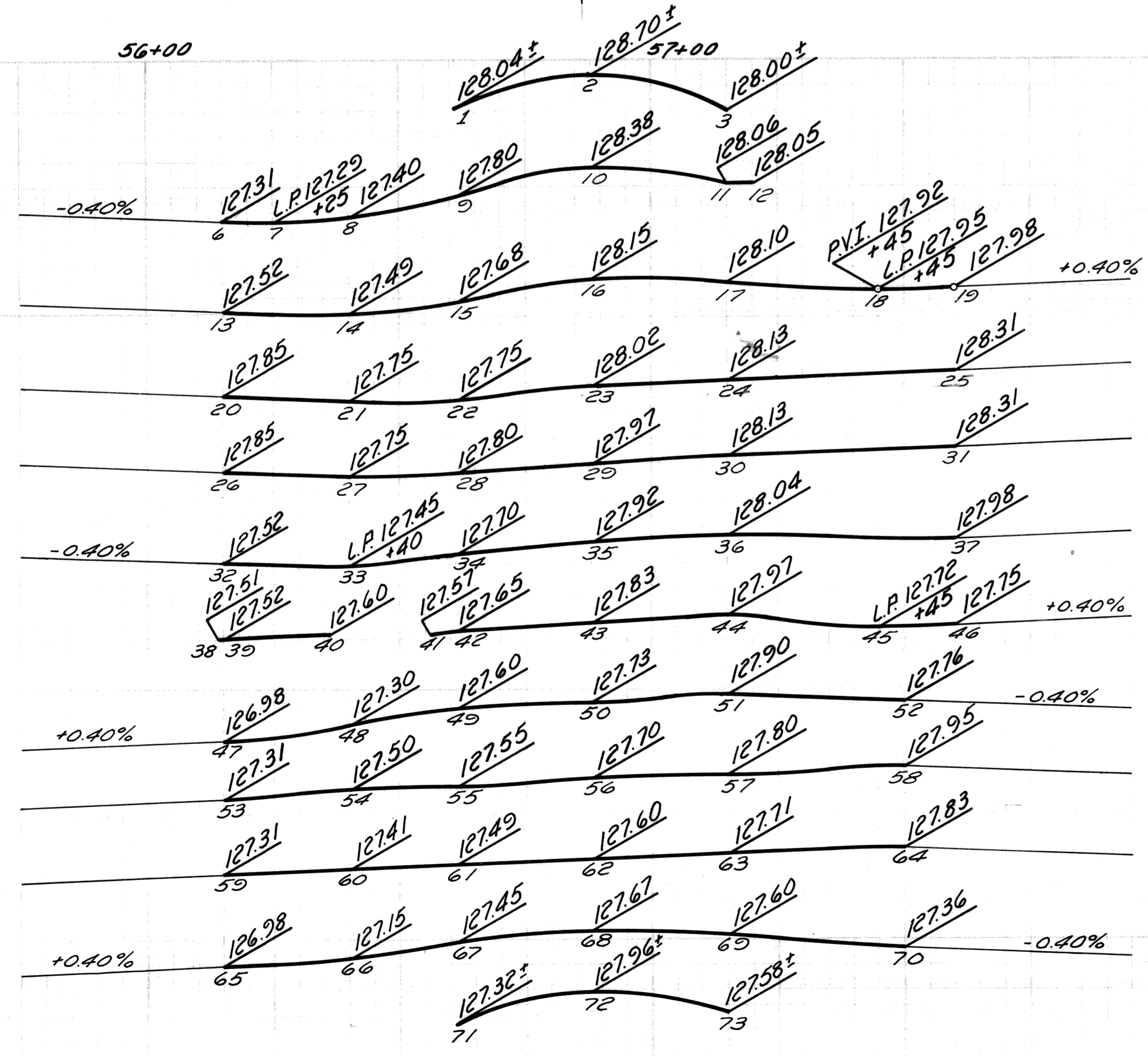
M.L.KING JR. BLVD. RECONSTRUCTION
 WEST GRAND BLVD. TO LINCOLN AVE.
 DETAILED GRADES FOR
 M.L.KING JR. BLVD. AT 16 TH ST.

INDEX NO.	SHEET 18 OF 77 SHEETS
CONTRACT NO.	19151 A
ASSIGNMENT NO.	
DATE	9-81



PLAN
Scale: 1"=20'

AS BUILT



PROFILES & CROSS-SECTIONS
SCALE: HOR. 1"=20'
VERT. 1"=2'

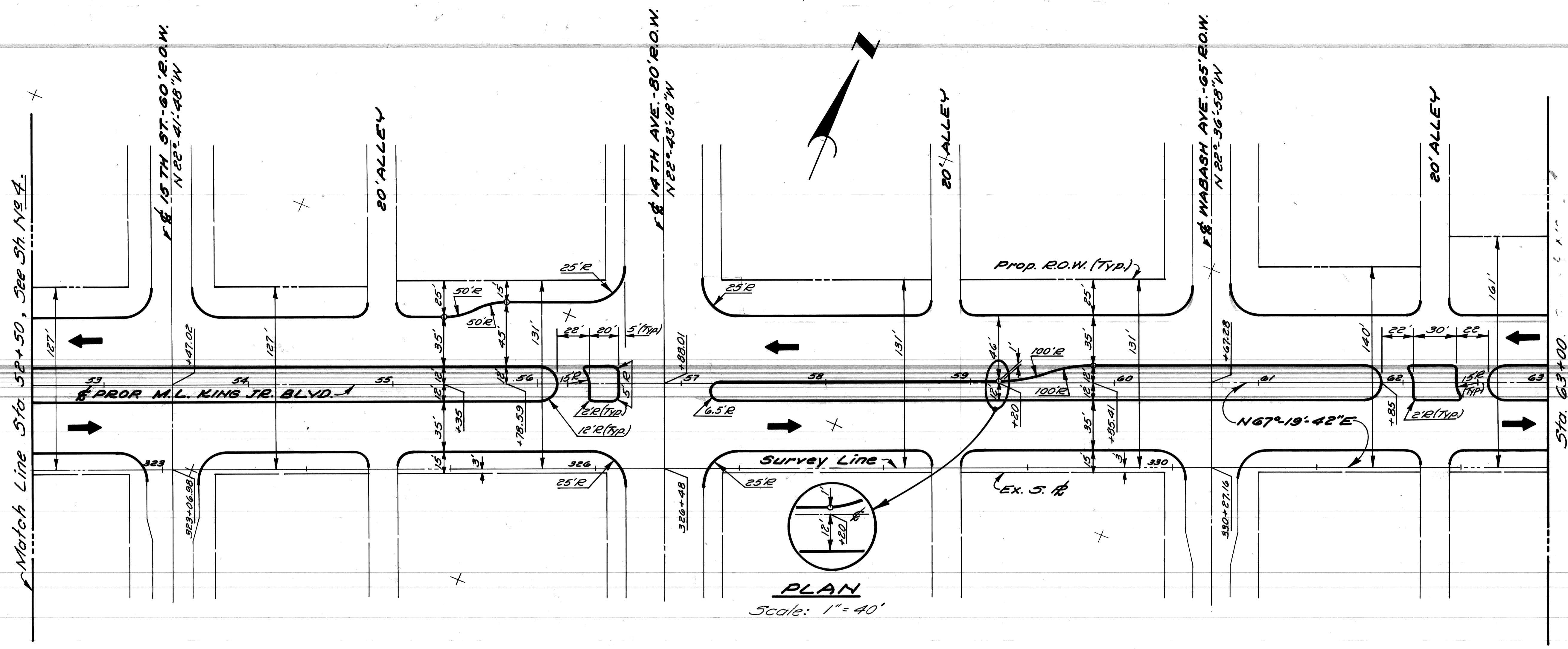
DESCRIPTION	DATE	BY	CHECKED BY	APPROVED:
PLAN		M.S.		
GRADE		M.S.		
ESTIMATE				
FINAL		W.B.		

CITY OF DETROIT
CITY ENGINEERING DIVISION-D.P.W.
BUREAU OF STREETS AND HIGHWAYS

ENGINEER OF STREETS
Allen E. Prager
HIGHWAY ENGINEER

M.L. KING JR. BLVD. RECONSTRUCTION
WEST GRAND BLVD. TO LINCOLN AVE.
DETAILED GRADES FOR
M.L. KING JR. BLVD. AT 14TH AVE.

INDEX NO.	SHEET 19 OF 77 SHEETS
CONTRACT NO.	19151A
ASSIGNMENT NO.	
DATE	9-81



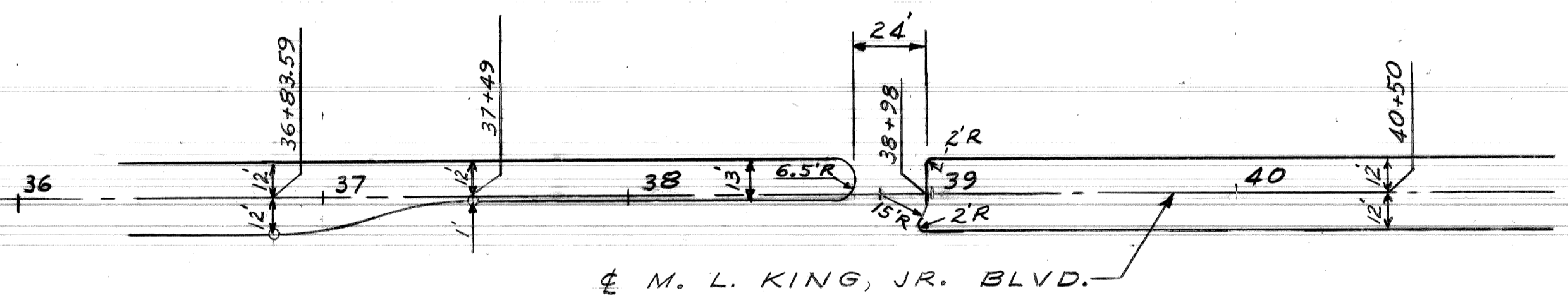
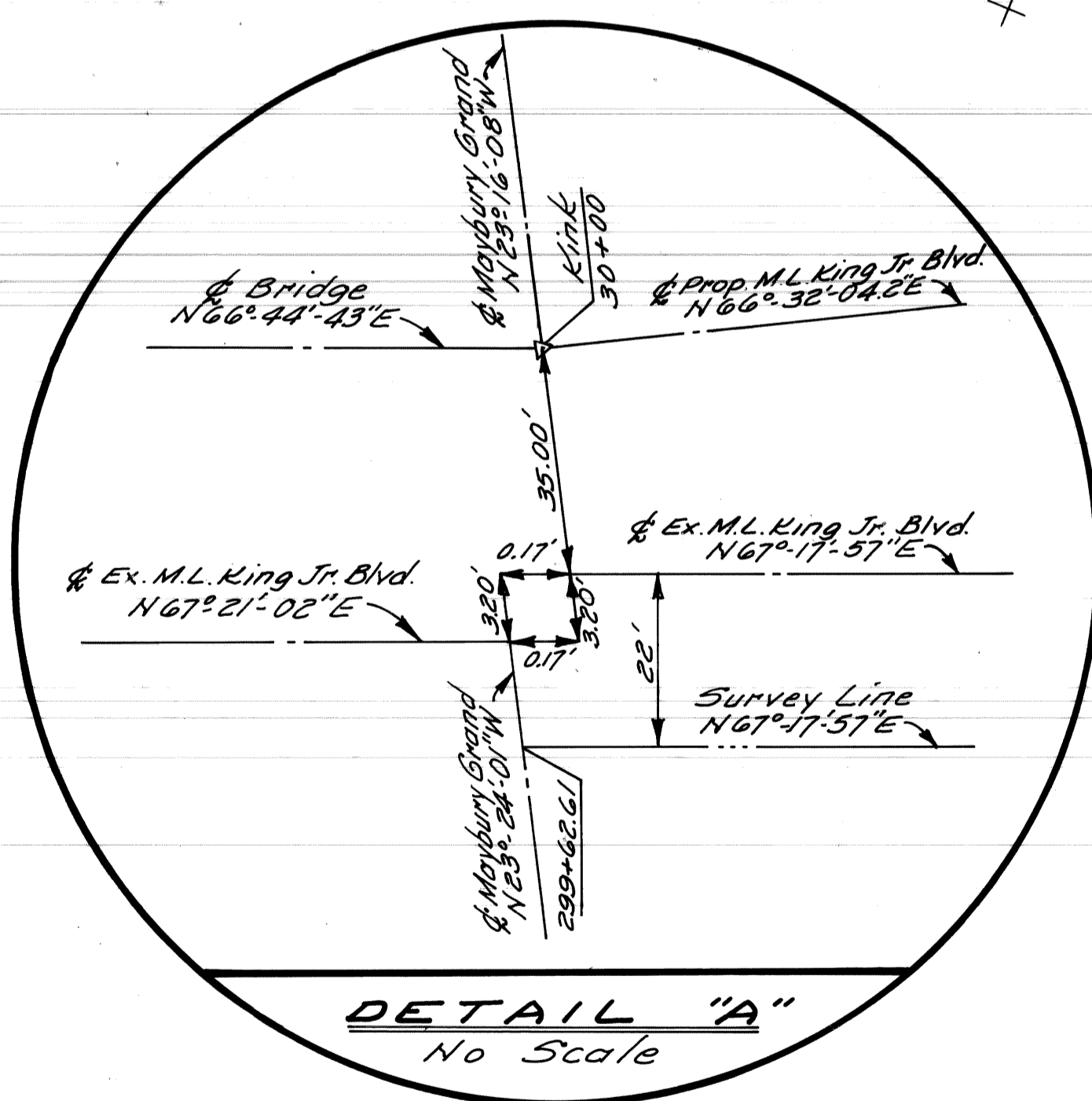
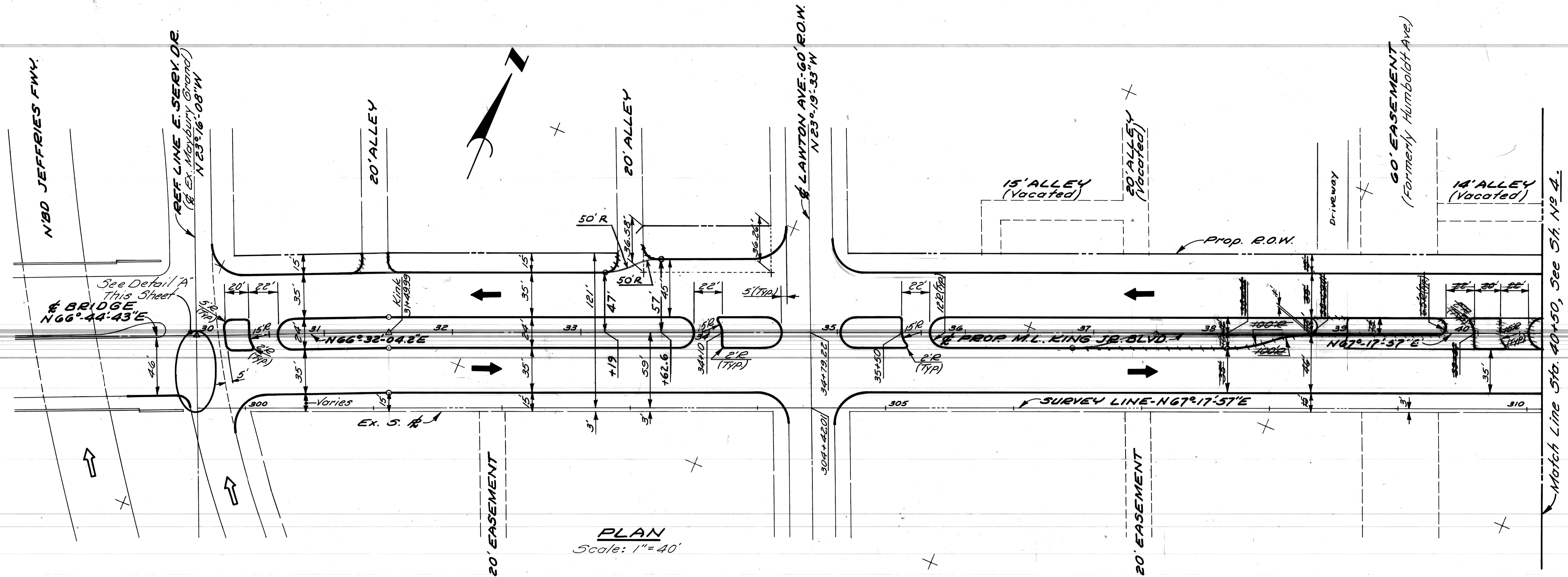
PLAN
Scale: 1" = 40'

NOTES:

1. All prop. street return radii shall be 20' unless otherwise noted.
2. All prop. alley return radii shall be 10' unless otherwise noted.

AS BUILT

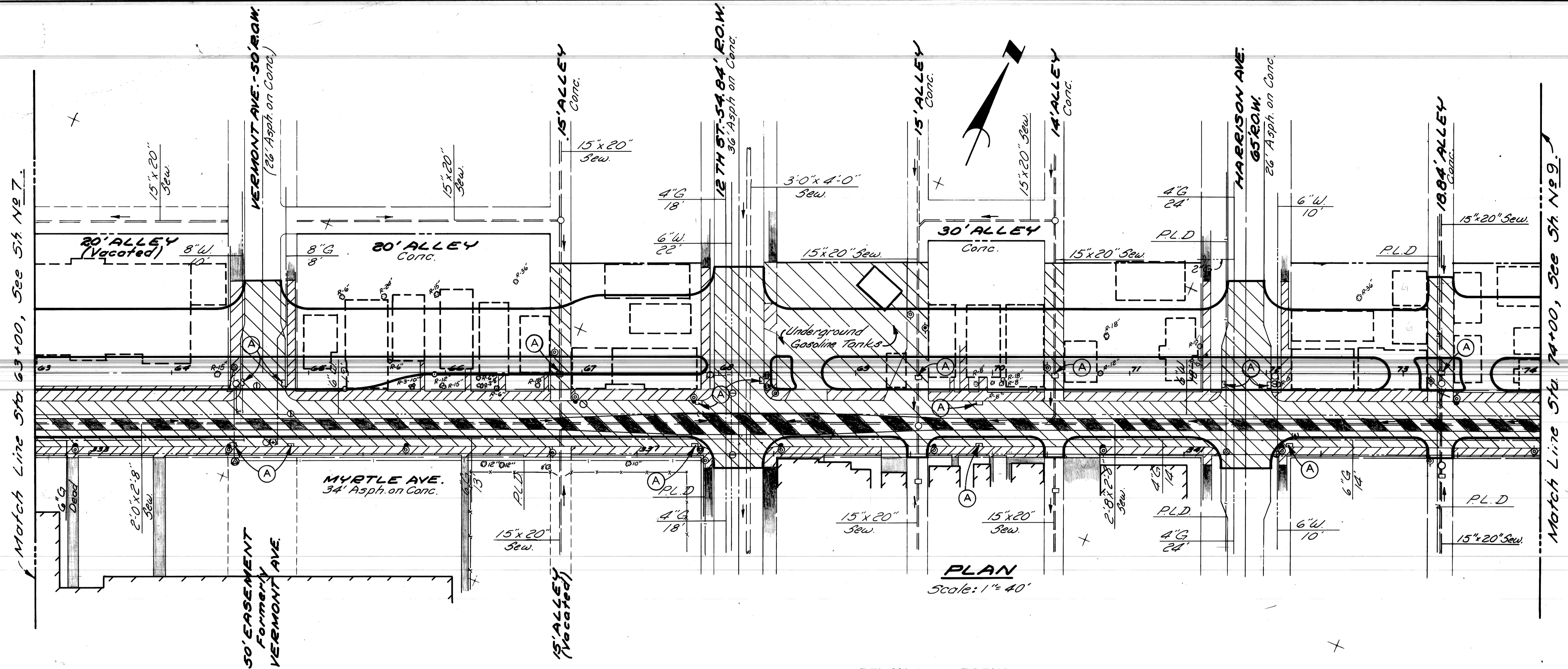
COORD	DESCRIPTION	DRN	CKD	APVD	DATE		
REVISIONS LOCATED BY COORDINATES ON SHEET						DESIGNED BY	APPROVED:
						DRAWN BY	ENGINEER OF STREETS
						TRACED BY	HIGHWAY ENGINEER
						CHECKED BY	
						CITY OF DETROIT CITY ENGINEERING DIVISION - D.P.W. BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. ALIGNMENT STA. 52 + 50 TO STA. 63 + 00
						SHEET 5 OF 77 SHEETS	CONTRACT NO. 19151A
						ASSIGNMENT NO.	DATE 9-81



- NOTES:**
1. All prop. street return radii shall be 25' unless otherwise noted.
 2. All prop. alley return radii shall be 10' unless otherwise noted.

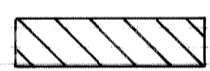


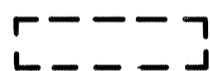

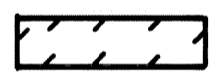


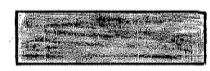
AS BUILT

REVISIONS LOCATED BY COORDINATES ON SHEET COORD. DESCRIPTION DRN. CHKD. APVD. DATE		DESIGNED BY DRAWN BY TRACED BY CHECKED BY	APPROVED: ENGINEER OF STREETS HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING DIVISION - D.R.W. BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. ALIGNMENT JEFFRIES FWY. TO STA. 40+50	SHEET 3 OF 77 SHEETS CONTRACT NO. 19151A ASSIGNMENT NO. DATE 9-81
C8-C11	Revise Median	RPWB	3/83	Allen E. Prager		
B3-B5	Eliminate Alley Returns					
	Revise Bus Bay					



PLAN
Scale: 1" = 40'

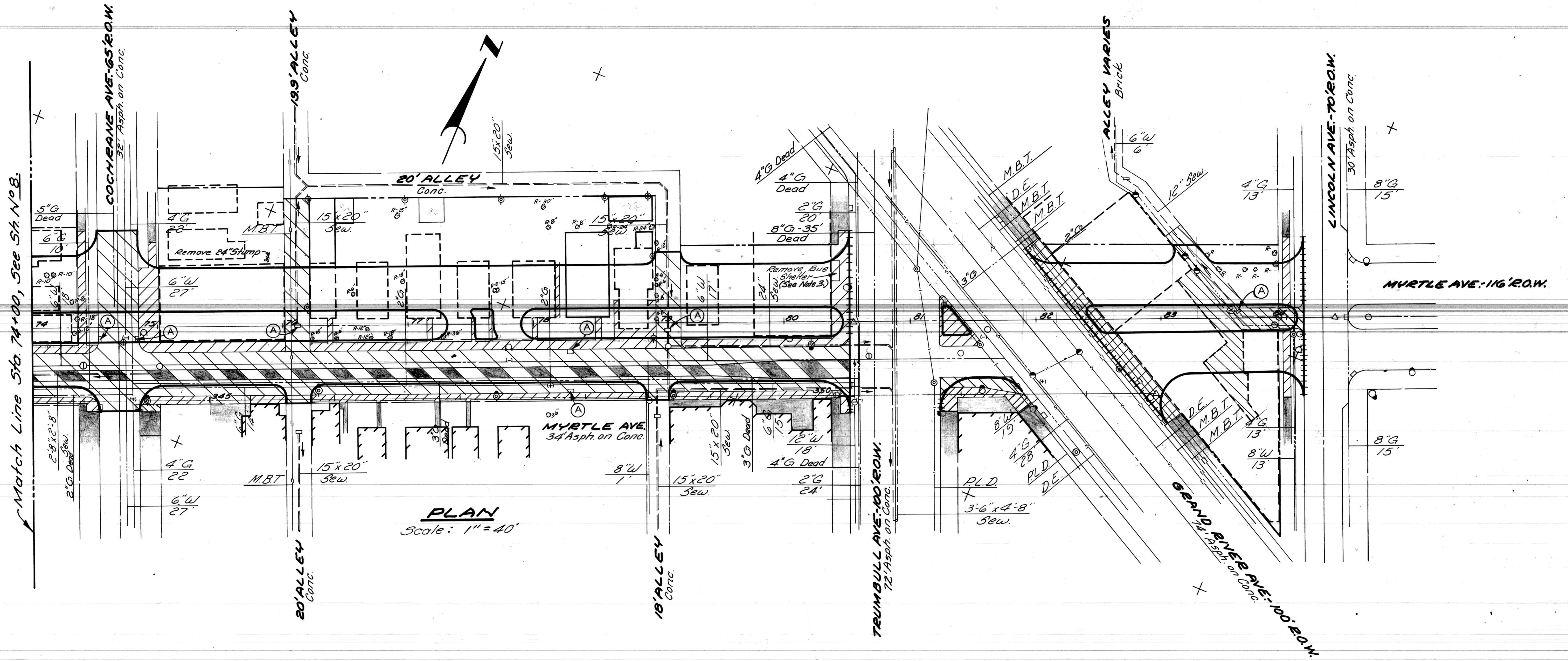
REMOVAL LEGEND

-  Removing Pavement
-  Removing Sidewalk
-  Removing Tree
-  Existing Basements
-  Existing Foundation
-  Existing Building to Remain
-  Abandoning Drainage Structures
-  Existing Sidewalk or Driveway to Remain
-  Removing Track Encasement

NOTES:
 1. For Utility Legend, See Sh # 27.
 2. Locations of exist. utility lines are based on the best available records & are not guaranteed for accuracy.

