DATE	REVISIONS

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-585	C-739	C-1608
C-327	C-586	C-739A	C-1783
C-390	C-587	C-739B	

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	
STREET WIDTH	NOTED
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER—12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A 01025	GROUP NO.	
BOOK NO.	PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW'G.	3-27-61		

	MADE BY	CHECKED BY
PLAN	L. CHAPMAN	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B
APPROVED	Erickson	
	Behm	
	ENGINEER OF STREETS	
	CITY ENGINEER	

INDEX NO.
—
TOPO NO.
—
INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

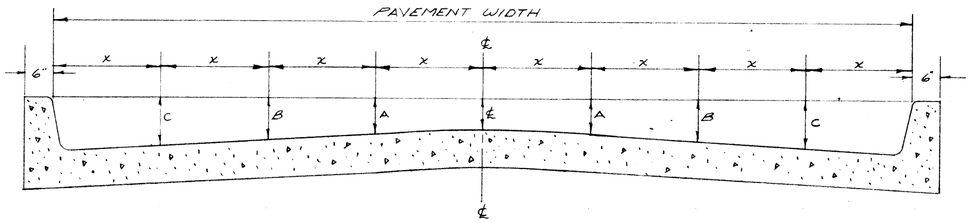
DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE

~~NAME OF STREET~~ FOREST — WARREN
~~FROM~~ REROUTING
~~TO~~ DETAIL SHEET

SHEET 11 OF 14 SHEETS

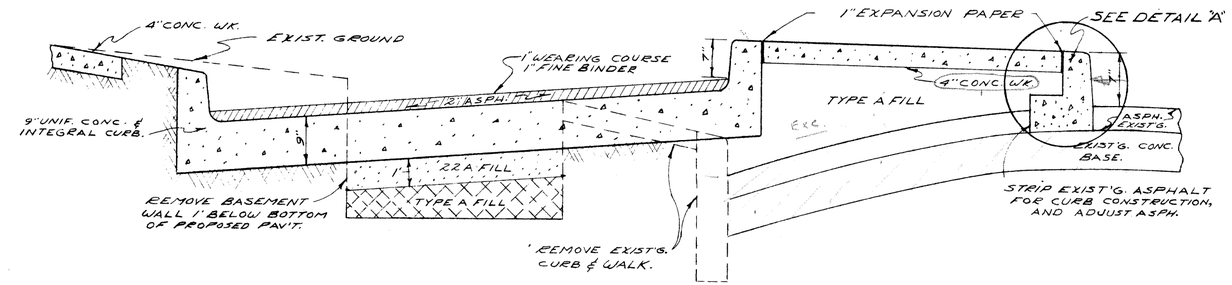
~~TYPE OF PAVEMENT 8 & 9 ONE COURSE CONCRETE INTEGRAL CURB~~
~~9" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~

~~FT. WD.~~

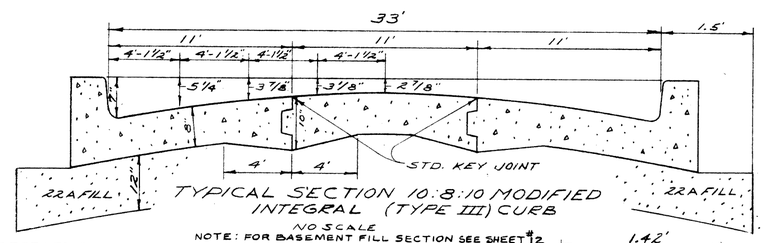


CONCRETE SLAB SECTION										
WIDTH	X	CURB DEPTH	C	B	A	℄	LANE WIDTH			
22'	2'-9"	7"	5 3/4"	5"	4 3/8"	4 1/4"	11'		11'	
33'	4'-1 1/2"	7"	5 1/4"	3 7/8"	3 1/8"	2 7/8"	11'	11'	11'	
44'	5'-6"	7"	4 5/8"	2 7/8"	1 7/8"	1 1/2"	11'	11'	11'	11'
48'	6'-0"	7"	4 3/8"	2 1/2"	1 3/8"	1"	12'	12'	12'	12'

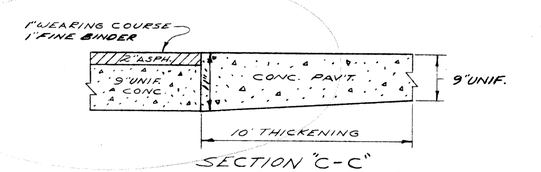
TYPICAL SECTION
NO SCALE



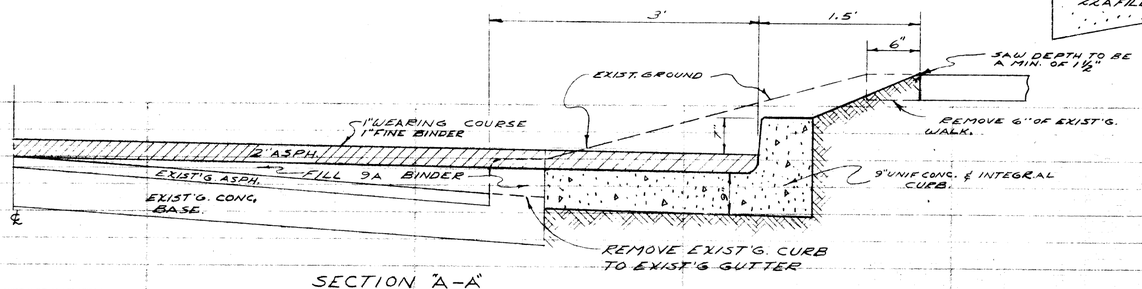
SECTION "F-F"
NO SCALE SEE SH. #6



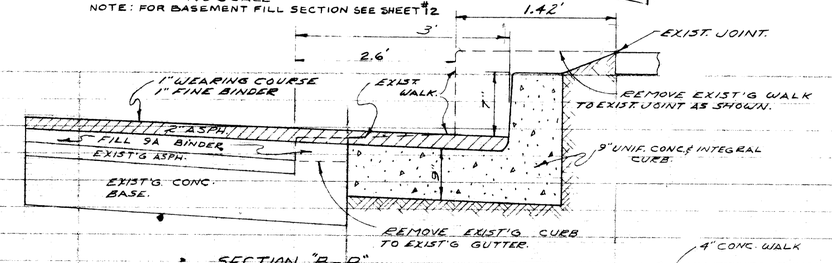
TYPICAL SECTION 10:8:10 MODIFIED INTEGRAL (TYPE III) CURB
NO SCALE
NOTE: FOR BASEMENT FILL SECTION SEE SHEET #12



SECTION "C-C"
NO SCALE SEE SH. #7



SECTION "A-A"
NO SCALE SEE SH. #6



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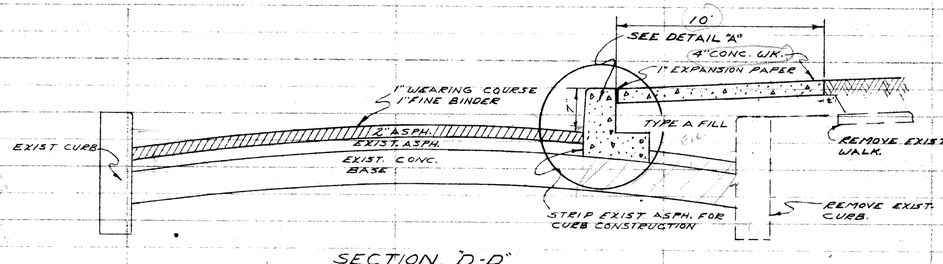
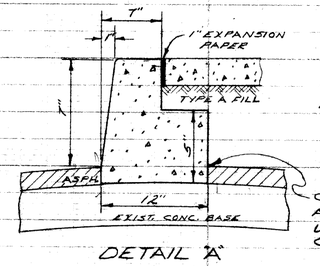
NO SCALE SEE SH. #6



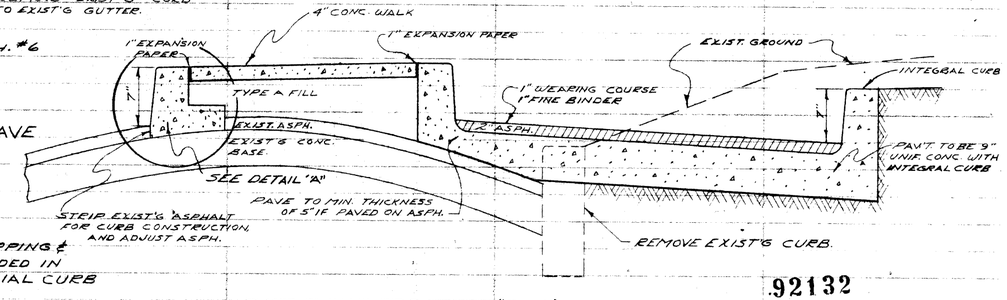
SECTION "D-D"
NO SCALE SEE SH. #7



DETAIL "A"
NO SCALE

NOTE:
ALL CURBS TO HAVE 1" BATTER

COST OF ASPH. STRIPPING & ADJUST TO BE INCLUDED IN UNIT PRICE FOR SPECIAL CURB CONSTRUCTION.



SECTION "E-E"
NO SCALE SEE SH. #10

92132

DATE	REVISIONS

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-585	C-739	C-1608
C-327	C-586	C-739A	C-1783
C-390	C-587	C-739B	

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE
LOTS & ALLEYS
STREET WIDTH
VERTICAL
DRAINAGE COMPLETED
PAVING COMPLETED

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
" "	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER–12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A0 1025	GROUP NO.	
BOOK NO.	PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW'G.	3-31-61		

	MADE BY	CHECKED BY
PLAN	L. CHAPMAN	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.

