

OUTER DRIVE - I-96 TO BURT ROAD

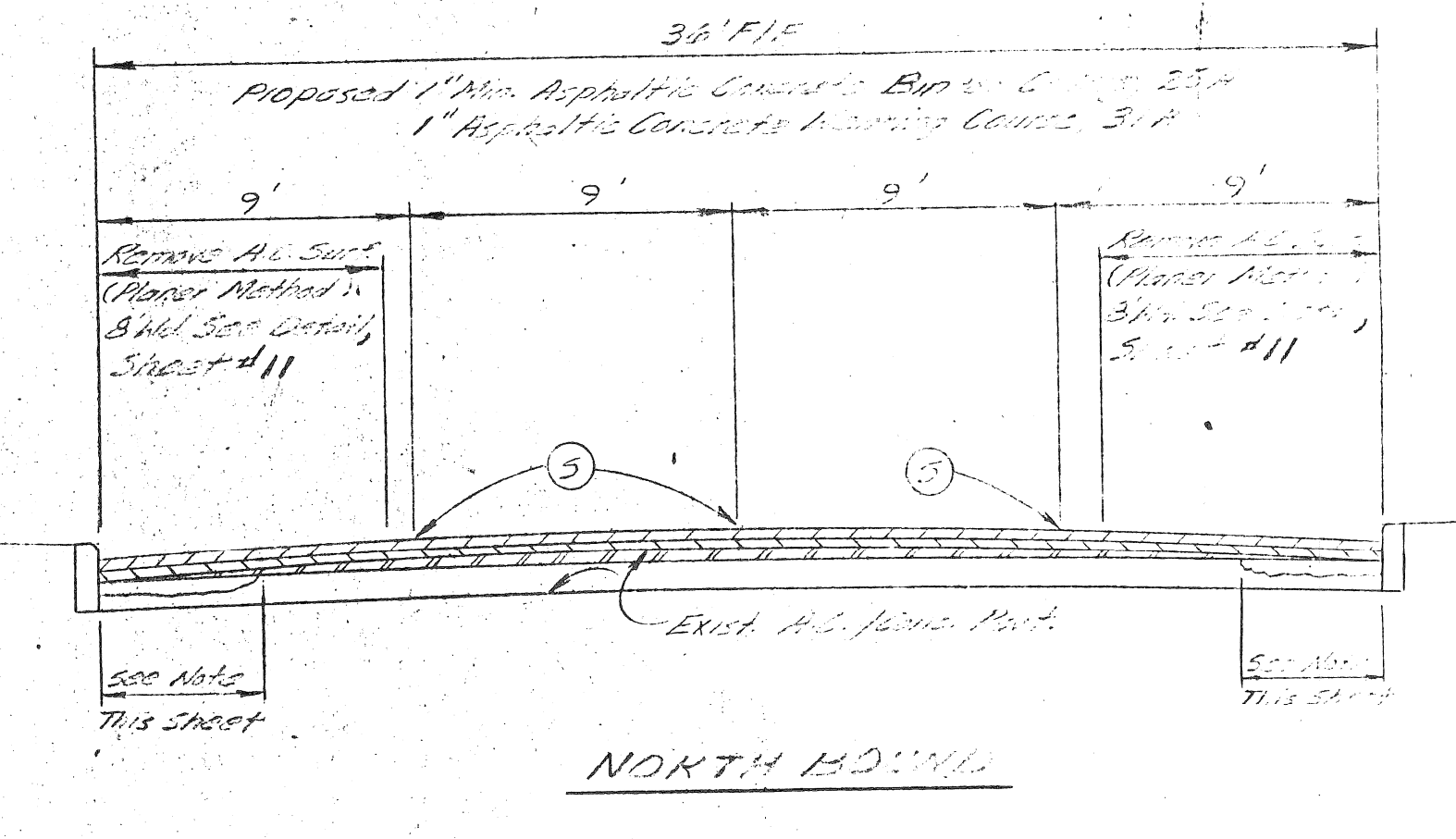
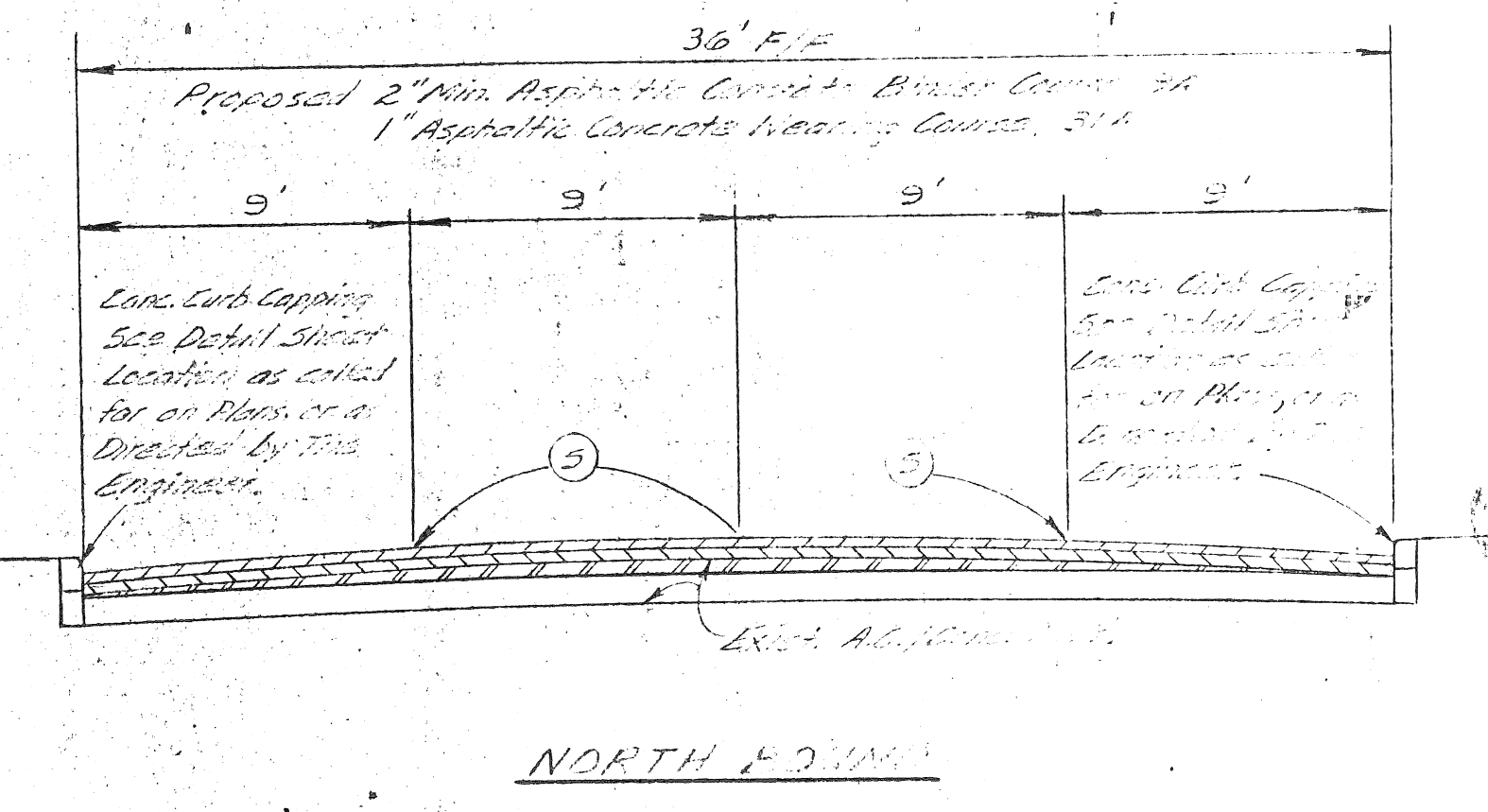
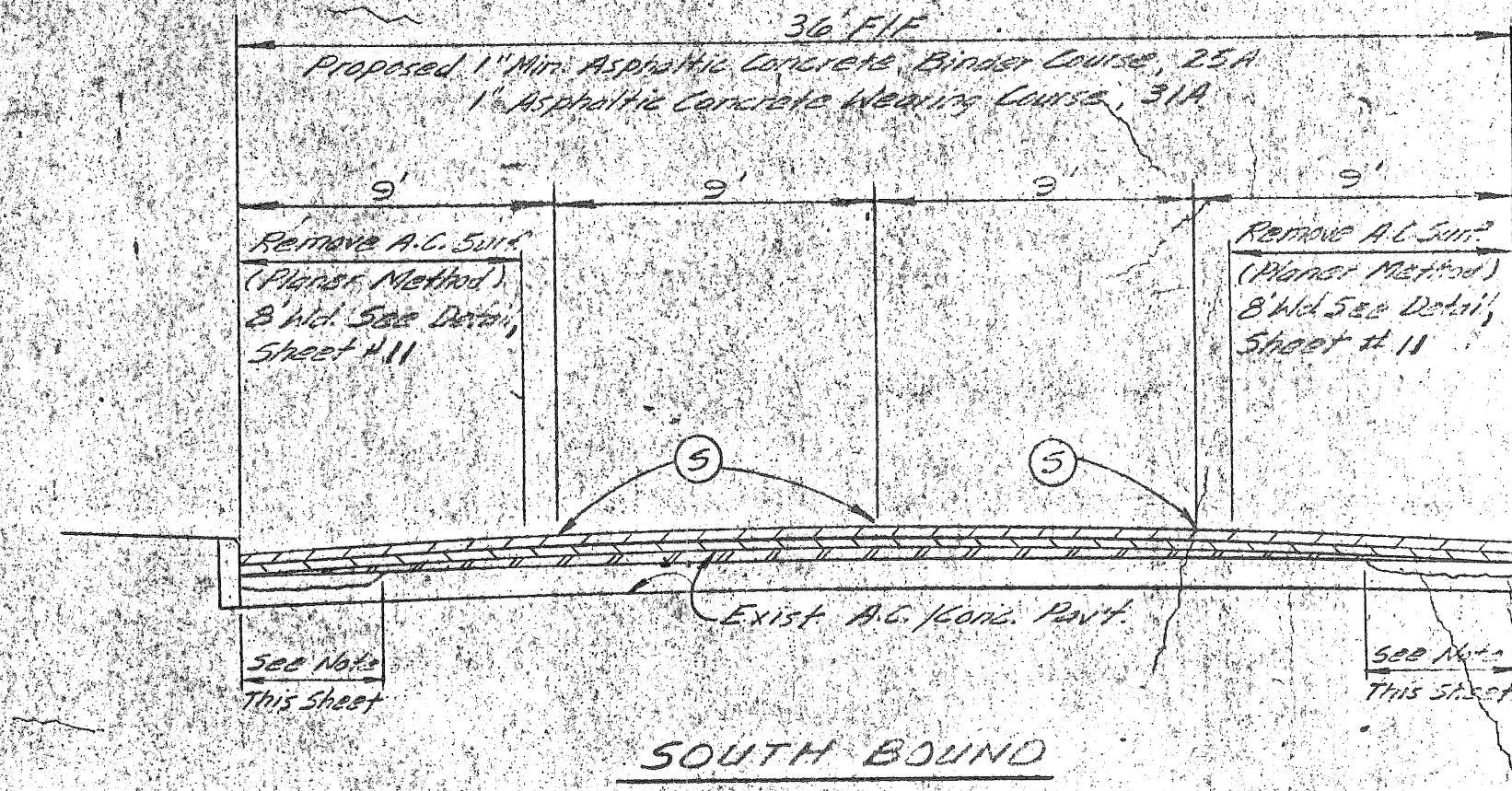
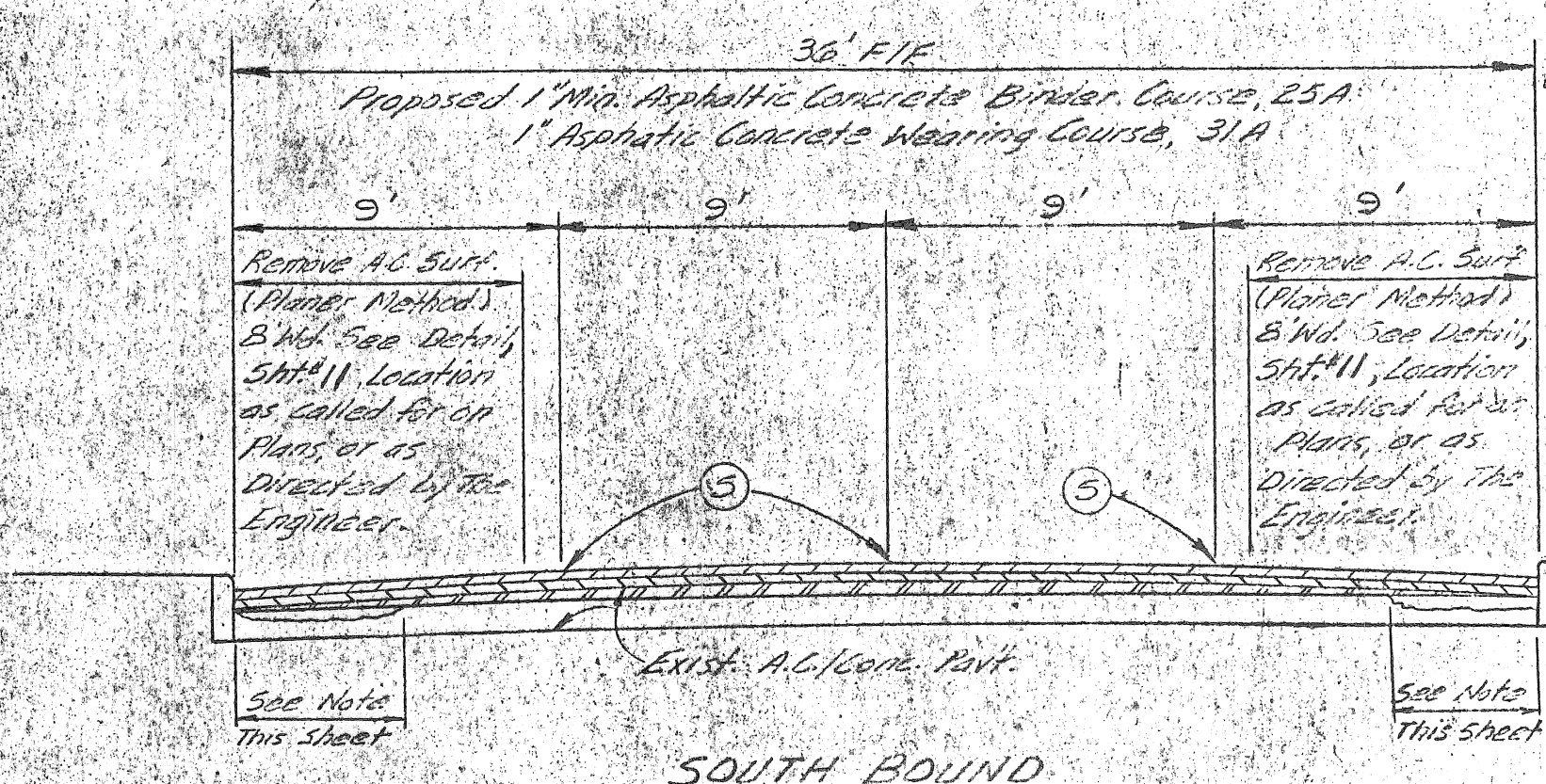
OUTER DRIVE