CALL MISS DIG
 48 HOURS PRIOR TO
 CONSTRUCTION 647-7344

COORD DESCRIPTION DRN CKD APVD DATE		REVISIONS LOCATED BY COORDINATES ON SHEET	DESIGNED BY DRAWN BY <i>M/S</i> TRACED BY CHECKED BY <i>WB</i>	APPROVED ENGINEER OF STREETS <i>Allen E. Rogus</i> HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. REMOVALS STA. 63 + 00 TO STA. 74 + 00	SHEET 8 OF 71 SHEETS CONTRACT NO. 17176A ASSIGNMENT NO. DATE AUG., 1984
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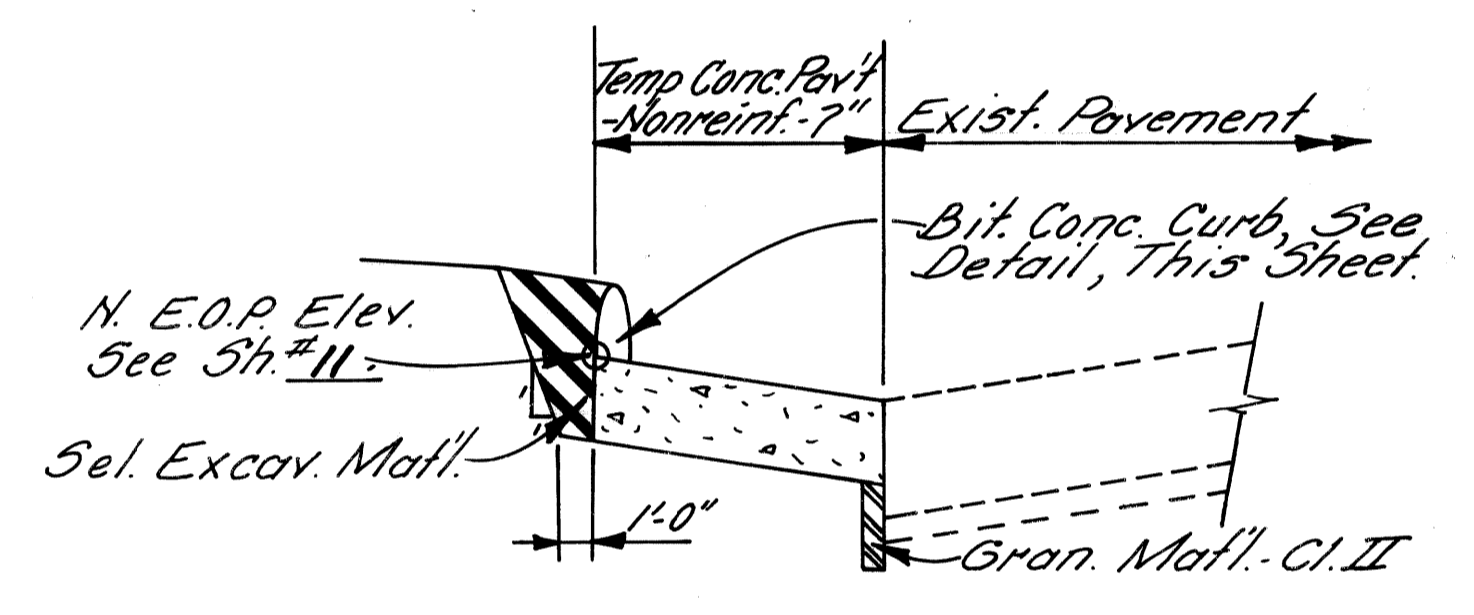
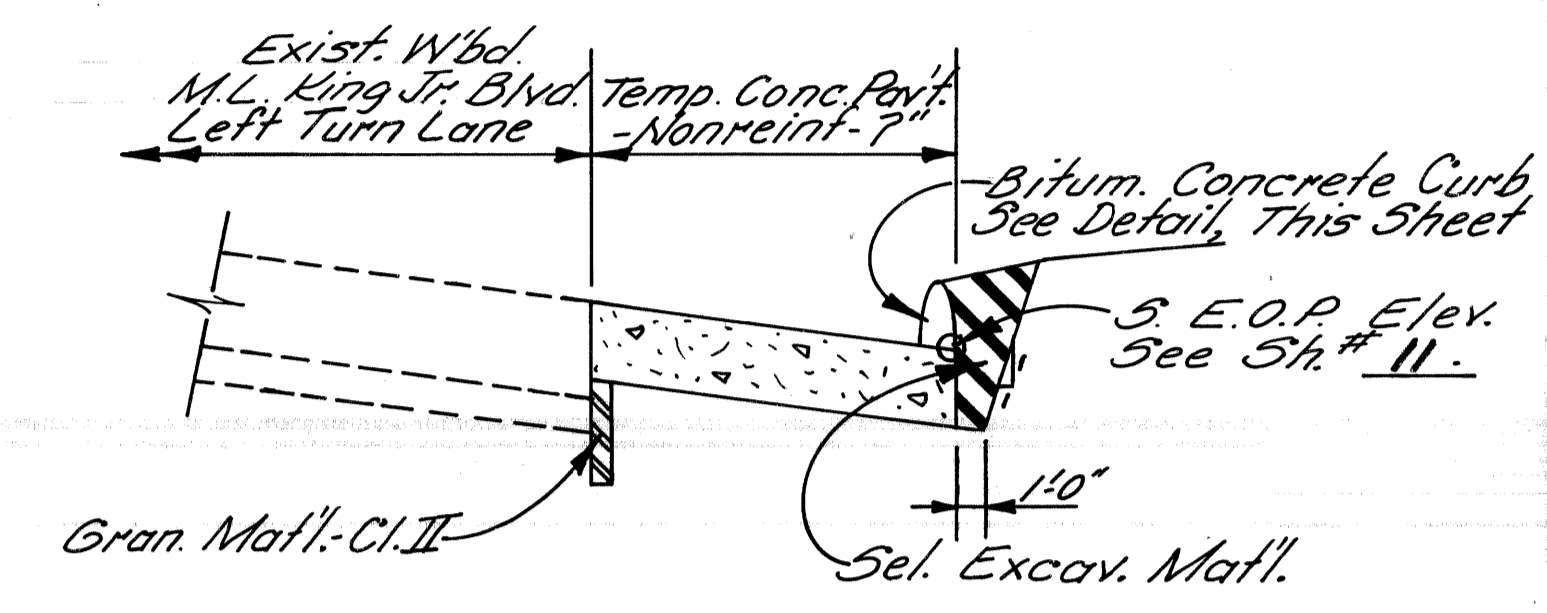
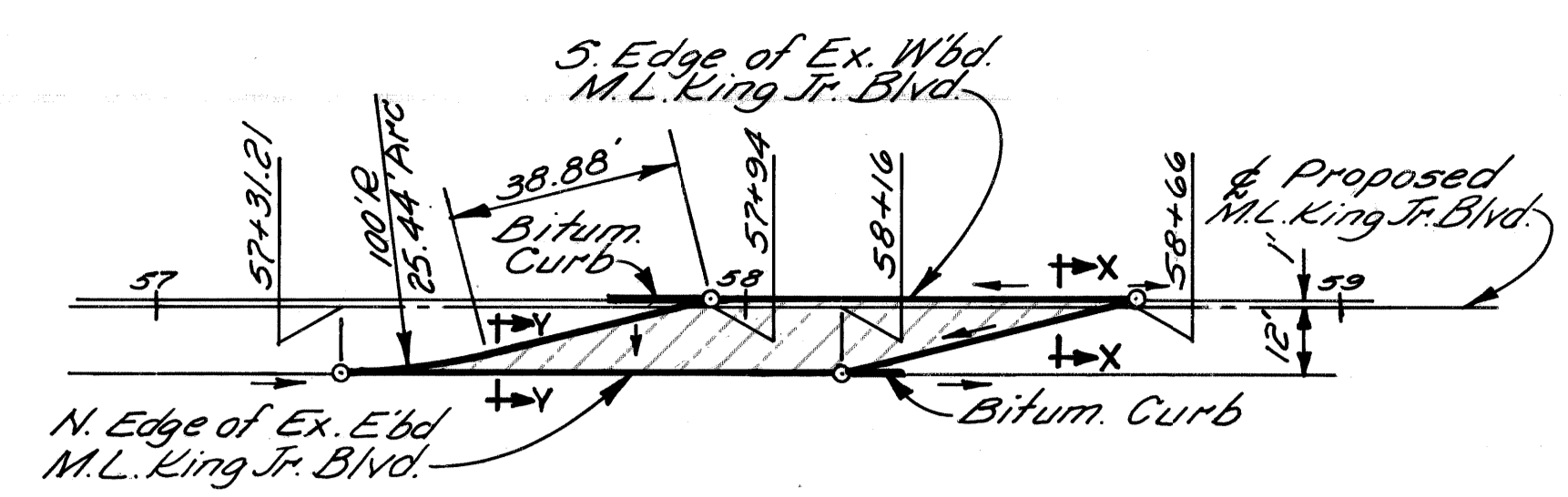
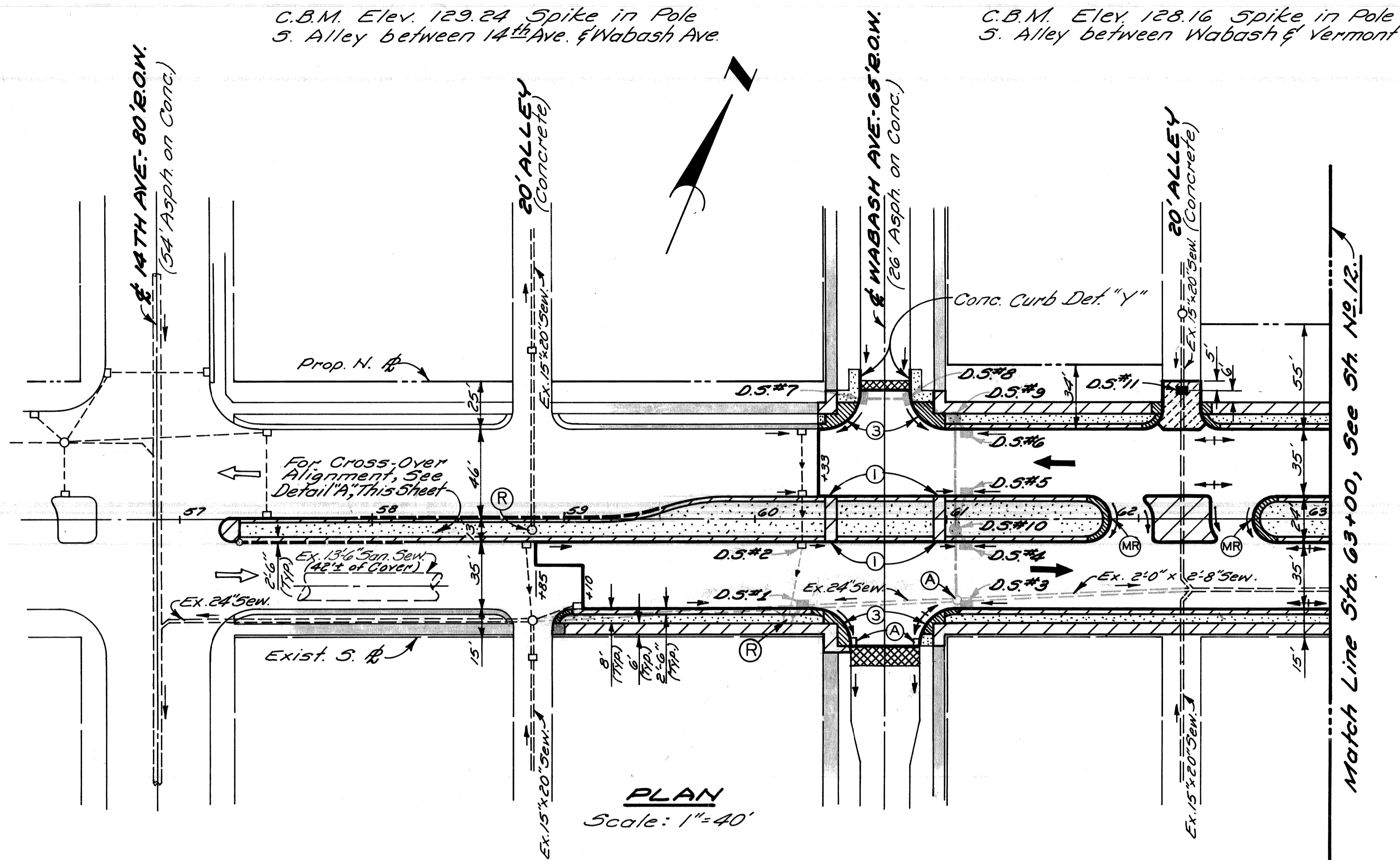
REMOVAL LEGEND

	Removing Pavement
	Removing Sidewalk
	R (size) Removing Tree
	Existing Basements
	Existing Foundation
	Existing Building to Remain
	Abandoning Drainage Structures
	Existing Sidewalk or Driveway to Remain
	Removing Track Encasement
	Removing Curb

NOTES:

1. For Utility Legend, See Sh. # 27.
2. Locations of exist utility lines are based on the best available records & are not guaranteed for accuracy.
3. To be removed by others. Call Mr. Cleveland Williams, (313) 833-7365.

<p>COORD. DESCRIPTION DRN CK'D AP'VD DATE</p> <p>REVISIONS LOCATED BY COORDINATES ON SHEET</p>				<p>DESIGNED BY</p> <p>DRAWN BY <i>MAS</i></p> <p>TRACED BY</p> <p>CHECKED BY <i>WB</i></p>	<p>APPROVED:</p> <p>ENGINEER OF STREETS</p> <p><i>Allen E. Rogers</i></p> <p>HIGHWAY ENGINEER</p>	<p>CITY OF DETROIT</p> <p>CITY ENGINEERING</p> <p>BUREAUS OF STREETS AND HIGHWAYS</p>	<p>M.L. KING JR. BLVD. RECONSTRUCTION</p> <p>WEST GRAND BLVD TO LINCOLN AVE.</p> <p>REMOVALS</p> <p>STA. 74 + 00 TO LINCOLN AVE.</p>	<p>SHEET 9 OF 71 SHEETS</p> <p>CONTRACT NO. 17176A</p> <p>ASSIGNMENT NO.</p> <p>DATE AUG., 1984</p>
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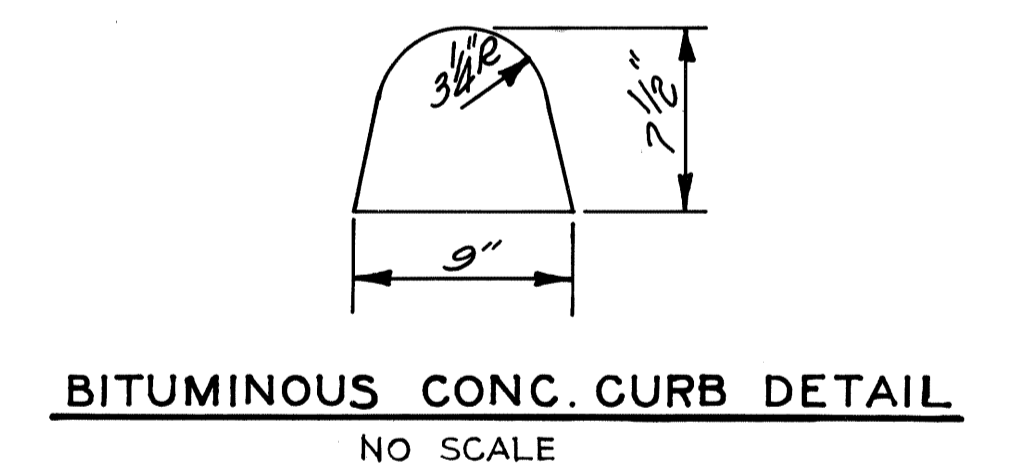


DRAINAGE STRUCTURE TABLE									
DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT.	WITH TRAP (T)	SEWER LENGTH & TYPE LEAVING (D.S.)			SEWER TAP SIZE (IN.)	TRENCH DETAIL (L.F.)		COMMENTS
			LENGTH (FT.)	SIZE (IN.)	CLASS C-76-		8	9	
1	C.B. "B"	T	2	12	III		2		
2	Ex. C.B. "B"	Use	Caution in exposing exist. structure.						Remove exist. solid cover. Replace with standard flat grate. Use C.B. "L" with half-trap if not enough room for C.B. "B".*
3	C.B. "B"	T	2	12	III	12	2		
4	C.B. "B"	T	2	12	III		2		
5	C.B. "B"	T	13	12	III		13		
6	C.B. "B"	T	2	12	III		2		
7	C.B. "A"		20	12	III		20		
8	C.B. "B"	T	20	12	III		20		
9	M.H. Std.		55	12	III		19	36	
10	M.H. Drop		34	12	III	12		34	
11	Std. 18" x 18" Spec. Y.C.B.		15	12	III	12			Ex. 15' x 20" Sewer approx. 9' deep See Detail C-4399, Sh. # 20.

*Incidental to Drainage Items.

SURFACING LEGEND

- Concrete Pavement-(Reinforced)-9" Uniform with Integral Curb.
- Concrete Pavt.-Nonreinforced-9"
- 4" Concrete Sidewalk.
- 6" Concrete Sidewalk-(Ramp areas are paid for separately.)
- Exist. Sidw. or Dr'wy. to Remain
- Exist. Building to Remain.
- Concrete Curb-Detail "X"
- Temp. Conc. Pavt.-Nonreinf.-7" with Bituminous Conc. Curb
- Removing Bitum. Surf. & Resurf. (Bituminous Approaches)
- Class "A" Sodding with 3" Topsoil.
- Drainage Flow Arrows.
- Adjust Drainage Structure Covers
- Reconstructing Drainage Structures
- Sidewalk Ramps, Types 1 & 3.
- Mower Ramp (Incidental to Contract Cost, For Details See Sh. # 20.)



NOTES:

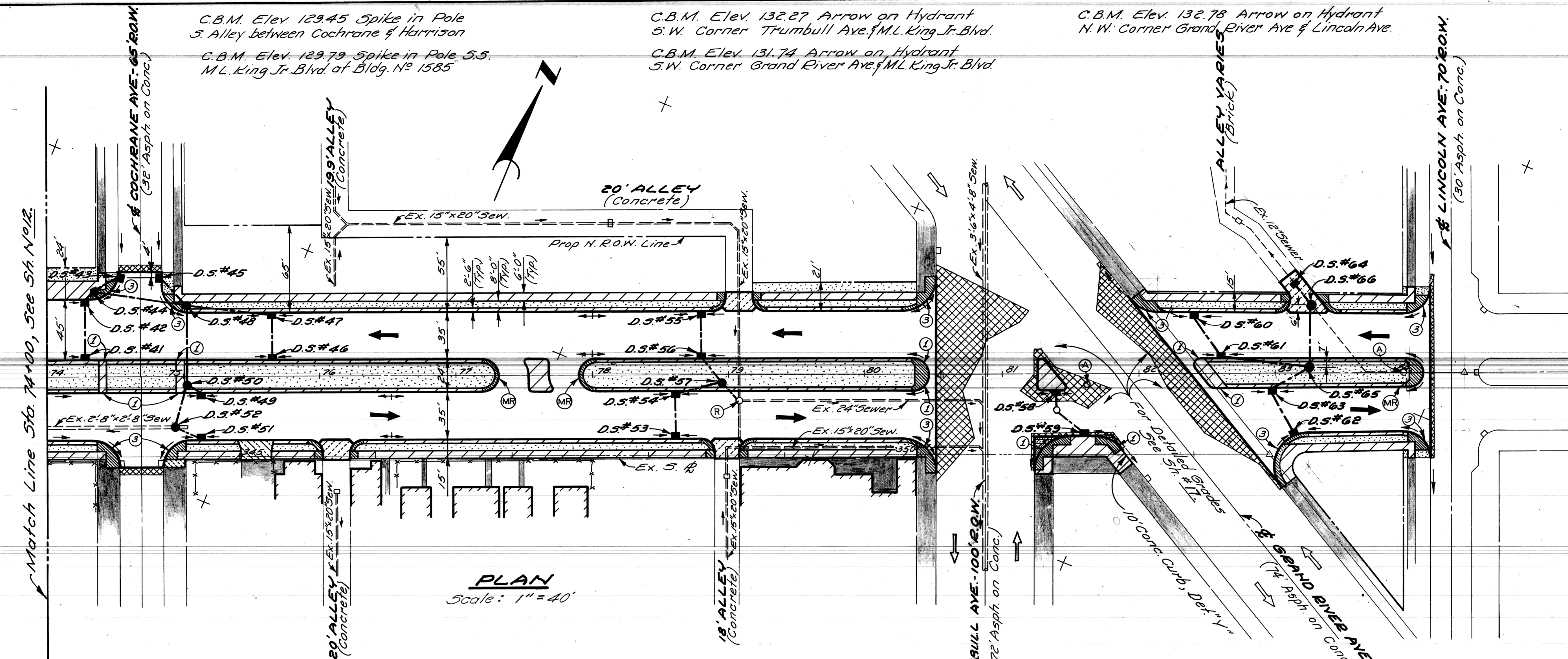
1. For Alignment, See Sh. # 3.
2. For Typical Cross-Section, See Sh. # 2.
3. For Construction Sequence, See Sh. # 6.
4. For Utility Legend & Symbols, See Sh. # 27.
5. For Driveway Details, See Sh. # 27.
6. For Landscaping, See Sh. # 21 thru 23.

REFERENCE DRAWINGS				DESIGNED BY	APPROVED:	CITY OF DETROIT CITY ENGINEERING BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.	SHEET 10 OF 71 SHEETS
				DRAWN BY	ENGINEER OF STREETS			CONTRACT NO. 17176A
				TRACED BY	ALLIEN S. [Signature]			ASSIGNMENT NO.
				CHECKED BY	WB			DATE AUG., 1984
COORD	DESCRIPTION	DRN	CKD	APVD	DATE			
REVISIONS LOCATED BY COORDINATES ON SHEET								



PROFILES
 Scale: Hor. 1" = 40'
 Vert. 1" = 4'

DRWN BY <i>MAP</i>		APPROVED <i>Allen E. Proger</i> ENGINEER OF EXPRESSWAY		CITY OF DETROIT CITY ENGINEERING		M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.		SHEET 11 OF 71 SHEETS CONTRACT NO. 17176A	
CK'D BY <i>WB</i>						PROFILES WABASH TO STA. 63+00		DATE: AUG., 1984	



PLAN
Scale: 1" = 40'

DRAINAGE STRUCTURE TABLE

DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT.	WITH TRAP (T)	SEWER LENGTH & TYPE LEAVING (D.S.)			SEWER TAP SIZE (IN.)	TRENCH DETAIL (L.F.)		COMMENTS
			LENGTH (FT.)	SIZE (IN.)	CLASS C-76		8	9	
41	C.B. "A"		41	12	III			41	
42	C.B. "B"	T	2	12	III			2	
43	C.B. "B"	T	14	12	III			14	
44	M.H. Std.		61	12	III			61	
45	C.B. "B"	T	15	12	III			15	
46	C.B. "A"		31	12	III			31	
47	C.B. "B"	T	58	12	III			58	
48	M.H. Std.		55	12	III		15	40	
49	C.B. "B"	T	6	12	III		6		
50	M.H. Std.		28	12	III			28	
51	C.B. "B"	T	15	12	III			15	
52	M.H. Drop								Construct over Exist. 2'-8" x 2'-8" Sewer.
53	C.B. "A"		31	12	III			31	
54	C.B. "B"	T	29	12	III		29		
55	C.B. "A"		31	12	III			31	
56	C.B. "B"	T	18	12	III		18		
57	M.H. Std.		13	12	III	12		13	
58	C.B. "B"	T	6	12	III	12		6	
59	C.B. "B"	T	20	12	III	12		20	
60	C.B. "A"		33	12	III			33	
61	C.B. "B"	T	59	12	III		59		
62	C.B. "A"		33	12	III			33	
63	C.B. "B"	T	27	12	III		27		
64	C.B. "B"	T	13	12	III			13	*
65	M.H. Std.		42	12	III			42	
66	M.H. Drop								On Existing 12" Sewer. *

SURFACING LEGEND

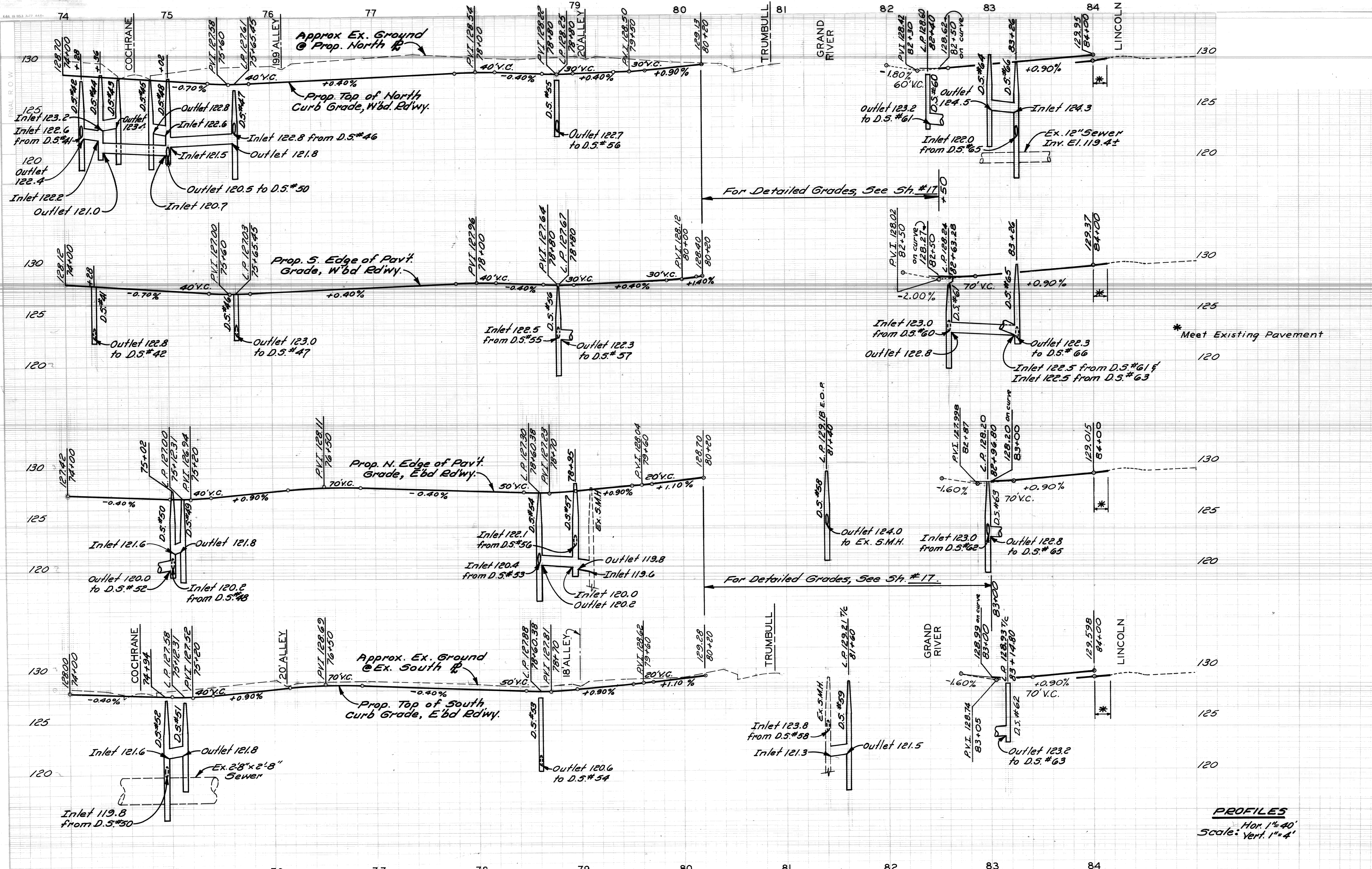
- Concrete Pavement - (Reinforced) - 9" Uniform with Integral Curb.
- Concrete Pavt. - Nonreinforced - 9"
- 4" Concrete Sidewalk
- 6" Concrete Sidewalk - (Ramp areas are paid for separately)
- Exist. Sd'wk. or Drwy. to Remain
- Exist. Building to Remain
- Bitum. on Conc. Base Course - (Reinf.) - 9" with Integral Curb.
- Removing Bitum. Surf. & Resurf. (Bituminous Approaches)
- Class "A" Sodding with 3" Topsoil
- Drainage Flow Arrows.
- Adjust Drainage Structure Covers
- Reconstructing Drainage Structures
- Sidewalk Ramps, Types 1 & 3
- Mower Ramp (Incidental to Contract Cost, For Details, See Sh. # 20.)
- Conc. Pavt. - Nonreinforced - 6" (Driveway)

NOTES:

1. For Alignment, See Sh. # 5.
2. For Typical Cross-Section, See Sh. # 2.
3. For Construction Sequence, See Sh. # 6.
4. For Utility Legend & Symbols, See Sh. # 27.
5. For Landscaping, See Sh. # 21 thru 23.