APPROVED Erickson
Behm
ENGINEER OF STREETS
CITY ENGINEER

INDEX NO.
TOPO NO.
INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

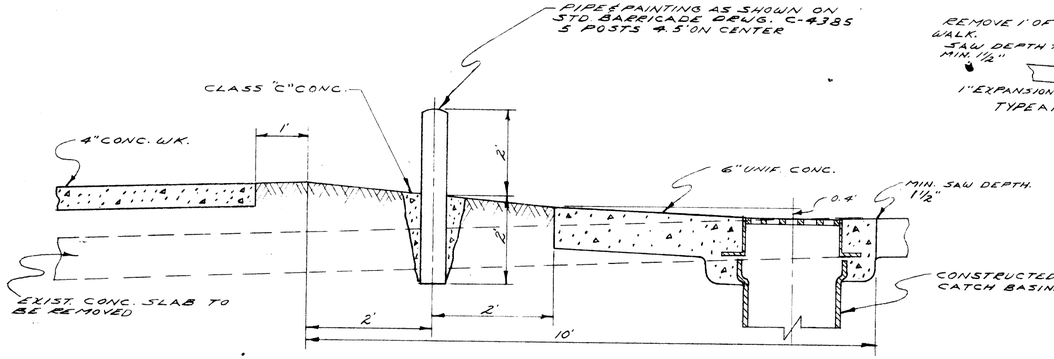
DEPARTMENT OF PUBLIC WORKS – CITY ENGINEERS OFFICE

~~NAME OF STREET~~ FOREST – WARREN
~~FROM:~~ REROUTING
~~TO~~ DETAIL SHEET

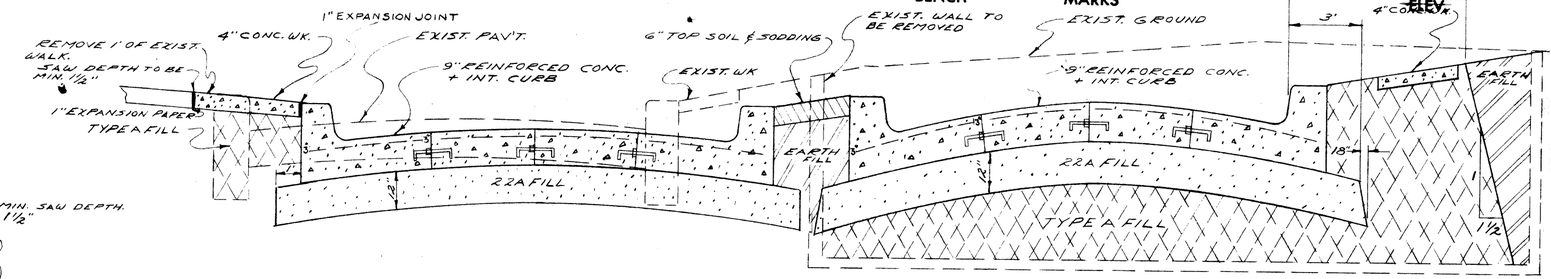
SHEET 12 OF 14 SHEETS

~~TYPE OF PAVEMENT 8 & 8 ONE COURSE CONCRETE INTEGRAL CURB~~
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~

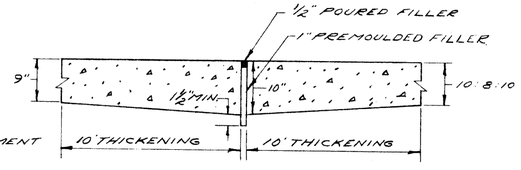
~~FT. WD.~~



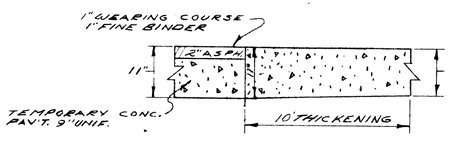
SECTION "G-G"
NO SCALE SEE SH. #7



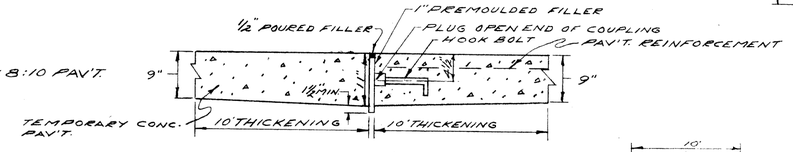
TYPICAL FILL SECTION – WARREN AVE. WIDENING
NO SCALE



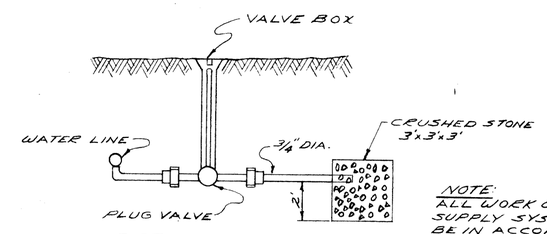
SECTION "M-M"
NO SCALE SEE SH. #9



SECTION "K-K"
NO SCALE SEE SH. #10



SECTION "H-H"
NO SCALE SEE SH. #7

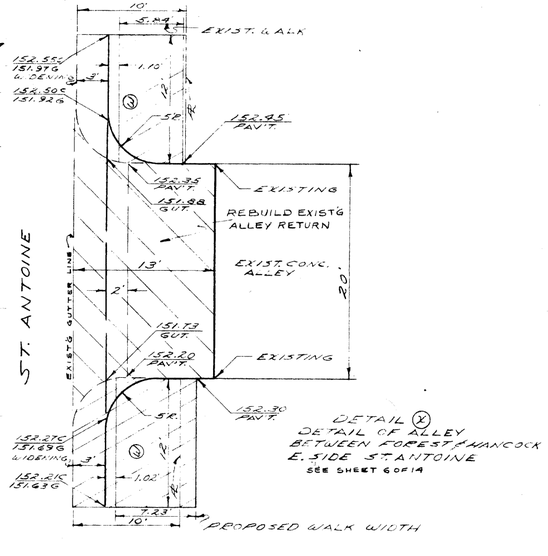


DETAIL OF MANUAL DRAIN
NO SCALE

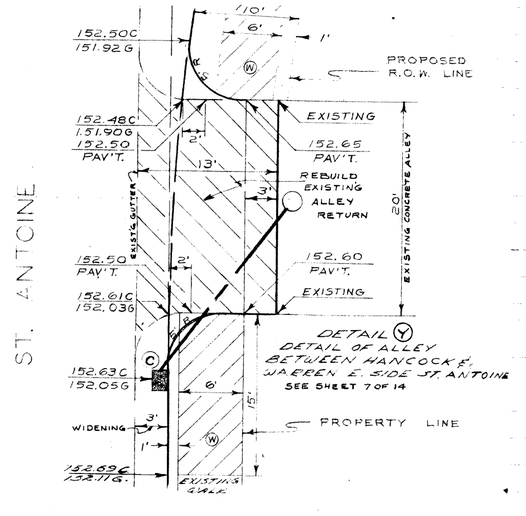
NOTE:
ALL WORK ON THE WATER SUPPLY SYSTEM SHALL BE IN ACCORDANCE WITH THE OFFICAL PLUMBING CODE OF THE CITY OF DETROIT.

LEGEND

- (M) METER PIT
- VB-O CONTROL VALVE & VACUUM BREAKER
- ● HOSE CONNECTION
- □ MANUAL DRAIN



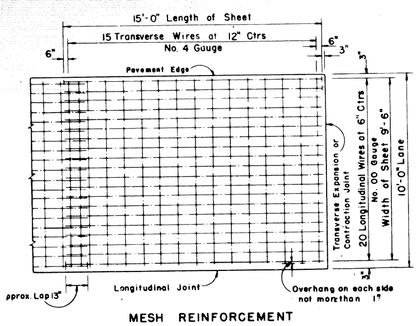
DETAIL (X)
DETAIL OF ALLEY BETWEEN FOREST & HANCOCK E. SIDE ST. ANTOINE
SEE SHEET 6 OF 14



DETAIL (Y)
DETAIL OF ALLEY BETWEEN HANCOCK & WARREN E. SIDE ST. ANTOINE
SEE SHEET 7 OF 14

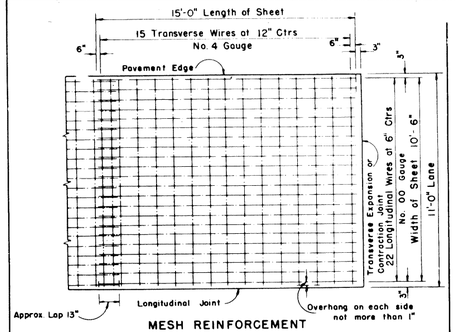
92133

REINFORCEMENT FOR 10 FT. LANES



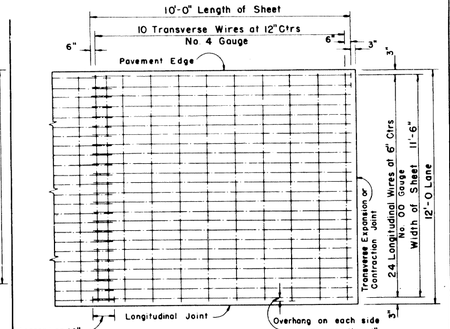
MESH REINFORCEMENT

REINFORCEMENT FOR 11 FT. LANES

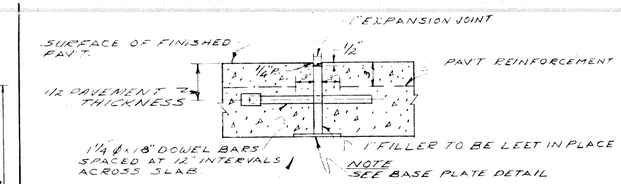


MESH REINFORCEMENT

REINFORCEMENT FOR 12 FT. LANES



MESH REINFORCEMENT

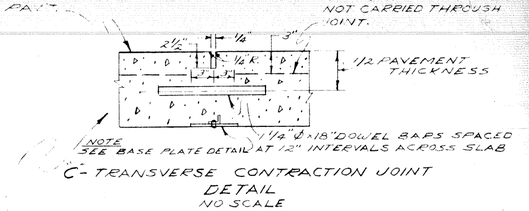


THE FILLER STRIP SHALL BE LEFT IN PLACE & THE JOINT FORMED WHILE THE CONCRETE IS STILL FRESH & SHALL BE TRUE TO POSITION & LINE. SEAL WITH POURED FILLER.

"E"-TRANSVERSE EXPANSION JOINT WITH LOAD TRANSFER

DETAIL

NO SCALE

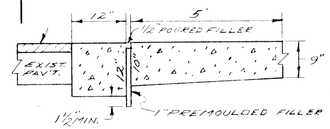


"C"-TRANSVERSE CONTRACTION JOINT

DETAIL

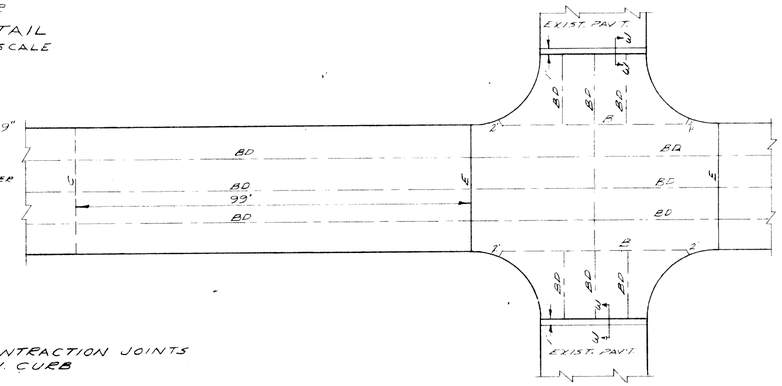
NO SCALE

THE JOINT FORMING STRIP ON THE LOAD TRANSFER DEVICE SHALL BE REMOVED & THE JOINT FORMED WHILE THE CONCRETE IS STILL FRESH & SHALL BE TRUE TO POSITION & LINE. THE JOINT SHALL BE SEALED WITH POURED FILLER.