I-96 FREEWAY TO BURT ROAD

TYPICAL CROSS-SECTION

Scale: 1"=5'

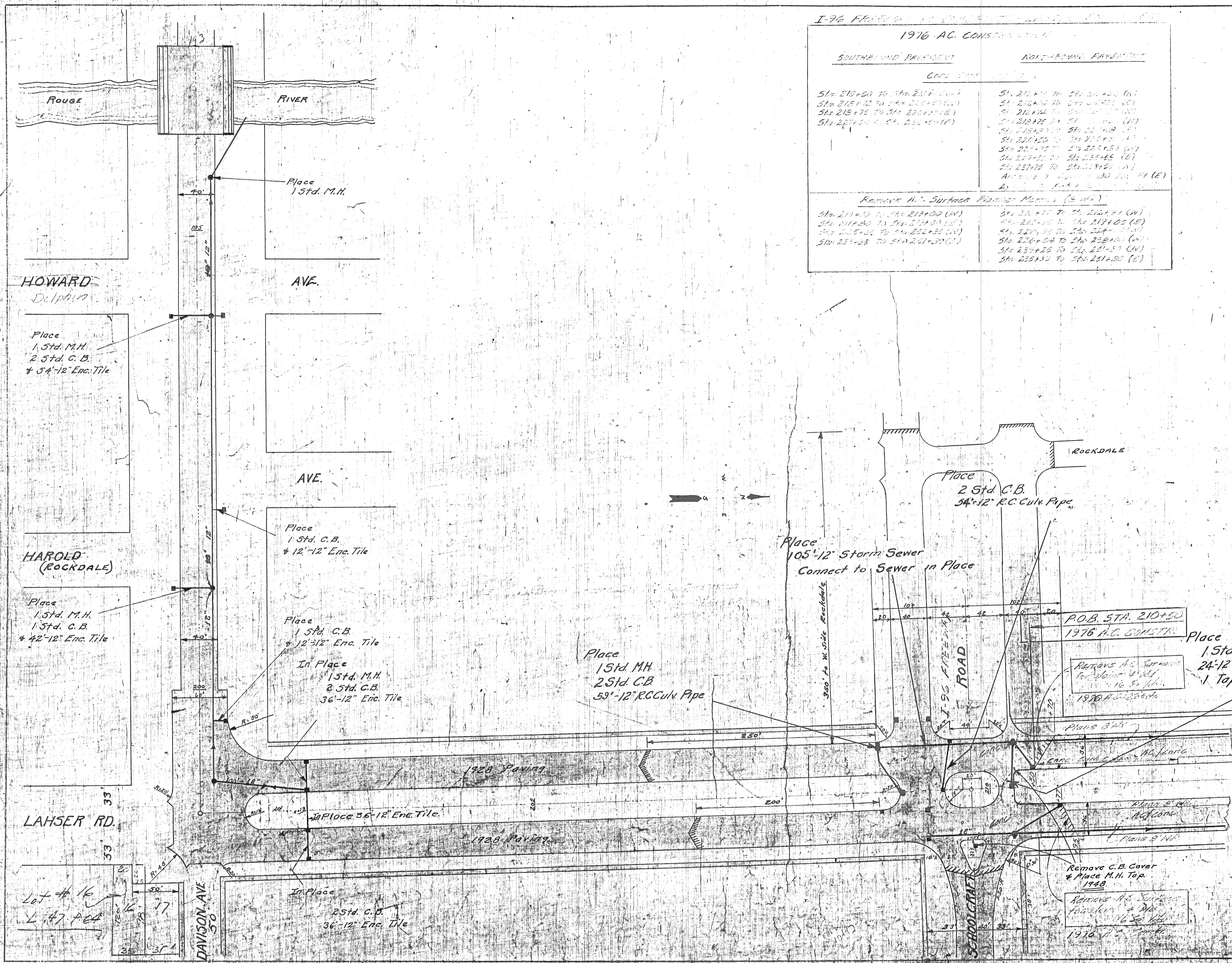
QUANTITIES	
ITEM	QUANTITY
REMOVING ASPHALT SURFACE	1265 Sa.Yd
REMOVING ASPHALT SURFACE, PLANER METHOD	18740 Sa.Yd
REMOVING SIDEWALK	38 Sa.Yd
REMOVING OLD PAVEMENT (PATCHING)	120 Sa.Yd
REMOVING CURB (PATCHING)	100 LIN.FT
ADJUSTING MANHOLE, FURNISHING COVER "A"	EA.
ADJUSTING CATCH BASIN, FURNISHING COVER "X"	10 EA.
ADJUSTING WATER SHUTOFF RISE & BOX	EA.
ADJUSTING MONUMENT BOX	EA.
RECONSTRUCTING M.H., C.B. OR W.G.W.	40 LIN.FT
ADJUSTING COVERS	165 EA.
6" PIPE UNDERDRAIN	LIN.FT
CONCRETE PAVEMENT, 9" (PATCHING)	Sa.Yd
CONCRETE CURB, STRAIGHT	100 LIN.FT
INTEGRAL CONCRETE CURB AND WALK	LIN.FT
AGGREGATE SHOULDER	Cu.Yd
CALCIUM CHLORIDE, APPLIED	TONS
4" CONCRETE SIDEWALK	150 Sa.Yd
BITUMINOUS BOND COAT (SS-1H)	6430 GAL.
BITUMINOUS PRIME COAT	GAL.
NOTCHING CONCRETE PAVEMENT FOR JOINT	LIN.FT
PREPARING EXISTING PAVEMENT	130 STA.
SAWED LONGITUDINAL PLANE OR WEAKNESS JOINT IN ASPHALTIC CONCRETE SURFACE	11400 LIN.FT
CHIPPING EXISTING CONCRETE PAVEMENT	Sa.Yd
4" CONCRETE SIDEWALK RAMP	250 Sa.Yd
CONCRETE CURB CAPPING	5350 LIN.FT
CONCRETE CURB CAP PATCHING	1340 LIN.FT
CLEANING EXIST. STORM DRAINAGE STRUCTURES	150 EA.
 (ESTIMATED QUANTITIES FOR ASPHALTIC CONCRETE MIXTURES ARE BASED ON THE USE OF NATURAL AGGREGATES)	
ASPH. CONC. BINDER COURSE, 25A	2325 TONS
ASPH. CONC. BINDER COURSE, 9A	1900 TONS
ASPH. CONC. WEARING COURSE, 31A	3945 TONS
ASPH. CONC. BASE COURSE, 25A	75 TONS



Note:
Remove deteriorated concrete by using compressed air or other approved method and fill with A.C. wearing course, 31A, by Machine Method (As Directed by the Engineer).

1976 A.C. PROJECT 45549

CORRECT: *R.D. Beckley* (Signature)
APPROVED: *[Signature]* (Signature)
Engineer of Highway Design



I-76 FREEWAY

1976 AC CONSTRUCTION

SOUTHERND PAVEMENT	NORTHBOUND PAVEMENT
COPS CURB	
Sta. 210+00 To Sta. 211+00 (W)	Sta. 210+00 To Sta. 211+00 (W)
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Remove A.C. Surface Paving Mats (3 in)

Remove A.C. Surface Paving Mats (3 in)

1948 Asphaltic Concrete Re-surfacing

8239 Sq. yds.

Break & Remove Exist. Pave.	- 100 Sq. yd.
Break & Remove Exist. Curb	- 40 Lin. ft.
9" Conc. Base	- 100 Sq. yd.
6" Conc. Straight Curb	- 40 Lin. ft.
Adjusting Castings	- 7 Each.
Preparing the Base	- 8239 Sq. yd.
Bond Coat #E2 - 0.15 gal. per Sq. yd.	- 1236 Gal.
Binder Course - 115 per Sq. yd.	- 474 Ton.
Wearing Course - 110 per Sq. yd.	- 453 Ton.