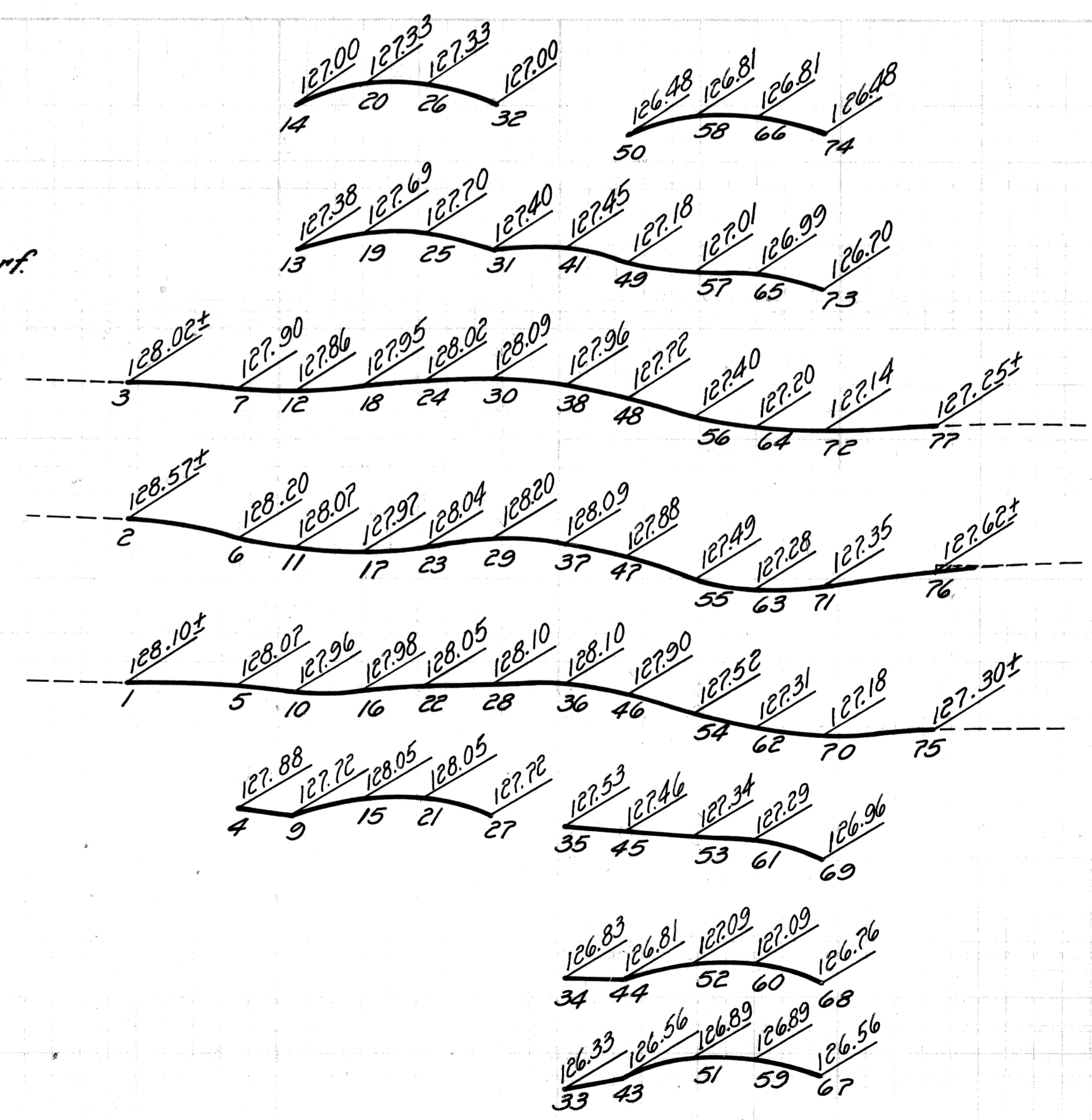
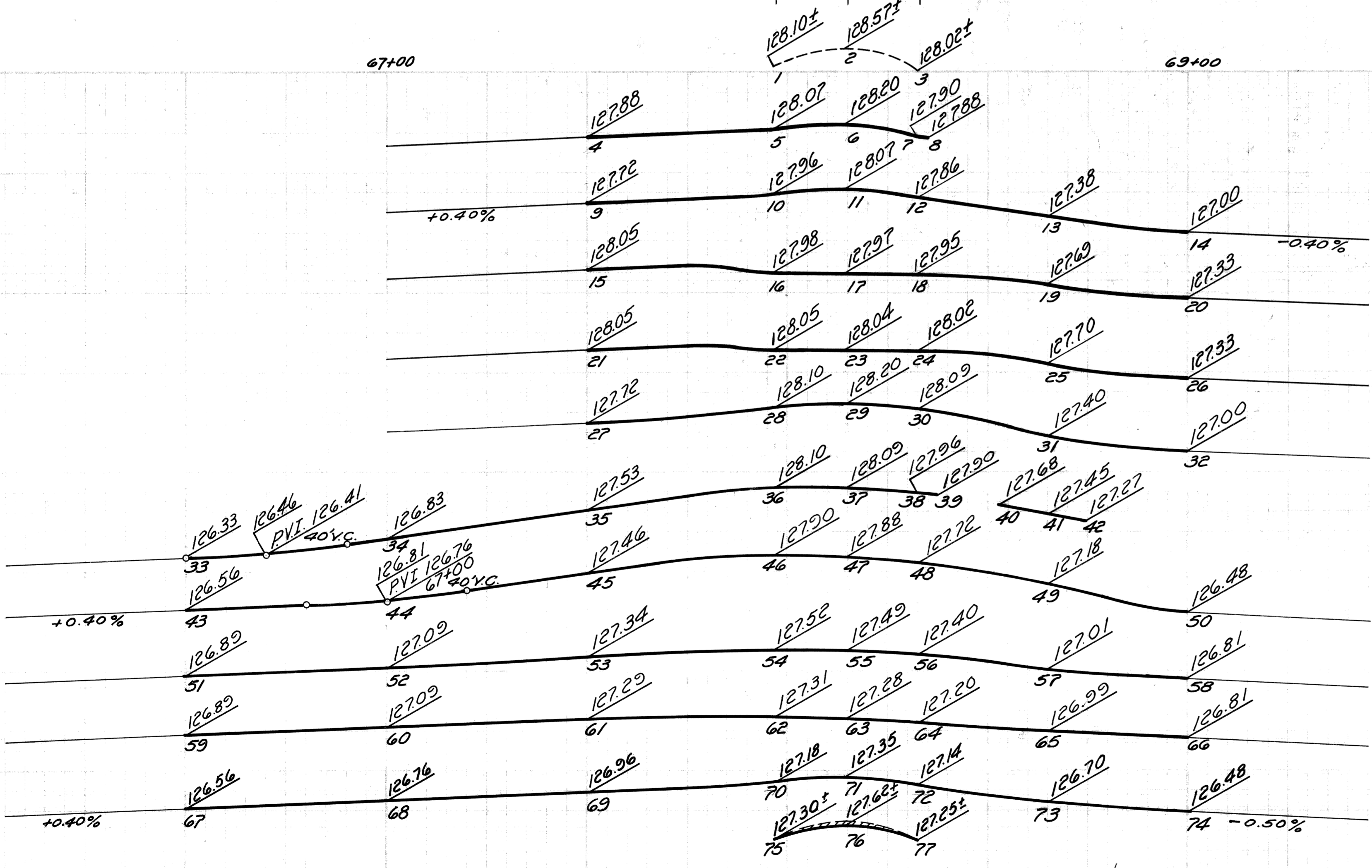
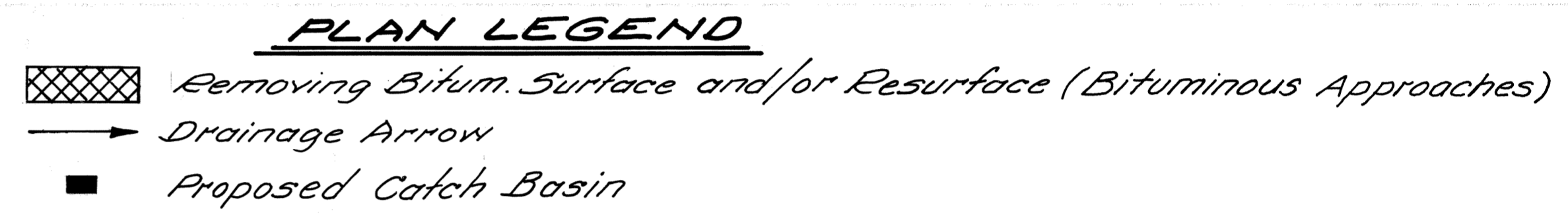
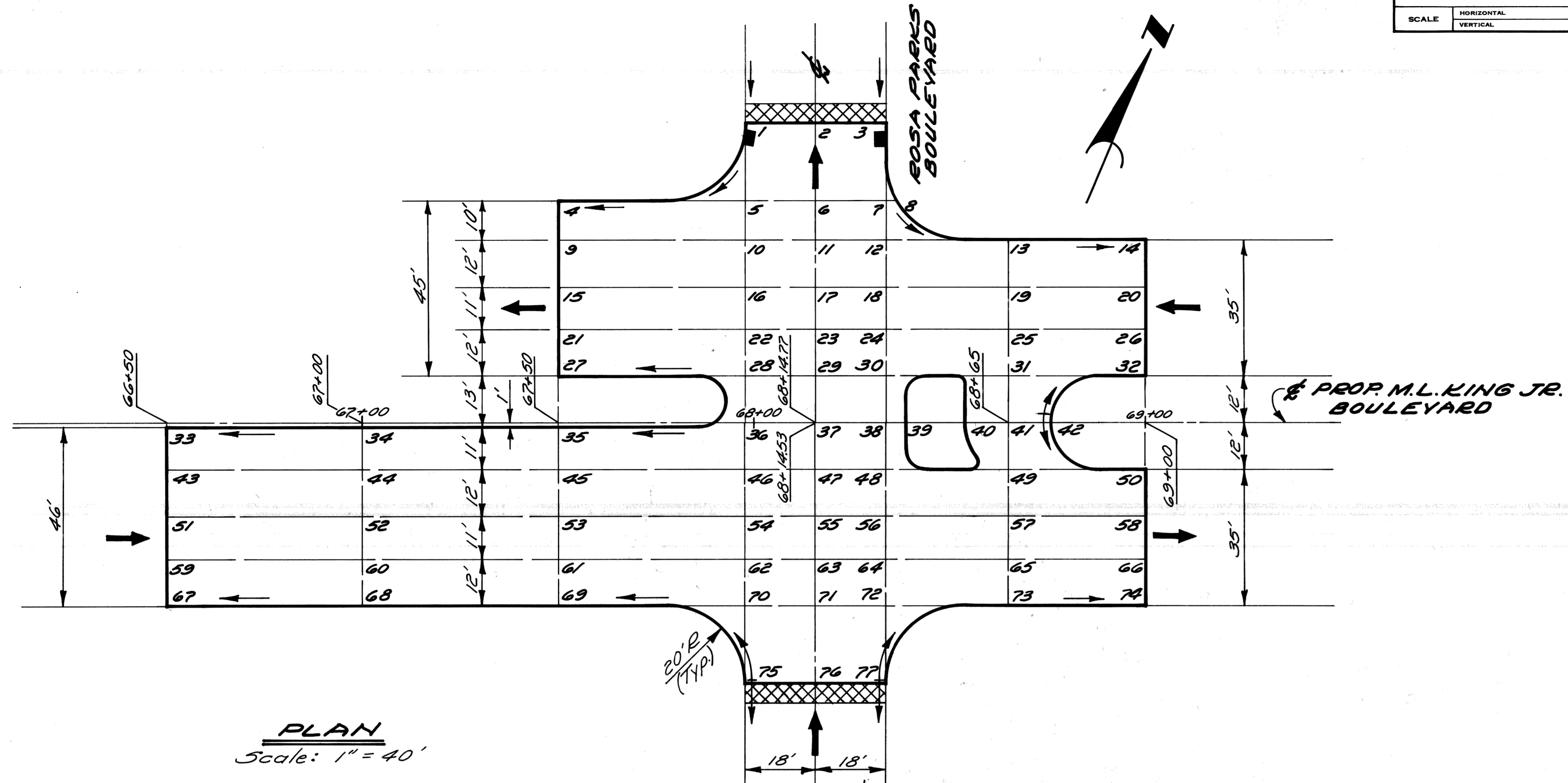
*If interference with existing 6" water main is encountered, modify drainage as directed by the Engineer.

REFERENCE DRAWINGS DESIGNED BY DRAWN BY <i>MAB</i> TRACED BY CHECKED BY <i>WB</i>	APPROVED: ENGINEER OF STREETS <i>Allen E. Pogue</i> HIGHWAY ENGINEER	CITY OF DETROIT CITY ENGINEERING BUREAUS OF STREETS AND HIGHWAYS	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD TO LINCOLN AVE. PLAN STA. 74 + 00 TO LINCOLN AVE.	SHEET 14 OF 71 SHEETS CONTRACT NO. 17176A ASSIGNMENT NO. DATE AUG., 1984
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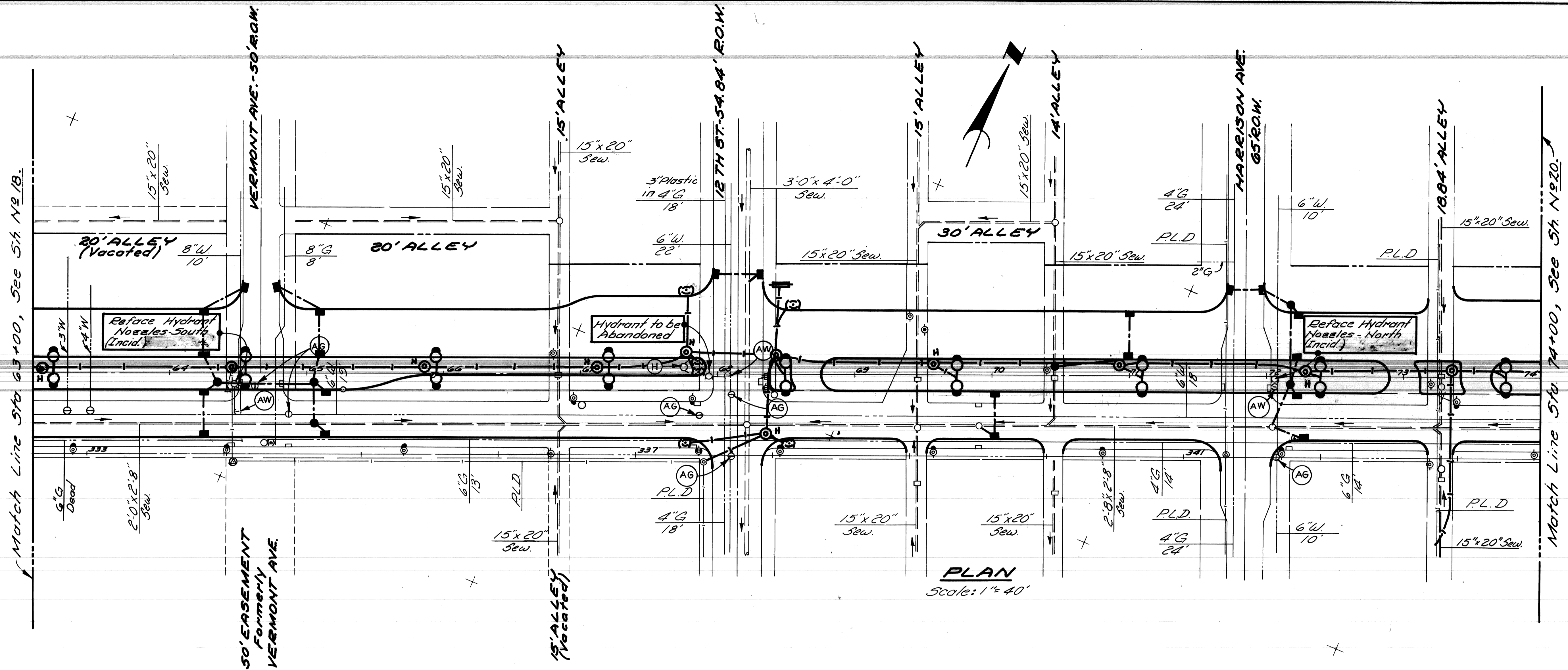


PROFILES
 Hor. 1"=40'
 Scale: Vert. 1"=4'

74	75	76	77	78	79	80	81	82	83	84
APPROVED <i>Allen E. Prosser</i> ENGINEER OF EXPRESSWAY			CITY OF DETROIT CITY ENGINEERING			M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.			SHEET 15 OF 71 SHEETS CONTRACT NO. 17176A	
DRWN BY MAP						PROFILES STA. 74+00 TO LINCOLN			DATE: AUG., 1984	
CK'D BY WB										



<table border="1"> <tr> <td>DESCRIPTION</td> <td>DR</td> <td>N</td> <td>CK</td> <td>D</td> <td>AP</td> <td>VD</td> <td>DATE</td> </tr> <tr> <td>REVISIONS</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </table>		DESCRIPTION	DR	N	CK	D	AP	VD	DATE	REVISIONS								<table border="1"> <tr> <td>BY</td> <td>CHECKED BY</td> <td>APPROVED:</td> </tr> <tr> <td>MAR</td> <td>RP</td> <td><i>Allen E. Rogan</i></td> </tr> <tr> <td></td> <td></td> <td>HIGHWAY ENGINEER</td> </tr> </table>	BY	CHECKED BY	APPROVED:	MAR	RP	<i>Allen E. Rogan</i>			HIGHWAY ENGINEER	<p align="center">CITY OF DETROIT CITY ENGINEERING DEPARTMENT BUREAU OF STREETS AND HIGHWAYS</p>	<p align="center">M.L. KING JR. BLVD RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.</p> <p align="center">DETAILED GRADES ROSA PARKS BLVD. AT M.L. KING JR. BLVD.</p>	<table border="1"> <tr> <td>INDEX NO.</td> <td>SHEET 16 OF 71 SHEETS</td> </tr> <tr> <td>CONTRACT NO.</td> <td>17176A</td> </tr> <tr> <td>ASSIGNMENT NO.</td> <td></td> </tr> <tr> <td>DATE</td> <td>AUG., 1984</td> </tr> </table>	INDEX NO.	SHEET 16 OF 71 SHEETS	CONTRACT NO.	17176A	ASSIGNMENT NO.		DATE	AUG., 1984
DESCRIPTION	DR	N	CK	D	AP	VD	DATE																															
REVISIONS																																						
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MAR	RP	<i>Allen E. Rogan</i>																																				
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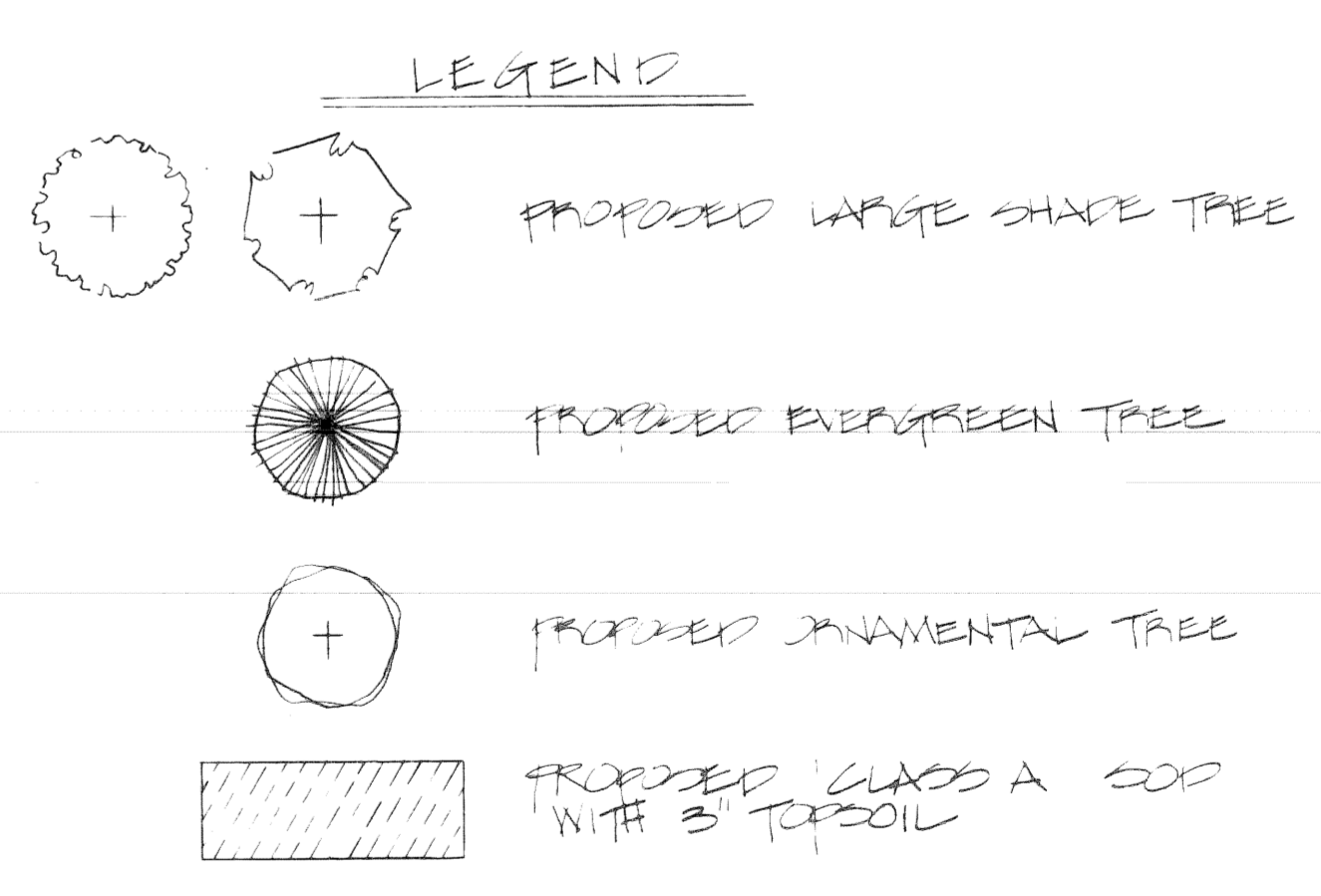
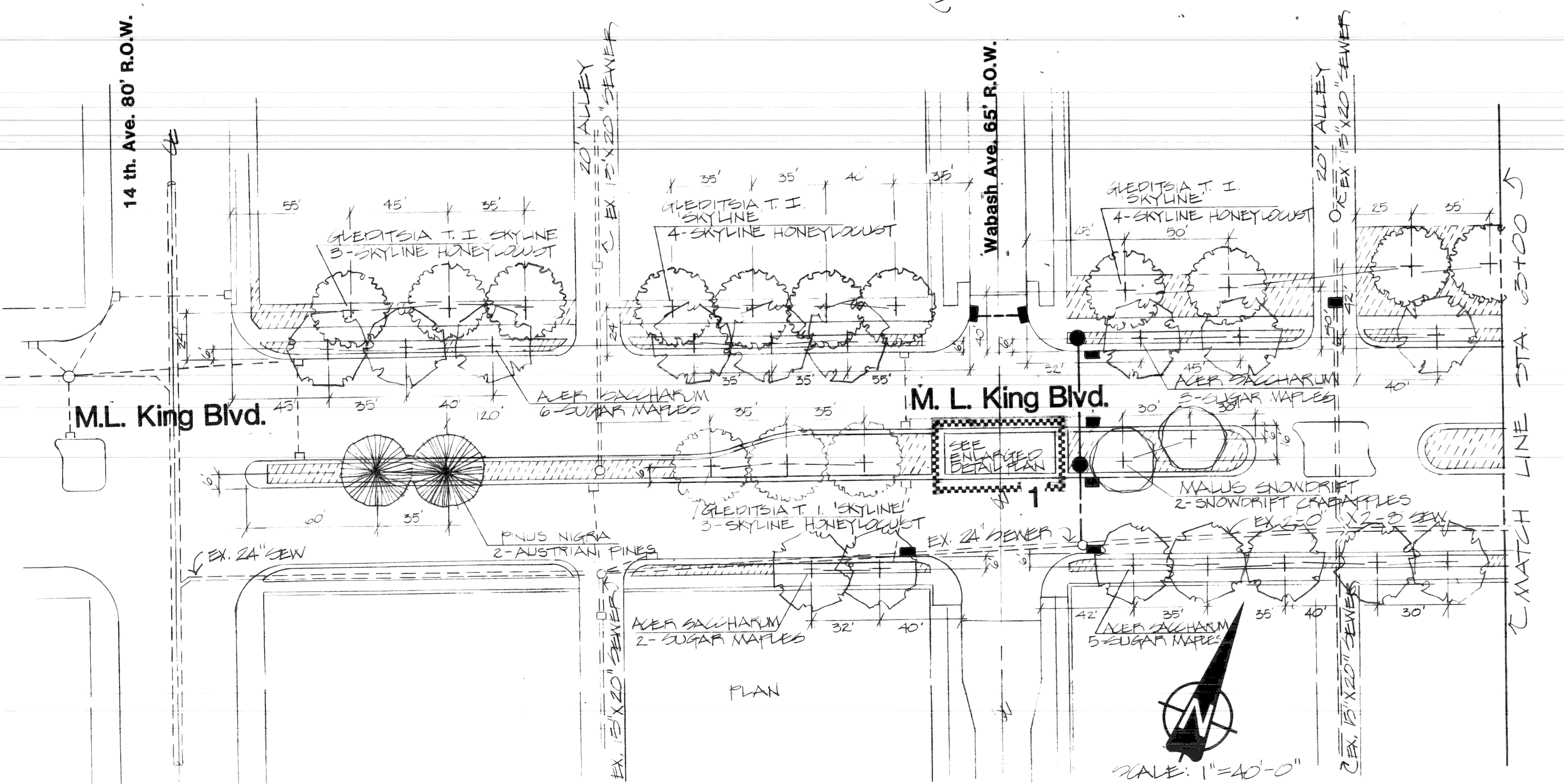
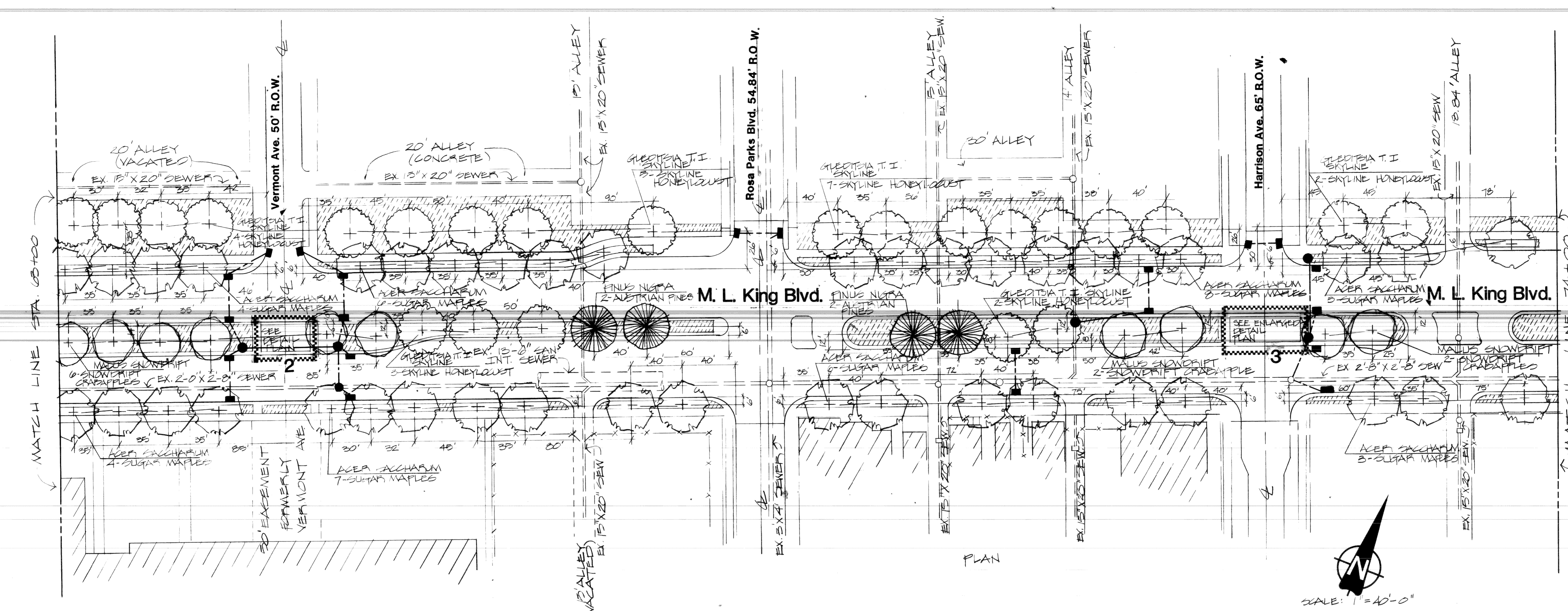
LEGEND

- (AW) Adjusting Water Shutoffs
- (AG) Adjusting Gatewell Manhole Cover
- (RG) Reconstruct Gatewell Manhole
- (H) Install New Fire Hydrant (Nozzles Facing East)

- NOTES:**
1. For Utility Legend, See Sh. # 27
 2. Locations of exist. utility lines are based on the best available records & are not guaranteed for accuracy.