SECTION W-W

NO SCALE



NOTE:

EXPANSION & CONTRACTION JOINTS TO EXTEND THRU. CURB

CONCRETE PAVEMENT-MULTIPLE LANES REINFORCED

The pavement reinforcement shall be shipped and delivered to the work in flat sheets or mats.

The reinforcement shall be placed 3 inches below the surface of the finished pavement.

Pavement reinforcement will be measured by area in square yards. The area paid for shall be equal to the square yards of pavement specified or required to be reinforced with no allowance for necessary laps or splices.

Where the unit width of pavement slab is more than shown, the reinforcement requirements shall be the same as shown on this standard plan except that the reinforcement shall be widened by adding split sheets or mats or by inserting special sheets or mats. Side laps shall be not less than the spacing of longitudinal wires or bars for wire mesh ~~and bar mat, and the width of diamond for expanded metal mesh~~.

Where the unit width of pavement slab is less than shown, the reinforcement requirements shall be the same as shown on this standard plan. Special sheets of the required width may be used or standard sheets may be cut to the required size. Side laps shall be not less than the spacing of longitudinal wires or bars for wire mesh ~~and bar mat, and the width of diamond for expanded metal mesh~~.

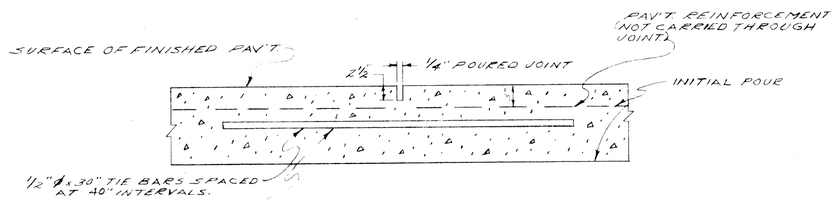
Size of wires are based on U.S. Steel Wire Gauge and weights for Mesh Reinforcement are according to the Wire Reinforcement Institute Tables. ~~Weights of bars in Bar Mat Reinforcement are according to the table for weights as shown in the Standard Specifications.~~

Reinforcement for 12 foot lanes may consist of sheets 15 feet long with a longitudinal lap. The reinforcement requirements shall be the same as shown on this standard plan. Side laps shall be as specified above.

COMPUTED WEIGHTS OF STEEL

Type Of Reinforcement	Average Weight per 100 Sq. Ft. Of Pavement (pounds)			Average Weight per Sq. Yard Of Pavement (pounds)			Weight per Standard Sheet (pounds)		
	10'-LANE	11'-LANE	12'-LANE	10'-LANE	11'-LANE	12'-LANE	10'-LANE	11'-LANE	12'-LANE
MESH	75.9	75.9	79.6	6.8	6.8	7.2	107.3	118.1	85.9

STANDARD PLAN FOR PAVEMENT REINFORCEMENT

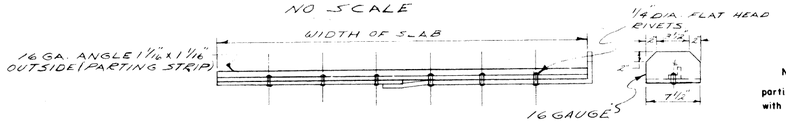


AFTER THE FIRST COURSE HAS BEEN POURED & STRUCK OFF 1/2" ϕ x 30" TIE BARS SHALL BE PLACED ON THE SURFACE OF THE FIRST COURSE AT THE PROPER SPACING LONGITUDINALLY & TRANSVERSELY & AT 90° WITH THE JOINT. THE REINFORCEMENT SHALL THEN BE PLACED & PROPERLY SPACED & EXTREME CARE SHALL BE EXERCISED TO AVOID DISPLACEMENT OF THE TIE BARS

"D"-LONGITUDINAL LANE TIE JOINT

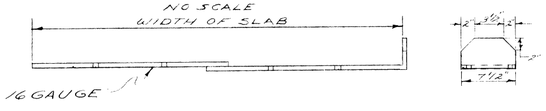
DETAIL

NO SCALE



BASE PLATE DETAILS FOR TRANSVERSE CONTRACTION JOINT

NO SCALE



BASE PLATE DETAILS FOR EXPANSION JOINT

NO SCALE

NOTES

The surfaces of the metal shields, metal end plates and metal parting strips, which will be in contact with concrete, shall be painted with form oil to prevent bonding with the concrete.

Changes in the lengths of the various pieces that make up the base plate will be permitted, provided that the correct overall length is obtained and a minimum of 3" lap as shown at all joints in base plates is provided.

End plates may be made as an integral part of the main base plate by bending, instead of separate pieces as shown, when approved by the Engineer.

MATERIAL:

The base plate metal shall be no. 16 gauge copper bearing steel having a 2-ounce uniform coating of prime spelter. The prime spelter coating shall be applied by the hot process to the base metal by an approved method which forms a perfect bond and amalgamation between the base metal and the coating. The finished sheet, coated by any process shall withstand the forming and shaping required for the sheet without flaking or powdering of the coating.

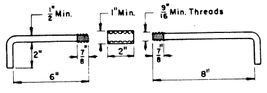
The sampling, testing and acceptance of the plates for chemical composition, gauge, and weight of spelter coating will be based on the current A.A.S.H.O. Designation: M 36.

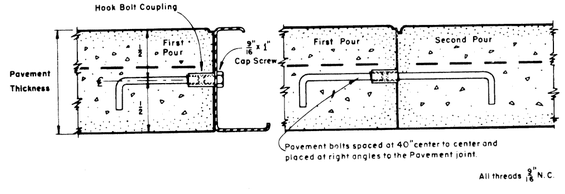
NOTES

B LONGITUDINAL BULKHEAD CONSTRUCTION JOINT
C TRANSVERSE CONTRACTION JOINT
D LONGITUDINAL LANE TIE JOINT
E 1" TRANSVERSE EXPANSION JOINT WITH LOAD TRANSFER DEVICE
BD OPTIONAL B OR D JOINT

LONGITUDINAL BULKHEAD CONSTRUCTION JOINT

Other methods of holding the first half of the Hook Bolt in position will be permitted, if approved by the Engineer.





JOINT DOWEL HOOK BOLT

B STANDARD PLAN FOR CONCRETE · PAVEMENT · JOINTS

F						
E						
D						
C						
B						
A						
	DESCRIPTION	DRW	CKD	APR	DATE	
	REVISIONS					

DESIGNED BY

DRAWN BY L. CHAPMAN

TRACED BY

CHECKED BY

APPROVED Erickson

Behm

CITY ENGINEER

CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE

FOREST—WARREN REROUTING DETAIL SHEET

BOOK NO. PG. NO. SCALE NO SCALE DATE 4-7-61

SHEET 13 OF 14 SHTS.

JOB NO. A-1125

CONTRACT NO. 66-3935

92134

DATE	REVISIONS

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-585	C-739	C-1608
C-327	C-586	C-739A	C-1783
C-390	C-587	C-739B	

BEFORE STARTING CONSTRUCTION CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	
STREET WIDTH	
VERTICAL	AS NOTED
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER 15"	LIN. FT.		

PETITION NO.	PETITION DATE
ORDINANCE NO. AO 1025	ORDER NO.
DOCK NO. PG.	SEWER DATE

LOTS 30	LENGTH
CITY 31	WIDTH
	LENGTH
DATE OF DRAW'G. 3-31-61	

	MADE BY	CHECKED BY
TRACED		
BROWN		
GRADE		
STRUCTURE		
SEWER APPROVED		
FINAL CHECK	G.E.B.	
APPROVED	Erickson	

CITY ENGINEER OR STREET COMMISSIONER

Wayne R. Thomas
CITY ENGINEER

INDEX NO.

TOPO NO.

INDEXED

CONTRACT NO. P.W. 3955

STREET PAVING PLAN

CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS - CITY ENGINEERS OFFICE

STREET FOREST - WARREN

FROM REROUTING

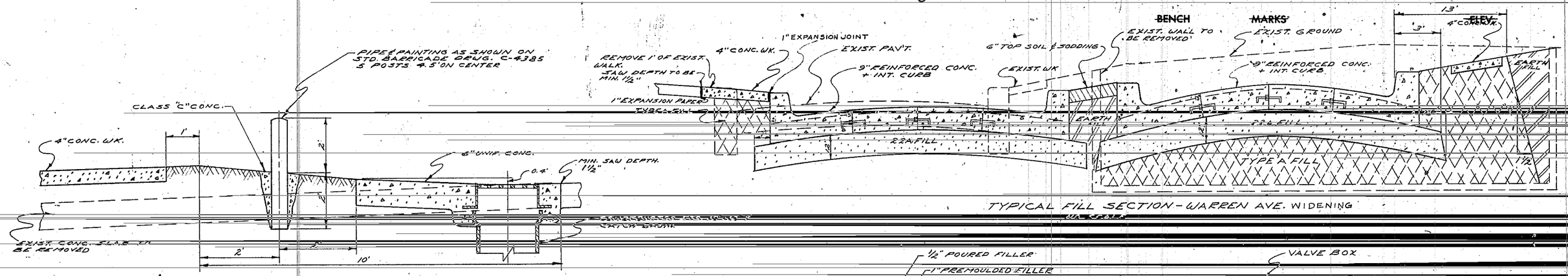
TO DETAIL SHEET

SHEET 7

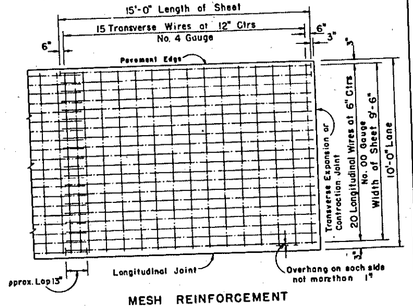
SHEETS

TYPE OF PAVEMENT 2 & 3 ONE COURSE CONCRETE INTEGRAL CURB FT. WIDE

8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED

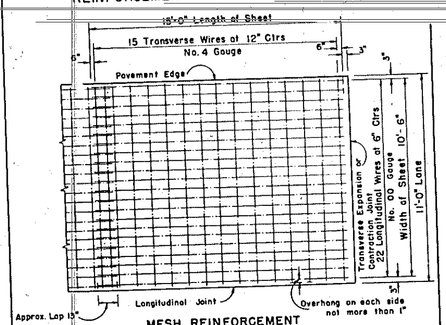


REINFORCEMENT FOR 10 FT. LANES



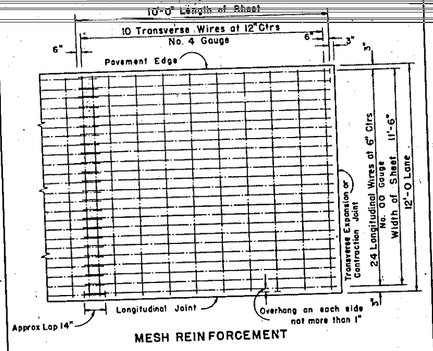
MESH REINFORCEMENT

REINFORCEMENT FOR 11 FT. LANES



MESH REINFORCEMENT

REINFORCEMENT FOR 12 FT. LANES



MESH REINFORCEMENT

STANDARD PLAN FOR PAVEMENT REINFORCEMENT

The pavement reinforcement shall be shipped and delivered to the work in flat sheets or mats.