1976 AC PROJECT 45539

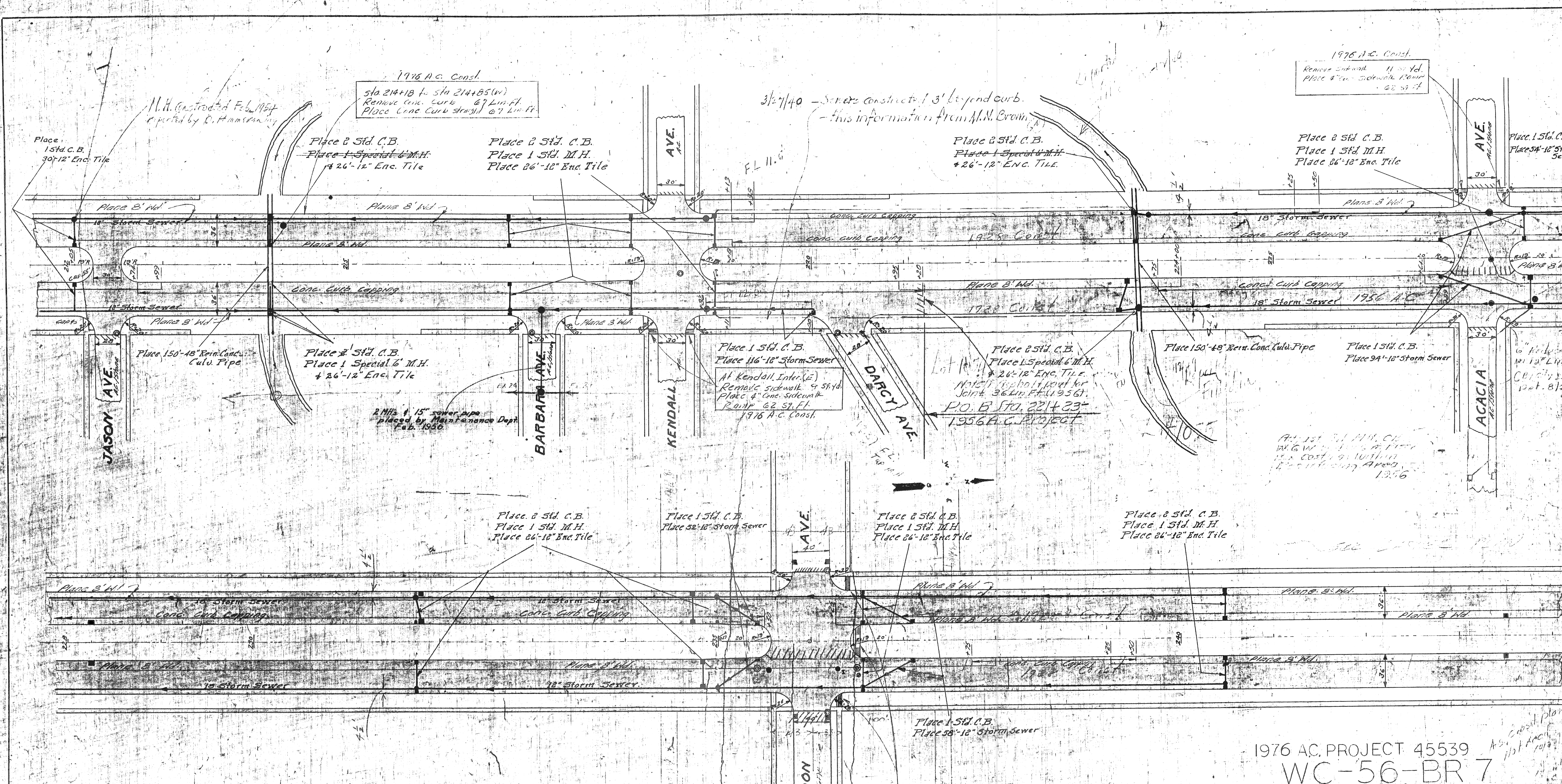
REVISIONS
7-2-28

BOARD OF
WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN
EDWARD N. HINES, JOHN S. HAGGERTY
CHAIRMAN COMMISSIONER
WM. F. BUTLER
COMMISSIONER

OUTER DRIVE SEC. E
PAVING PLAN ROUGE RIVER TO SCHOOLCRAFT
193+00 210+00

DESIGNED BY _____ DRAWN BY MEY CHECKED BY _____
DATE _____ SCALE 1"=50'
CORRECT _____
APPROVED _____ ENGINEER NUMBER _____ REG. CIVIL ENG. _____
SHEET NO. _____

E-11-3
F-8-1
3



M.H. Constructed Feb. 1954 reported by C. Hammon

1976 A.C. Const.
Sta 214+18 to Sta 214+85 (W)
Remove Conc. curb 67 Lin. Ft.
Place Conc. Curb straight 67 Lin. Ft.

3/27/40 - Sewers constructed 1' 3" beyond curb.
- this information from M.M. Brown

1976 A.C. Const.
Remove sidewalk 115 sq. Yd.
Place 4" Conc. sidewalk 130 sq. Ft.
62 SF FT.

Place 150'-18" Reinf. Conc. Curb Pipe
Jason Ave

Place 2 Std. C.B.
Place 1 Special 6" M.H.
Place 26'-12" End. Tile

Place 1 Std. C.B.
Place 16'-18" Storm Sewer
At Kendall Inter. (E)
Remove sidewalk 9 sq. Yd.
Place 4" Conc. sidewalk 102 SF FT.
1976 A.C. Const.

Place 2 Std. C.B.
Place 1 Special 6" M.H.
Place 26'-12" End. Tile
Lot 10
Note: 1" depth joint for joint 36 Lin. Ft. (1956)
P.O. B. Sta. 2214+23-
1956 A.C. Project

Place 150'-18" Reinf. Conc. Curb Pipe
Place 1 Std. C.B.
Place 94'-18" Storm Sewer
Acacia Ave

At 1st St. M.H. Ch.
10' G.W. 1' 6" dia. Pipe
12" Cost 19. 10' dia. 12' long
1956 A.C. Project 1356

Place 2 Std. C.B.
Place 1 Std. M.H.
Place 26'-12" End. Tile

Place 1 Std. C.B.
Place 32'-18" Storm Sewer

Place 2 Std. C.B.
Place 1 Std. M.H.
Place 26'-12" End. Tile

Place 2 Std. C.B.
Place 1 Std. M.H.
Place 26'-12" End. Tile

Place 1 Std. C.B.
Place 38'-12" Storm Sewer

Place:

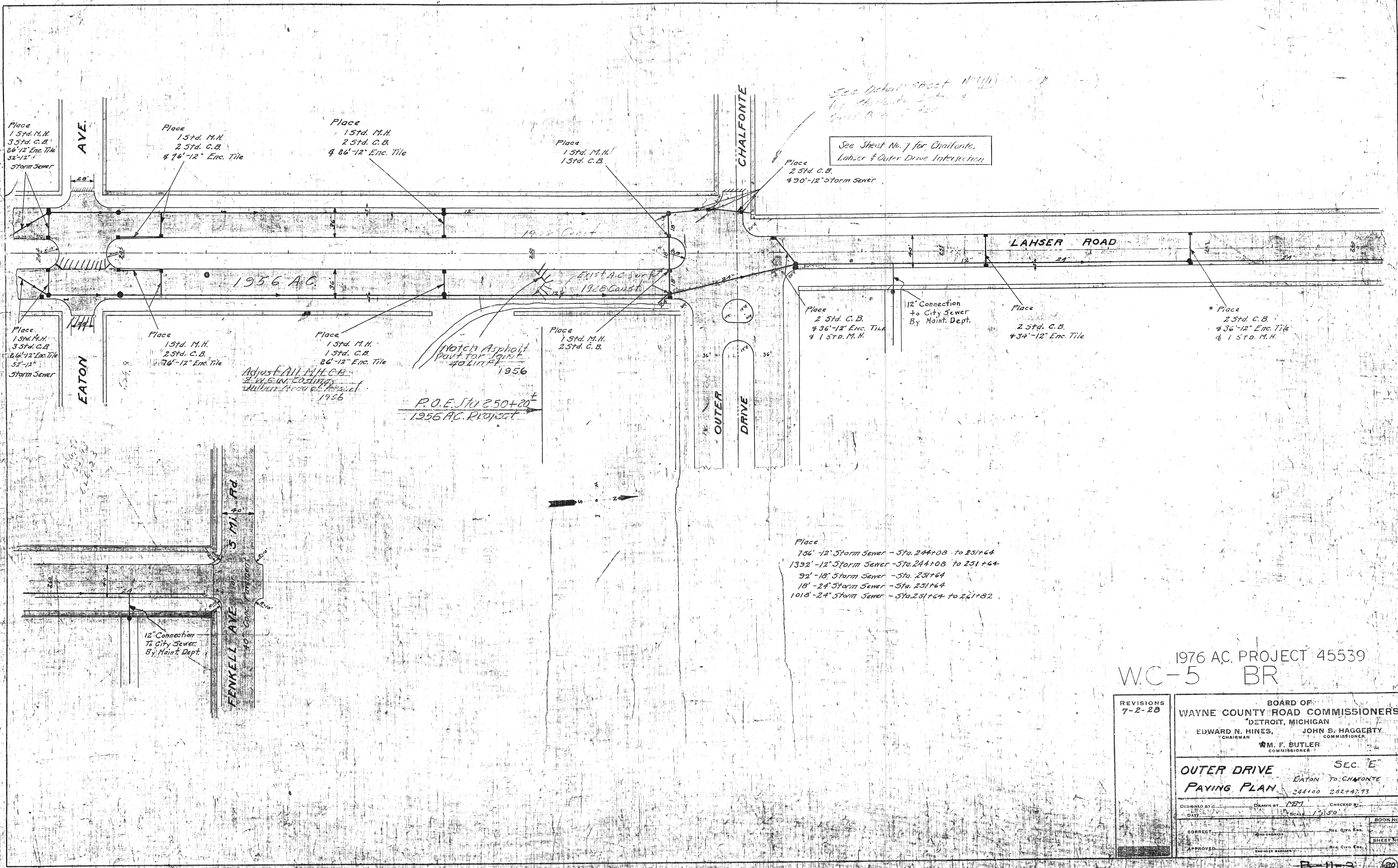
186'-18" Storm Sewer Right & Left Sides	Sta. 209+30 To Sta. 214+16	
150'-18" "	214+16	218+96
828'-18" "	223+58	231+74
878'-42" "	231+74	240+46

Remove sidewalk 16 sq. Yd.
Place 4" Conc. sidewalk 130 SF FT.
4" Conc. sidewalk Ramp 124 SF FT.
Remove Conc. Curb 14 Lin. Ft.
Place Conc. Curb straight 14 Lin. Ft.
1976 A.C. Const.

1976 A.C. PROJECT 45539
WC-56-BR 7

REVISIONS 7-2-28	BOARD OF WAYNE COUNTY ROAD COMMISSIONERS DETROIT, MICHIGAN EDWARD N. HINES, CHAIRMAN JOHN S. HAGGERTY, COMMISSIONER WM. F. BUTLER, COMMISSIONER
OUTER DRIVE PAVING PLAN	SEC. E JASON TO EATON 213+00 TO 244+00
DESIGNED BY DATE	DRAWN BY DATE
CHECKED BY DATE	BOOK NO. SHEET NO.

B-11-23
F.B. 11/1



1976 AC. PROJECT 45539
 W.C-5 BR

REVISIONS 7-2-28	BOARD OF WAYNE COUNTY ROAD COMMISSIONERS DETROIT, MICHIGAN EDWARD N. HINES, CHAIRMAN JOHN S. HAGGERTY, COMMISSIONER W. F. BUTLER, COMMISSIONER
OUTER DRIVE SEC. E PAYING PLAN EATON TO CHAFONTE 244+00 252+47.93	
DESIGNED BY: _____	CHECKED BY: _____
DATE: _____	SCALE: 1"=50'
CORRECT: _____	BOOK NO. _____
APPROVED: _____	SHEET NO. _____

B-11-3
 F-8-1

See Detail Sheet
for construction
of
Lahser
to
Five Mile

Note: 6" throttles placed by
City of Detroit, Sept 1958

NOTE: Adjust all C.B.'s, M.H.'s, W.G.W.'s
to G. Riser Boxes within area
to be resurfaced.
1955

P.O.E. STA 254+00±
1955 A.C. RESURF.

P.O.E. STA 218+54
1976 A.C. CONST.

P.O.E. STA 279+45±
1955 A.C. RESURF.