CALL MISS DIG
48 HOURS PRIOR TO
CONSTRUCTION 647-7344

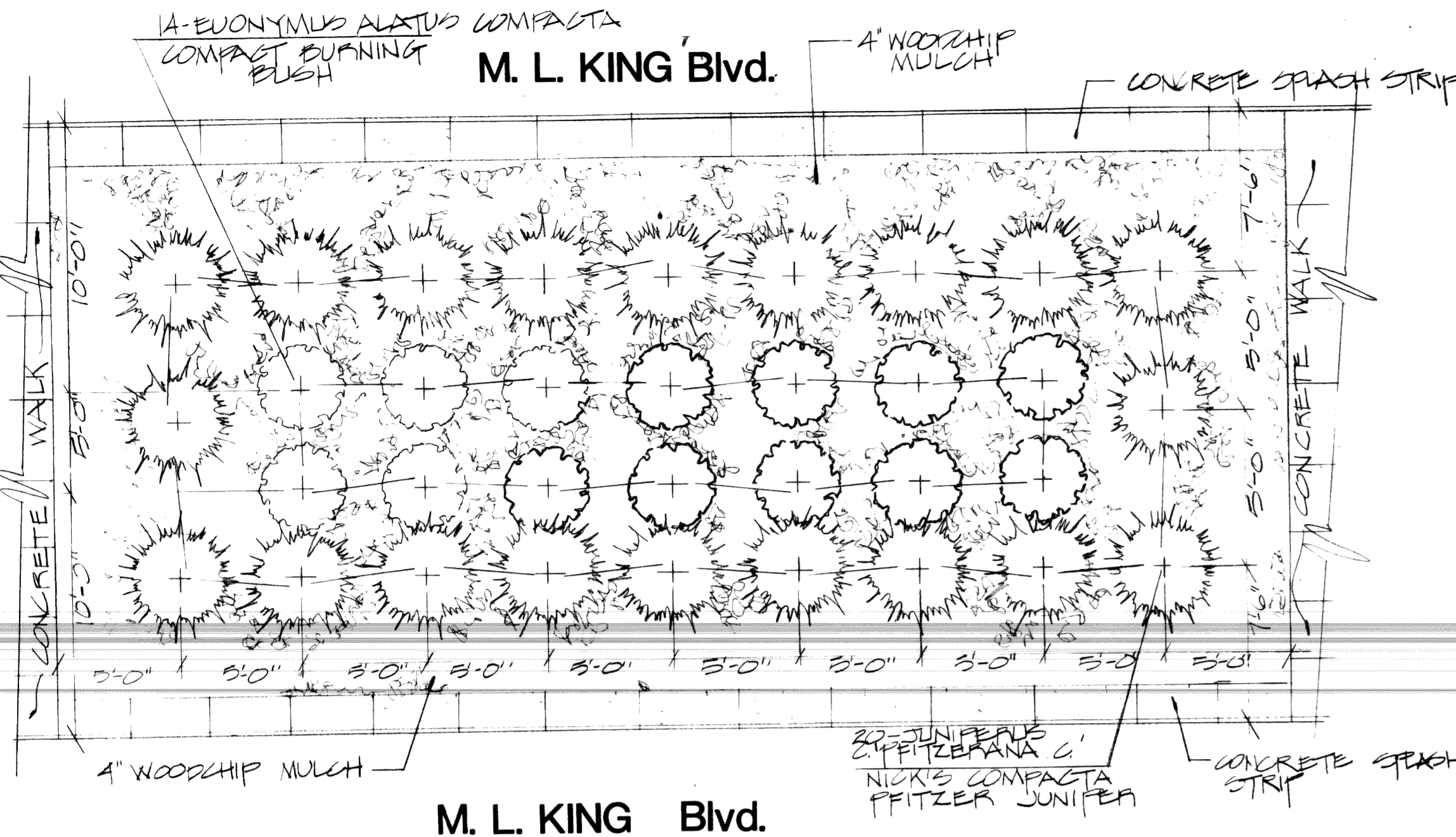
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DRAWN BY			ENGINEER OF STREETS <i>Allen E. Reagin</i> HIGHWAY ENGINEER		UTILITIES		CONTRACT NO. 17176A
TRACED BY					STA. 63 + 00 TO STA. 74 + 00		ASSIGNMENT NO.
CHECKED BY	WB						DATE AUG., 1984
COORD	DESCRIPTION	DRN	CK'D	APVD	DATE		
REVISIONS LOCATED BY COORDINATES ON SHEET							



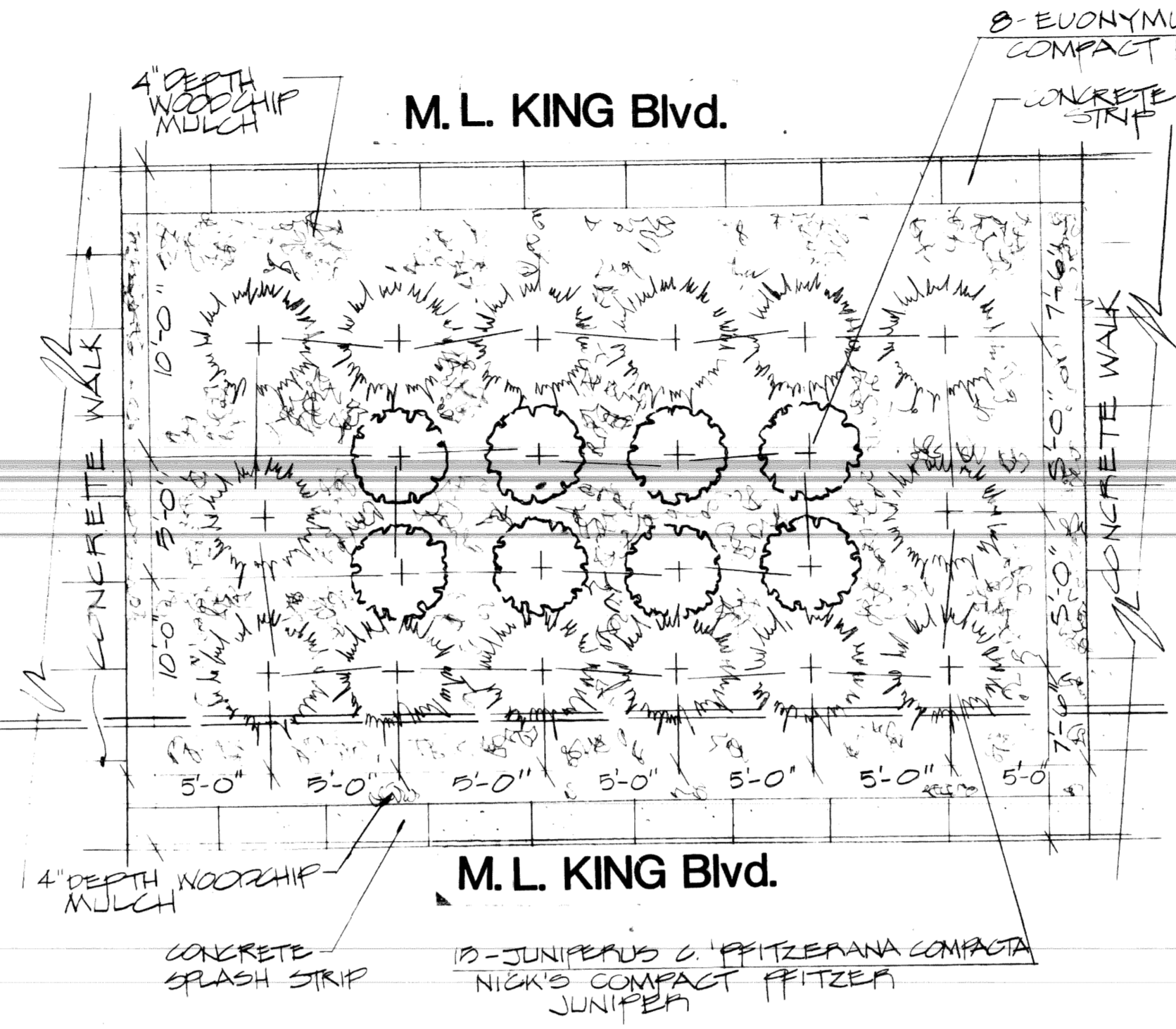
CITY OF DETROIT RECREATION DEPARTMENT LANDSCAPE DESIGN UNIT FOR CITY ENGINEERING DEPARTMENT		M. L. KING JR. BLVD. RECONSTRUCTION LANDSCAPE PLAN WABASH TO LINCOLN AVE. Sta. 63+00 to Sta. 74+00		SHEET <u>21</u> OF <u>71</u> SHEETS CONTRACT No. 17176A DRWG No. DATE AUG., 1984
DESIGNED BY U.D. O'NEAL	APPROVED L. D. U. Dick Hawtan Forestry <i>J. J. Lyons</i>	DRAWN BY U.D. O'NEAL	TRACED BY U.D. O'NEAL	
CHECKED BY J.L. ECKERT				

COORD	DESCRIPTION	DR	CR	AP	DATE
1					
2					

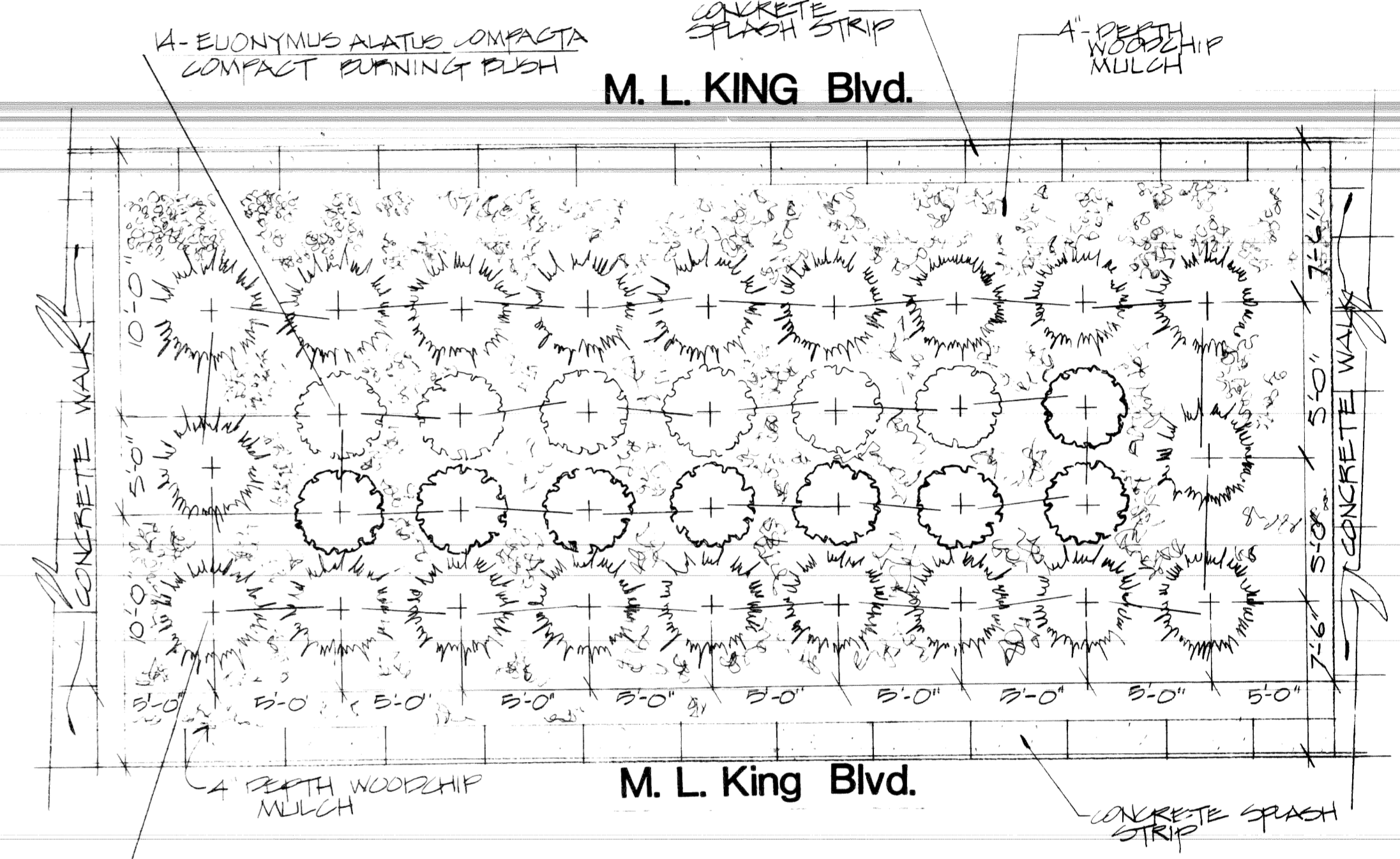
REVISIONS LOCATED BY COORDINATES ON SHEET



1
3
PLANTING BED DETAIL #1 & #3
SCALE: 1"=5'-0"



2
3
PLANTING BED DETAIL #2
SCALE: 1"=5'-0"



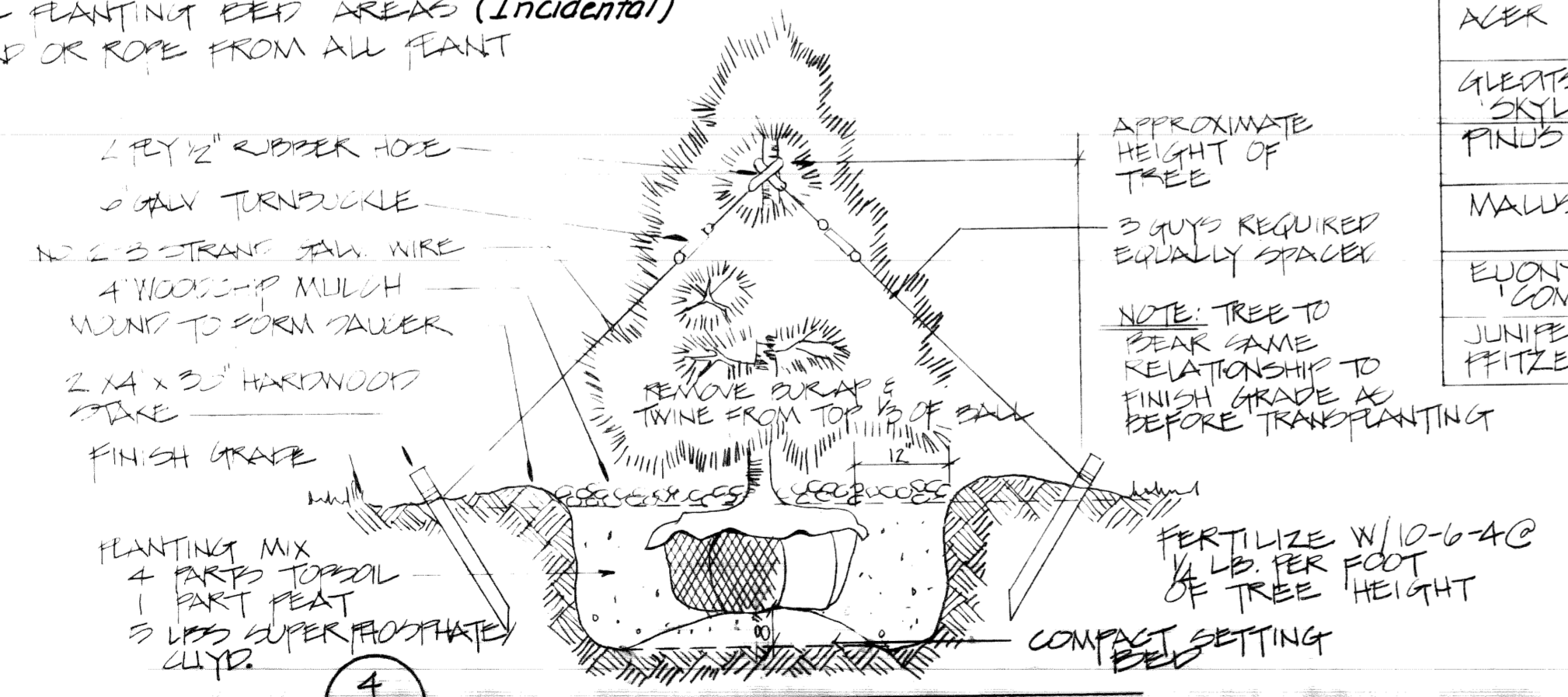
3
3
PLANTING BED DETAIL #4
SCALE: 1"=5'-0"

NOTES

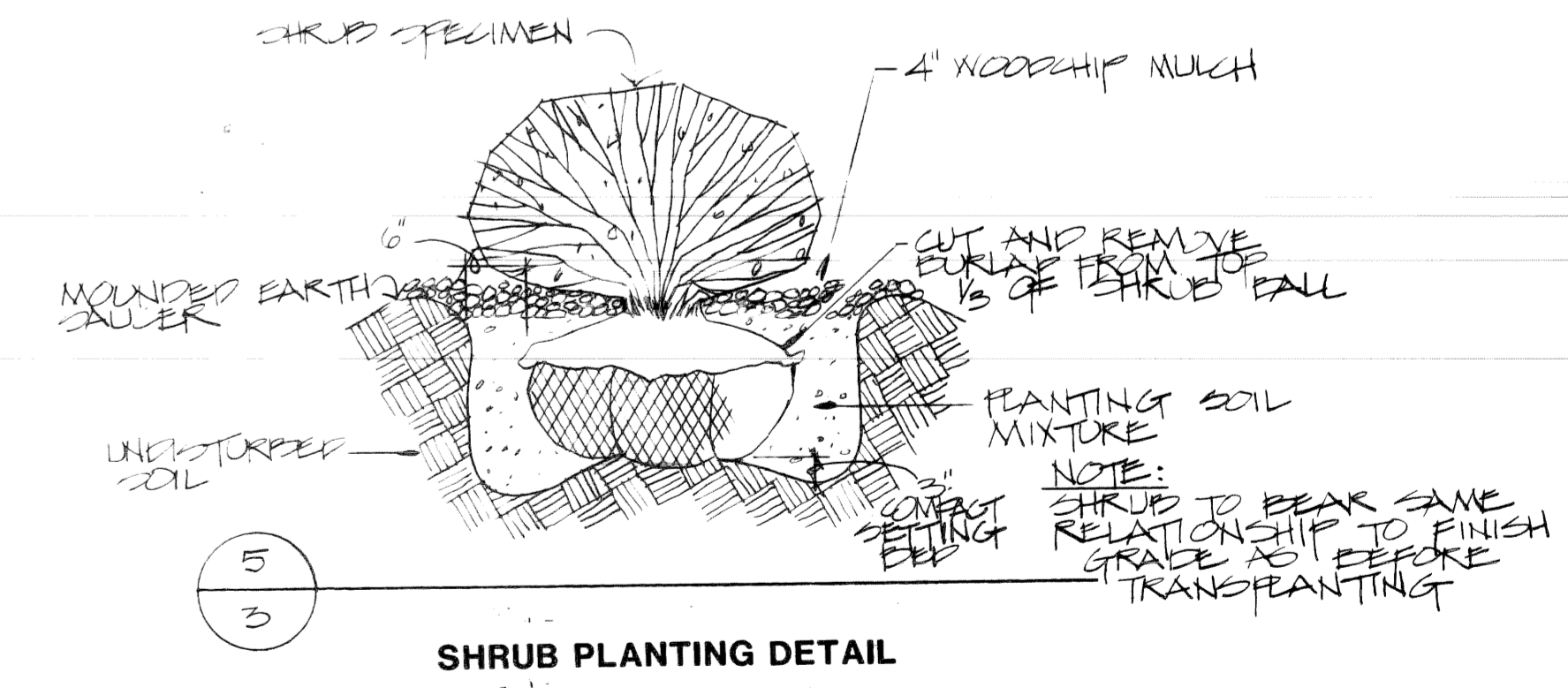
- 1) PRIOR TO ANY EXCAVATION CARE SHOULD BE TAKEN TO INSURE NO DAMAGE TO UNDERGROUND UTILITIES BY CONTRACTING MISERY AT
- 2) ALL BORDERING JUNIPERS ARE TO BE SPACED 5'-0" TO 5'-6" AS INDICATED IN THE ENLARGED DRAWINGS (1/3), (2/3) AND (3/3)
- 3) ALL INTERIOR EUONYMUS PLANTINGS ARE TO BE SPACED 5'-0" O.C. STAGGERED BETWEEN JUNIPERS
- 4) 4" DEPTH WOODCHIP MULCH SHALL BE SPREAD CONSISTENTLY THROUGHOUT THE PLANTING BED AREAS (Incidental)
- 5) REMOVE ALL CORDS OR ROPE FROM ALL PLANT SPECIMEN

PLANT MATERIAL LIST					
BOTANICAL NAME	COMMON NAME	QTY.	SIZE	ROOT	SPECIFICATION
ACER SACCHARUM	SUGAR MAPLE	25	5'-2 1/2" CAL	B&E	
QUERCUS TRIACANTHOS INERMIS SKYLINE	SKYLINE HONEYLOCUST	49	5'-3 1/2" CAL	B&E	
PINUS NIGRA	AUSTRIAN PINE	15	7'-8" HT.	B&E	
MALUS SNOWDRIFT	SNOWDRIFT CRABAPPLE	13	2'-2 1/2" CAL	B&E	
EUONYMUS ALATUS COMPACTA	COMPACT BURNING BUSH	50	24"-30"	B&E	
JUNIPERUS CHINENSIS FITZINGERIANA COMPACTA	NICK'S COMPACT FITZGER JUNIPER	74	30"-30" STD	B&E	

(For Info. Only)
WOODCHIP MULCH @ 4" DEPTH 275 WYD

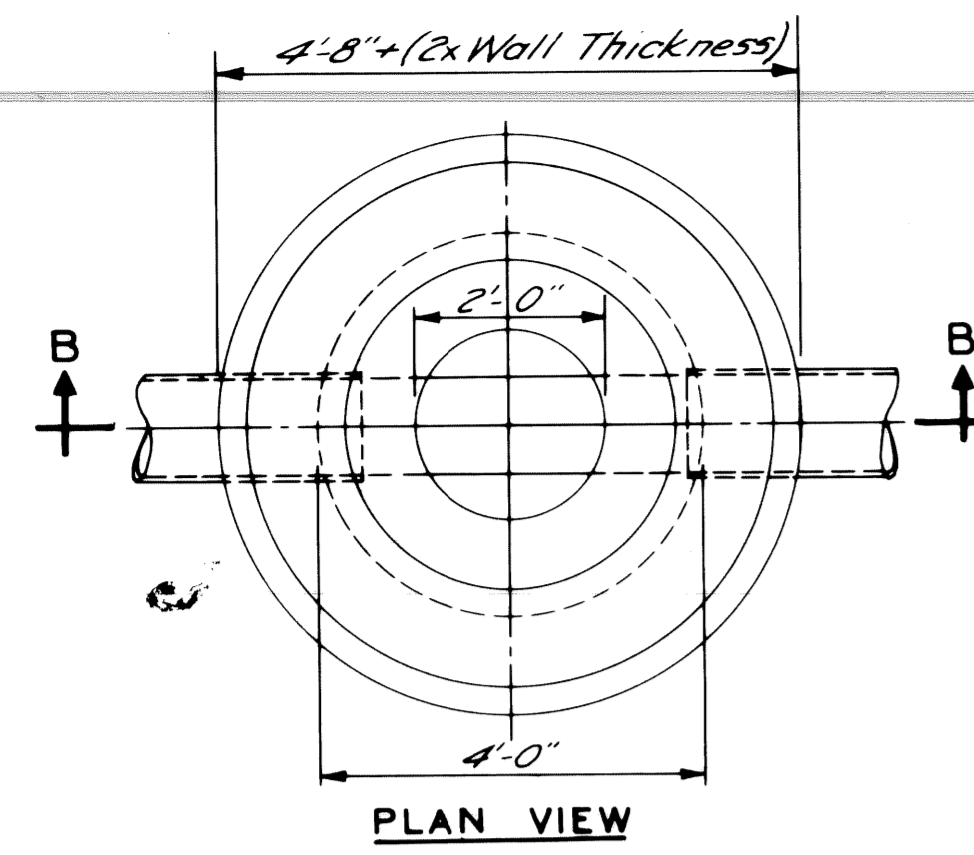


4
3
TREE PLANTING DETAIL - EVERGREEN

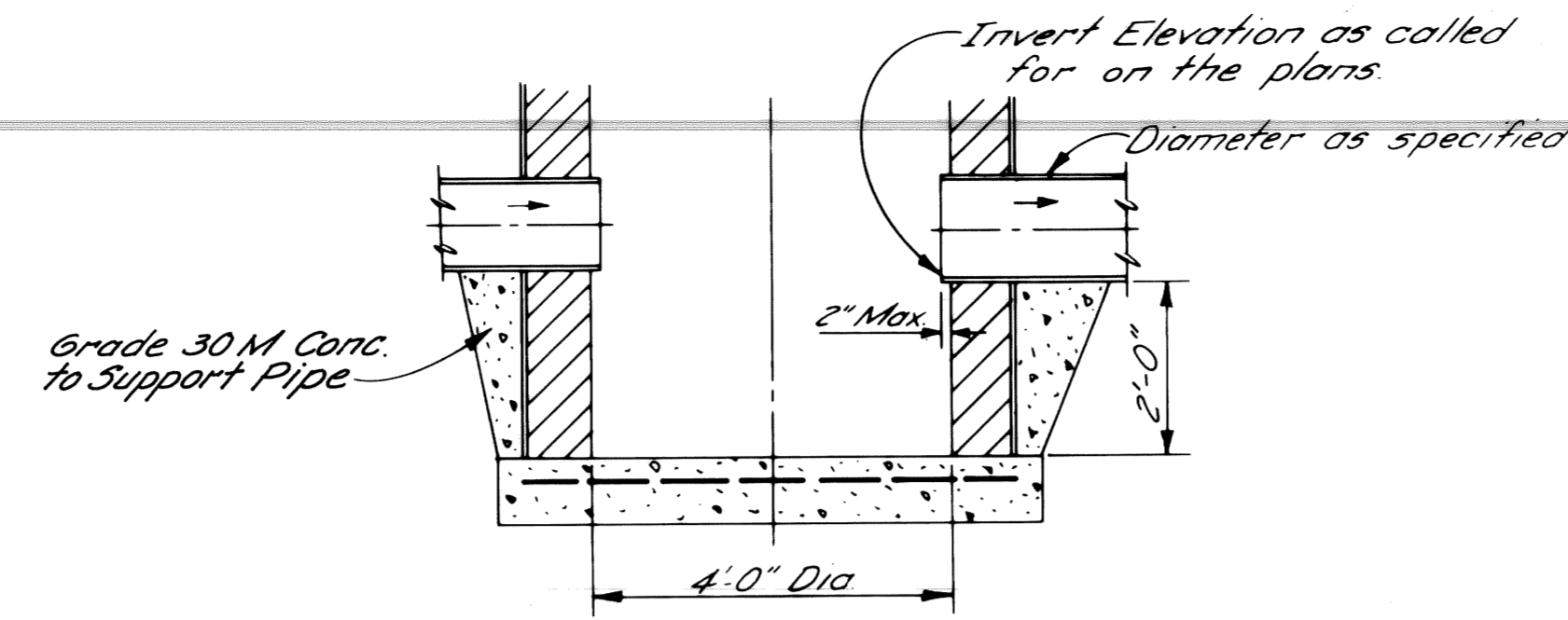
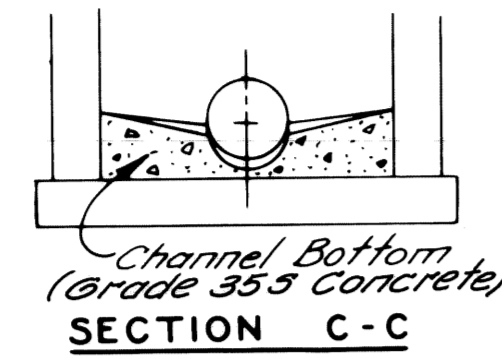


5
3
SHRUB PLANTING DETAIL

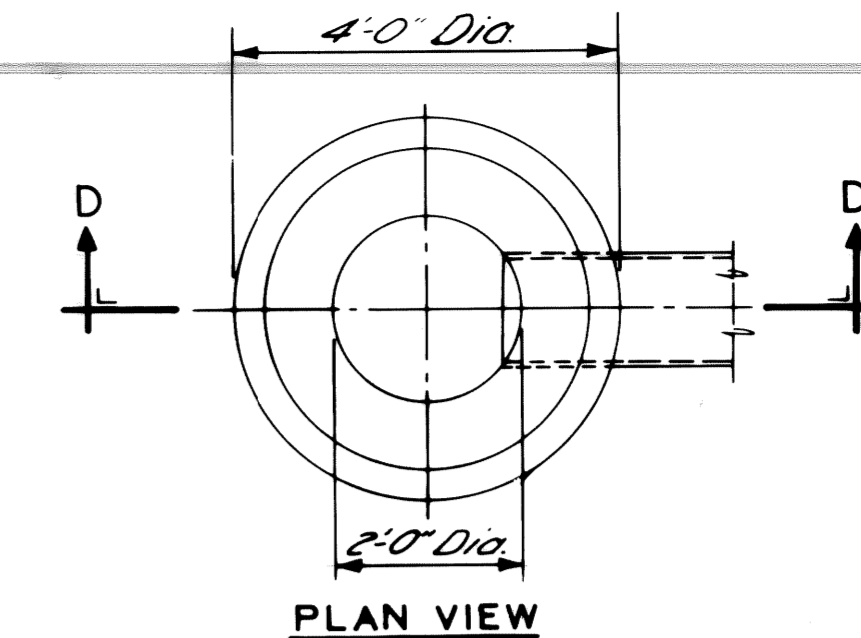
	DESIGNED BY: C.D. OATES DRAWN BY: C.D. OATES TRACED BY: C.D. OATES CHECKED BY: J.L. ECKERT	APPROVED: D. Hawley Forestry: [Signature]	CITY OF DETROIT RECREATION DEPARTMENT LANDSCAPE DESIGN UNIT FOR CITY ENGINEERING DEPARTMENT	M. L. KING JR. BLVD. RECONSTRUCTION LANDSCAPE PLAN SHRUB BED (WABASH TO LINCOLN)	SHEET 23 OF 71 SHEETS CONTRACT NO. 17176A DRWG NO. DATE AUG., 1984
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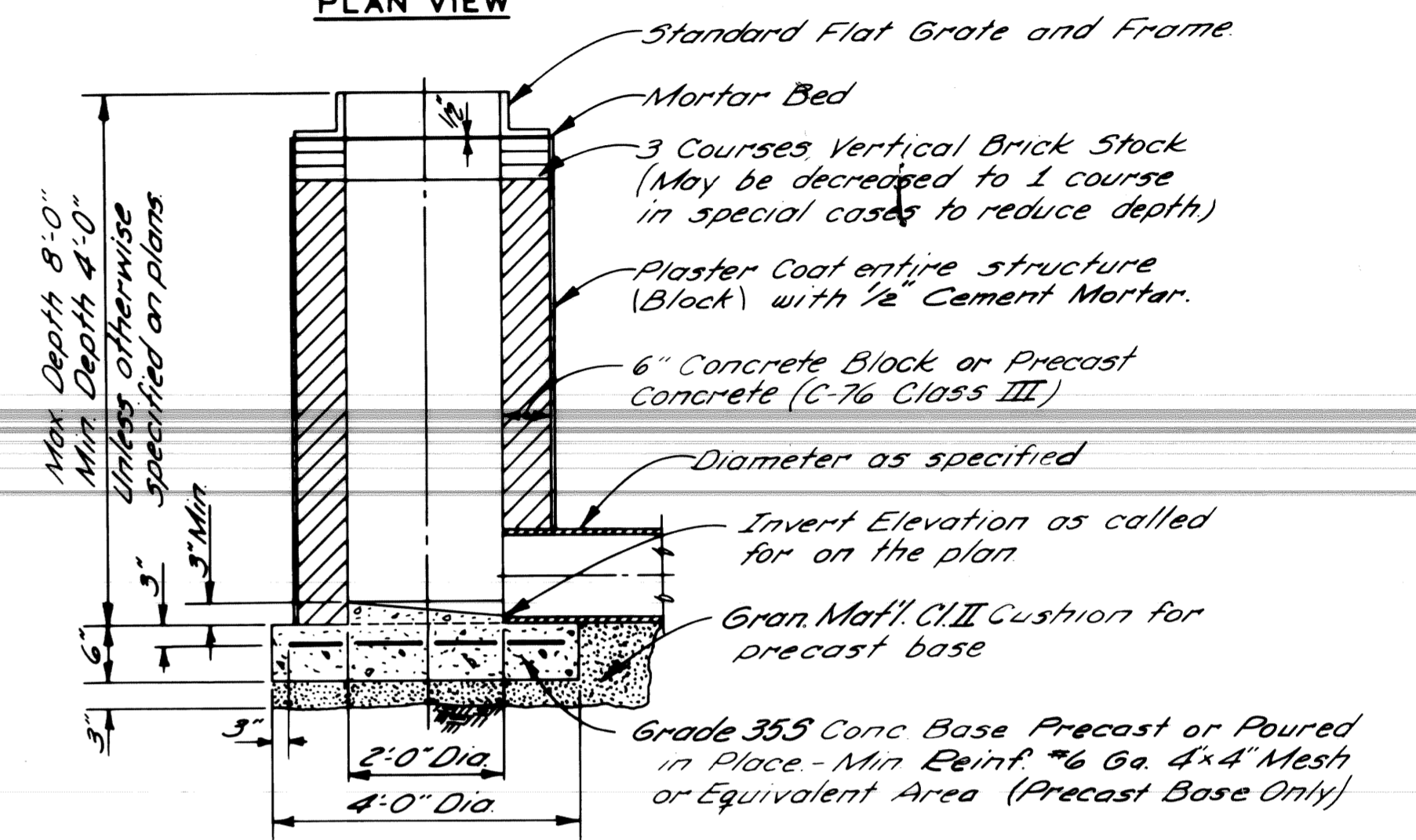
NOTE "A"
Wall thickness below a depth of 15 feet shall be 12 inches.