The reinforcement shall be placed 3 inches below the surface of the finished pavement.

Pavement reinforcement will be measured by area in square yards the area paid for shall be equal to the square yards of pavement specified or required to be reinforced with no allowance for necessary laps or splices.

Where the unit width of pavement slab is more than shown, the reinforcement requirements shall be the same as shown on this standard plan except that the reinforcement shall be widened by adding split sheets or mats or by inserting special sheets or mats. Side laps shall be not less than the spacing of longitudinal wires or bars for wire mesh ~~and bar mat, and the width of diamond for expanded metal mesh~~.

Where the unit width of pavement slab is less than shown, the reinforcement requirements shall be the same as shown on this standard plan. Special sheets of the required width may be used or standard sheets may be cut to the required size. Side laps shall be not less than the spacing of longitudinal wires or bars for wire mesh ~~and bar mat, and the width of diamond for expanded metal mesh~~.

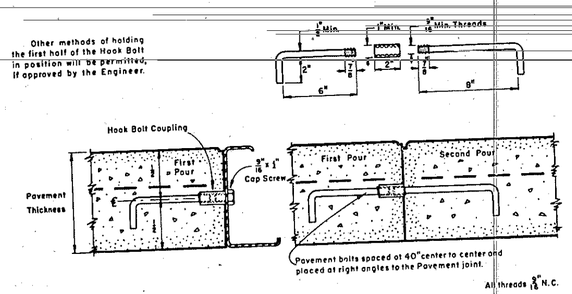
Size of wires are based on U.S. Steel Wire Gauge and weights for Mesh Reinforcement are according to the Wire Reinforcement Institute Tables. ~~Weights of bars in Bar Mat Reinforcement are according to the tables for weights as shown in the Standard Specifications.~~

Reinforcement for 12 foot lanes may consist of sheets 15 feet long with a longitudinal lap. The reinforcement requirements shall be the same as shown on this standard plan. Side laps shall be as specified above.

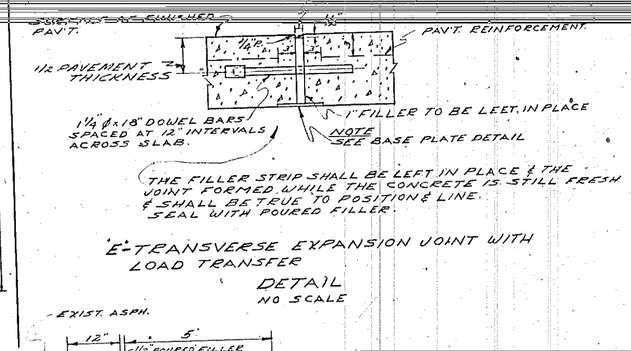
COMPUTED WEIGHTS OF STEEL

Type Of Reinforcement	Average Weight per 100 Sq.Ft. Of Pavement (pounds)			Average Weight per Sq.Yard Of Pavement (pounds)			Weight per Standard Sheet (pounds)		
	10' LANE	11' LANE	12' LANE	10' LANE	11' LANE	12' LANE	10' LANE	11' LANE	12' LANE
MESH	75.9	75.9	79.6	6.8	6.8	7.2	107.3	118.1	89.9

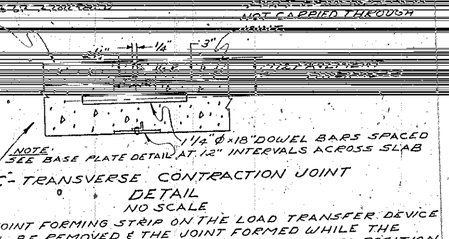
LONGITUDINAL BULKHEAD CONSTRUCTION JOINT



JOINT DOWEL HOOK BOLT

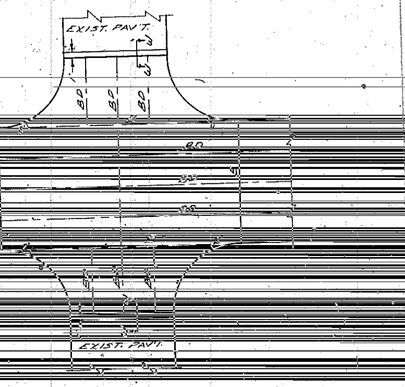


'E'-TRANSVERSE EXPANSION JOINT WITH LOAD TRANSFER
DETAIL
NO SCALE

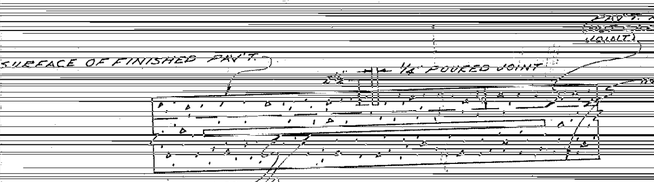


'C'-TRANSVERSE CONTRACTION JOINT
DETAIL
NO SCALE

THE JOINT FORMING STRIP ON THE LOAD TRANSFER DEVICE SHALL BE REMOVED & THE JOINT FORMED WHILE THE CONCRETE IS STILL FRESH & SHALL BE TRUE TO POSITION & LINE. THE JOINT SHALL BE SEALED WITH POURED FILLER.



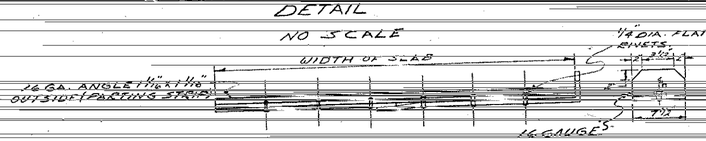
CONCRETE PAVEMENT-MULTIPLE LANES



NOTE:
SEE CONSTRUCTION LEGEND FOR CONTRACTION JOINTS

(AFTER THE FIRST COURSE HAS BEEN TRAVERSED ON THE SURFACE OF THE FIRST COURSE OF THE BRIDGE SPACING TRANSVERSELY AT 30" WITH THE JOINT THE REMAINING SHALL THEN BE PLACED & PROPERLY SPACED & EXTRA TIE PLACE SHALL BE CONTINUED TO AVOID DISPLACEMENT OF THE TIE BARS)

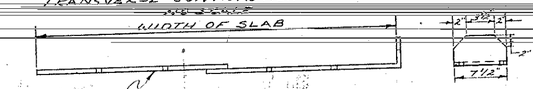
'D' LONGITUDINAL LANE TIE JOINT
DETAIL
NO SCALE



NOTES

B LONGITUDINAL BULKHEAD CONSTRUCTION JOINT
C TRANSVERSE CONTRACTION JOINT
D LONGITUDINAL LANE TIE JOINT
E 1" TRANSVERSE EXPANSION JOINT WITH LOAD TRANSFER DEVICE.

BASE PLATE DETAILS FOR TRANSVERSE CONTRACTION JOINT
NO SCALE



BASE PLATE DETAILS FOR EXPANSION JOINT
NO SCALE

NOTES

The surfaces of the base plate metal, anchors and metal sealing strips, which will be in contact with concrete shall be painted with form oil to prevent bonding with the concrete.

Changes in the lengths of the various pieces that make up the base plate will be permitted, provided that the assembled overall length is obtained and the dimensions of the parts as shown in the plans is maintained.

End plates may be made as an integral part of the main base plate or sealing, instead of separate pieces as shown, when approved by the Engineer.

MATERIAL:

The base plate metal shall be no. 16 gauge copper bearing steel having a 2-ounce uniform coating of prime spelter. The prime spelter coating shall be applied by the hot process to the base metal by an approved method which forms a perfect bond and amalgamation between the base metal and the coating. The finished sheet, coated by any process shall withstand the forming and shaping required for the sheet without flaking or powdering of the coating.

The sampling, testing and acceptance of the plates for chemical composition, gauge, and weight of spelter coating will be based on the current A.A.S.H.O. Designation: M 36.