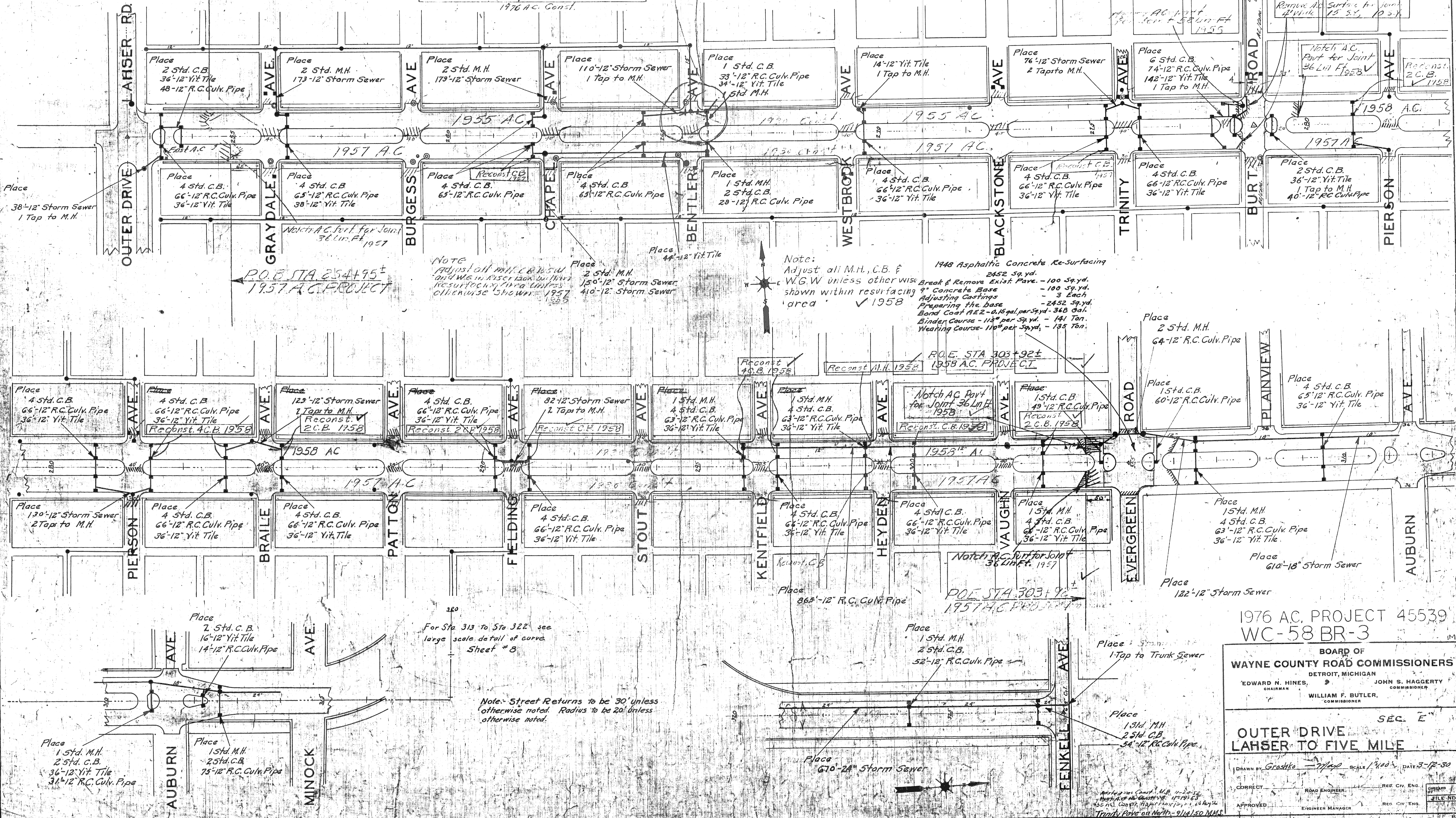
P.O.B. STA 273+00±
1958 A.C. PROJECT
WEST BOUND

Lahser-Chauffeur Sts. to be
Removed & surf. (place method)
8' wid. Both sides of the East & West bound
1976 A.C. Const.

Remove All Surface for joint
4' wide 15' x 10' x 1'

Notch A.C. Part for Joint
36 Lin Ft. 1958

Reconst. C.B. 1958



1976 A.C. PROJECT 45539
WC-58 BR-3

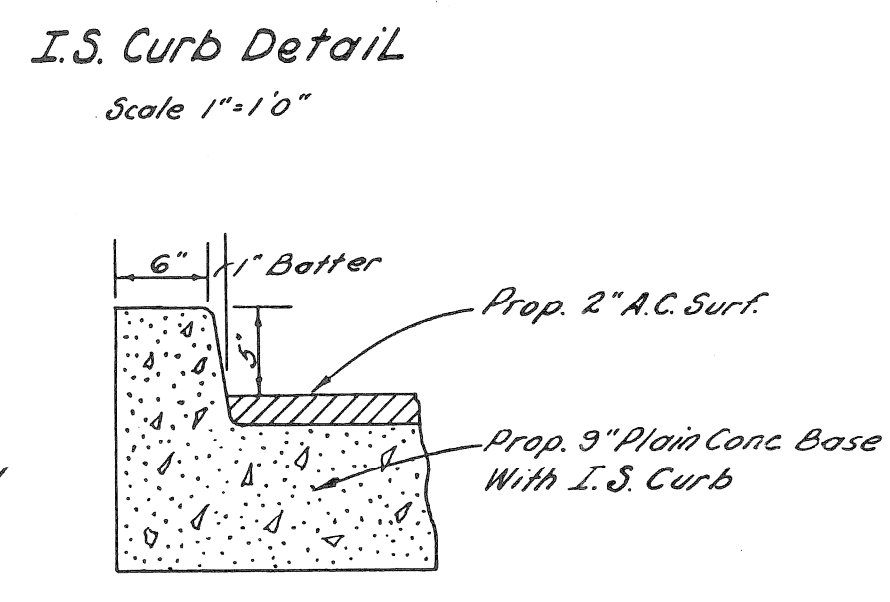
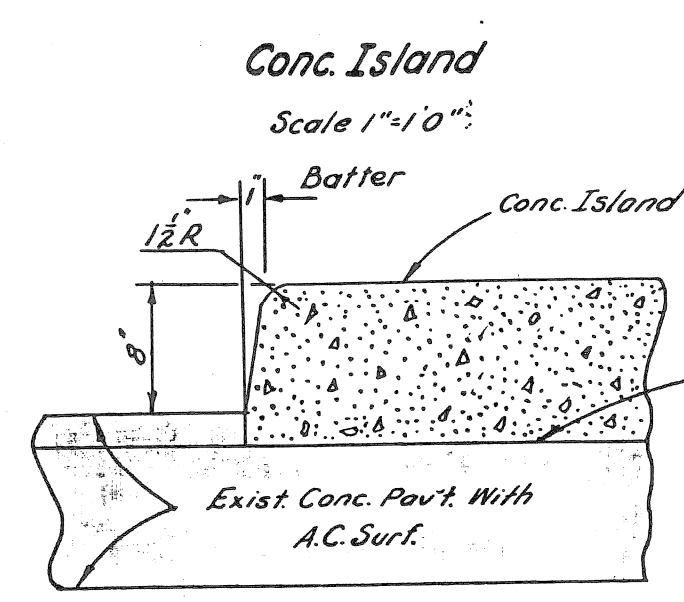
BOARD OF
WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN
EDWARD N. HINES, CHAIRMAN
JOHN S. HAGGERTY, COMMISSIONER
WILLIAM F. BUTLER, COMMISSIONER

SEC. E
OUTER DRIVE
LAHSER TO FIVE MILE

DRAWN BY: G. H. G. scale 1/4" = 1'-0" DATE 3-12-50
CORRECT: ROAD ENGINEER REG. CIV. ENG.
APPROVED: ENGINEER MANAGER REG. CIV. ENG.

B-11-3
F-8-1
6

B.M. Elev. = 621.29
 S. Cor. Lamp Base #257 N.W. Cor.
 Chalfonte & Outer Drive



Median Island
 Break & Remove AC Surface 14 Sq. Yds.
 Break & Remove Conc. Curb 119 L.F.
 Break & Remove Exist. Pavt. 82 Sq. Yds.
 Place 9" Plain Conc. Base, 13C
 & 2" AC Surface 66 Sq. Yds.
 Std. Straight Curb 55 L.F.

Break & Remove AC Surface 17 Sq. Yds.
 Break & Remove Conc. Curb 156 L.F.
 Break & Remove Conc. Walk 21 Sq. Yds.
 Place 9" Plain Conc. Base, 13C
 & 2" AC Surface 66 Sq. Yds.
 Place 4" Conc. Walk 55 Sq. Ft.

Break & Remove AC Surface 78 Sq. Yds.
 Place 10" Plain Conc. Pavt. 78 Sq. Yds.

Remove A.C. Surf. for Joint
 (4' Wd.) 40 Sq. Yds.
 1915 A.C. Const.

Adj. CB, Place MH Cover
 Place Std. CB with Trap
 7'-12" Sewer (C76-1) PB

Adj. CB, Place MH Cover
 Place Std. CB
 over exist. Sewer

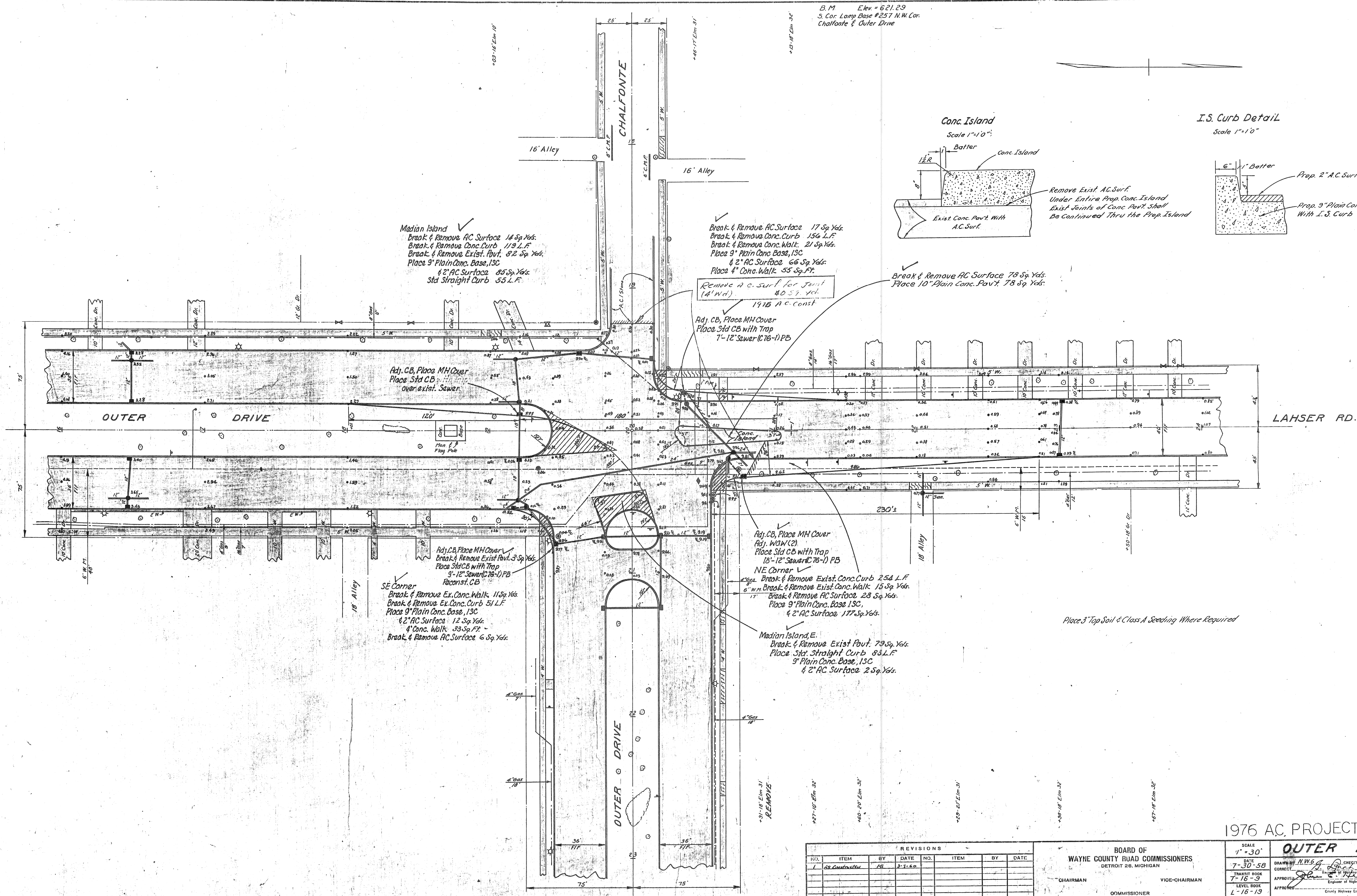
Adj. CB, Place MH Cover
 Adj. W&W (2)
 Place Std. CB with Trap
 18'-12" Sewer (C76-1) PB
 NE Corner

Break & Remove Exist. Conc. Curb 254 L.F.
 Break & Remove Exist. Conc. Walk 15 Sq. Yds.
 Break & Remove AC Surface 28 Sq. Yds.
 Place 9" Plain Conc. Base, 13C
 & 2" AC Surface 177 Sq. Yds.