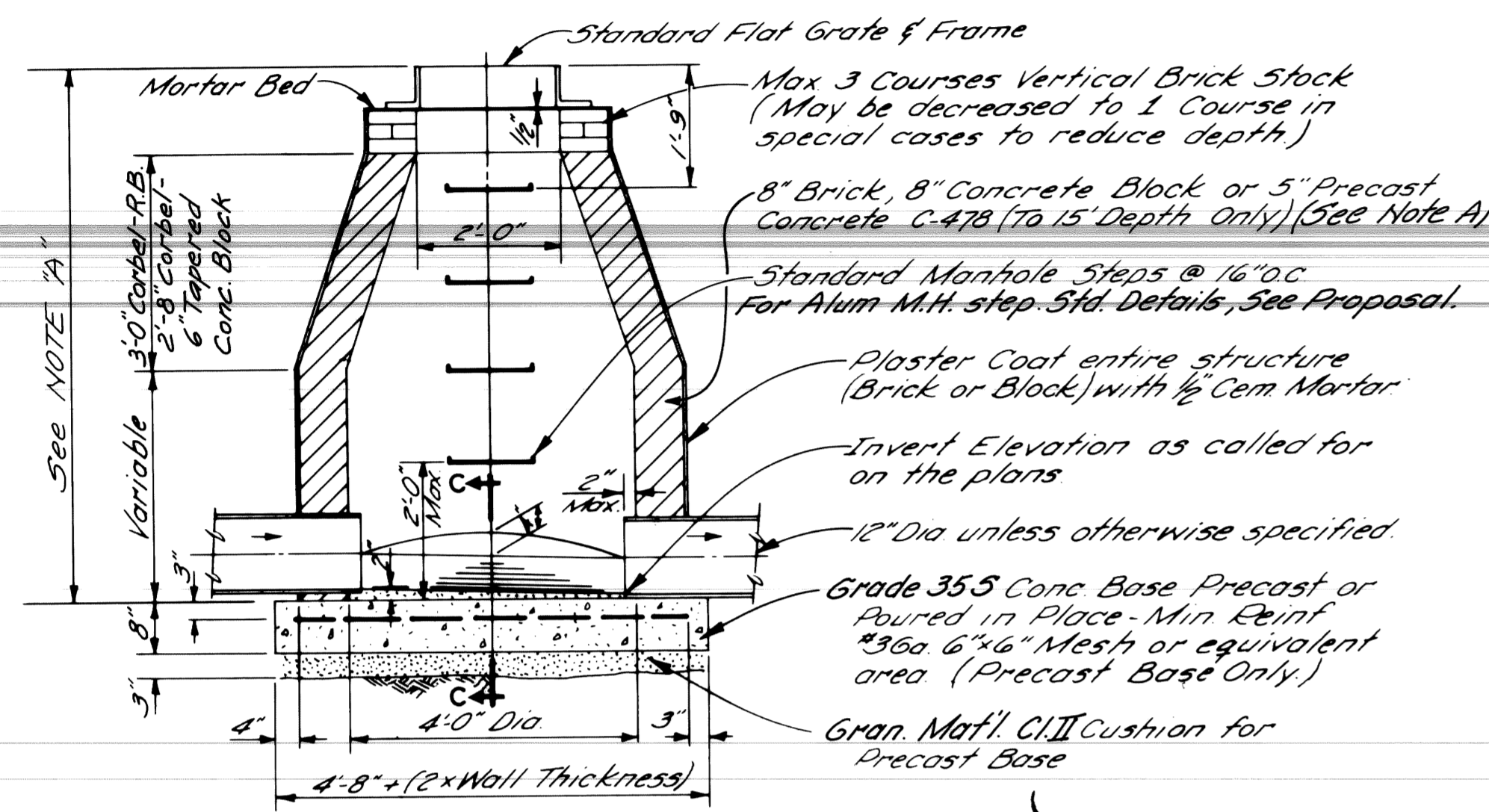
DETAIL OF SUMP FOR CATCH BASIN "B"
NO SCALE



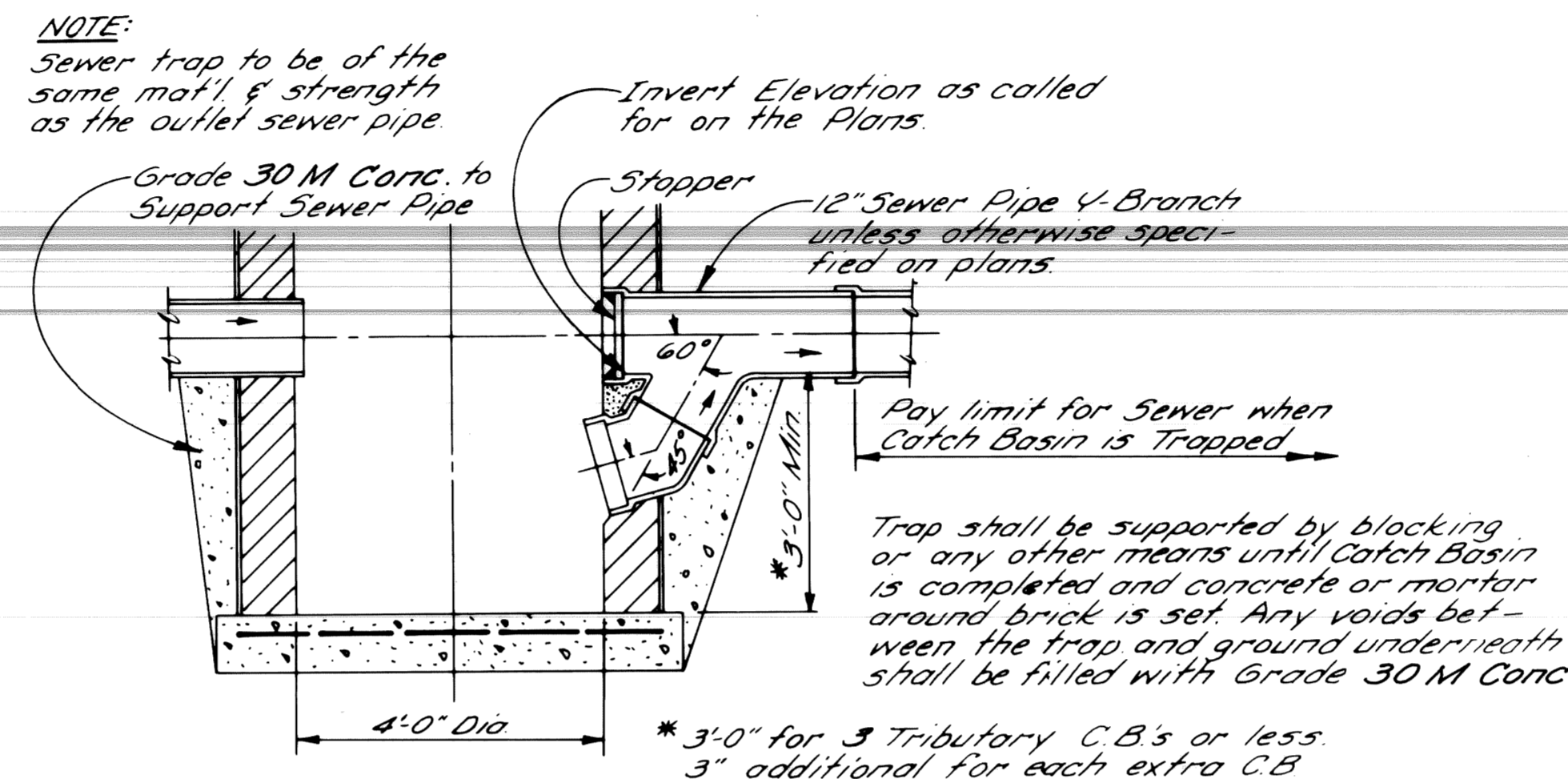
NOTE:
Catch Basin "A" will be used only when outletting to a Catch Basin "B"



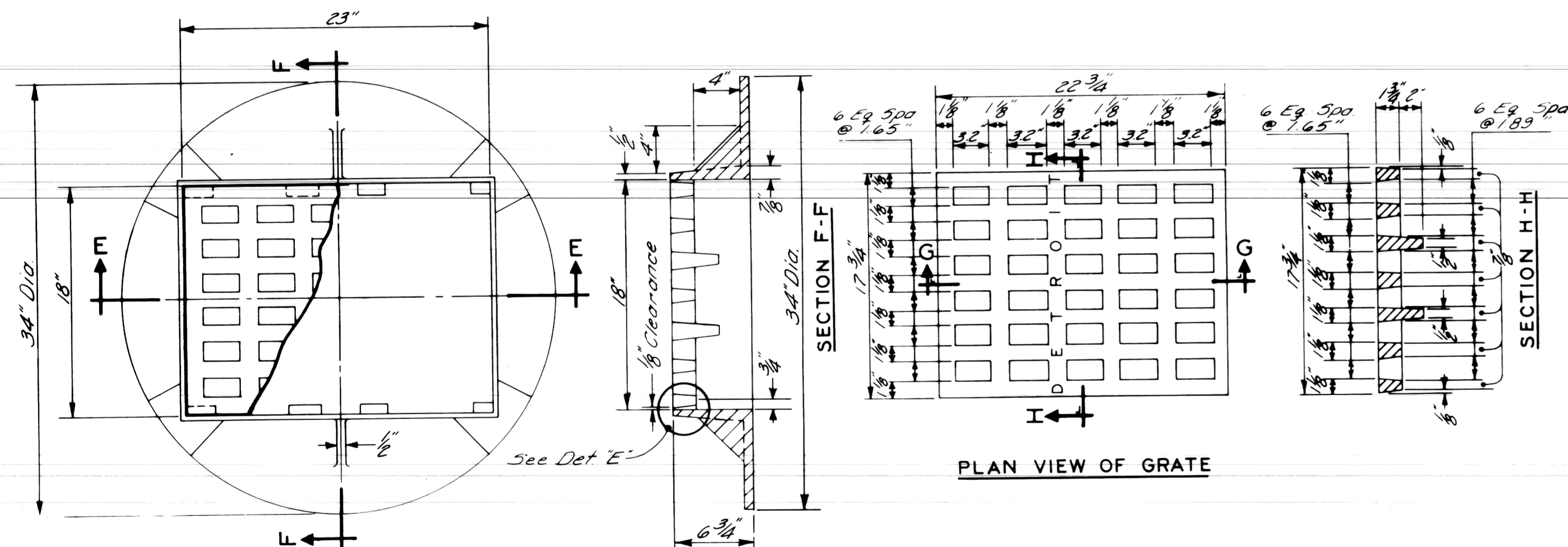
SECTION D-D
CATCH BASIN "A"
NO SCALE



SECTION B-B
CATCH BASIN "B"
NO SCALE

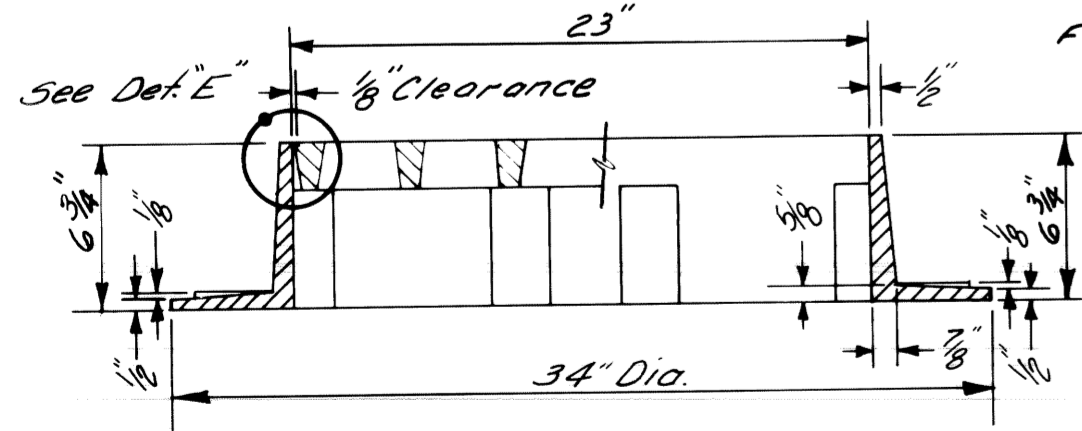


DETAIL OF TRAP FOR CATCH BASIN "B"
NO SCALE

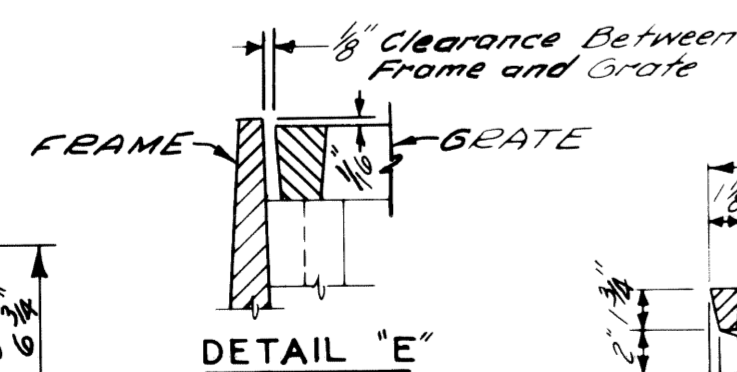


PLAN VIEW OF FRAME

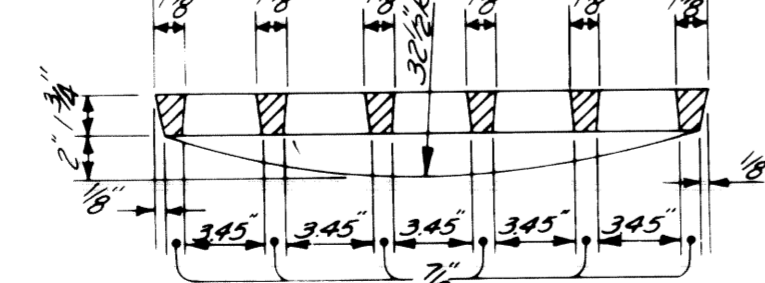
PLAN VIEW OF GRATE



SECTION E-E



DETAIL "E"



SECTION G-G

STANDARD FLAT GRATE AND FRAME
NO SCALE

GENERAL NOTES

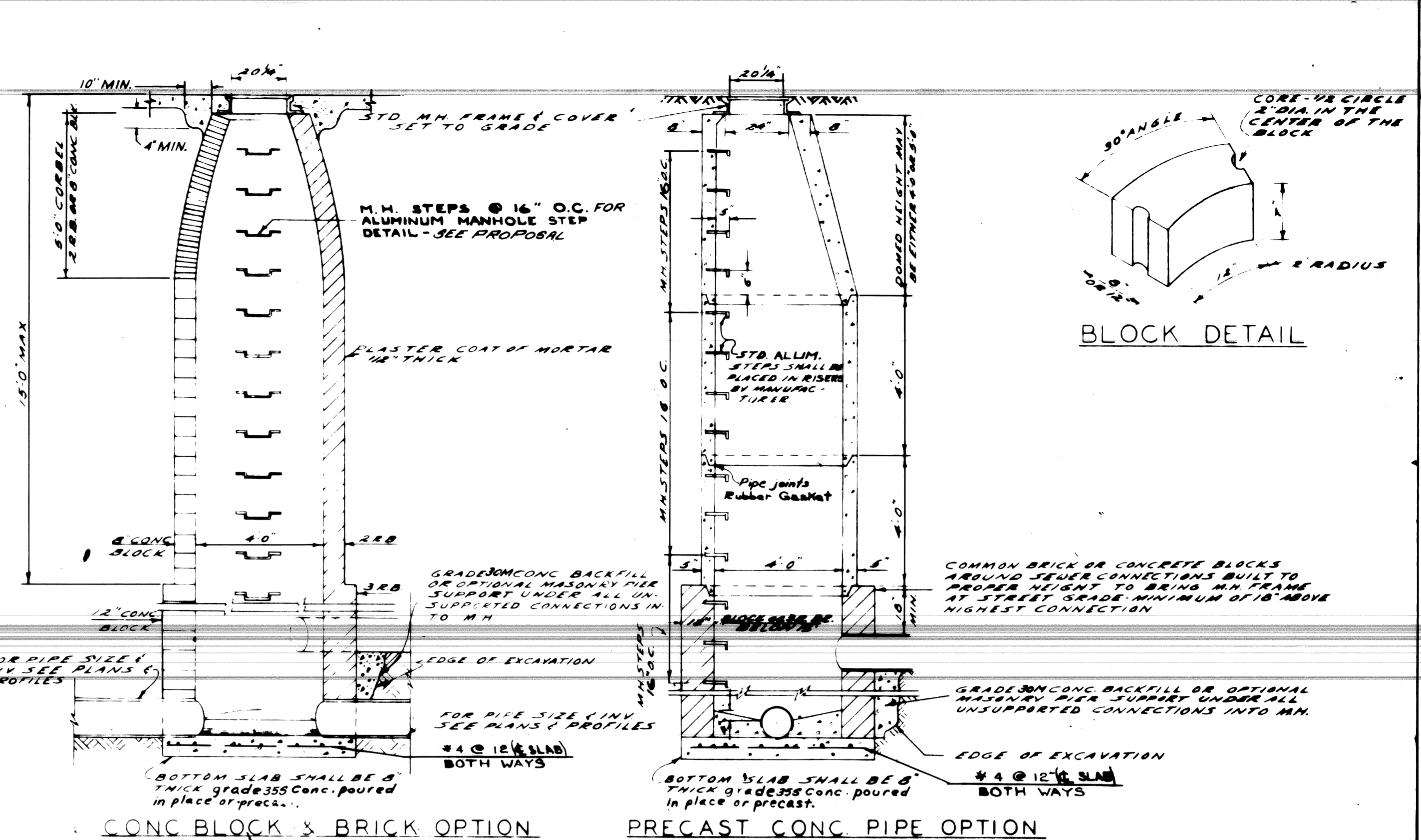
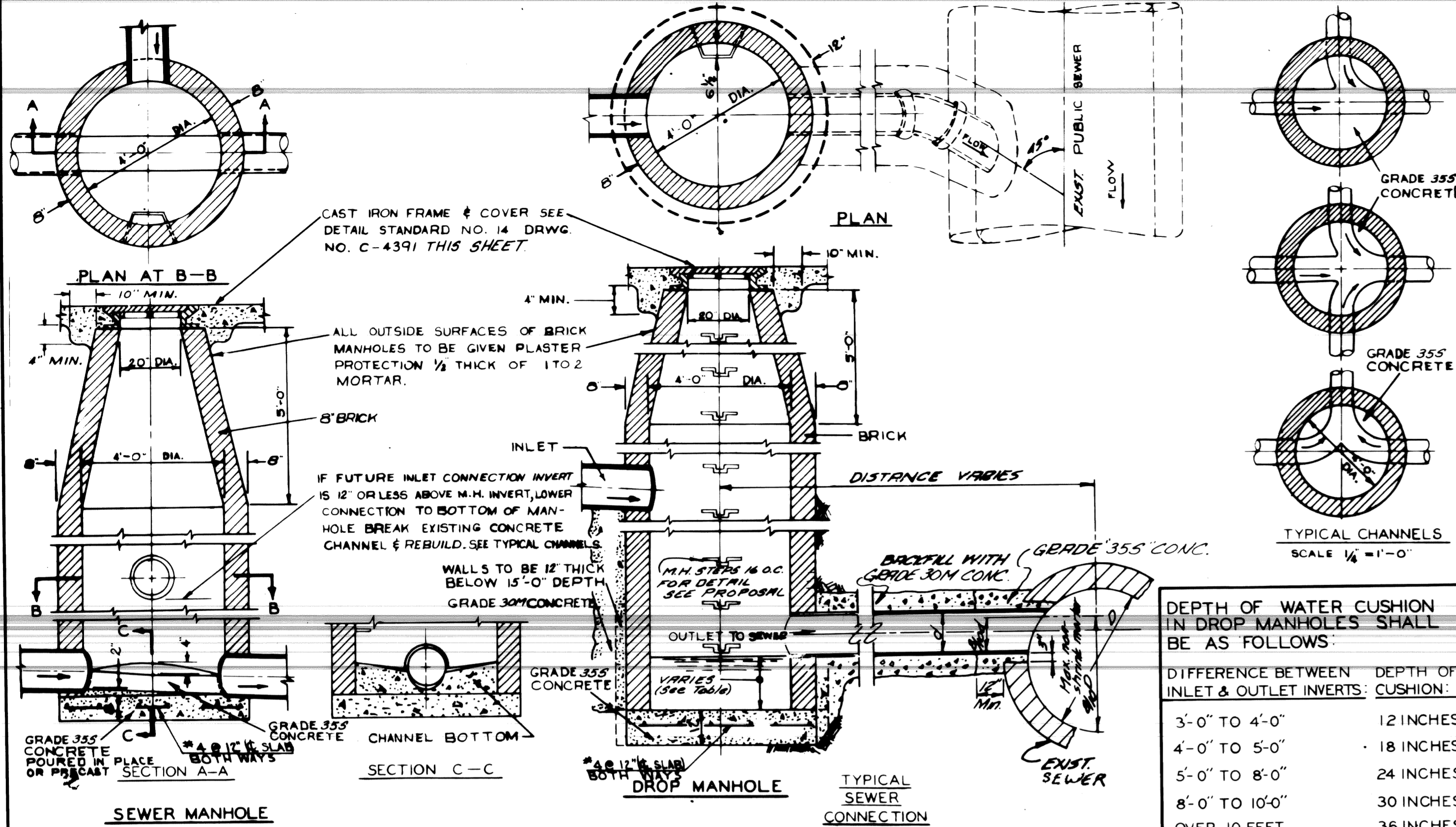
- The materials & workmanship shall be in accordance with the current standard specifications.
- Center of Catch Basin shall be 20 inches from back of curb.
- All sizes & flow lines of pipe, and elevations for top & bottom of structures shall be determined from the plans or construction requirements.
The bell shall be removed from the first length of outlet pipe projecting through the wall of the structures.
When any structure is constructed of precast concrete or concrete block, the top of the masonry shall be left sufficiently low to permit proper adjustment of cover to grade by the use of mortar or bricks as directed by the Engineer.
- A Trap, as detailed on this sheet, shall be placed where called for in the outlet sewer line of Catch Basins "B".
This trap shall be set into the masonry wall as shown on the detail. The space between the faces of the wall & the trap shall be completely filled with cement, mortar or concrete, so as to hold trap securely in place.
The Traps will be paid for separately of the Contract Unit Price each, which price shall include the extra catch basin construction required and for furnishing and installing the trap.
- A plaster coat of mortar 1/2 inch in thickness shall be applied to the outer surface of the structure as shown. A 1/2 inch cement plaster coat shall be placed on the inside of all sumps.
- Contractor shall verify elevations of existing utilities to enable construction to indicated elevations shown on drawings. If necessary, invert elevations shown on the drawings may be altered in the field to clear existing utilities. Such alterations, upward or downward, shall be at no change in contract price.
- When precast concrete pipe sections are used for catch basins, either a section of the inlet and outlet pipes or an opening or eye for the inlet & outlet pipes shall be cast into the wall of the catch basin pipe when it is being manufactured. Eyes in precast pipe sections shall be furnished to accommodate a flexible joint connection, such as Press-Seal by Press Seal Gasket Corp., Res-Seal by Scales Mfg. Corp., or Link-Seal by Thunderline Corp.
- Pay limit for sewers shall be inside faces of structures unless otherwise noted.

DESIGNED BY	APPROVED:
DRAWN BY	<i>John Erickson</i>
TRACED BY	ENGINEER OF STREETS
CHECKED BY	<i>W. A. Bannard</i>
	HEAD CIVIL ENGINEER

CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE

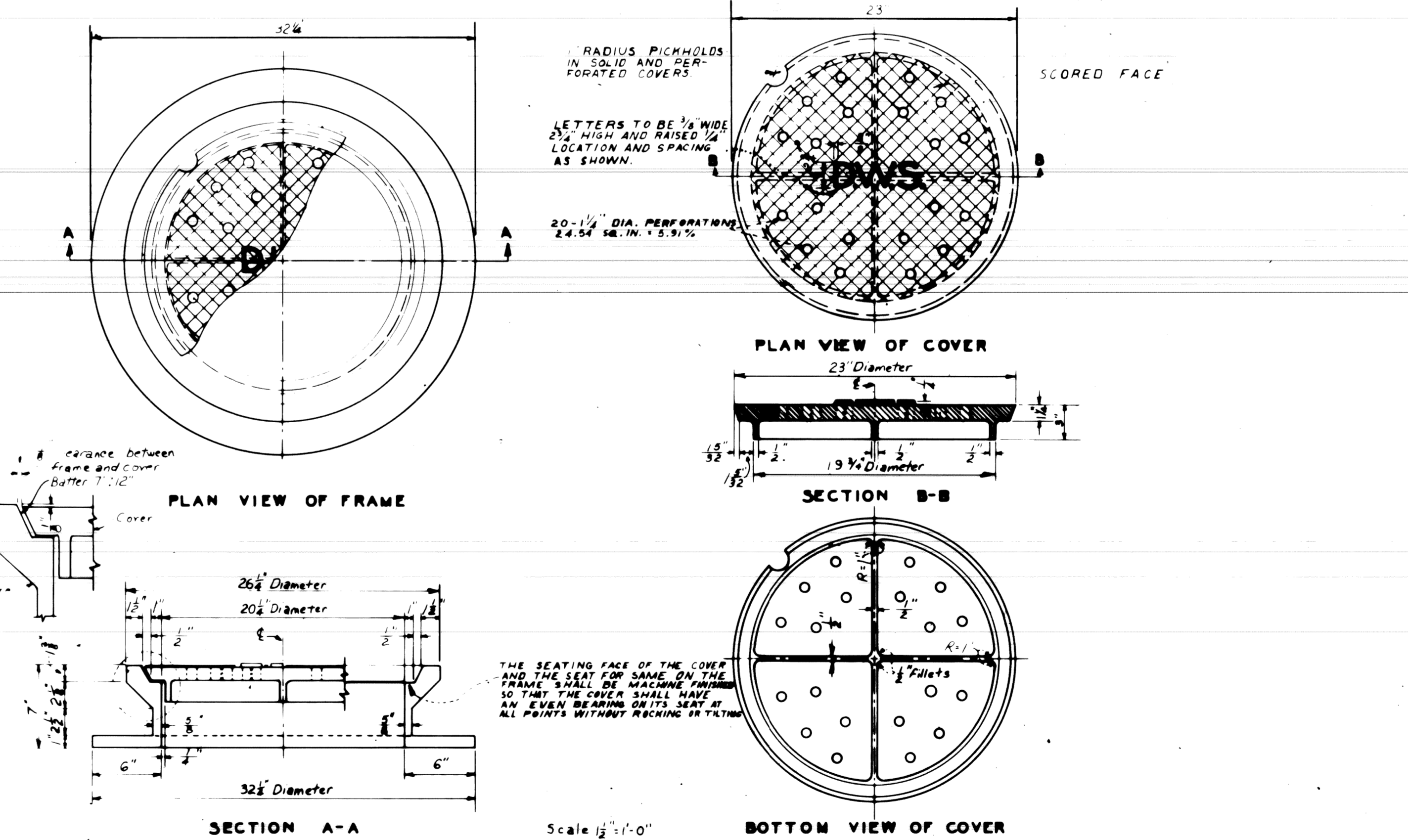
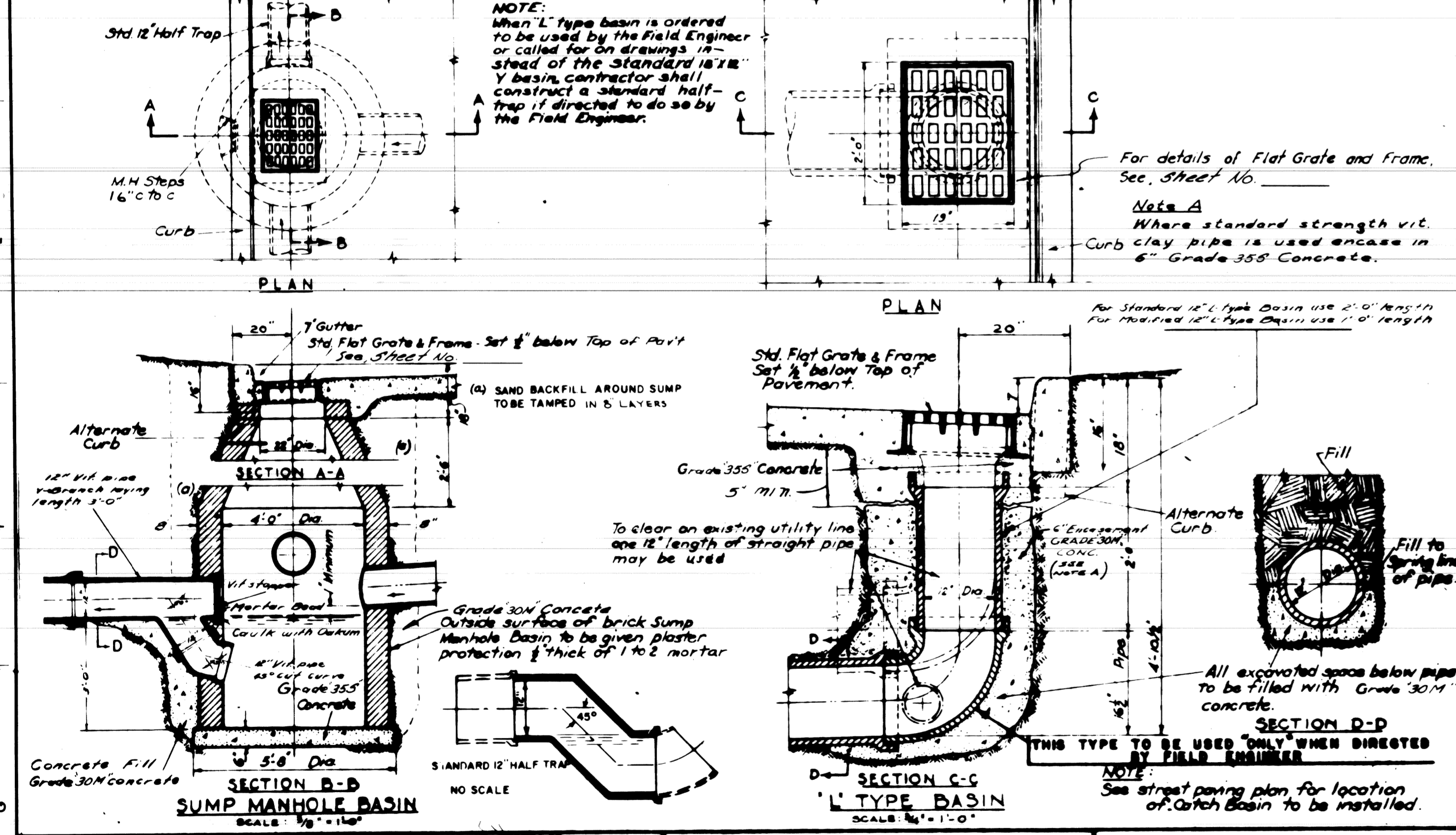
DETAILS OF STANDARD
CATCH BASINS "A" & "B"
AND FLAT GRATE & FRAME