C.C. 8-21-62

B - STANDARD PLAN FOR
CONCRETE · PAVEMENT · JOINTS

F						
E						
D						
C						
B						
A						
	DESCRIPTION REVISIONS	DRW	CKD	APR	DATE	

DESIGNED BY
DRAWN BY
TRACED BY
CHECKED BY

APPROVED
CITY ENGINEER

CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE

FOREST—WARREN
REROUTING
DETAIL SHEET

SHEET 13 OF 14 SHTS.
JOB NO. AU 7020
CONTRACT NO. PW-3935

ITEM NO.	DESCRIPTION OF ITEM	QUANTITY AND PAY UNIT
1	Excavation	6,986 Cyds.
2	Pavement Removal (Street, Including Curb and Backing)	7,516 Syds.
3	Pavement Removal (Alley)	280 Syds.
4	Concrete Drive Removal	54 Syds.
5	Concrete Walk Removal	24,983 Sft.
6	Foundation Removal	712 Cyds.
7	Tree Removal (4"-12" Dia.)	14 Each
8	" " (13"-18" Dia.)	21 Each
9	" " (19" Dia.)	1 Each
10	" " (20" Dia.)	3 Each
11	" " (24" Dia.)	3 Each
12	" " (30" Dia.)	3 Each
13	Curb Removal (Including Backing)	1,512 Lft.
14	Fill, Type A	3,409 Cyds.
15	Fill, 22A	5,390 Cyds.
16	Manhole (Sewer), Adjust	9 Each
17	Manhole (Water), Adjust	17 Each
18	Manhole - Reconstruct Cone	1 Each
19	Manhole - Standard, New	4 Each
20	Catch Basin - Standard, New	34 Each
21	Catch Basin - Adjust	2 Each
22	Sewer - 12" Pipe	1,745 Lft.
23	Sewer - 12" Pipe Under Pavement	125 Lft.
24	Sewer - 15" Pipe	315 Lft.
25	Paving - 10:8:10 Concrete**, Modified (Plain-Unreinforced)	1,528 Syds.
26	Paving - 10" Uniform Concrete**, (Plain-Unreinforced)	326 Syds.
27	Paving - 9" Uniform Concrete**	8,532 Syds.
28	Pavement Reinforcement	8,532 Syds.
29	Paving - 9" Uniform Concrete** (Plain-Unreinforced)	10,175 Syds.
30	Paving - 6" Uniform Concrete*** (Plain-Unreinforced)	37 Syds.
31	Curb - Special	117 Lft.
32	Curb - Type II	30 Lft.
33	Test Cores	20 Each
34	Sidewalk, Std. 4"	32,272 Sft.
35	Concrete Drive - 6" Std.	234 Sft.
36	" " - 8" Std.	100 Sft.
37	Barricade (Pipe Type)	9 Each
38	Top Soil	722 Cyds.
39	Sodding	4,346 Syds.
40	Steel Beam Guard Rail - Single	65 Lft.
41	Steel Beam Guard Rail - Double	70 Lft.
42	Maintain Traffic	Lump Sum
43	Piping for Lawn Watering System	Lump Sum
44	Stripping Existing Surface Including Gutter Stripping	155 Syds.
45	Manhole Adapters	7 Each
46	Catch Basin Adapters	2 Each
47	Resetting Stop Boxes	4 Each
48	Rebuilding Catch Basins	1 Each
49	Bond Coat in Place	1,112 Gals.
50	Sheet Asphalt Wearing Surface in Place	5 Tons
51 a	Fine Binder Course Mixture ~~2330-B~~, (All Natural Aggregates)	180 Tons
51 b	Fine Binder Course Mixture ~~2330-B~~, (All Slag Aggregates)	170 Tons
52 a	Binder Course Mixture 3A (All Natural Aggregates)	175 Tons
52 b	Binder Course Mixture 3A (All Slag Aggregates)	160 Tons
53 a	Wearing Surface Mixture 31A, Type IX Asphaltic Concrete (All Natural Aggregates)	290 Tons
53 b	Wearing Surface Mixture 31A, Type IX Asphaltic Concrete (All Slag Aggregates)	270 Tons

** Includes Type II or Type III Curbs, See Std. Drawings C-4380 or C-4381.

*** No Curbing Included.

NOTE: Levels will be retaken and quantities recomputed after buildings have been removed and before construction begins.

REVISED 4-7-61

HEARING

REVISED

DATE | REVISIONS

SUPPLEMENTARY REPORTS

C-252 C-253 C-254 C-255 C-256 C-257 C-258 C-259 C-260 C-261 C-262 C-263 C-264 C-265 C-266 C-267 C-268 C-269

BEFORE STARTING CONSTRUCTION CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE

LOTS & ALLEYS

STREET WIDTH

VERTICAL

DRAINAGE COMPLETED

PAVING COMPLETED

QUANTITIES: EXCAVATION CU. YDS.; PAVING SQ. YDS.; TEST CORES; SIDEWALK SQ. FT.; MARKERS CONSTRUCT ADJUST; BASINS CONSTRUCT ADJUST; SEWER-12" LIN. FT.

ESTIMATED: LOTS CITY

PETITION NO. ASSIGNMENT NO. BOOK NO. PG.

LOTS 2L CITY 2L TOTAL

LENGTH

DATE OF ORDER 4-7-61

PETITION DATE ORDER NO. SERVICE DATE

LENGTH

PLAN PROJECT DESIGN CHECKED BY G.E.B.

SEWER APPROVED FINAL CHECK APPROVED

ENGINEER-OF-STREETS

CITY ENGINEER

INDEXED

INDEX NO.

~~NAME OF STREET~~ FOREST - WARREN

~~LIMITS~~ REROUTING

TO QUANTITY SHEET

SHEET 14 OF 14 SHEETS

CONTRACT NO. 39-35

STREET PAVING PLAN

CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS

CONTRACT NO. P.W. 3955

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — BUREAU OF ENGINEERING

NAME OF STREET: FOREST - WARREN

FROM: BERDELLING

TO: DOUGLAS

TYPE OF PAVEMENT B & B ONE COURSE CONCRETE INTEGRAL CURB ON UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED

DATE	REVISIONS
6/4/62	ADDED QUANTITY CHANGE L.P.K.

BEFORE STARTING CONSTRUCTION CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE

LOTS & ALLEYS

STREET WIDTH

VERTICAL

DRAINAGE COMPLETED

PAVING COMPLETED

SUPPLEMENTARY DETAILS			
C-325	C-324	C-733	C-938
C-326A	C-385	C-739	C-1608
C-327	C-386	C-739A	C-1783
C-590	C-587	C-739B	

QUANTITIES		LOTS	80%
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

PETITION NO.	PETITION 80%
ASSIGNMENT NO.	GROUP NO.
BOOK NO. PG.	LEVEL DATE

LOTS 2L		LENGTH
CITY 2L		LENGTH
TOTAL		LENGTH

DATE OF PAVING 4-7-61

	MADE BY	CHECKED BY
TRACING		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.
APPROVED	Graham	
	Dobson	
	Roger L. Dalman ENGINEER OF STREETS	
	CITY ENGINEER	

INDEX NO. ✓✓

TOPO NO.

INDEXED

QUANTITY AND PAY UNIT	ITEM NO.	DESCRIPTION OF ITEM
17,081 Cyds.	1	Excavation
7,516 Syds.	2	Pavement Removal (Street, Including Curb and Backing)
234 Syds.	3	Pavement Removal (Alley)
61 Syds.	4	Concrete Drive Removal
24,983 Sft.	5	Concrete Walk Removal
712 Syds.	6	Foundation Removal
14 Each	7	Tree Removal (4"-12" Dia.)
21 Each	8	" " (13"-18" Dia.)
1 Each	9	" " (19" Dia.)
3 Each	10	" " (20" Dia.)
3 Each	11	" " (24" Dia.)
3 Each	12	" " (30" Dia.)
5,512 Lft.	13	Curb Benchwall (Including Backing)
3,409 Cyds.	14	Fill, Type A
5,310 Cyds.	15	Fill, 22A
9 Each	16	Manhole (Sewer), Adjust
17 Each	17	Manhole (Water), Adjust
1 Each	18	Manhole - Reconstruct Cone
4 Each	19	Manhole - Standard, New
24 Each	20	Manhole - Standard, New
2 Each	21	Catch Basin - Standard, New
1,745 Lft.	22	Catch Basin - Adjust
125 Lft.	23	Sewer - 12" Pipe
115 Lft.	24	Sewer - 12" Pipe Under Pavement
1,528 Syds.	25	Sewer - 15" Pipe
34 Syds.	26	Paving - 10:1:3 Concrete Modified (Plain-Unreinforced)
9,533 Syds.	27	Paving - 10" Uniform Concrete (Plain-Unreinforced)
9,532 Syds.	28	Paving - 7" Uniform Concrete
10,115 Syds.	29	Pavement Reinforcement
37 Syds.	30	Paving - 9" Uniform Concrete (Mesh-Reinforced)
117 Lft.	31	Paving - 6" Uniform Concrete (Mesh-Reinforced)
30 Lft.	32	Curbs - Special
2 Each	33	Curbs - Type II
32,27 Sft.	34	Curb Cones
23 Sft.	35	Sidewalk, Std. 4"*
10 Sft.	36	Concrete Drive - 6" Std.
1 Each	37	" - 8" Std.
795 Syds.	38	Bituminous (Pipe Type)
1,346 Syds.	39	Sod Mat
290 Lft.	40	Guardrail
70 Lft.	41	Steel Beam Guard Rail
Lump Sum	42	Steel Beam Guard Rail
Lump Sum	43	Maintain Traffic
125 Syds.	44	Topsoil for Lawn Waterway
7 Each	45	Relocating Existing Buoyance Center Striping
2 Each	46	Miscellaneous Adjust (Residential)
4 Each	47	Catch Basin Adapters
1 Each	48	Flushing Stop Boxes
138 Gals.	49	Flushing Catch Basins
4 Tons	50	Tack Coat in Place
180 Tons	51	Asphalt Wearing Course
165 Tons	52 a	Prime Binder Course (All Bituminous Mixture)
175 Tons	52 b	(All Sand Asphalt)
350 Tons	53 a	Under Course Mixture (All Natural Aggregate)
290 Tons	53 b	Under Course Mixture (All Sand Asphalt)
275 Tons	54	Surface Mixture (All Natural Aggregate)
	55	Surface Mixture (All Sand Asphalt)
	56	Concrete (All Curb, Gutter and Driveways)
	57	Concrete Surface (All Curb, Gutter and Driveways)
	*	CONSTRUCTION NOTES:
		Type I or Type II Curb, Gutter and Driveway Cement Concrete
		C-4 Oil.
		Paving will be weather permitting.
		Drainage Inspection Manhole.
		Drainage Inspection Manhole.

QUANTITY CHANGE	DATE
CATCH BASIN (ABANDON) + 13 EACH	

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS – CITY ENGINEERS OFFICE
FOREST – WARREN REROUTING

NAME OF STREET RUSSELL

FROM UTILITY LOCATION AND

TO REMOVAL SHEET

SHEET 5 OF 14 SHEETS

~~TYPE OF PAVEMENT 8-6-8 ONE COURSE CONCRETE INTEGRAL CURB~~ FT. WD.
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~

DATE	REVISIONS

SUPPLEMENTARY DETAILS

C-325	C-524	C-733	C-930
C-326A	C-585	C-739	C-1608
C-327	C-586	C-739A	C-1783
C-398	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