SE Corner
 Break & Remove Ex. Conc. Walk 11 Sq. Yds.
 Break & Remove Ex. Conc. Curb 51 L.F.
 Place 9" Plain Conc. Base, 13C
 & 2" AC Surface 12 Sq. Yds.
 & Conc. Walk 33 Sq. Ft.
 Break & Remove AC Surface 6 Sq. Yds.

Median Island, E.
 Break & Remove Exist. Pavt. 79 Sq. Yds.
 Place Std. Straight Curb 83 L.F.
 9" Plain Conc. Base, 13C
 & 2" AC Surface 2 Sq. Yds.

Place 3" Top Soil & Class A Seeding Where Required



REVISIONS							
NO.	ITEM	BY	DATE	NO.	ITEM	BY	DATE
1	AS Constructed	AR	2-1-60				

BOARD OF
 WAYNE COUNTY ROAD COMMISSIONERS
 DETROIT 26, MICHIGAN

CHAIRMAN
 VICE-CHAIRMAN
 COMMISSIONER

1976 AC. PROJECT 45539

SCALE 1"=30'

DATE 7-30-58

TRANSIT BOOK 7-16-9

LEVEL BOOK L-16-19

OUTER DRIVE

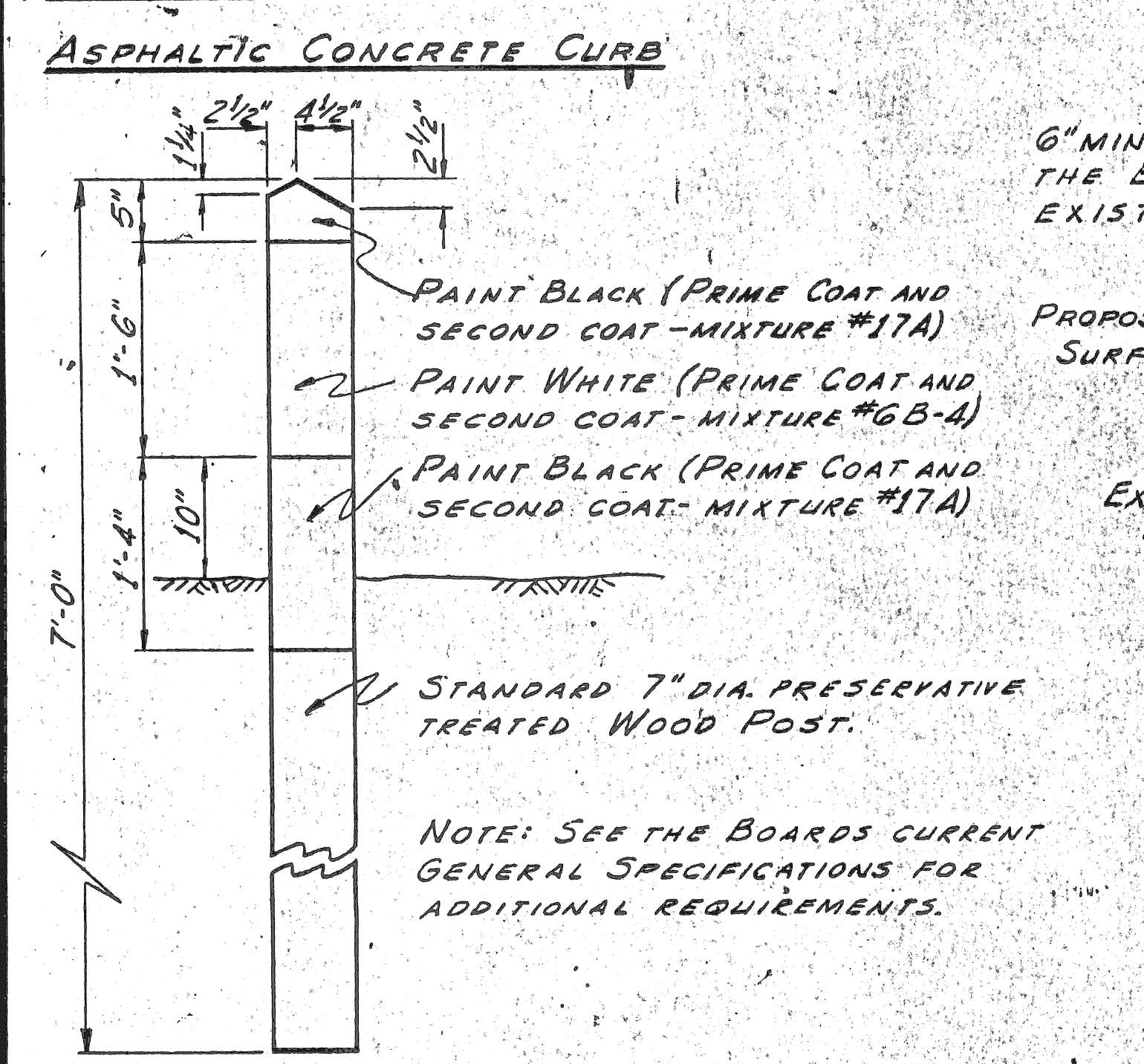
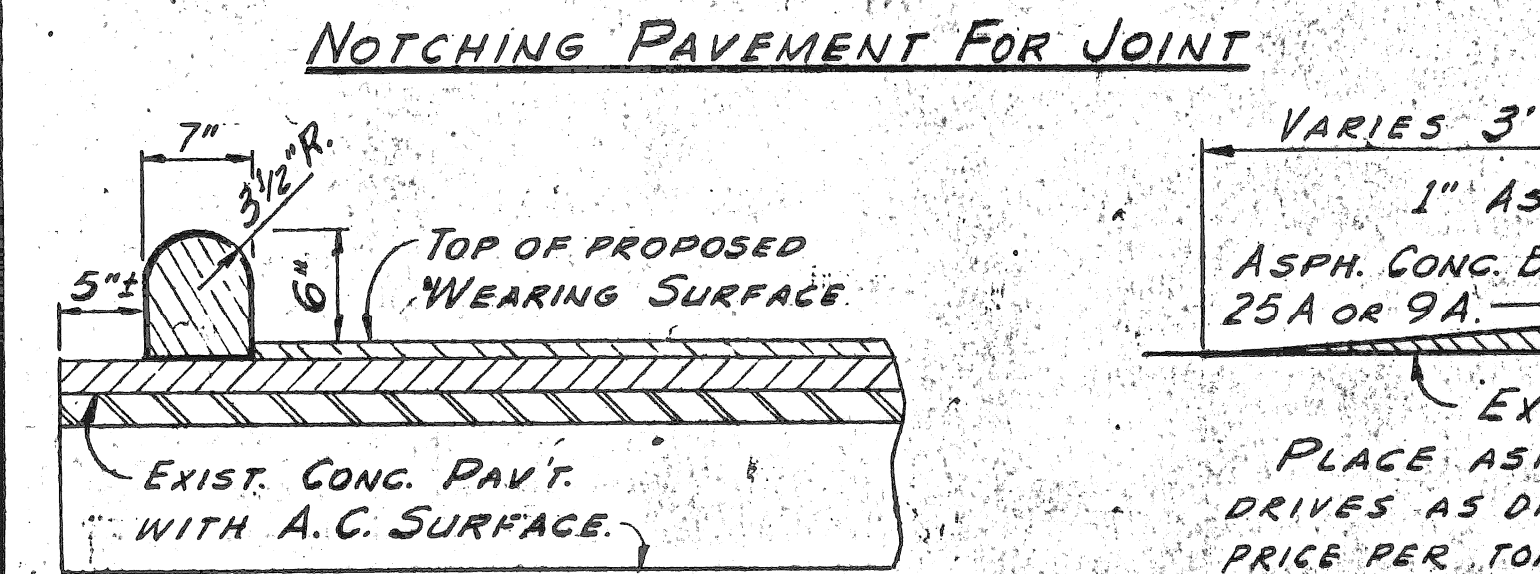
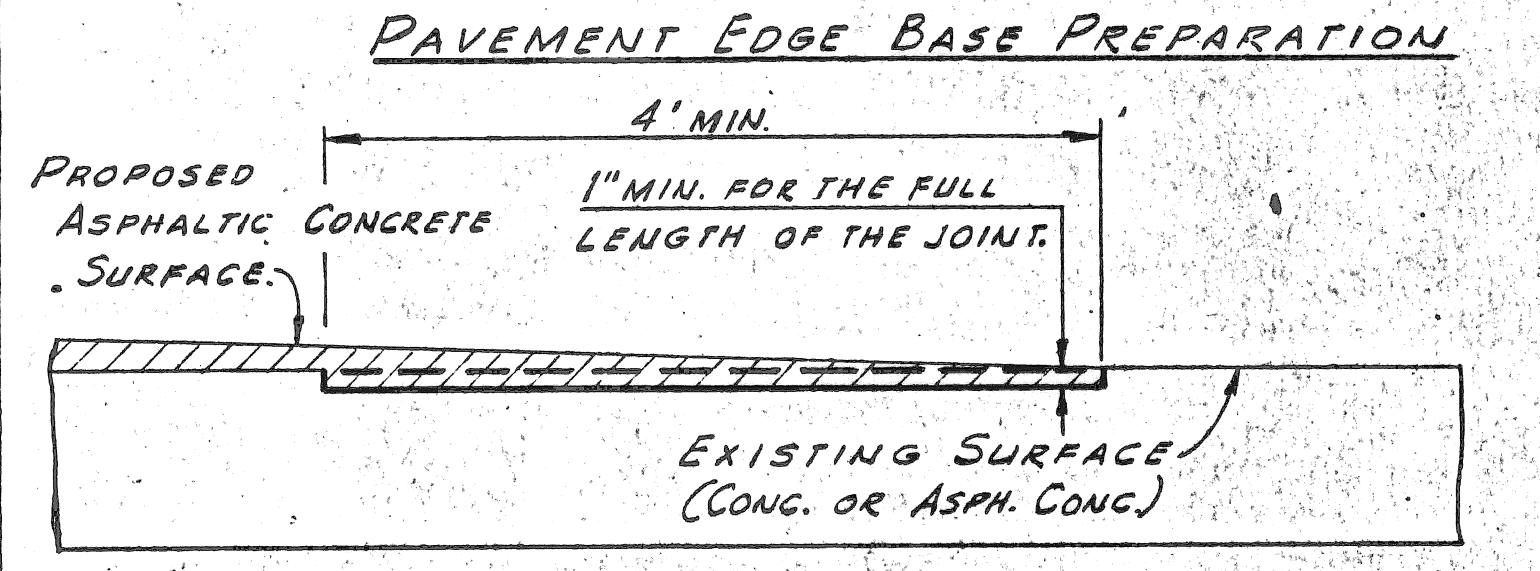
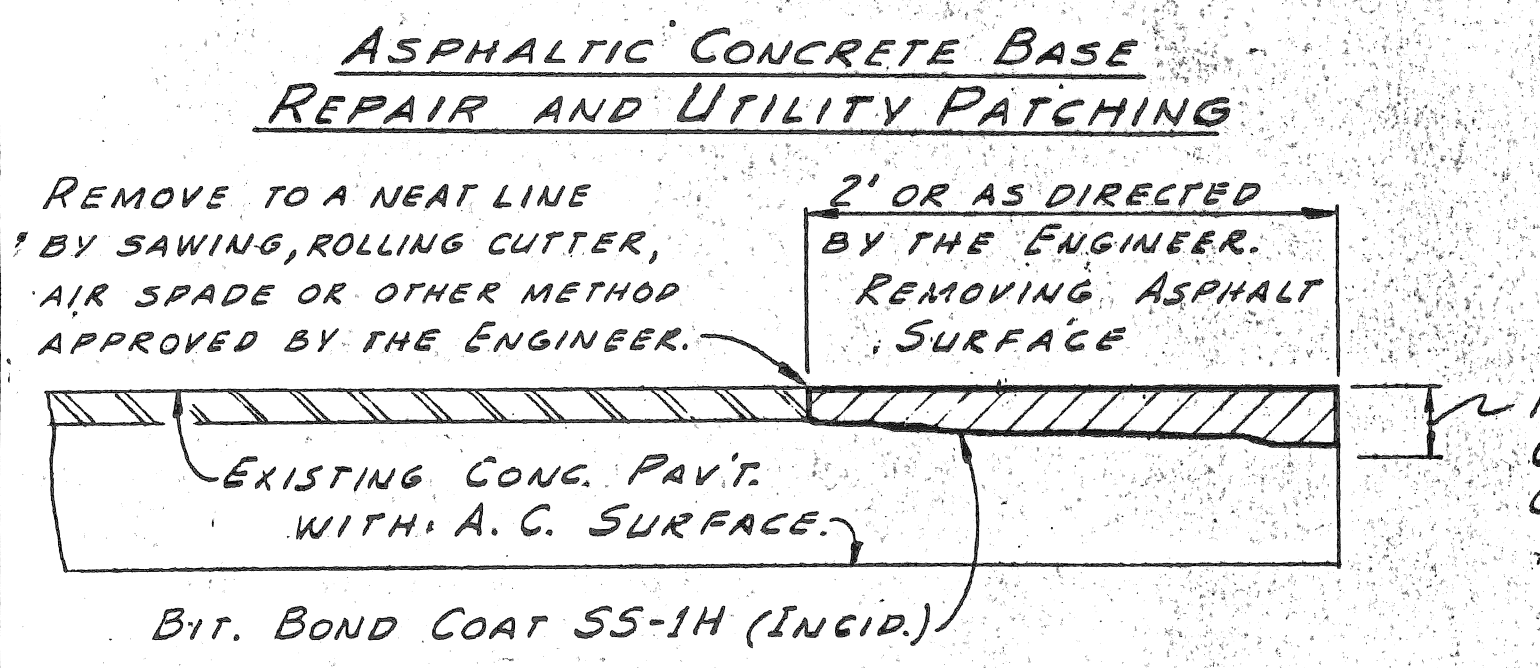
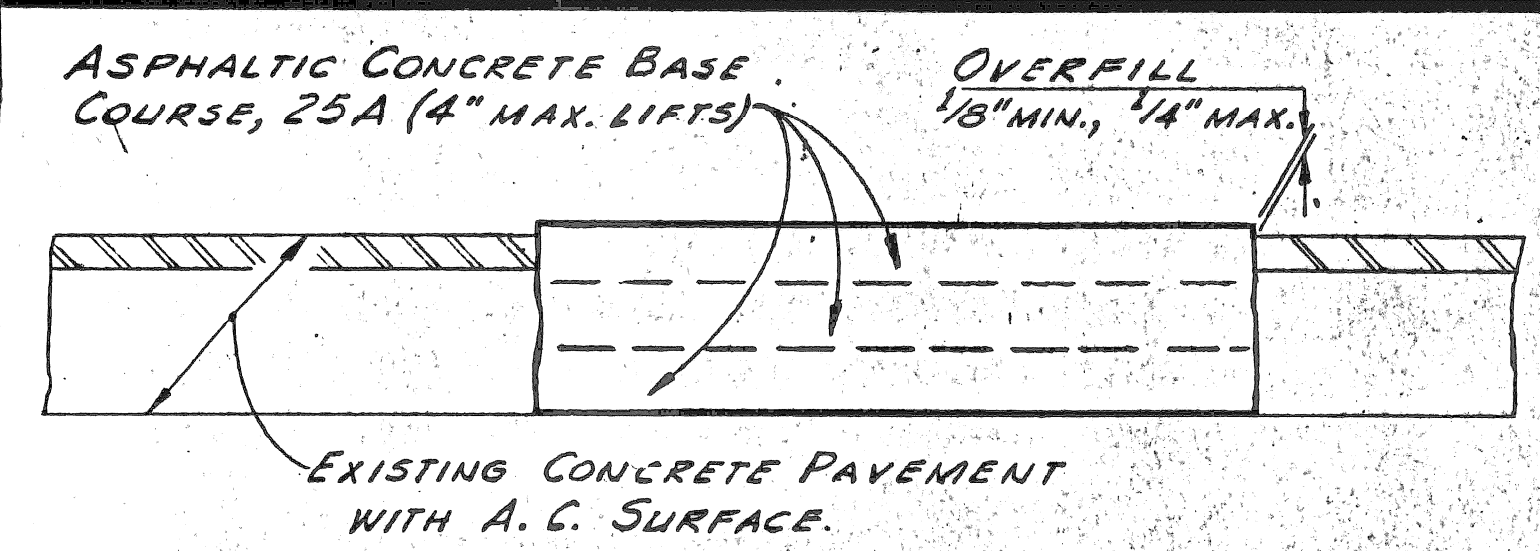
DRAWN BY M.W.G.