SHEET	24 OF 71 SHEETS
CONTRACT NO.	17176A
ASSIGN. NO.	
DATE	AUG., 1984



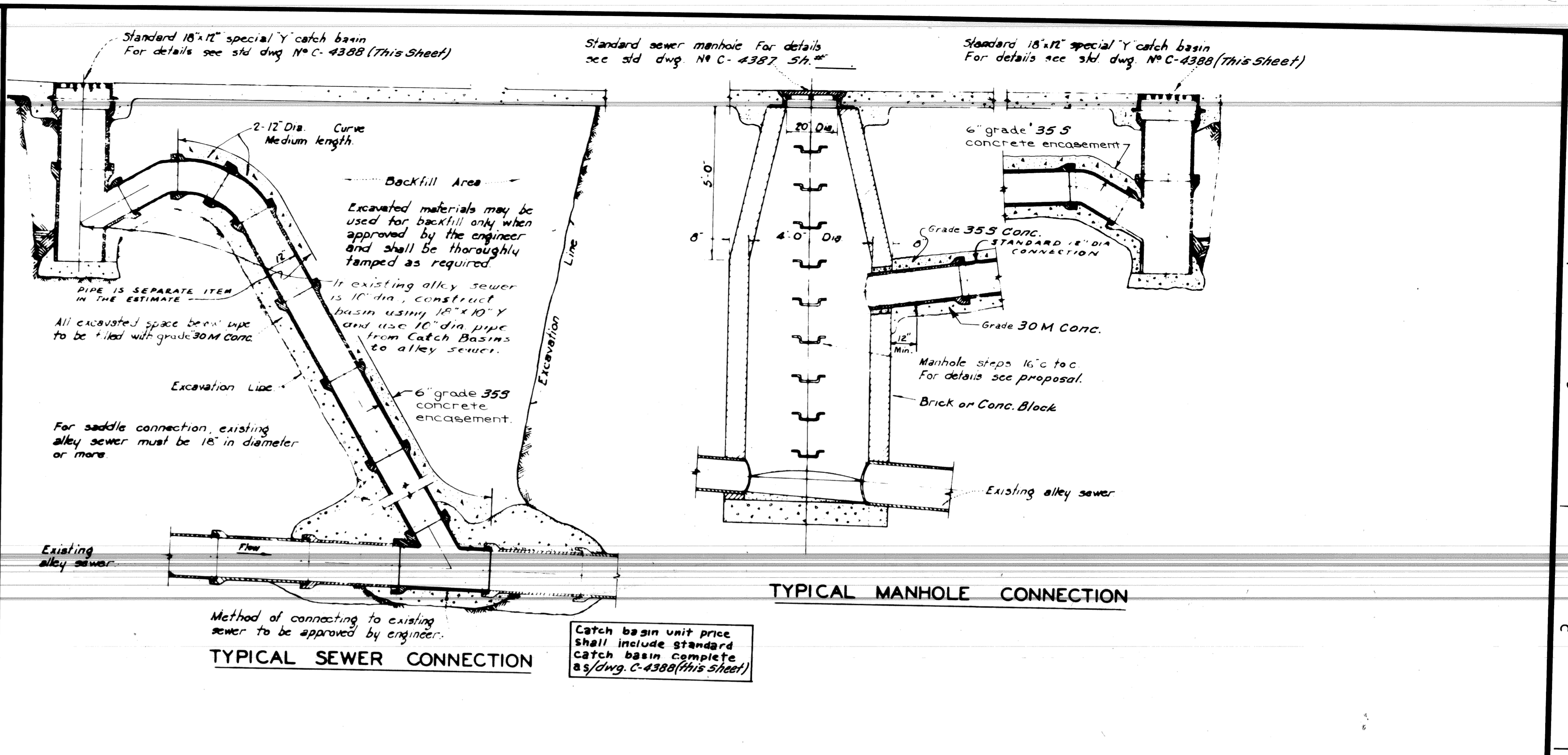
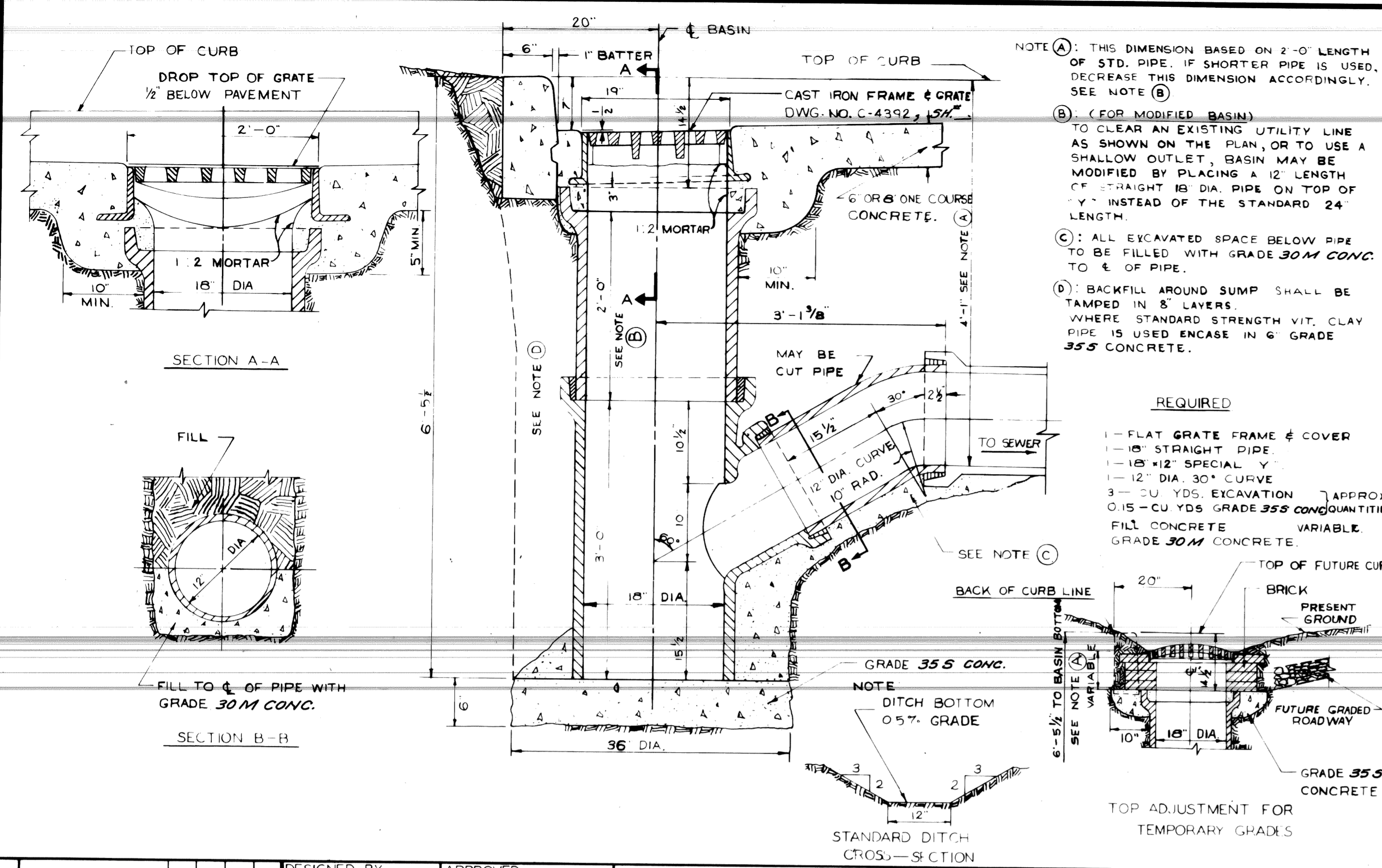
DESIGNED BY	APPROVED BY	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEER'S OFFICE BUREAU OF DESIGN
DRAWN BY	ENGINEER OF STREETS	
TRACED BY	ASST. CITY ENGINEER	DETAILS OF STANDARD SEWER & DROP MANHOLES
CHECKED BY	CITY ENGINEER	
REVISED M.H. STEPS	J.C.G.D. 11/1/82	BOOK NO.
DESCRIPTION	DRWG. NO. DATE	PG.
REVISIONS		SCALE 3/8" = 1'-0" DATE: 8-27-66

DESIGNED BY	APPROVED BY	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEER'S OFFICE BUREAU OF DESIGN
DRAWN BY	ENGINEER OF STREETS	
TRACED BY	ASST. CITY ENGINEER	STD SEWER MANHOLES CONSTRUCTION ALTERNATES
CHECKED BY	CITY ENGINEER	
REVISED M.H. STEPS	J.C.G.D. 11/1/82	BOOK NO.
DESCRIPTION	DRWG. NO. DATE	PG.
REVISIONS		SCALE NONE DATE: 8-28-68



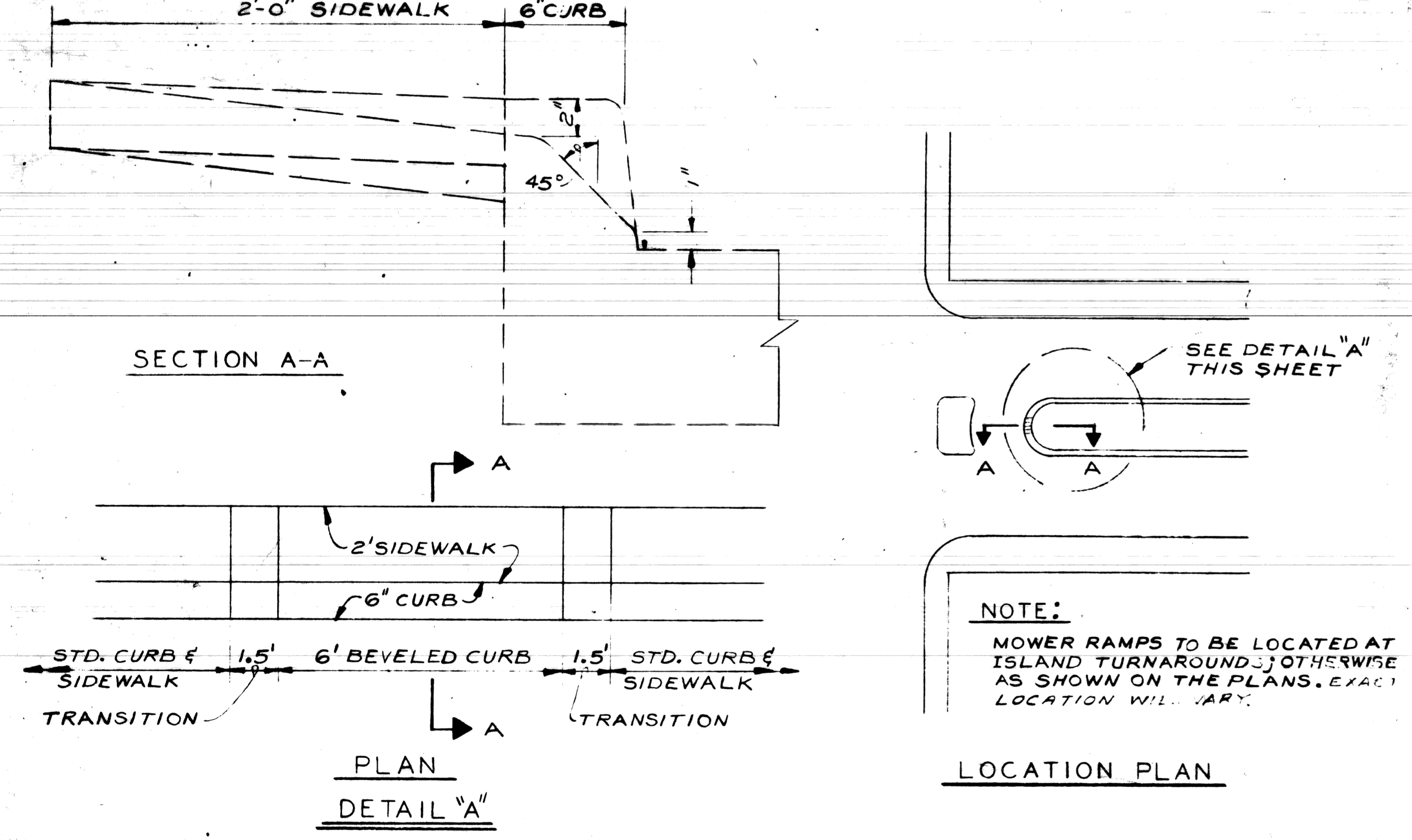
DESIGNED BY	APPROVED BY	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEER'S OFFICE BUREAU OF DESIGN
DRAWN BY	ENGINEER OF STREETS	
TRACED BY	ASST. CITY ENGINEER	DETAILS OF STANDARD SEWER & DROP MANHOLES
CHECKED BY	CITY ENGINEER	
REVISED M.H. STEPS	J.C.G.D. 11/1/82	BOOK NO.
DESCRIPTION	DRWG. NO. DATE	PG.
REVISIONS		SCALE 3/8" = 1'-0" DATE: 8-27-66

DESIGNED BY	APPROVED BY	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEER'S OFFICE BUREAU OF DESIGN
DRAWN BY	ENGINEER OF STREETS	
TRACED BY	ASST. CITY ENGINEER	DETAILS OF STANDARD MANHOLE FRAME AND COVER
CHECKED BY	CITY ENGINEER	
REVISED M.H. STEPS	J.C.G.D. 11/1/82	BOOK NO.
DESCRIPTION	DRWG. NO. DATE	PG.
REVISIONS		SCALE: AS SHOWN DATE: 10-8-1966



DESIGNED BY	APPROVED	CITY OF DETROIT	STANDARD DESIGN
DRAWN BY <i>W. Jones</i>	ENGINEER OF STREETS	DEPARTMENT OF PUBLIC WORKS	18" x 12" SPECIAL 'Y' CATCH BASIN
TRACED BY	ASST. CITY ENGINEER	CITY ENGINEER'S OFFICE	TO BE USED UNLESS DIRECTED
CHECKED BY	CITY ENGINEER	BUREAU OF DESIGN	OTHERWISE BY FIELD ENGINEER
BOOK NO.	SCALE	DATE	DWG. NO.
	1" = 1'-0"	8-19-66	C-4388

DESIGNED BY	APPROVED	CITY OF DETROIT	ALLEY INSTALLATION
DRAWN BY <i>W. Jones</i>	ENGINEER OF STREETS	DEPARTMENT OF PUBLIC WORKS	STANDARD 18" x 12" SPECIAL
TRACED BY	ASST. CITY ENGINEER	CITY ENGINEER'S OFFICE	'Y' CATCH BASIN.
CHECKED BY	CITY ENGINEER	BUREAU OF DESIGN	
BOOK NO.	SCALE	DATE	DWG. NO.
	1" = 1'-0"	8-16-59	C-4399

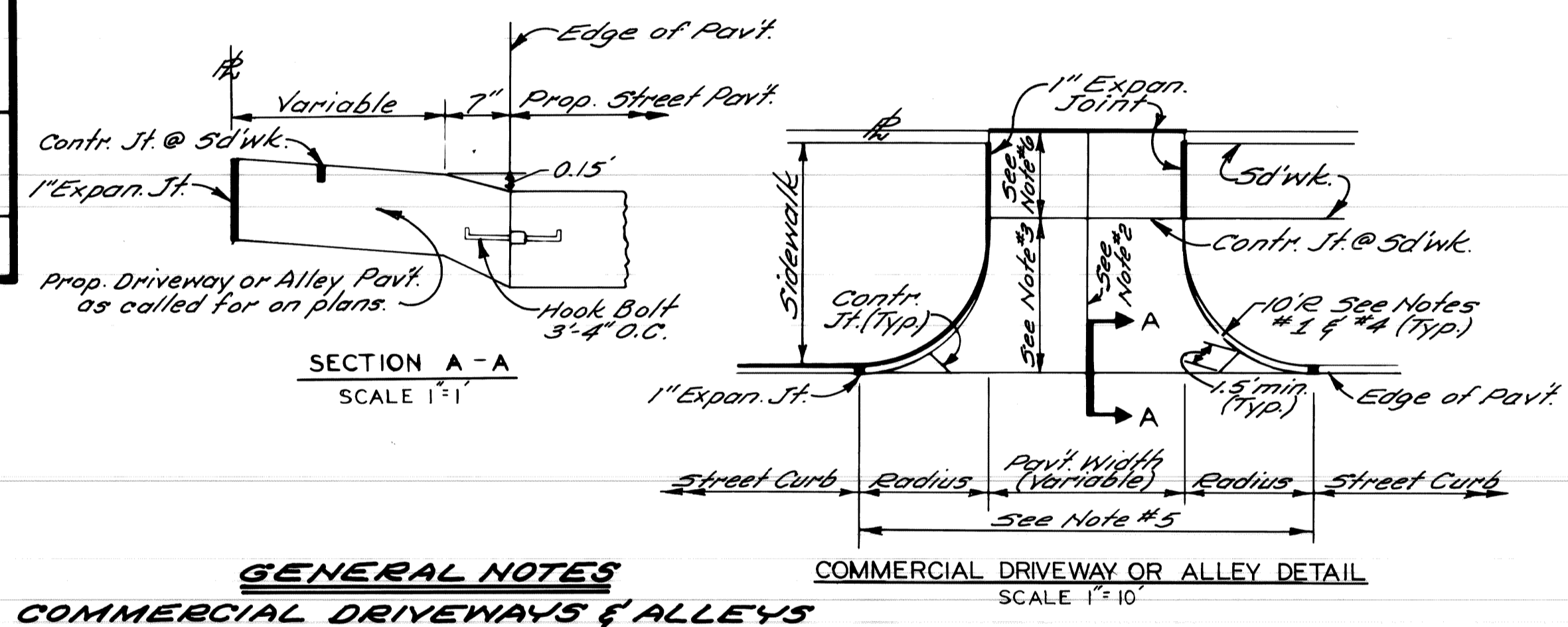


DESIGNED BY	APPROVED	CITY OF DETROIT	1975
DRAWN BY JONES	ENGINEER OF STREETS	CITY ENGINEERING DEPARTMENT	SPECIAL MOWER RAMP
TRACED BY	DEPUTY DIRECTOR		
CHECKED BY	DIRECTOR		
BOOK NO.	SCALE	DATE	DWG. NO.
	NONE	JULY 22, 1974	C-5239

CITY OF DETROIT		M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE.		SHEET 28 OF 71 SHEETS	
FOR		SPECIAL DETAILS		CONTRACT No. 17176A	
				DRWG. No.	
				DATE AUG., 1984	

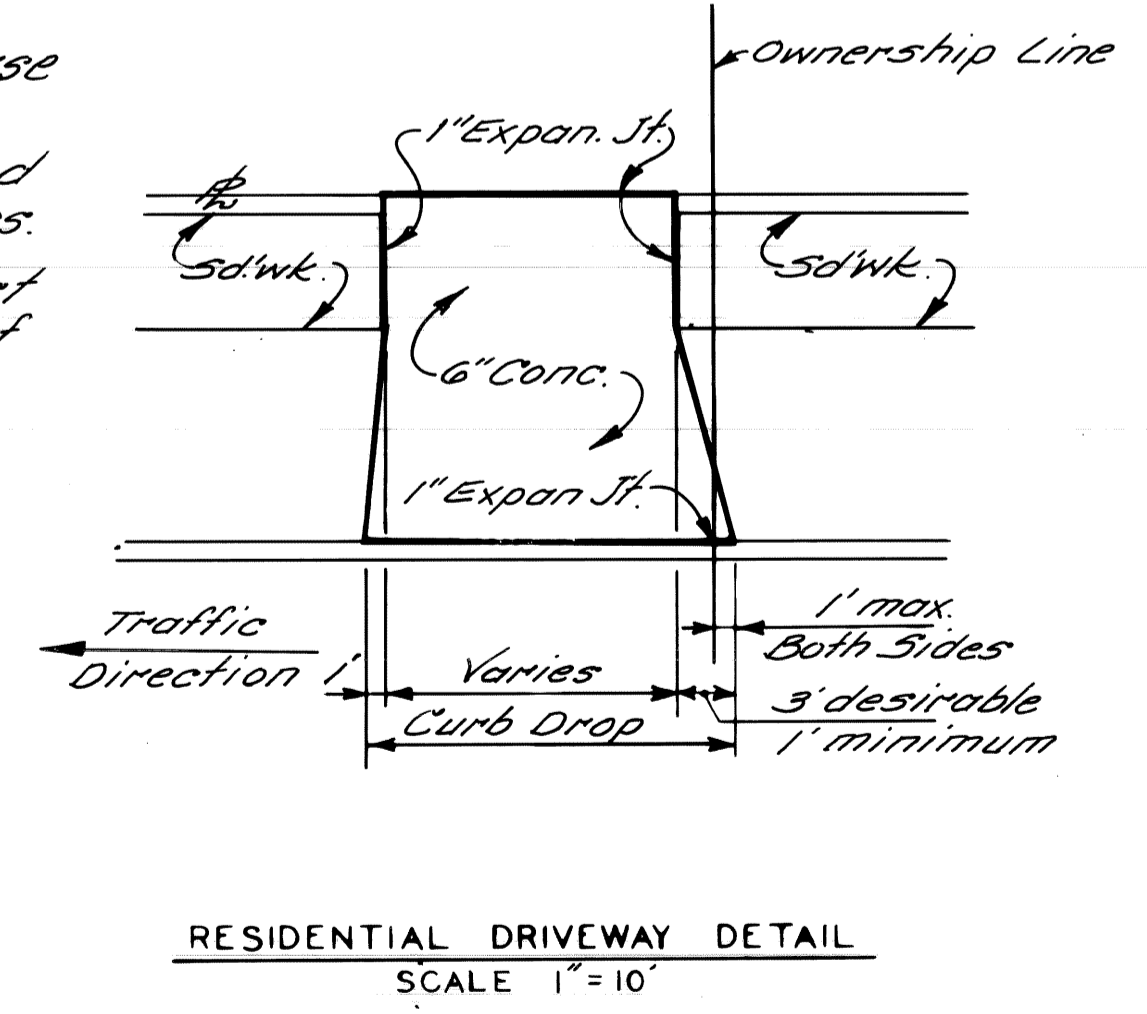
COLOR	SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
BLACK	---	PROPERTY AND LOT LINES	○	SEWER MANHOLE	#	STREET SIGN POST
	---	PROPOSED CURB LINES AND HEADERS AT RETURNS	⊙	P.L.C. HANDHOLE	□	CATCH BASIN
	---	EXISTING CURB LINES OR EDGE OF PAVEMENT	⊕	WATER MANHOLE OR GRATE	(W)	WATER METER
	---	PROPOSED CURB OR PAVING ON INTERSECTING STREET	⊖	GAS MANHOLE OR GRATE	(-)	WATER SHUT OFF
	---	PROPOSED SEWER TO CATCH BASINS	⊗	WESTERN UNION MANHOLE	(+)	GAS SHUT OFF
	---	EXISTING LATERAL SEWERS	⊙	MICHIGAN BELL MANHOLE	⊞	GAS DRIP
	---	EXISTING PUBLIC SEWERS	⊕	EDISON STEAM MANHOLE	□	DRAIN OR VENT
	---	EXISTING GAS LINES	⊖	EDISON ELECTRIC MANHOLE	⊞	GUY POLE
	---	EXISTING WATER LINES	⊕	FIRE DEPT. MANHOLE	+	MAIL BOX
	---	PROPOSED C.B. M.H. & INLET SEWER	⊖	POLICE DEPT. MANHOLE	⊞	AWAY OR COAL CHUTE
	---	EXISTING C.B. M.H. & INLET SEWERS	⊕	D.S.R. MANHOLE	⊙	FLAN POLE
	---	PROPOSED C.B. M.H. & INLET SEWERS ON INTERSECTING STREETS	⊖	FIRE DEPT. STANDARD HYDRANT	⊞	SPRINKLER BOX
	---		⊕	FIRE DEPT. HIGH PRESSURE HYDRANT	⊞	DEAD MANHOLE
	---		⊖	FIRE DEPT. CALL BOX	⊞	TREE
	---		⊕	FIRE DEPT. HIGH PRESSURE MANHOLE	⊞	P.L.C. 4' D. MANHOLE FILLED WITH SAND
	---		⊖	POLICE DEPT. CALL BOX	⊞	TRACK DRAIN
	---		⊕	P.L.C. LIGHT POLE	+	TRAFFIC SIGN POST
	---		⊖	P.L.C. POLE	(2)	TRAFFIC SIGNAL LIGHT
	---		⊕	TELEPHONE POLE	⊙	TRAFFIC SIGNAL ON LIGHT POST
	---		⊖	EDISON POLE	⊞	D.S.R. DANGER PLATE
	---		⊕	TRAFFIC CONTROL BOX	⊞	R.R. SIGNAL CONTROL BOX
	---		---	DETROIT EDISON ELECTRIC CONDUIT		
	---		---	DETROIT EDISON STEAM CONDUIT		
	---		---	WESTERN UNION DUCTS		
	---		---	MICHIGAN BELL TELEPHONE DUCTS		
	---		---	P.L.C. CONDUITS		
	---		---	DETROIT FIRE OR POLICE DEPT. CONDUITS (P.L.C. COMMUNICATIONS)		

DESIGNED BY	DRIVEN BY	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEER'S OFFICE BUREAU OF DESIGN	STANDARD SYMBOLS FOR UTILITIES
DRAWN BY T. KANDON	CHECKED BY J. H. KERR	PAVING DETAIL NO. 25	SCALE: NO. SCALE
DATE: 10/1/83	DATE: 10/1/83	NO. C-902-A	

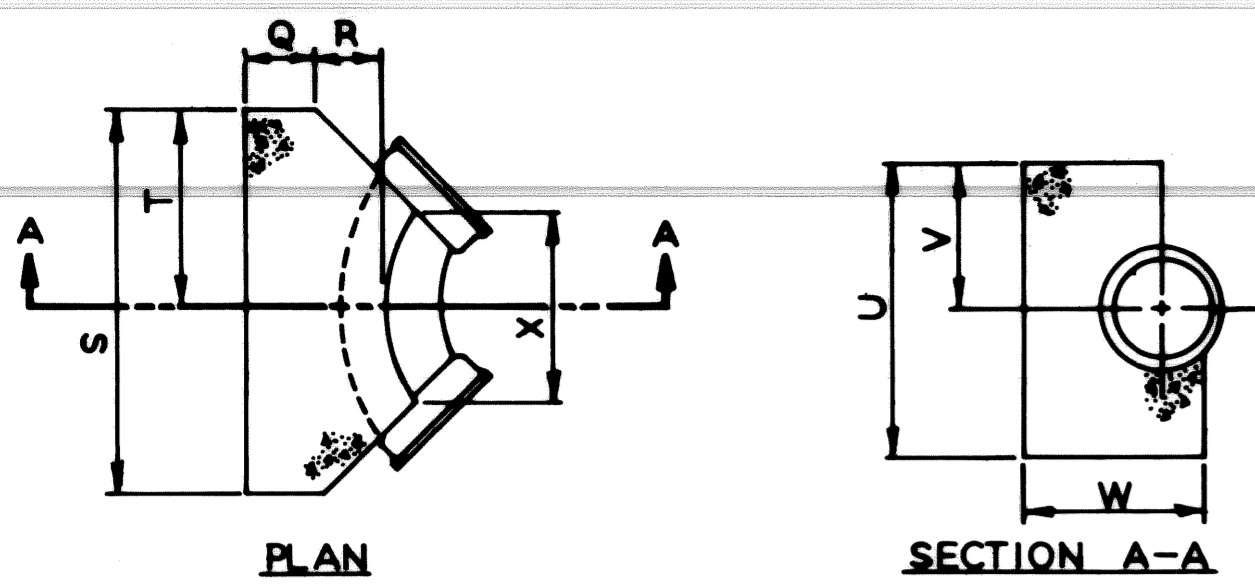


**GENERAL NOTES
COMMERCIAL DRIVEWAYS & ALLEYS**

1. Transition edge of driveway or alley from curb at the street to no curb at the end of the driveway or alley radius.
2. Where driveway or alley exceeds 15' in width a contraction joint shall be placed longitudinally along it.
3. When distance exceeds 15', a transverse contraction joint will be required.
4. Radius 10' unless otherwise directed by the Engineer, or as shown on plans.
5. All work & materials req'd to construct the driveway or alley between the end of returns will be paid for as "Concrete Pavement" of the specified thickness.
6. Minimum & Maximum Slopes of 1/4" ft. & 3/4" ft. shall be used in the sidewalk area.