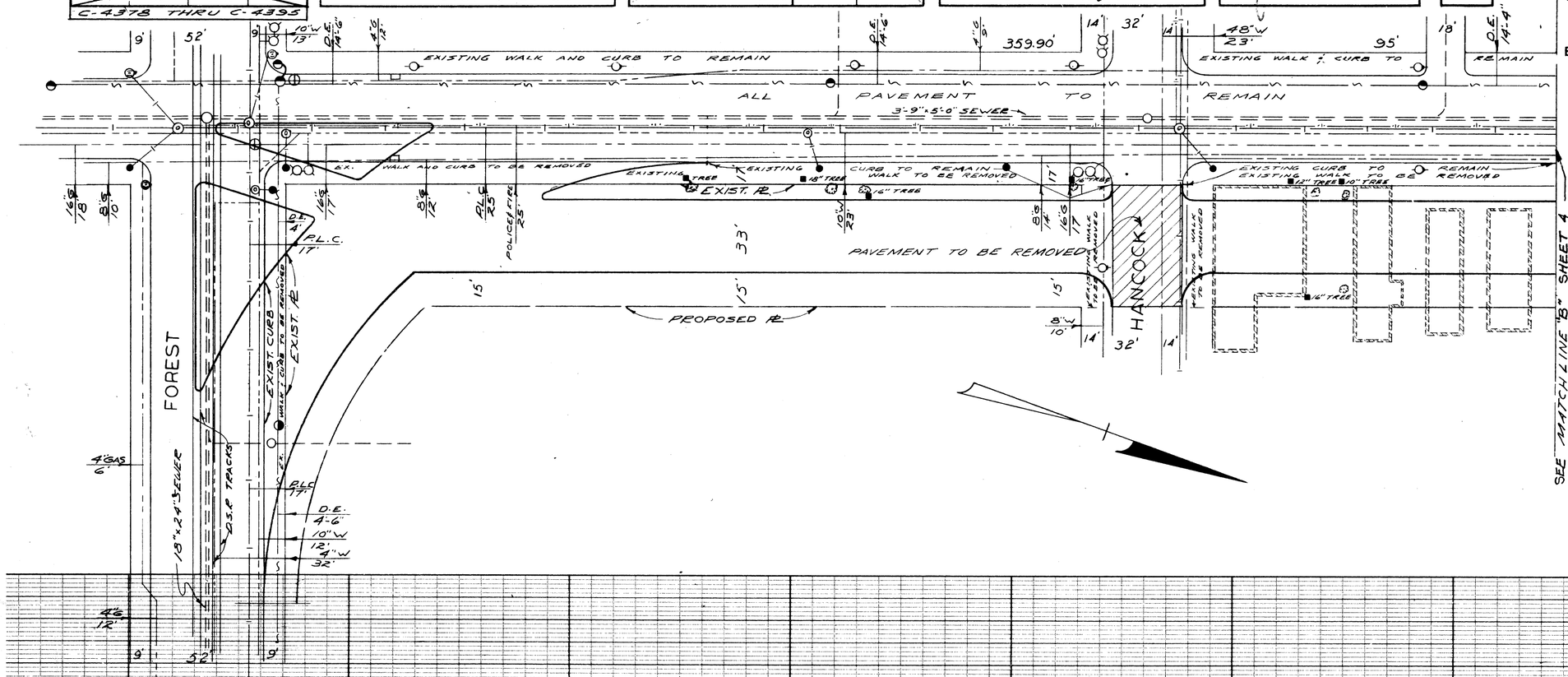
PETITION NO.	—	PETITION DATE	—
ASSIGNMENT NO.	A0 1025	GROUP NO.	
BOOK	1911 4 / 1909 37	LEVEL	3-8-60 / 3-27-60
NO.	1842 PG. 1 / 1851 19	DATE	FEB. 1955 / FEB. 1955
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW'G.	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	L. CHAPMAN	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.
APPROVED	Erickson	
	Behm	
	ENGINEER OF STREETS	
	CITY ENGINEER	

INDEX NO. 6

TOPO NO. 40

INDEXED



BENCH MARKS ELEV.

LEGEND

DETROIT EDISON — MANHOLE
DETROIT FIRE & POLICE
DETROIT GAS CO. — MANHOLE
PUBLIC LIGHT
WATER LINE
TRAFFIC LIGHT
LIGHT POLE
HYDRANT
SEWER & MANHOLE
SEWER
POLE (ALL TYPES)
CATCH BASIN

NOTE
EXISTING CURB, WALK, AND PAVEMENT TO BE REMOVED AS NOTED ON DRAWING.

SEE SHEET 10 OF 14 FOR PAVING PLAN

■ REMOVE TREES IF NECESSARY

92126

DATE	REVISIONS
6-4-62	ADDED C. BASIN TO ABANDON N.S. FOREST AT 143' E. RUSSEL

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-585	C-739	C-1608
C-327	C-586	C-739A	C-1783
C-390	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	1" = 2'
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"			
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A01025	GROUP NO.	
BOOK 909 NO. 1911	PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW G.	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	CHAPMAN	—
PROFILE	CHAPMAN	—
GRADE	W. OWENS	GRANTIGAN
ESTIMATE		
SEWER APPROVED	3/30/61	
FINAL CHECK		G.E.B.
APPROVED	Erickson	
	Behm	
	ENGINEER OF STREETS	
	CITY ENGINEER	

INDEX NO.	TOPO NO.	INDEXED
6	40	

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS – CITY ENGINEERS OFFICE
FOREST – WARREN REROUTING

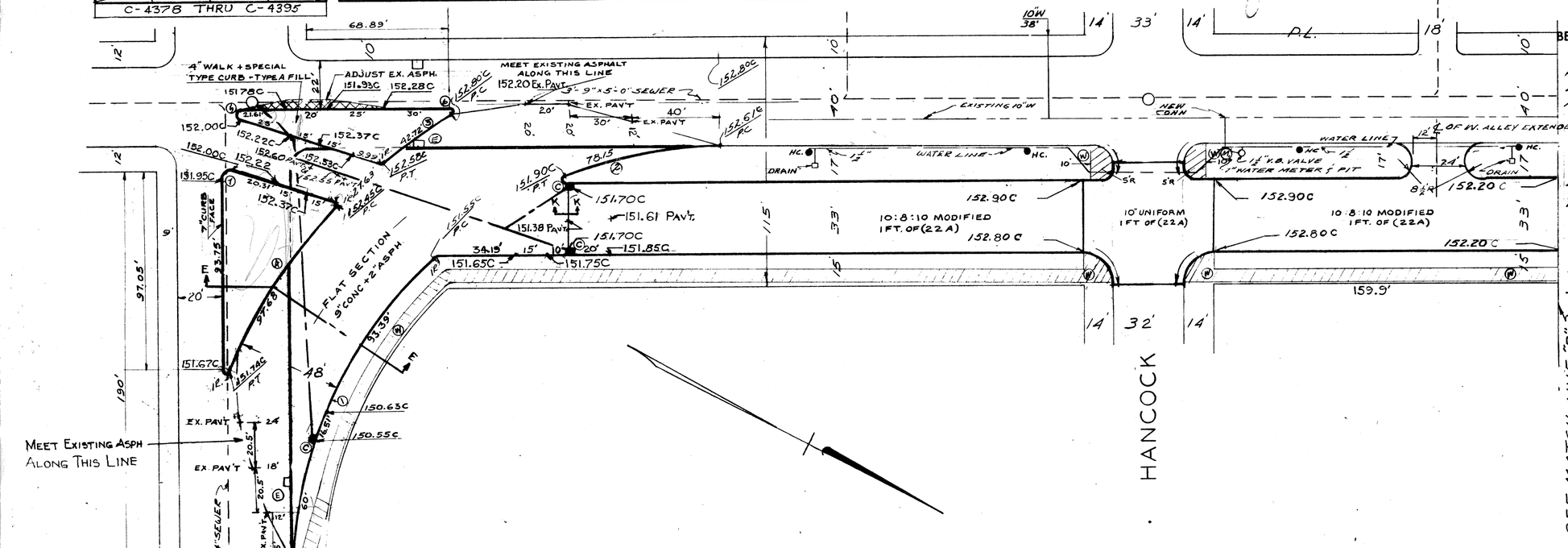
NAME OF STREET RUSSELL

FROM 29' E. OF W.P.L. OF FOREST

TO 154.10' E. OF E.P.L. OF HANCOCK

SHEET 10 OF 14 SHEETS

TYPE OF PAVEMENT 9" UNIFORM CONCRETE-INTEGRAL CURB EXCEPT AS NOTED
10" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED



BENCH MARKS	ELEV.
P.L.B.M. # 40-251 N.E. COR. HASTINGS & WARREN	151.840 R.
P.L.B.M. # 40-251A N.E. COR. RIVARD & WARREN	151.405 R.
ARROW ON HYD. S.E. COR. OF ST. ANTOINE & FOREST	153.52
ARROW ON HYD. S.E. COR. OF RUSSELL & WARREN	156.11

NOTE FOR SYMBOLS (A) ETC. SEE STD. DET. DWG. C-4378.

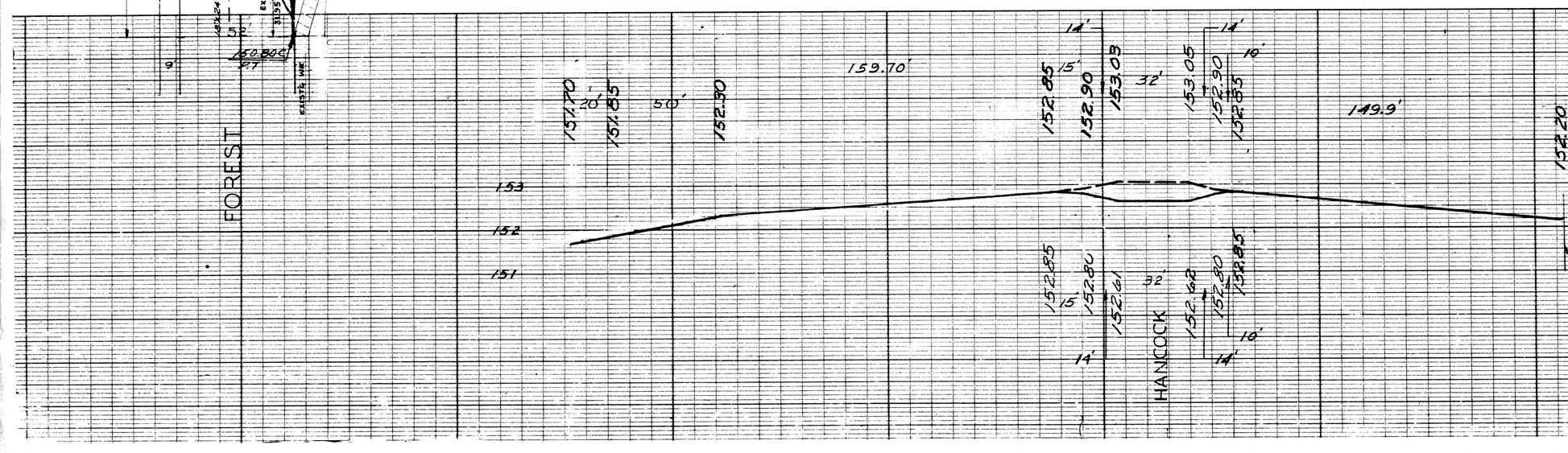
FOR DETAIL SECTIONS SEE DETAIL SHEETS 11 OF 14 & 12 OF 14

FOR WATER SUPPLY SYSTEM SYMBOLS SEE SHEET 12

HORIZONTAL CONTROL FOR HORIZONTAL CONTROL SEE BUREAU OF SURVEYS DWG. S-170.