CHECKED BY

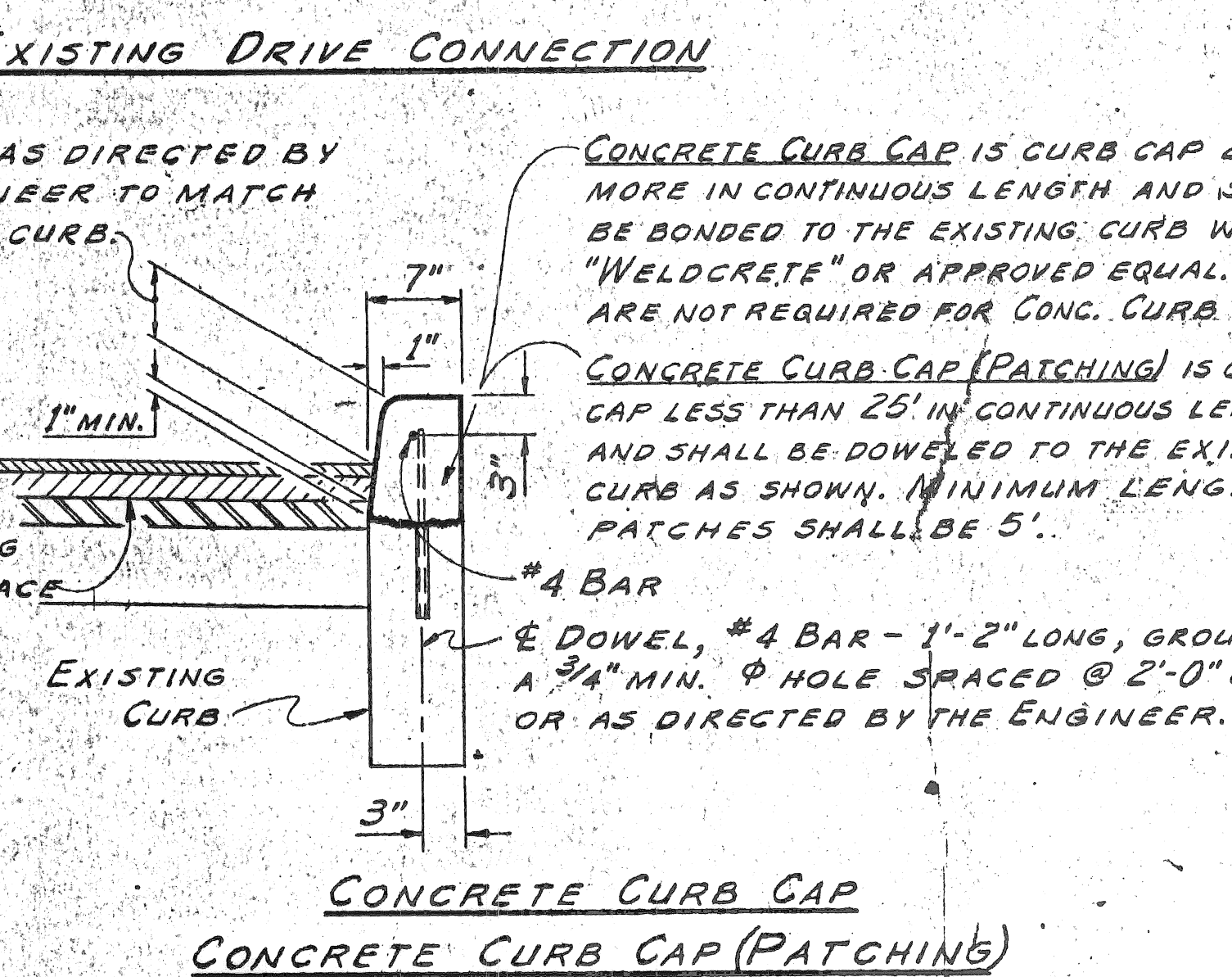
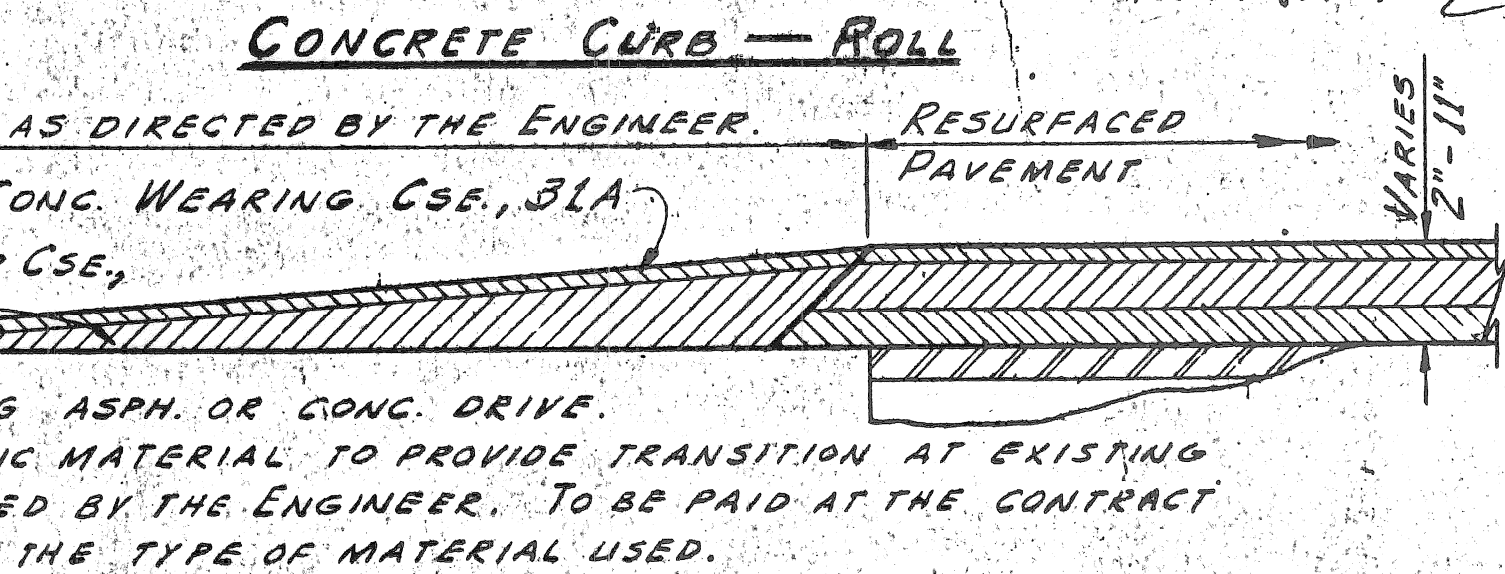
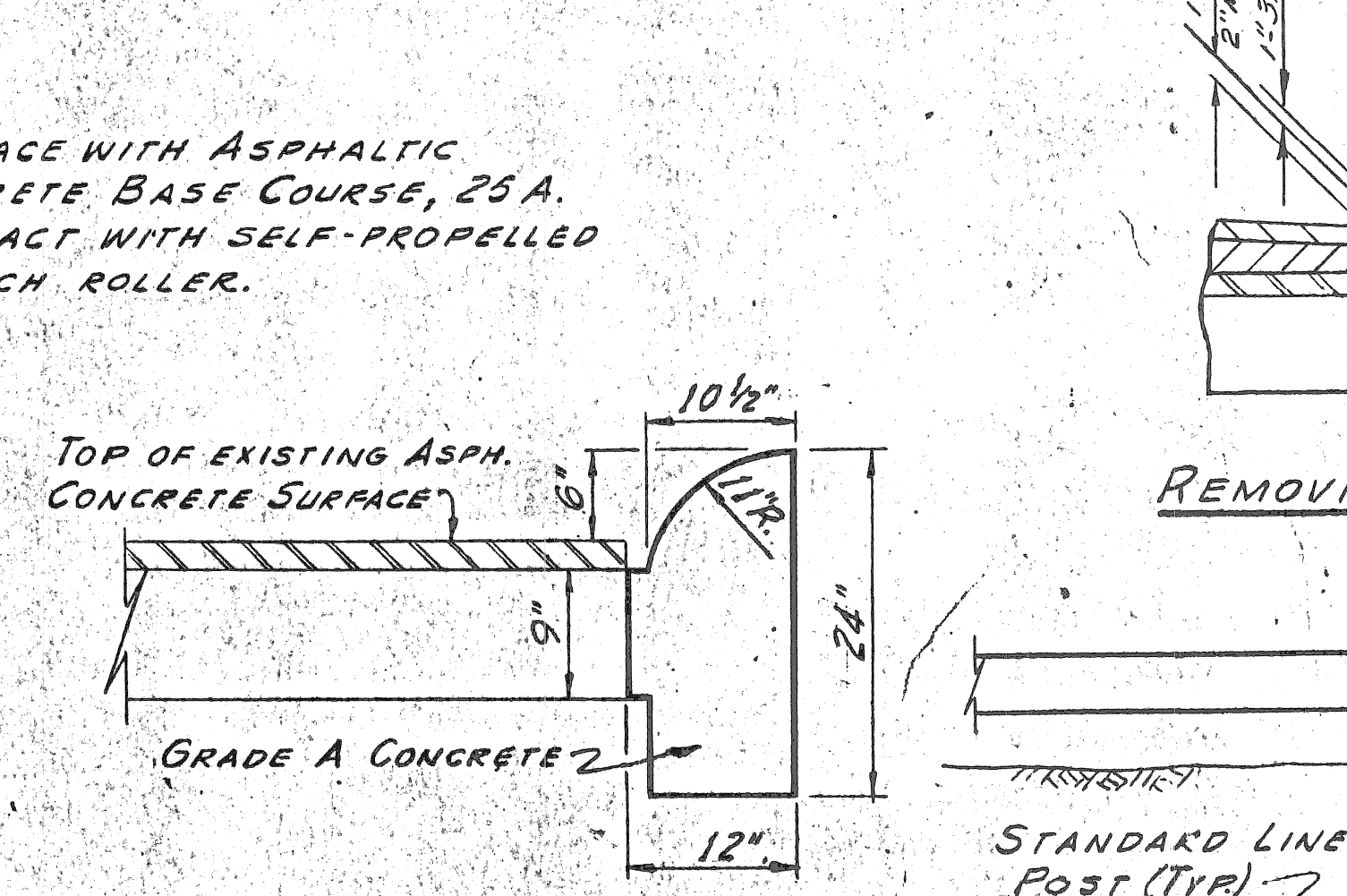
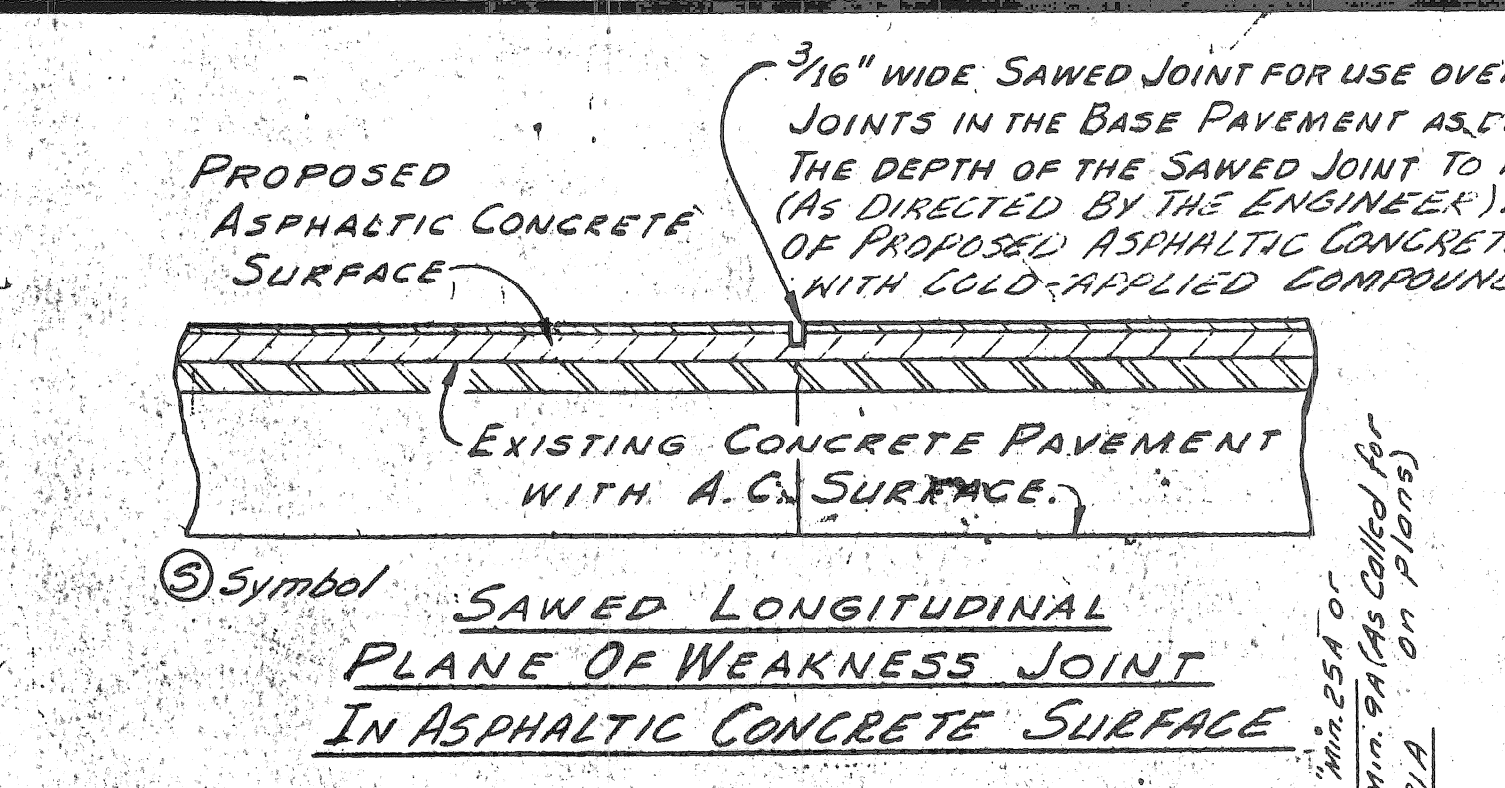
APPROVED