CITY OF DETROIT CITY ENGINEERING DIVISION E. P. M. D.	M.L. KING JR. BLVD. RECONSTRUCTION WEST GRAND BLVD. TO LINCOLN AVE. SPECIAL STANDARDS & SPECIAL DETAILS	SHEET 27 OF 71 SHEETS CONTRACT NO. 17176A ASSIGNMENT NO. A.O. DATE AUG., 1984
DESIGNED BY: J.R.D. DRAWN BY: J.R.D. CHECKED BY: N.B.	APPROVED: [Signature] ENGINEER OF EXPRESSWAYS	

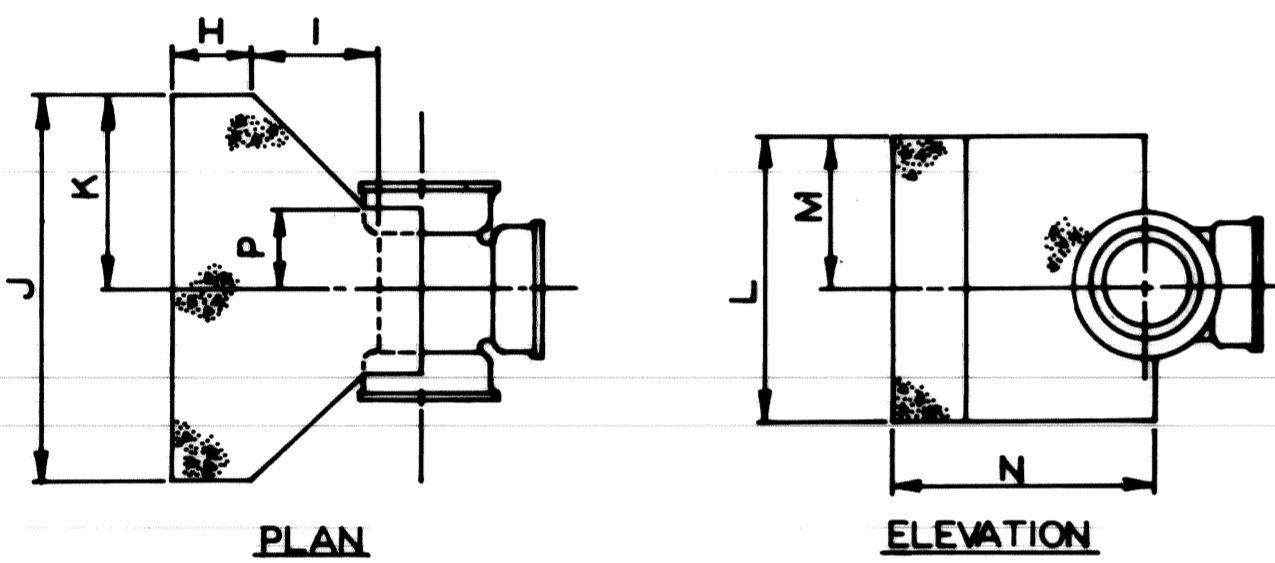


SCHEDULE OF THRUST BLOCK DIMENSIONS

BEND SIZE	Q	R	S	T	U	V	W	X
6"-22 1/2" 445°	0'-9"	1'-0"	2'-0"	1'-0"	1'-6"	0'-9"	1'-11"	1'-4"
6"-90°	0'-9"	1'-0"	3'-0"	1'-6"	2'-0"	1'-0"	1'-11"	1'-2"
8"-22 1/2" 445°	0'-9"	1'-0"	2'-4"	1'-2"	2'-0"	1'-0"	1'-11"	1'-4"
8"-90°	0'-9"	1'-0"	3'-4"	1'-8"	2'-6"	1'-3"	1'-11"	1'-2"
12"-22 1/2" 445°	0'-9"	1'-0"	2'-6"	1'-3"	2'-0"	1'-0"	2'-0"	1'-4"
12"-45°	0'-9"	1'-4"	3'-6"	1'-9"	2'-6"	1'-3"	2'-4"	1'-4"
12"-90°	0'-9"	1'-4"	5'-6"	2'-9"	3'-0"	1'-6"	2'-4"	1'-8"
16"-22 1/2" 445°	1'-0"	1'-8"	3'-4"	1'-8"	2'-6"	1'-3"	3'-0"	1'-2"
16"-45°	1'-0"	1'-8"	5'-4"	2'-8"	3'-0"	1'-6"	3'-0"	2'-6"
16"-90°	1'-0"	1'-8"	6'-0"	3'-0"	5'-0"	2'-6"	3'-0"	2'-8"

DETAIL OF STANDARD THRUST BLOCKS FOR BENDS
Not to Scale

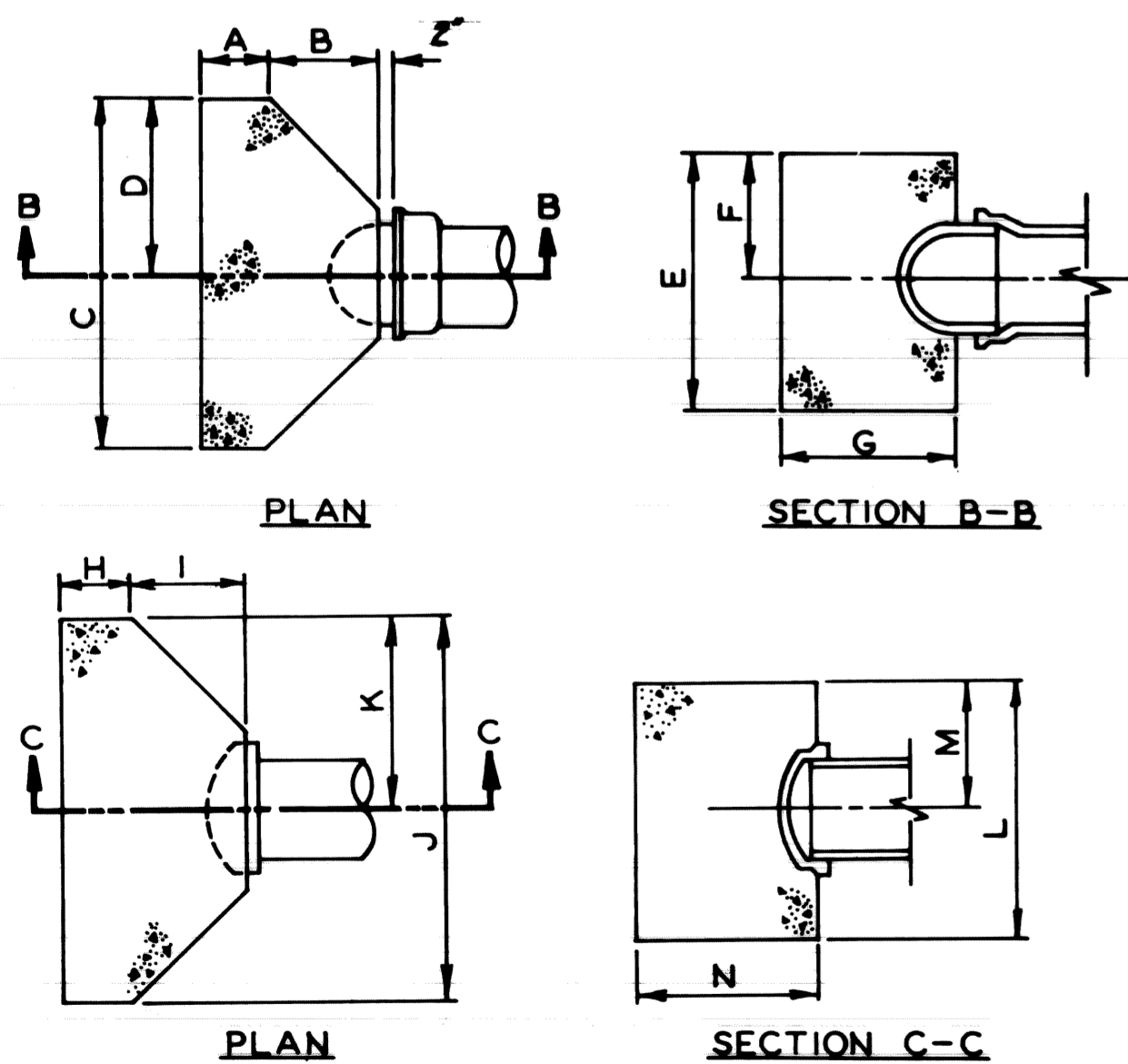
NOTES
The bearing face of all Thrust Blocks shall be poured against undisturbed earth.
Thrust Blocks when required for vertical bends are shown by special details.



SCHEDULE OF THRUST BLOCK DIMENSIONS

TEE SIZE	H	I	J	K	L	M	N	P
6" x 6"	0'-9"	1'-0"	2'-6"	1'-3"	1'-6"	0'-9"	2'-3"	0'-6"
8" x 8"	0'-9"	1'-0"	3'-0"	1'-6"	2'-0"	1'-0"	2'-3"	0'-6"
10" x 10"	0'-9"	1'-2"	3'-6"	1'-9"	2'-6"	1'-3"	2'-7"	0'-7"
12" x 8"	0'-9"	1'-0"	3'-0"	1'-6"	2'-0"	1'-0"	2'-5"	0'-6"
12" x 12"	0'-9"	1'-4"	4'-0"	2'-0"	3'-0"	1'-6"	2'-9"	0'-8"
16" x 8"	0'-9"	1'-0"	3'-0"	1'-6"	2'-0"	1'-0"	2'-9"	0'-6"
16" x 12"	0'-9"	1'-4"	4'-0"	2'-0"	3'-0"	1'-6"	3'-1"	0'-8"
16" x 16"	1'-0"	1'-8"	5'-0"	2'-6"	4'-0"	2'-0"	3'-8"	0'-11"

DETAIL OF STANDARD THRUST BLOCKS FOR TEES
Not to Scale



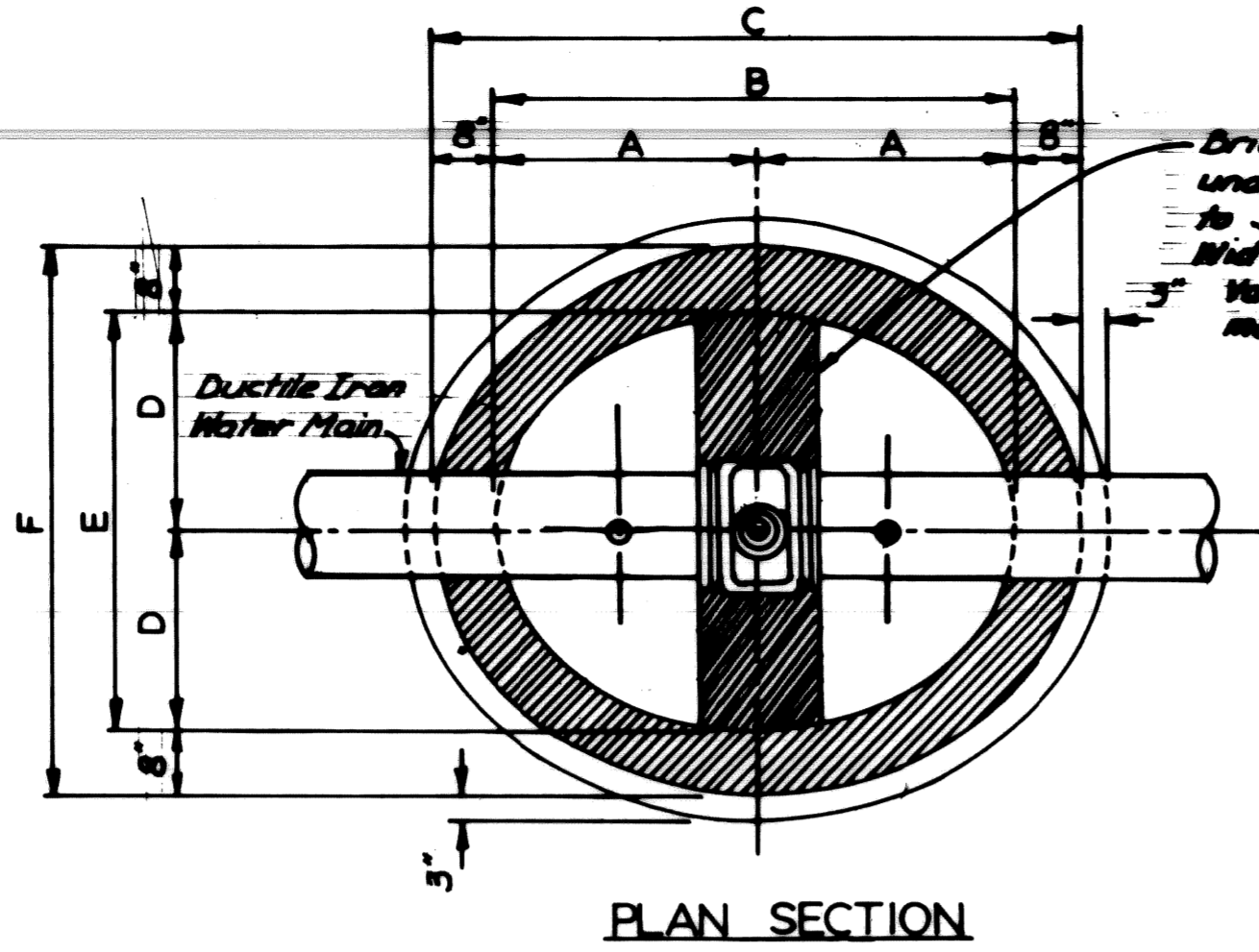
SCHEDULE OF THRUST BLOCK DIMENSIONS

PLUG SIZE	A	B	C	D	E	F	G
6"	0'-9"	1'-2"	3'-0"	1'-6"	2'-0"	1'-0"	1'-11"
8"	0'-9"	1'-2"	3'-0"	1'-6"	2'-0"	1'-0"	1'-11"
12"	0'-9"	1'-4"	4'-0"	2'-0"	3'-0"	1'-6"	2'-1"
16"	1'-0"	1'-8"	5'-0"	2'-6"	4'-0"	2'-0"	2'-8"

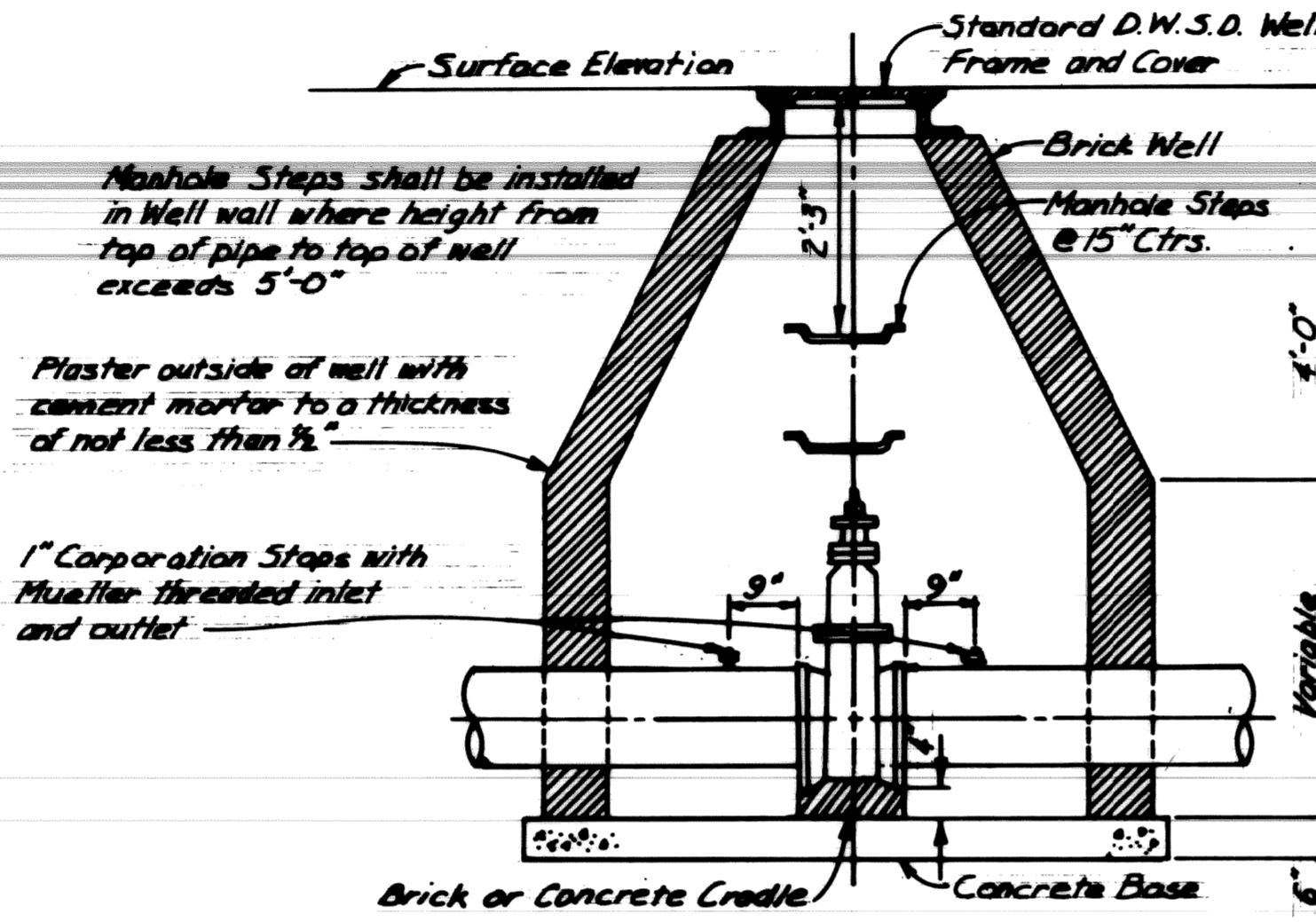
SCHEDULE OF THRUST BLOCK DIMENSIONS

CAP SIZE	H	I	J	K	L	M	N
6"	0'-9"	1'-2"	3'-0"	1'-6"	2'-0"	1'-0"	1'-11"
8"	0'-9"	1'-2"	3'-0"	1'-6"	2'-0"	1'-0"	1'-11"
12"	0'-9"	1'-4"	4'-0"	2'-0"	3'-0"	1'-6"	2'-1"
16"	1'-0"	1'-8"	5'-0"	2'-6"	4'-0"	2'-0"	2'-8"

DETAIL OF STANDARD THRUST BLOCKS FOR PLUGS AND CAPS
Not to Scale



PLAN SECTION

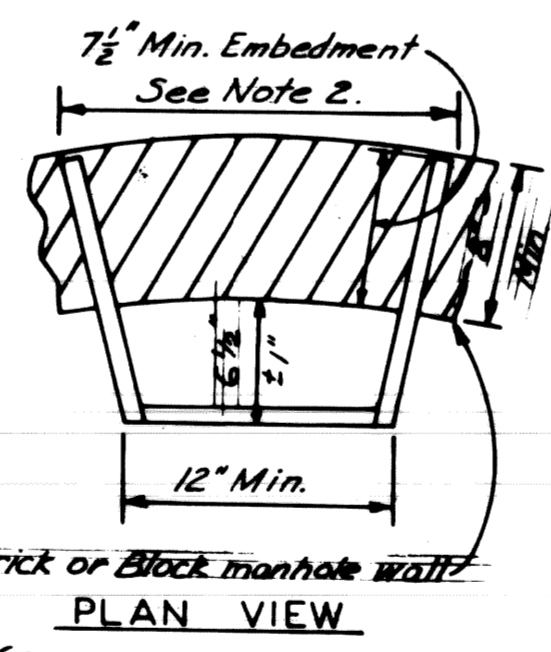


SECTION THROUGH GATE WELL

SCHEDULE OF GATE WELL DIMENSIONS

SIZE	A	B	C	D	E	F
6"	2'-6"	5'-0"	6'-4"	2'-0"	4'-0"	5'-4"
8"	2'-6"	5'-0"	6'-4"	2'-0"	4'-0"	5'-4"
12"	2'-9"	5'-6"	6'-10"	2'-3"	4'-6"	5'-10"
16"	3'-0"	6'-0"	7'-4"	2'-9"	5'-6"	6'-10"

DETAIL OF STANDARD GATE WELLS
Not to Scale



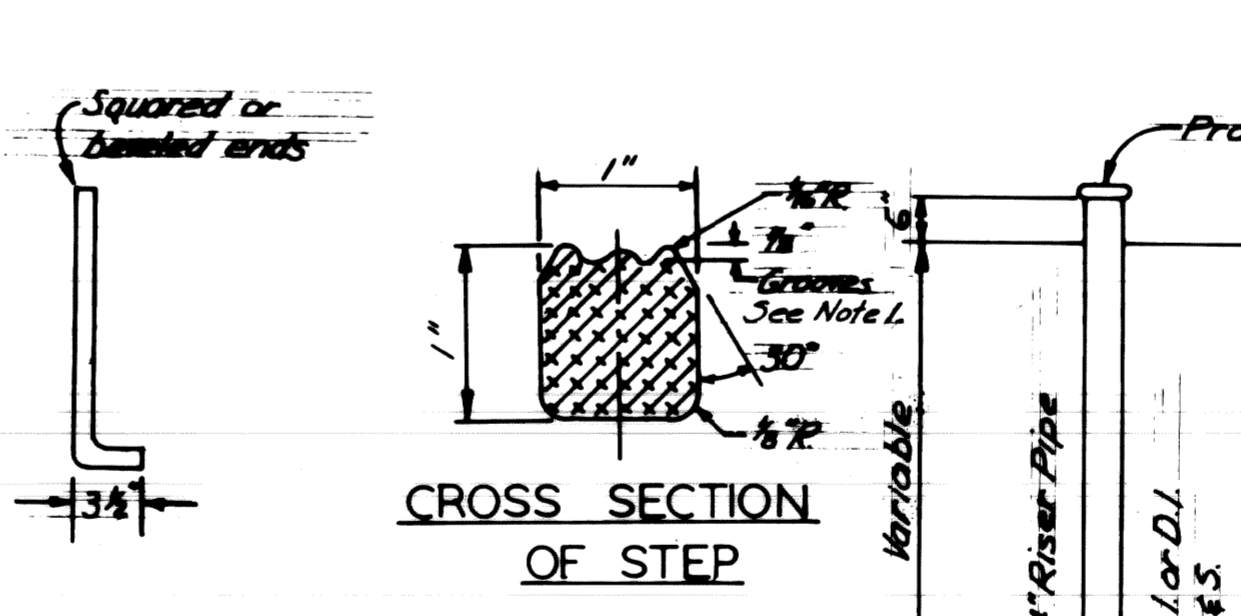
ALUMINUM MANHOLE STEP
Not to Scale

Note: Manhole steps shall conform to the requirements for "Aluminum Alloy Extruded Bars, Rods, Shapes and Tubes", A.S.T.M. B 221 (current), Alloy 6061, Temper T-6, or approved equal.

SCHEDULE OF STANDARD THRUST BLOCK DIMENSIONS FOR TAPPING VALVE

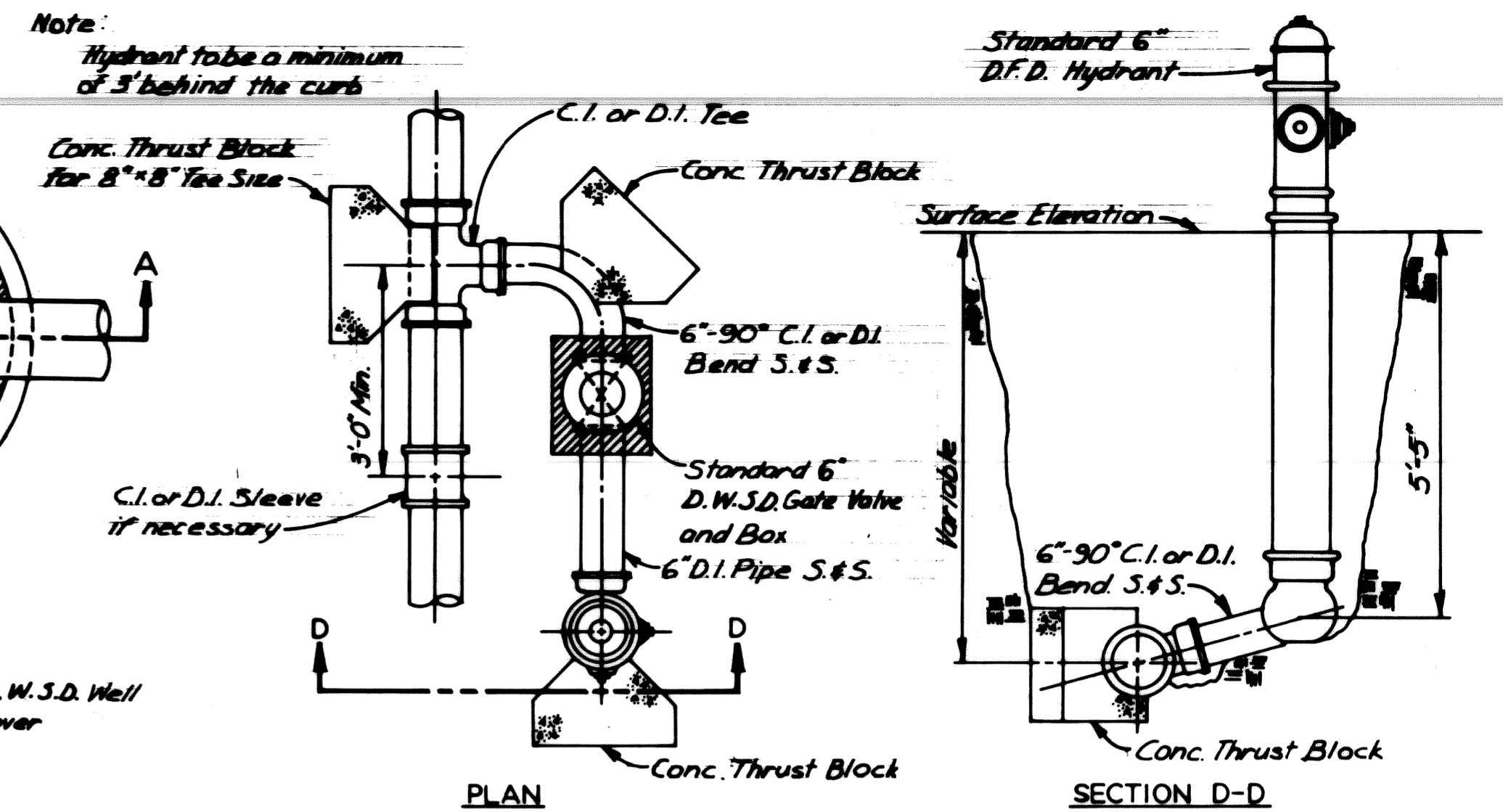
SIZE	A	B	C	E	F	G	H	J
8" x 8" 1'-6"				0'-7"				
12" x 8" 1'-9"				0'-7"				
16" x 8" 1'-11 1/2"				0'-7 1/2"				
12" x 12" 1'-8 1/2"	1'-6"	0'-9"	2'-3"	0'-7"	1'-9"	3'-6"	1'-6"	3'-0"
16" x 12" 1'-11 1/2"	1'-6"	0'-9"	2'-3"	0'-10 1/4"	1'-9"	3'-6"	1'-6"	3'-0"
16" x 16" 2'-2"	2'-7"	1'-0"	3'-7"	0'-10 1/4"	2'-11"	5'-10"	1'-8"	3'-4"