SEE SHEET 5 OF 14 FOR UTILITY LOCATION & REMOVAL

FOR SECTION E-E SEE DETAIL SHEET 1 OF 14
FOR SECTION K-K SEE DETAIL SHEET 2 OF 14



(1)	(2)	(3)
Δ 48° 40' 27"	Δ 22° 23' 22"	Δ 9° 52' 20"
R 200'	R 200'	R 248'
T 90.46'	T 39.68'	T 22.18'
A 169.90'	A 78.15'	A 42.72'
(4)	(5)	(6)
Δ 22° 14' 00"	Δ 162° 3' 30"	Δ 150° 37' 56"
R 248'	R 3'	R 1'
T 49.48'	T 19'	T 3.82'
A 97.68'	A 8.49'	A 2.63'
(7)		
Δ 107° 56' 31"		
R 5'		
T 6.66'		
A 9.42'		

9213

KEEP IN FILE UNDER FOREST

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE
FOREST — WARREN REROUTING

NAME OF STREET ST. ANTOINE

FROM UTILITY LOCATION AND

TO REMOVAL SHEET

SHEET 1 OF 14 SHEETS

~~TYPE OF PAVEMENT 8 & 8 ONE COURSE CONCRETE INTEGRAL CURB~~
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~ ~~FT. WD.~~

DATE	REVISIONS

SUPPLEMENTARY DETAILS

C.525	C.524	C.733	C.1600
C.525A	C.525	C.1789	C.1600
C.527	C.526	C.529A	C.1783
C.528	C.527	C.1793	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" — 40'
STREET WIDTH	1" — 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER 12"	LIN. FT.		

SEE SHEET 14 OF 14

PETITION NO. —		PETITION DATE —	
ASSIGNMENT NO.	A0-1025	GROUP NO. —	
BOOK 1311	4	LEVEL	9-27-60
NO. 1309	37		9-27-60
1362 PG.	1	DATE	FEB. 1955
1434	19		FEB. 1955
LOTS 2%		LENGTH	
CITY 2%		LENGTH	
TOTAL		LENGTH	
DATE OF DRAWING	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	WANYCKIJ	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.

APPROVED Erickson
Behm
Clyde R. Palmer
ENGINEER OF STREETS
CITY ENGINEER

INDEX NO. 5-6

TOPO NO. 30

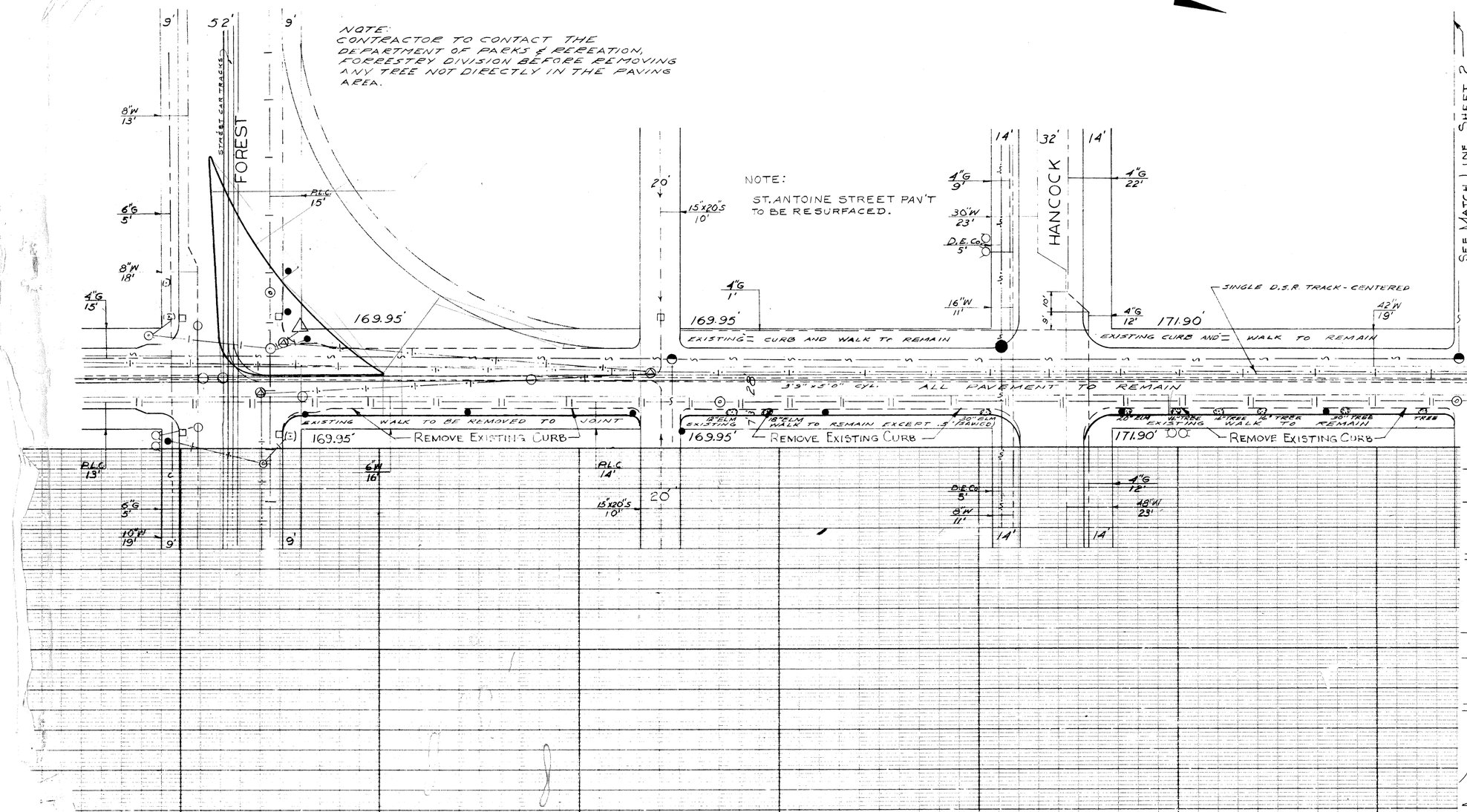
INDEXED

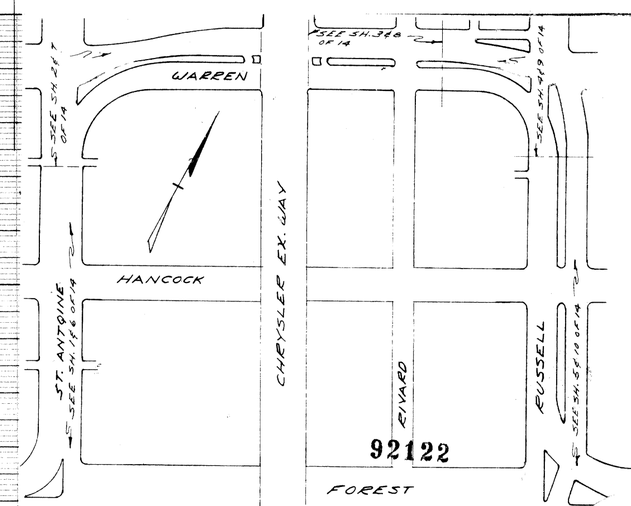
BENCH MARKS ELEV.

LEGEND

- DETROIT EDISON ELECTRIC CONDUIT
- DETROIT EDISON STEAM CONDUIT
- P.L.C. CONDUITS
- DETROIT FIRE DEPT. CONDUITS
- EXISTING GAS LINES
- EXISTING WATER LINES
- EXISTING INLET SEWERS
- P.L.C. HIGH TENSION CONDUIT
- EDISON STEAM MANHOLE
- EDISON ELECTRIC MANHOLE
- P.L.C. MANHOLE
- POLICE DEPT. MANHOLE.
- FIRE DEPT. STD. HYDRANT.
- POLES (ALL TYPES)
- TRAFFIC SIGNAL LIGHT
- POLICE DEPT. CALL BOX.
- SEWER MANHOLE
- WATER MANHOLE OR GRATE
- CATCH BASIN

SEE SHEET 6 OF 14 FOR PAVING PLAN





92122

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS – CITY ENGINEERS OFFICE
FOREST – WARREN REROUTING

NAME OF STREET ST. ANTOINE

FROM 29' E. OF W.P.L. OF FOREST

TO 171.90' N. OF N.P.L. OF HANCOCK

SHEET 6 OF 14 SHEETS

TYPE OF PAVEMENT 9" UNIFORM CONCRETE-INTEGRAL CURB EXCEPT AS NOTED
9" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED

DATE	REVISIONS

SUPPLEMENTARY DETAILS

C-525	C-524	C-733	C-938
C-326A	C-685	C-739	UC-1608
C-327	C-326	C-739A	UC-1783
C-350	C-587	C-739B	

C-4378 THRU C-4385

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE

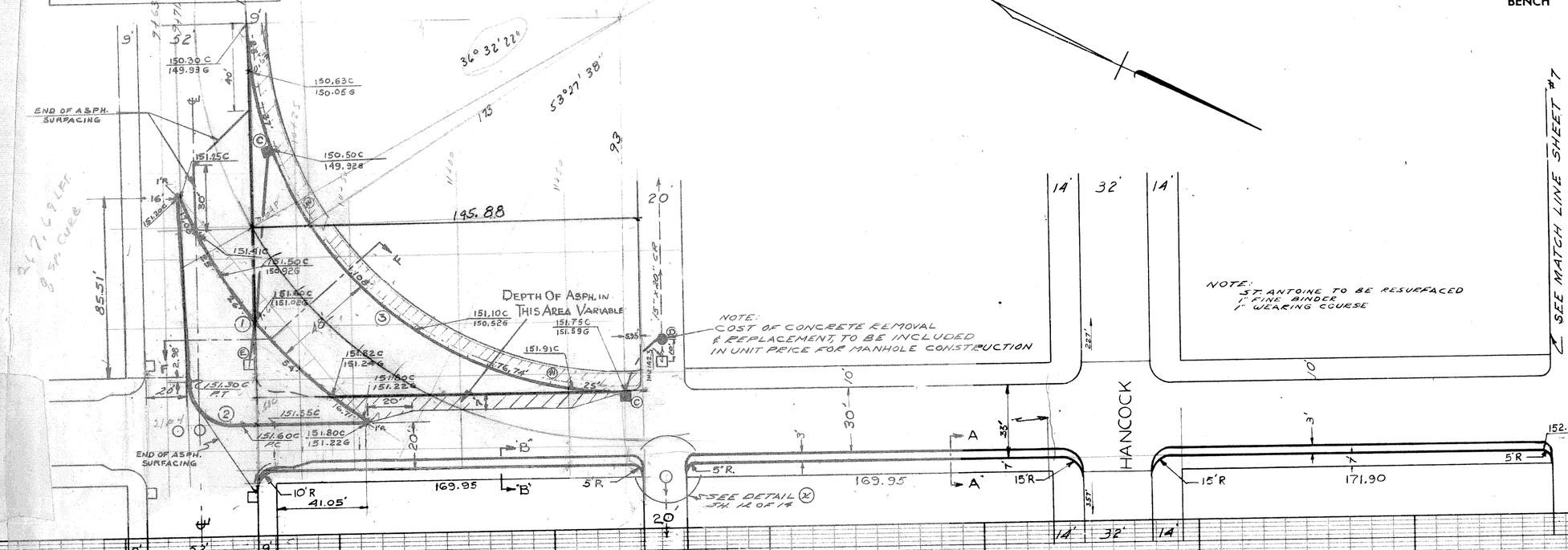
LOTS & ALLEYS	1" 40'
STREET WIDTH	1" 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER–12"	LIN. FT		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A0-1025	GROUP NO.	
BOOK 1909 NO. 1911	PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW'G.	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	IWANYCKIJ	—
PROFILE	IWANYCKIJ	—
GRADE	W. OWENS	BRANTIGAN
ESTIMATE		Winfield Morris
SEWER APPROVED 5/23/61		
FINAL CHECK		G.E.B.
APPROVED	Erickson	
	Behm	ENGINEER OF STREETS
		CITY ENGINEER