ENGINEER OF HIGHWAYS

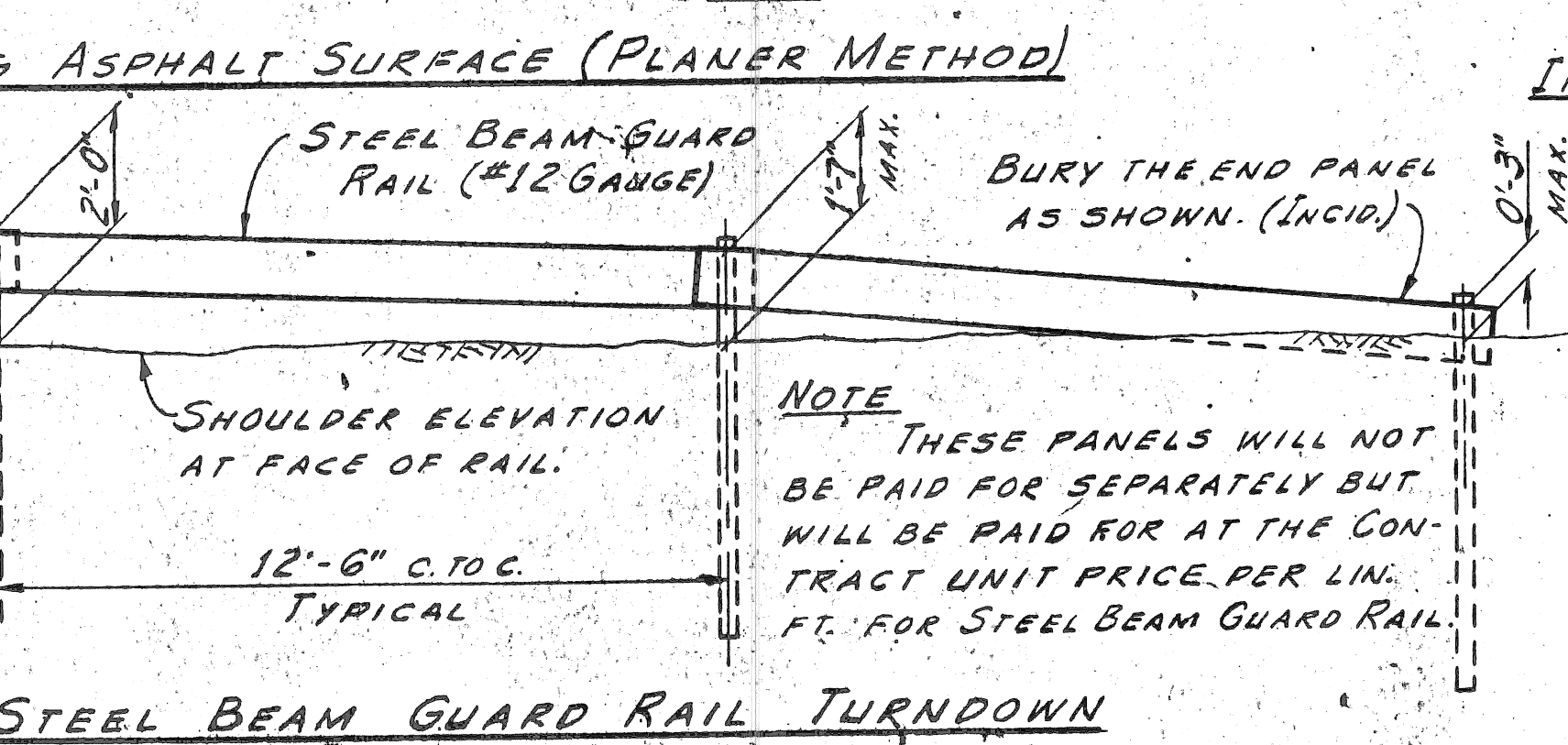
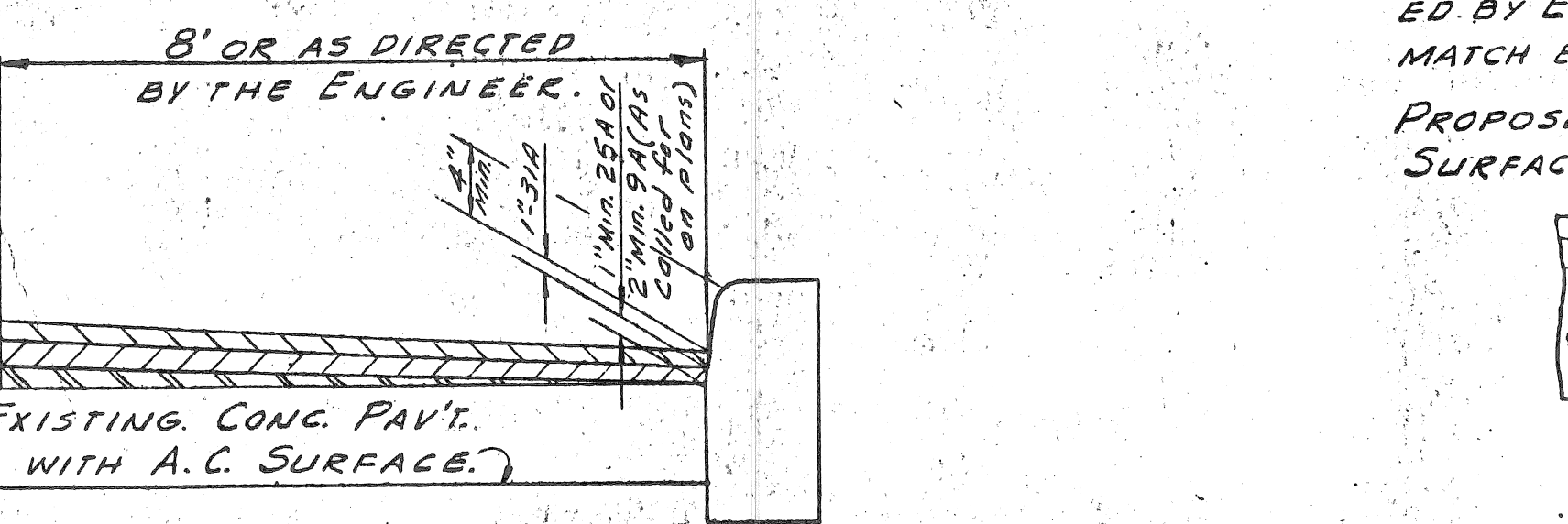
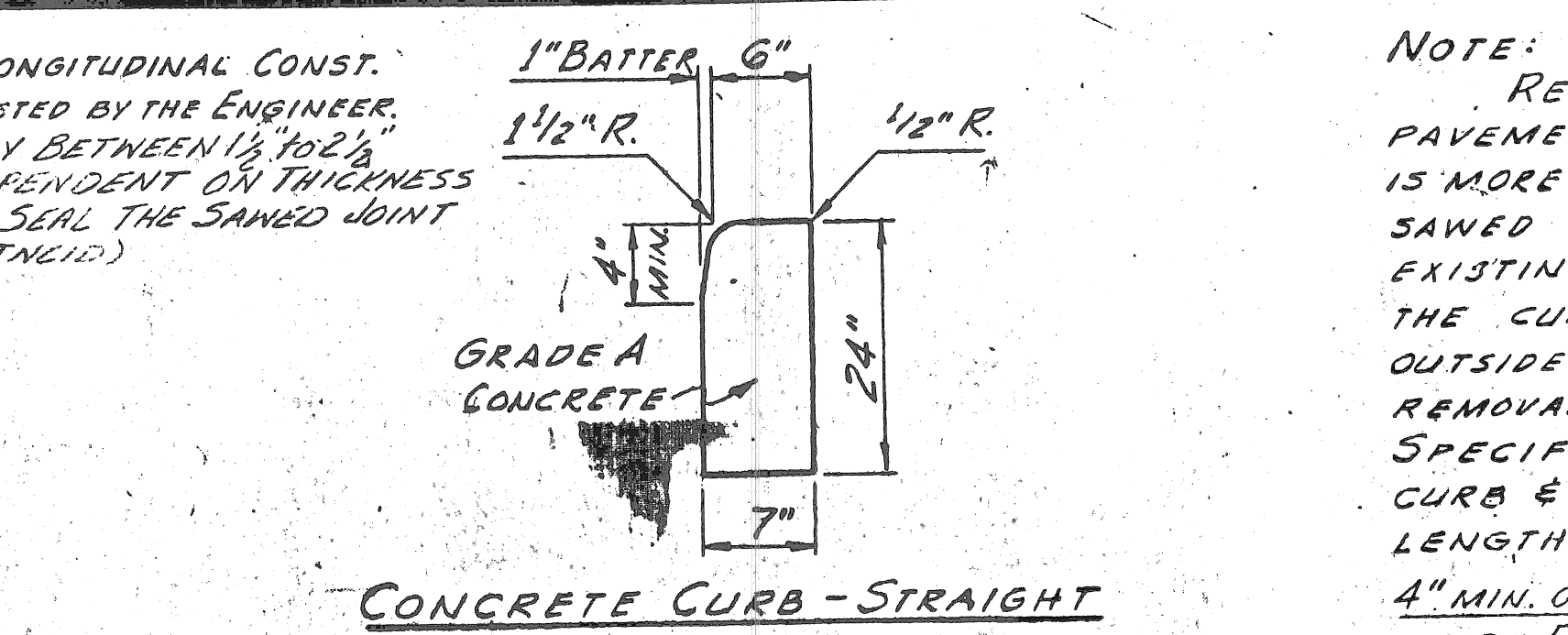
CHIEF HIGHWAY ENGINEER