DETAIL OF STANDARD THRUST BLOCKS FOR TAPPING VALVES
Not to Scale

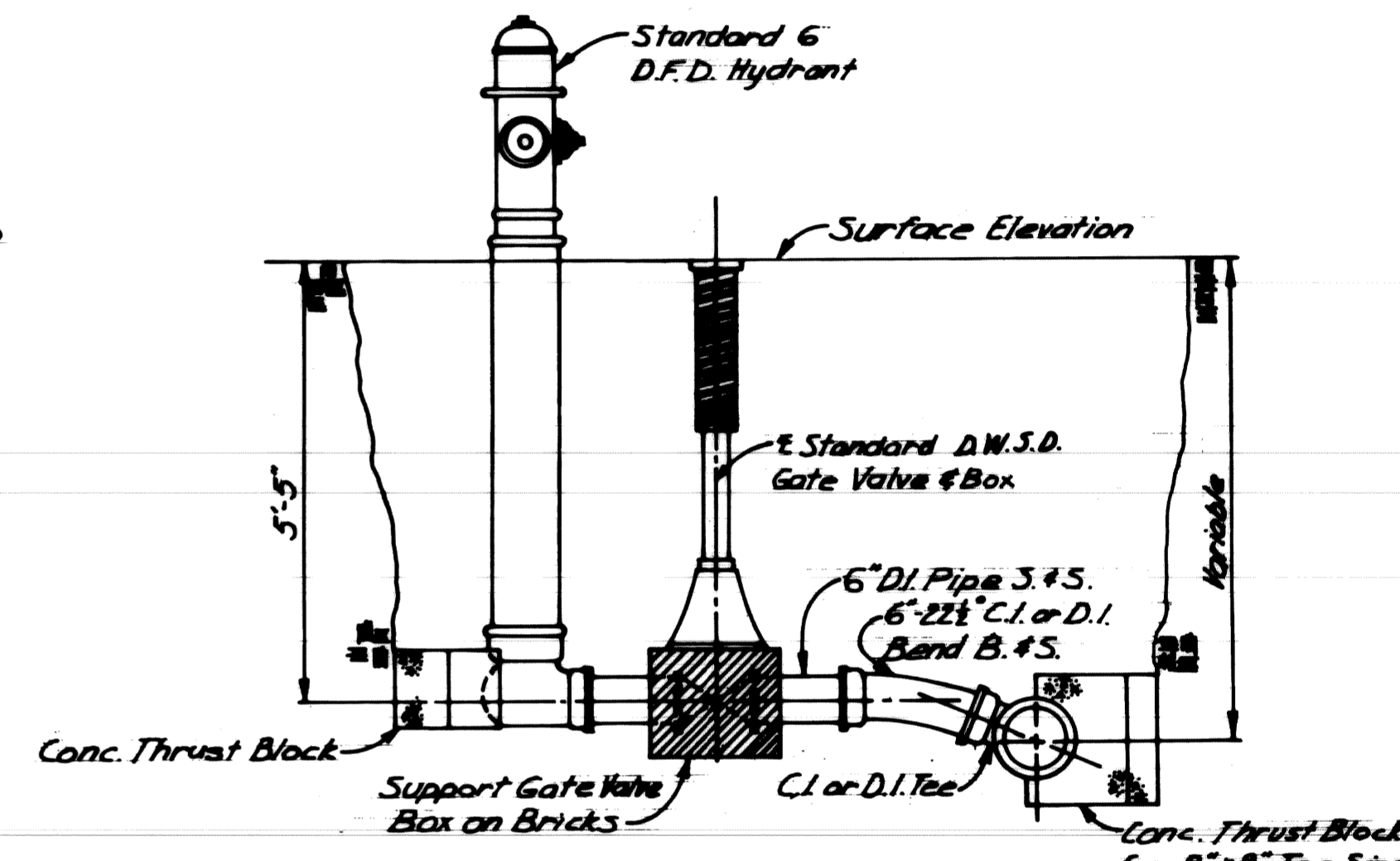


CROSS SECTION OF STEP

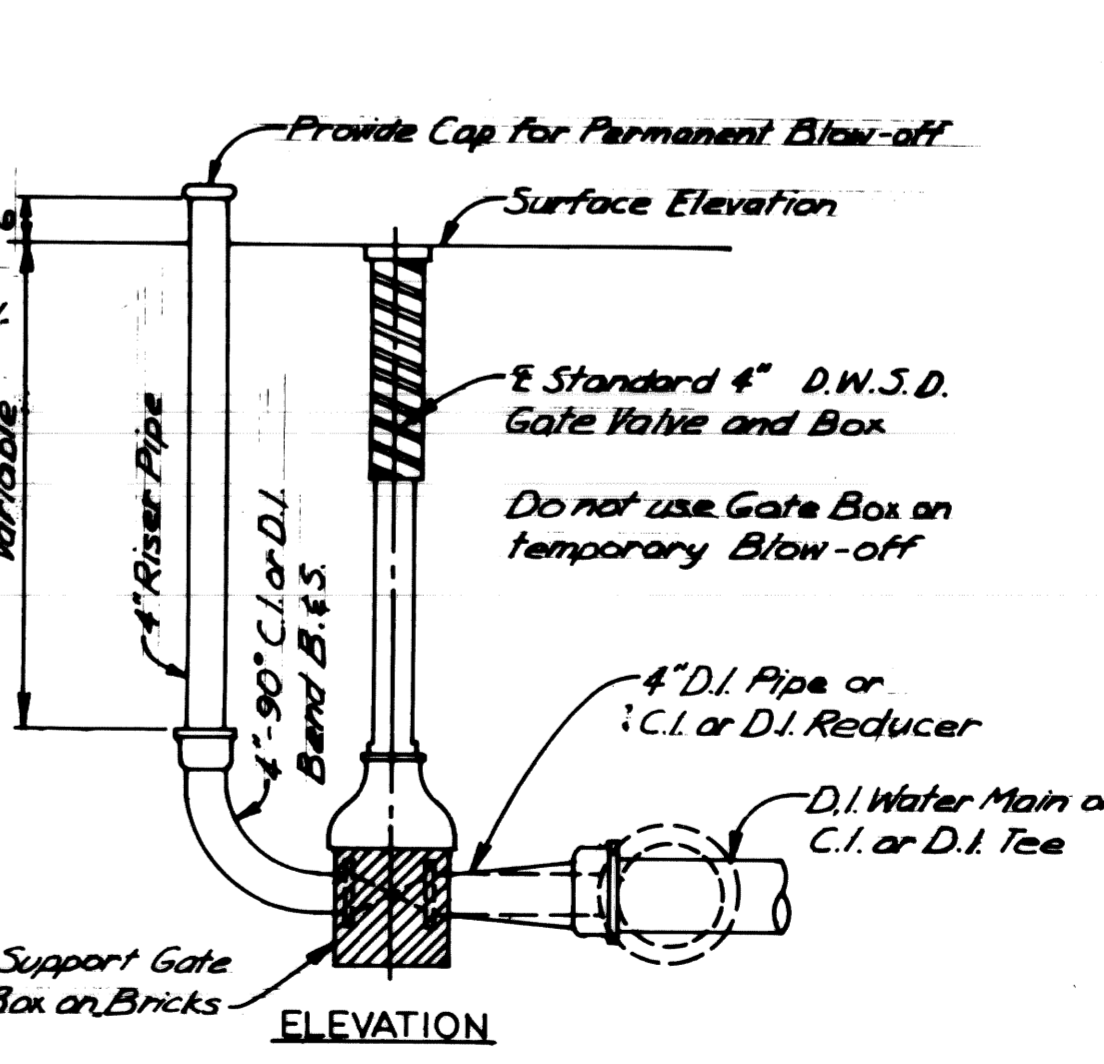
Note: 1. In lieu of grooves, alternate foot hold configurations will be considered for approval.
2. Flared legs shall be radial to the manhole wall.



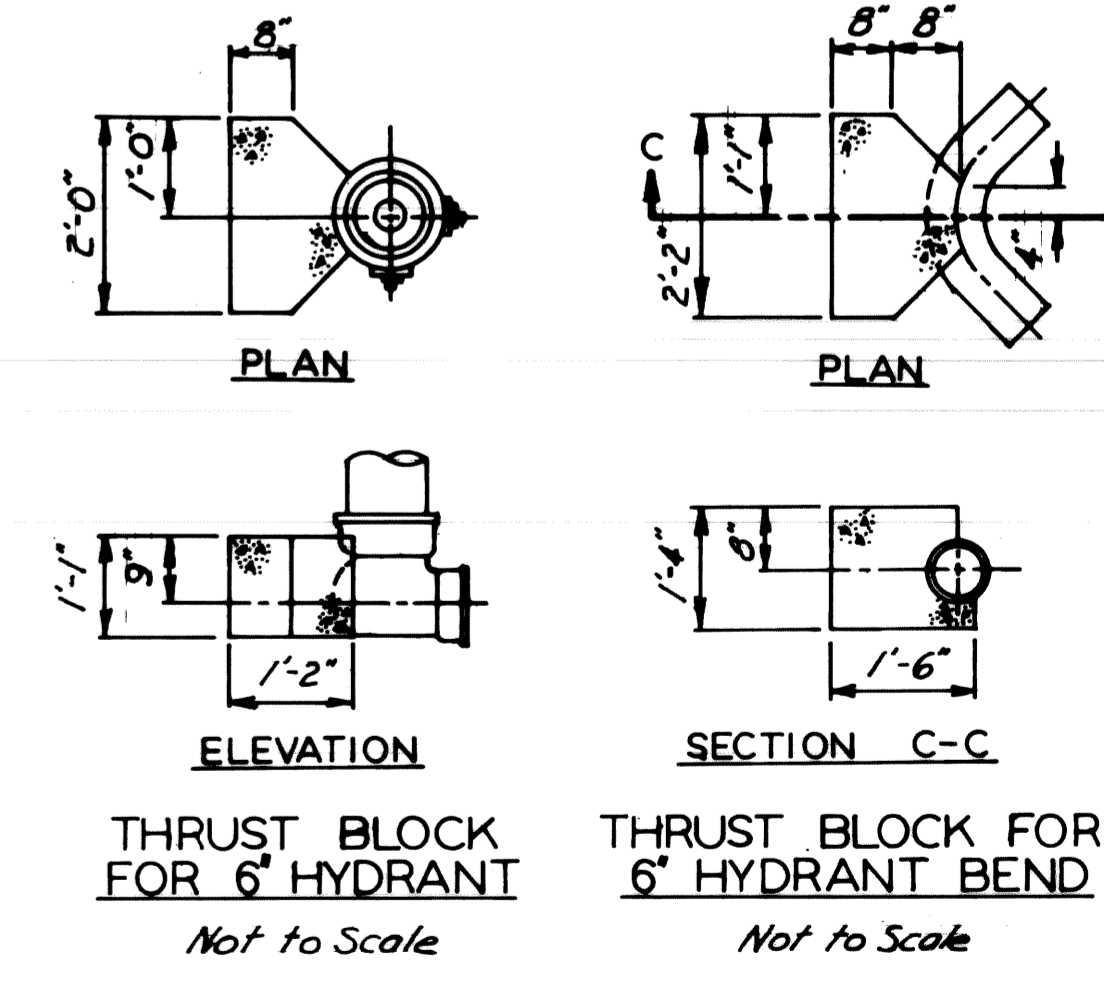
DETAIL OF 6" HYDRANT INSTALLATION OFF SET
Not to Scale



DETAIL OF 6" HYDRANT INSTALLATION STRAIGHT AWAY
Not to Scale



DETAIL OF 4" BLOW-OFF
Not to Scale



THRUST BLOCK FOR 6" HYDRANT
Not to Scale

THRUST BLOCK FOR 6" HYDRANT BEND
Not to Scale

Note: The bearing face of Thrust Blocks shall be poured against undisturbed earth.

DESIGNED BY		APPROVED		CITY OF DETROIT WATER AND SEWERAGE DEPARTMENT				M.D.P.H. PERMIT NO.
DRAWN BY R.A.C.				MISCELLANEOUS DETAILS				FED. REF. NO.
CHECKED BY R.T.D.		DIRECTOR OF ENGINEERING						CONTRACT NO.
APRVD. DATE		DIRECTOR		SCALE AS SHOWN				DRAWING NO. 17176A
REVISIONS				DATE				SHEET 28 OF 71

LEGEND

UNDERGROUND PLAN

- EXISTING DIRECT BURIAL OR PARKWAY CABLE
- ABANDON DIRECT BURIAL OR PARKWAY CABLE
- INSTALL DIRECT BURIAL OR PARKWAY CABLE (NO. & SIZE AS INDICATED)
- 1-3" --- INSTALL DIRECT BURIAL CONDUIT (1-3" SHOWN)
- EXISTING SINGLE P.L.D. DUCT RUN
- 4-3" --- EXISTING P.L.D. DUCT RUN (4-3" SHOWN)
- 2-3" --- ABANDON EXISTING P.L.D. DUCT RUN (2-3" SHOWN)
- 4-4" --- BUILD ENCASED CONDUIT (4-4" SHOWN)
- 2-3" --- GALVANIZED STEEL CONDUIT, JACKING-BORING (2-3" SHOWN)
- EXISTING P.L.D. HANDHOLE
- M.H.1234 EXISTING P.L.D. MANHOLE
- BUILD ROUND HANDHOLE
- BUILD TYPE "D" HANDHOLE
- M.H.7890 BUILD NEW MANHOLE (2-WAY)
- △ M.H.1234 BUILD NEW MANHOLE (3-WAY)
- ◊ M.H.5678 BUILD NEW MANHOLE (CORNER)
- ◇ M.H.9012 BUILD NEW MANHOLE (4-WAY)
- BUILD TRAFFIC SIGNAL CONTROLLER FDN.
- BUILD T.S. MAST ARM STANDARD FDN.
- BUILD T.S. PEDESTAL FDN.
- INSTALL 30FT. STEEL STRAIN POLE ON NEW FDN.
- EXISTING FIRE OR POLICE CALL BOX
- ▲ INSTALL NEW FIRE OR POLICE CALL BOX (EXCEPT AS OTHERWISE INDICATED)
- ▲ INSTALL SALVAGED FIRE OR POLICE CALL BOX (EXCEPT AS OTHERWISE INDICATED)
- ⊠ INSTALL MULT. ST. LTG. CONTROL CABINET ON NEW FDN.
- ☀ EXISTING U.G.-FED ST. LTG. UNIT
- ☀ EXISTING U.G.-FED ST. LTG. UNIT WITH DBL. ARM
- ☀ FUTURE U.G.-FED ST. LTG. UNIT
- ☀ REMOVE U.G.-FED ST. LTG. UNIT & FOUNDATION (EXCEPT AS OTHERWISE NOTED)
- ☀ INSTALL SALVAGED U.G.-FED ST. LTG. UNIT ON NEW FDN.
- "T" INDICATES TRAFFIC SIGNAL CONTACT ON ST. LTG. STD.
- "WL" INDICATES WITHOUT LUMINAIRE
- ☀ INSTALL CODE 117 ST. LTG. UNIT ON NEW FDN.
- ☀ INSTALL ONE 400W TYPE III RECTANGULAR SODIUM VAPOR LUMINAIRE (240V.) & ONE SALVAGED 400W. TYPE III RECTANGULAR SODIUM VAPOR LUMINAIRE (240V)
- ☀ NEW LUMINAIRE
- ☀ SALVAGED LUMINAIRE
- ☀ INSTALL CODE 009-00 ST. LTG. STD., UNIVERSAL TRANSFORMER BASE, 6 FT. CLAMP-ON BRACKET ARM (3'-0" RISE) & 400W. TYPE III MERCURY VAPOR LUMINAIRE WITH SERIES COIL ON NEW FOUNDATION.

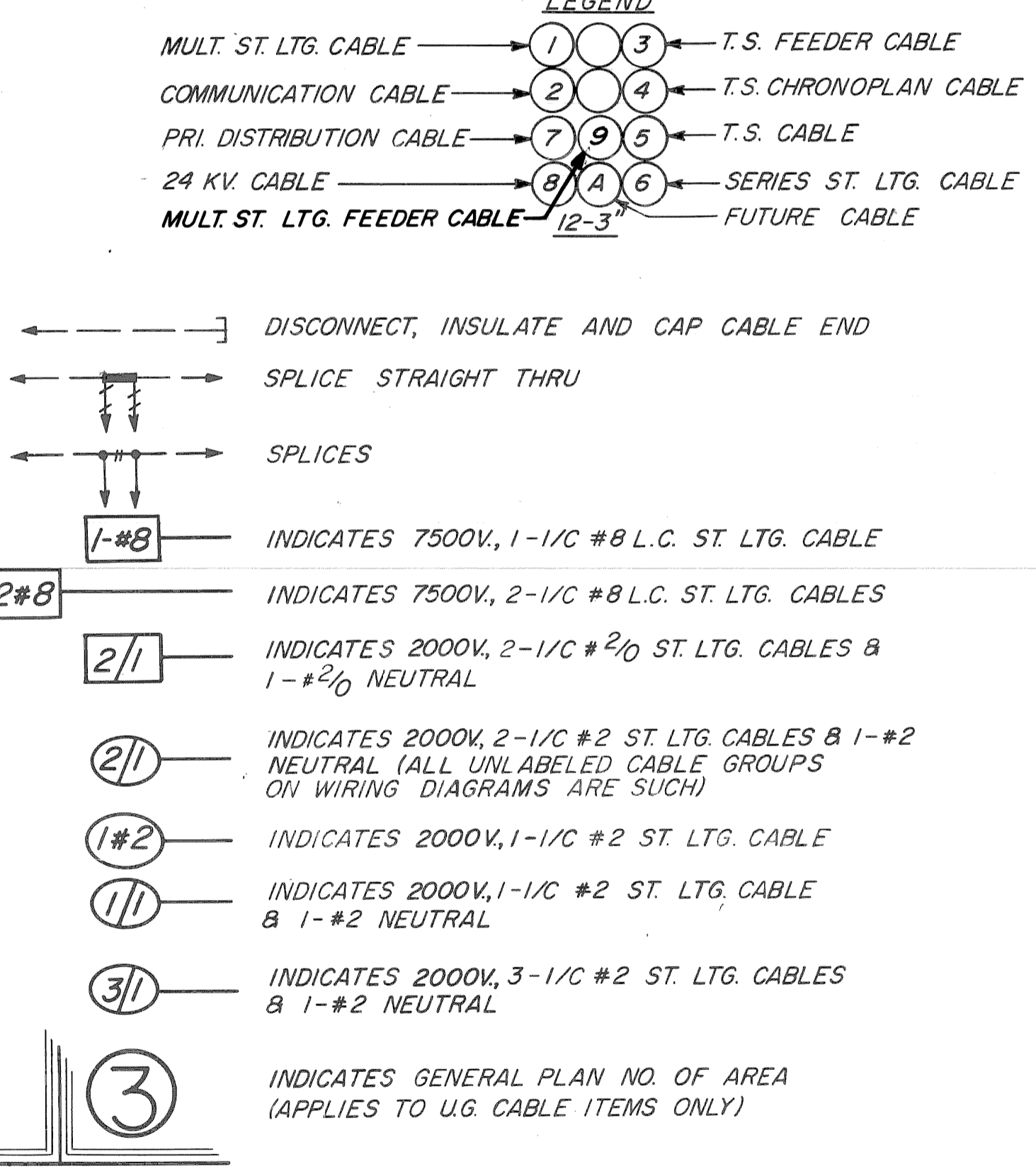
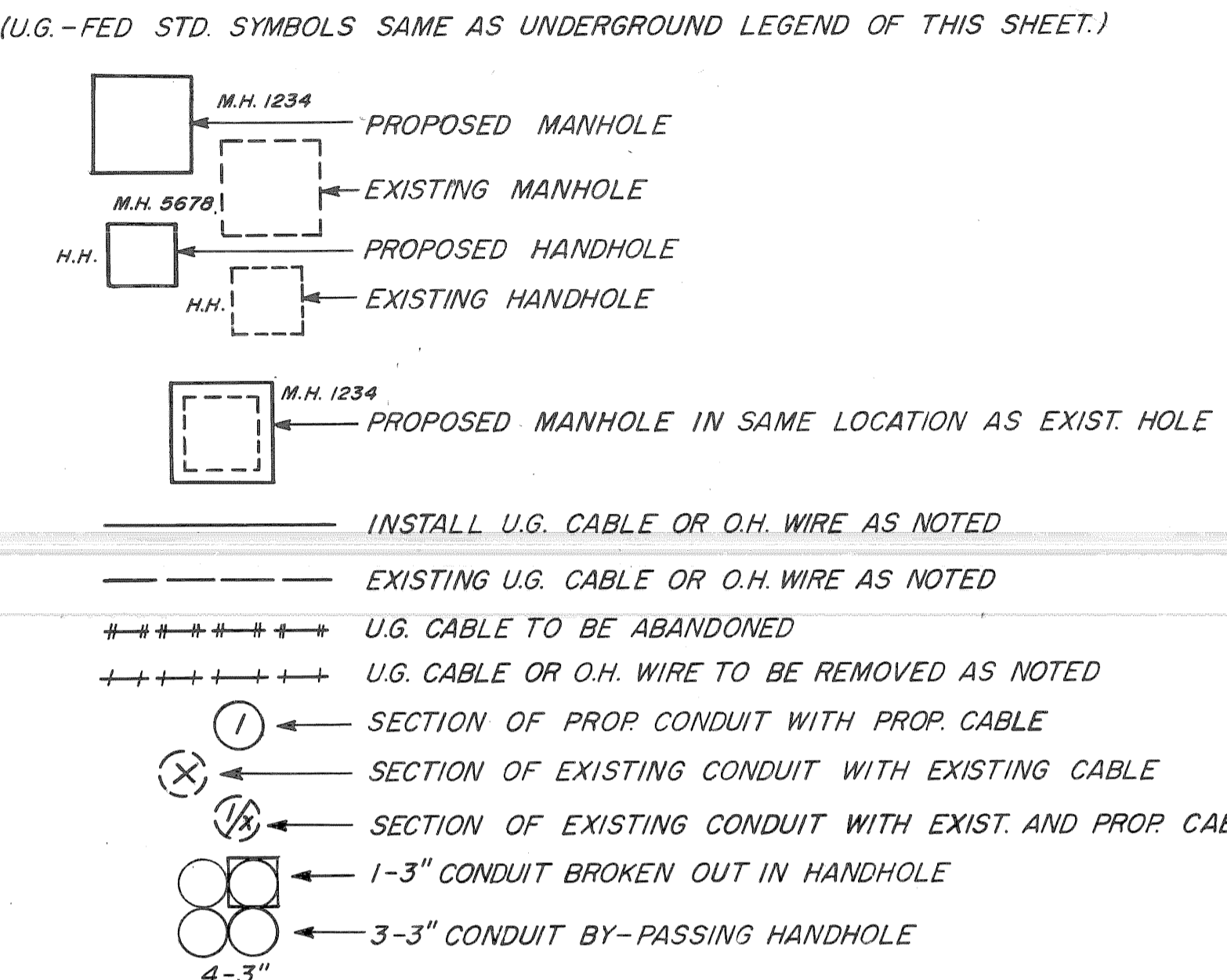
OVERHEAD PLAN

- OE EXISTING WOOD POLE (D.E. CO. POLE SHOWN)
- C REMOVE WOOD POLE (P.L.D. POLE SHOWN)
- ⊙ REPLACE WOOD POLE (HEIGHT & CLASS AS INDICATED)
- ✕ INSTALL 35FT. CLASS 4 WOOD POLE (EXCEPT WHERE OTHERWISE NOTED) (USE SALVAGED POLE WHERE INDICATED)
- EXISTING OVERHEAD ST. LTG. UNIT
- REMOVE OVERHEAD ST. LTG. UNIT
- INSTALL 8FT BRACKET ARM & 400W TYPE III RECTANGULAR SODIUM VAPOR LUMINAIRE (240V) (EXCEPT AS OTHERWISE NOTED)
- INSTALL NEW 100W MERCURY VAPOR LUMINAIRE FOR ALLEY LTG. WITH NEW SERIES COIL & NEW 6FT. BRACKET ARM. (USE SALVAGED UNIT WHERE INDICATED)
- INDICATES 2-WAY
- STA-24 (2-#6) EXISTING OVERHEAD LINE (2-#6 OF STANTON-24 SHOWN)
- 3-#2 (STA-662) REMOVE OVERHEAD LINE (3-#2 OF STANTON-662 SHOWN)
- 2-#6 (STA-24) INSTALL OVERHEAD LINE (2-#6 OF STANTON-24 SHOWN)
- 3-#6 (MULT) INSTALL & LATER REMOVE OVERHEAD LINE (3-#6 MULT. SHOWN)
- 1/2" GUY INSTALL GUY & ANCHOR (1/2" GUY SHOWN)
- REMOVE GUY & ANCHOR
- 1/2" P GUY INSTALL POLE GUY (1/2" GUY SHOWN)
- 3/8" A GUY INSTALL ARM GUY (3/8" GUY AS SHOWN)
- POR A REMOVE GUY (TYPE AS INDICATED)
- IN. MATERIAL TO BE INSTALLED
- RM. MATERIAL TO BE REMOVED
- M.S.S. MAKE WOOD POLE SELF-SUPPORTING IN CRUSHED STONE
- XYZ PHASES OF P.L.D. DISTRIBUTION WIRES OR EQUIPMENT
- E D.E. CO. DISTRIBUTION WIRE
- es D.E. CO. SECONDARY WIRE
- C.P. CABLE POLE
- C INSTALL SUSPENSION INSULATOR
- EXISTING → C PROPOSED → C P.L.D. DISTRIBUTION WIRE
- EXISTING → CS PROPOSED → CS P.L.D. SECONDARY WIRE
- EXISTING → A PROPOSED → A P.L.D. SERIES ST. LTG. WIRE
- EXISTING → M PROPOSED → M P.L.D. MULTIPLE STREET LIGHTING WIRE

GENERAL

- PROPERTY LINE
- EXISTING FACE OF CURB WHEN NEW STREET CONST.
- PROPOSED FACE OF CURB WHEN NEW STREET CONST. OR EXISTING FACE OF CURB WHEN NO STREET CONST.
- FUTURE PAVEMENT
- S SEWER LINE, MANHOLE & CATCH BASIN
- E DETROIT EDISON COMPANY U.G. LINE & MANHOLE
- T MICH. BELL TEL. COMPANY U.G. LINE & MANHOLE
- W WATERMAIN & GATEWELL
- G MICH. CONSOLIDATED GAS CO. U.G. LINE & WELL (OTHER UTILITIES SHOWN SIMILAR)

DIAGRAMS & CONDUIT ALLOCATION



POR. 301 A1-1
POR. 24-26
INDICATES NEW ST. LTG. STANDARD NUMBER
INDICATES OLD ST. LTG. STANDARD NUMBER

TRAFFIC SIGNAL PLAN

- ← INSTALL 3-SECTION TRAFFIC SIGNAL (1-WAY SHOWN)
- ← INSTALL 3-SECTION TRAFFIC SIGNAL WITH SALVAGED HEADS (2-WAY SHOWN)
- ← REMOVE 3-SECTION TRAFFIC SIGNAL (3-WAY SHOWN)
- ← EXISTING 3-SECTION TRAFFIC SIGNAL (4-WAY SHOWN)
- ← INSTALL 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (2-WAY SHOWN)
- ← INSTALL 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL WITH SALVAGED HEAD (1-WAY SHOWN)
- ← REMOVE 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (1-WAY SHOWN)
- ← EXISTING 2-SECTION PEDESTRIAN (WALK-DON'T WALK) TRAFFIC SIGNAL (1-WAY SHOWN)
- INSTALL JUNCTION BOX
- INSTALL SALVAGED JUNCTION BOX
- REMOVE JUNCTION BOX
- ⊠ EXISTING JUNCTION BOX
- INSTALL OVERHEAD PLASTIC JACKETED CABLE
- EXISTING OVERHEAD PLASTIC JACKETED CABLE
- - - - - REMOVE OVERHEAD PLASTIC JACKETED CABLE
- INSTALL TRAFFIC SIGNAL CONTROLLER (NEW OR SALVAGED AS INDICATED) ON NEW FDN.
- INSTALL CANTILEVER TYPE TRAFFIC SIGNAL MAST ARM STANDARD & MAST ARM ON NEW FOUNDATION. (EXCEPT AS OTHERWISE INDICATED)
- ⊠ INSTALL 8FT. TRAFFIC SIGNAL PEDESTAL ON NEW FOUNDATION (EXCEPT AS OTHERWISE INDICATED)
- ⊕ INSTALL STEEL STRAIN POLE ON NEW FDN.
- ⊠ EXISTING TRAFFIC SIGNAL CONTROLLER
- ⊠ EXISTING MAST ARM STANDARD
- ⊠ EXISTING PEDESTAL
- ⊕ EXISTING STEEL SHAFT POLE
- ☀ BACK-OUT LAMPS & HOOD SIGNALS (INCIDENTAL TO INSTALLATION OF T.S. ON THIS CONTRACT).
- † REMOVE HOOD & INSTALL LAMPS (INCIDENTAL TO INSTALLATION OF T.S. ON THIS CONTRACT).
- POCH POLE CONTACT HEIGHT OF T.S. SPAN WIRE
- L.C.H. LOW CONTACT HEIGHT OF SPAN WIRE T.S. TO SPAN WIRE
- ◇ 2 INDICATES TYPE OF SIDEWALK RAMP TO BE CONSTRUCTED.

PLAN INDEX	
DRWG. NO.	SUB-TITLE
1	LEGEND
2	GENERAL INFORMATION
3 THRU 8	GENERAL PLAN
9	MULT. ST. LTG. WIRING DIAGRAMS
10	MISC. WIRING DIAGRAMS
11	SERIES ST. LTG. WIRING DIAGRAMS
12 & 13	T.S. AT M.L. KING BLVD. & ROSA PARKS BLVD.
14 & 15	T.S. AT M.L. KING BLVD., TRUMBULL & GRAND RIVER
16 & 17	QUANTITY SHEETS
18 THRU 41	DETAILS