GUARD POST

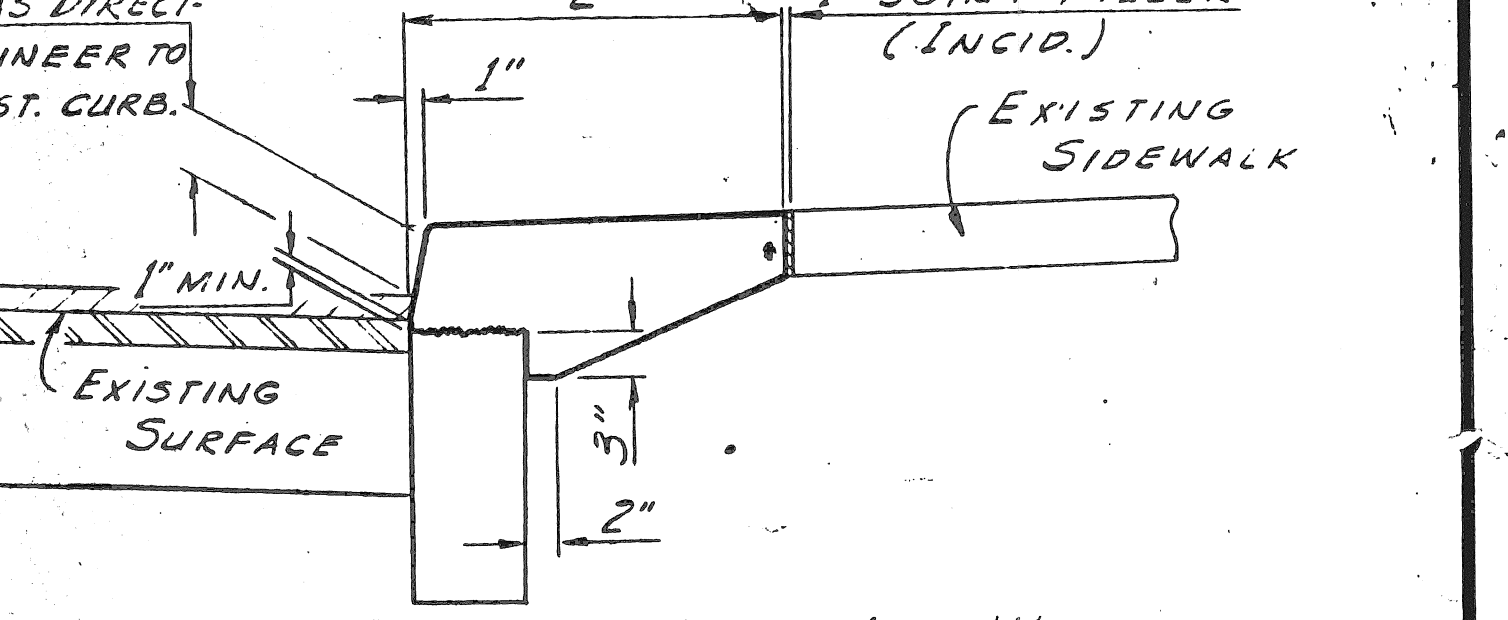


CONCRETE CURB CAP



STEEL BEAM GUARD RAIL TURNDOWN

NOTE: REMOVE EXISTING CURB TO A MINIMUM OF 1" BELOW EXISTING PAVEMENT SURFACE. IF AN EXISTING LONGITUDINAL SIDEWALK JOINT IS MORE THAN 3 1/2' FROM THE CURB FACE THE SIDEWALK SHALL BE SAWED FOR THE INTEGRAL CONG. CURB & WALK SECTION. IF AN EXISTING LONGITUDINAL SIDEWALK JOINT IS CLOSER THAN 3 1/2' FROM THE CURB FACE THE SIDEWALK SHALL BE REMOVED AND REPLACED OUTSIDE THE INTEGRAL CONG. CURB & WALK SECTION. SIDEWALK REMOVAL AND REPLACEMENT SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS. INTEGRAL CONCRETE CURB & WALK (PATCHING) IS CURB & WALK LESS THAN 25' IN CONTINUOUS LENGTH. MINIMUM LENGTH OF PATCHES WILL BE 5'.



INTEGRAL CONCRETE CURB AND WALK

INTEGRAL CONCRETE CURB AND WALK (PATCHING)

MIXTURE	YIELD FACTORS	
	NATURAL STONE (LBS.)	SLAG (LBS.)
9A BINDER	114.0	104.0
25A BINDER	107.0	105.0
25A WEARING	107.0	105.0
31A WEARING	108.0	102.5

BITUMINOUS BOND COAT (SS1H)
0.10 GAL/SQ. YD. ON EXISTING SURFACE.
0.05 GAL/SQ. YD. ON PREVIOUSLY PLACED SURFACE.

INTEGRAL CONCRETE CURB AND WALK (PATCHING)

GENERAL NOTES

ON PROJECTS WITHOUT CURB WHERE A WIDENING IS NOT PROPOSED EXCESS SHOULDER MATERIAL SHALL BE EXCAVATED TO LEVEL WITH OR BELOW THE EXISTING SURFACE (INCIDENTAL) TO PROVIDE FOR THE PLACEMENT OF A 5" MIN. DEPTH OVERLAY OF ASPHALTIC CONCRETE ON UNDISTURBED SHOULDER TO A SUFFICIENT WIDTH, AS DIRECTED BY THE ENGINEER, TO PROVIDE A STABLE BASE AND BACK-UP AT THE EDGE OF PAVEMENT FOR THE PLACING OF PROPOSED BINDER AND WEARING COURSES.

THE PREPARATION OF EXISTING AGGREGATE INTERSECTIONS AND DRIVEWAYS SHALL BE INCIDENTAL TO THE PLACING OF ASPHALTIC CONCRETE WEARING COURSE, 31A.

MAIL BOXES IN THE WAY OF SHOULDER CONSTRUCTION SHALL BE REMOVED AND RELOCATED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO PLACING AGGREGATE SHOULDERS.

THE ADJUSTMENT AND RECONSTRUCTION OF UTILITY STRUCTURES NOT INDICATED ON THE PLANS WILL BE PERFORMED BY THE UTILITY OWNERS.

ANY EXISTING CASTINGS DAMAGED BY THE CONTRACTOR IN THE COURSE OF WORK SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

STREET SIGNS AND TRAFFIC SIGNS IN THE WAY OF CONSTRUCTION SHALL BE REMOVED, TEMPORARILY SET AND FINALLY RESET BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. INCIDENTAL TO THE PROJECT.

THE LOCATION OF ALL PUBLIC UTILITIES SHOWN ON THESE PLANS IS TAKEN FROM THE BEST AVAILABLE DATA. THE BOARD OF WAYNE COUNTY ROAD COMMISSIONERS WILL NOT BE RESPONSIBLE FOR ANY OMISSIONS OR VARIATIONS FROM THE LOCATIONS SHOWN.

UNDERGROUND UTILITIES MAY EXIST WHERE THE LOCATION OF UTILITIES ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS REGARDING THEIR FACILITIES PRIOR TO STARTING ANY WORK OF EXCAVATION AND SHALL BE LIABLE FOR ALL DAMAGES CAUSED BY HIS OPERATIONS TO OTHER UTILITIES AND STRUCTURES.

TRANSIT MIX CONCRETE MAY BE USED.

ALIGNMENT SHALL BE MAINTAINED WHERE THERE ARE NO CURBS AND SHALL NOT VARY MORE THAN 1" FROM A LINE APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH MATERIALS AND LABOR TO SET A STRINGLINE TO CONTROL THE ALIGNMENT.

ASPHALTIC CONCRETE USED TO CONSTRUCT EXISTING A.C. OR CONCRETE DRIVE CONNECTIONS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER TON OF ASPHALTIC CONCRETE WEARING COURSE OR BINDER COURSE OF THE MIXTURE DESIGNATION PLACED.

ASPHALTIC CONCRETE RESURFACING DETAILS

NOT TO SCALE

1976 A.C. PROJECTS

CORRECT: [Signature]

APPROVED: [Signature]