INDEX NO.
5-6
TOPO NO.
30
INDEXED



NOTE FOR SYMBOLS (A) ETC. SEE STD. DET. DWG. C-4378

SEE SHEET 1 OF 14 FOR UTILITY LOCATION & REMOVAL

HORIZONTAL CONTROL FOR HORIZONTAL CONTROL SEE BUREAU OF SURVEYS DWG. S-170

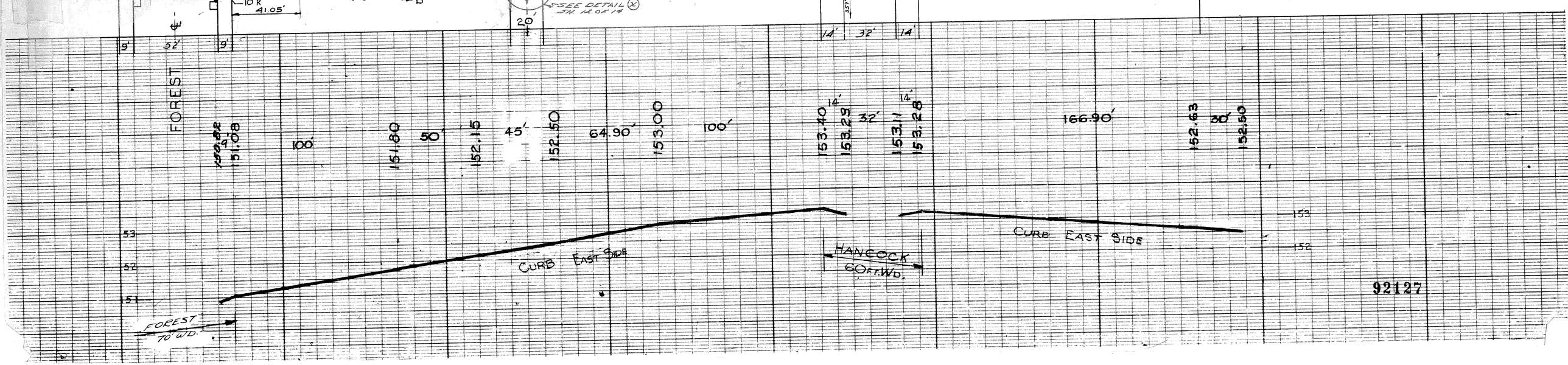
BENCH MARKS

	ELEV.
P.L.B.M. #40-251 N.E. COR. HASTINGS & WARREN	151.840 R.
P.L.B.M. #40-251A N.E. COR. RIVARD & WARREN	151.405 R.
ARROW ON HYD. S.E. COR. ST. ANTOINE & FOREST	153.52
ARROW ON HYD. S.E. COR. OF RUSSELL & WARREN	156.11

CURVE DATA

①	②	③
Δ 35°59'04"	Δ 87°19'40"	Δ 90°00'00"
R 221 FT.	R 20 FT.	R 173 FT
T 72.10 FT.	T 19.18 FT.	T 173 FT.
A 138.79	A 30.53 FT.	A 271.74 FT.

FOR SECTION A-A, B-B F-F SEE DETAIL SH# 11 OF 14



92127

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE
FOREST — WARREN REROUTING

NAME OF STREET WARREN AVE.

FROM UTILITY LOCATION AND

TO REMOVAL SHEET

SHEET 2 OF 14 SHEETS

~~TYPE OF PAVEMENT 8 & 8 ONE COURSE CONCRETE INTEGRAL CURB FT. WD.~~
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~

DATE	REVISIONS

SUPPLEMENTARY DETAILS

C-325	C-524	C-733	C-[illegible]
C-326A	C-525	C-[illegible]	C-1603
C-327	C-[illegible]	C-[illegible]A	C-1783
C-328	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER—12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	AO-1025	GROUP NO.	
BOOK NO.	1811, 1903, 1862, 1684	LEVEL DATE	3-2-60, 3-27-60, FEB. 1953, FEB. 1953
PG.	4, 57, 1, 13		
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	

DATE OF DRAW'G. MARCH 29, 1961

	MADE BY	CHECKED BY
PLAN	L. CHAPMAN	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.

APPROVED Erickson

Behm

ENGINEER OF STREETS

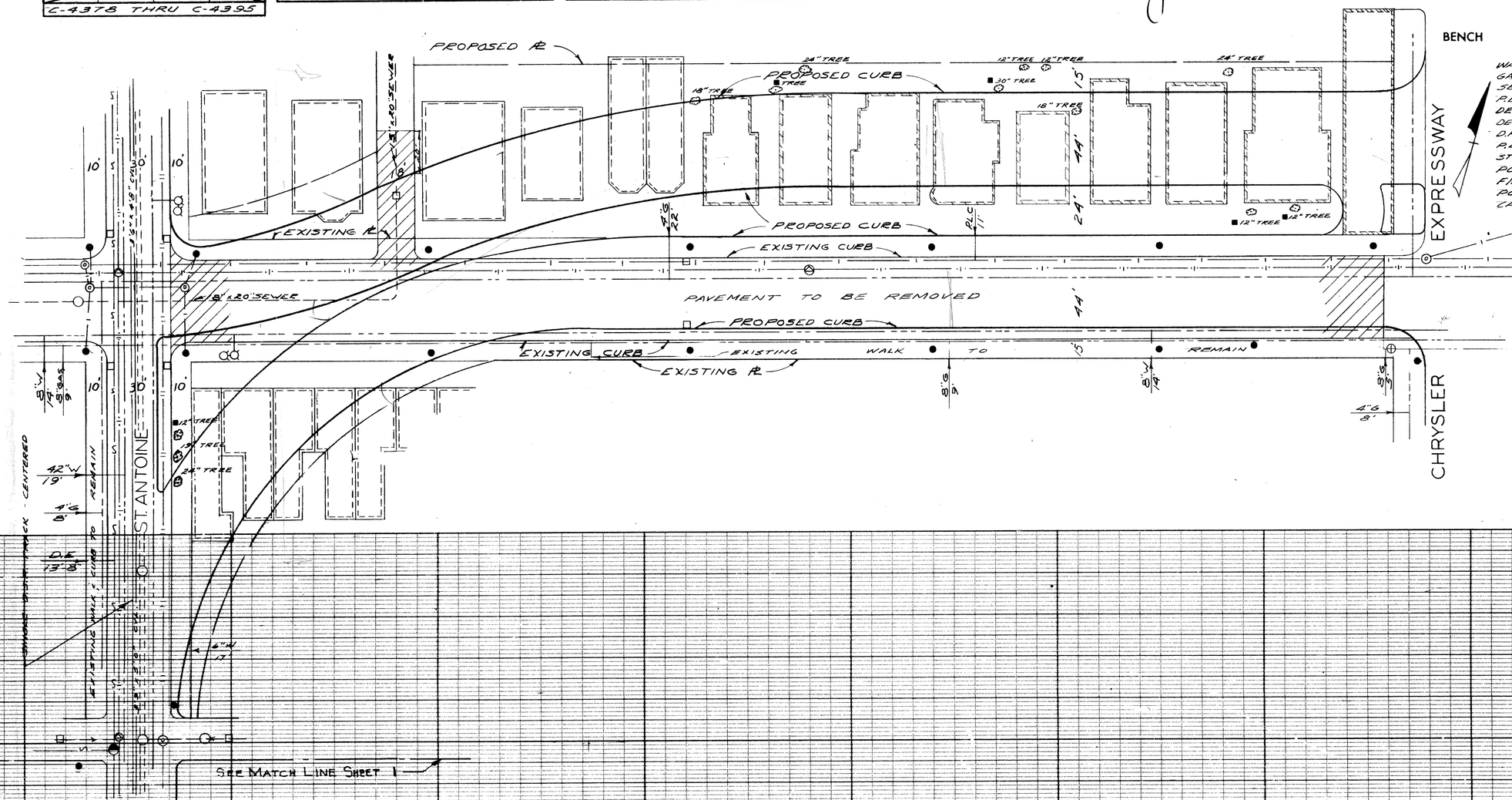
Clyde L. Solmen

CITY ENGINEER

INDEX NO. 5-6

TOPO NO. 30

INDEXED



BENCH MARKS ELEV.

LEGEND FOR UTILITIES

WATER LINE & M.H. OR GATE
GAS LINE & M.H.
SEWER LINE & M.H.
P.L.C. CONDUIT & M.H.
DET. EDISON ELEC. CONDUIT & M.H.
DET. EDISON STEAM CONDUIT & M.H.
D.F.D. & D.P.D. LINE & M.H.
P.L.C. HIGH TENSION CONDUIT
STREET CAR TRACKS
POLE (ALL TYPES)
FIRE DEPT. STD. HYDRANT
POLICE DEPT. CALL BOX
CATCH BASIN

NOTE REMOVE EXISTING PAVEMENT AND WALKS IN PROPOSED R.O.W. EXCEPT AS NOTED.

SEE SHEET 7 OF 14 FOR PAVING PLAN

■ REMOVE TREES IF NECESSARY

92123

DATE	REVISIONS

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-995
C-326A	C-525	C-997	C-1608
C-327	C-586	C-299A	C-1783
C-350	C-587	C-7298	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A.O. 1025	GROUP NO.	
BOOK / NO.	1311 / 1303 / 1945 / 1634 PG. 4 / 31 / 13	LEVEL / DATE	9-8-60 / 9-27-60 / FEB. 1958 / FEB. 1955
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	

DATE OF DRAWG. MARCH 29, 1961

	MADE BY	CHECKED BY
PLAN	D. HAUER	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.

APPROVED Erickson
Behm
Cleyde R. Palmer
ENGINEER OF STREETS
CITY ENGINEER

INDEX NO.
6
TOPO NO.
40
INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

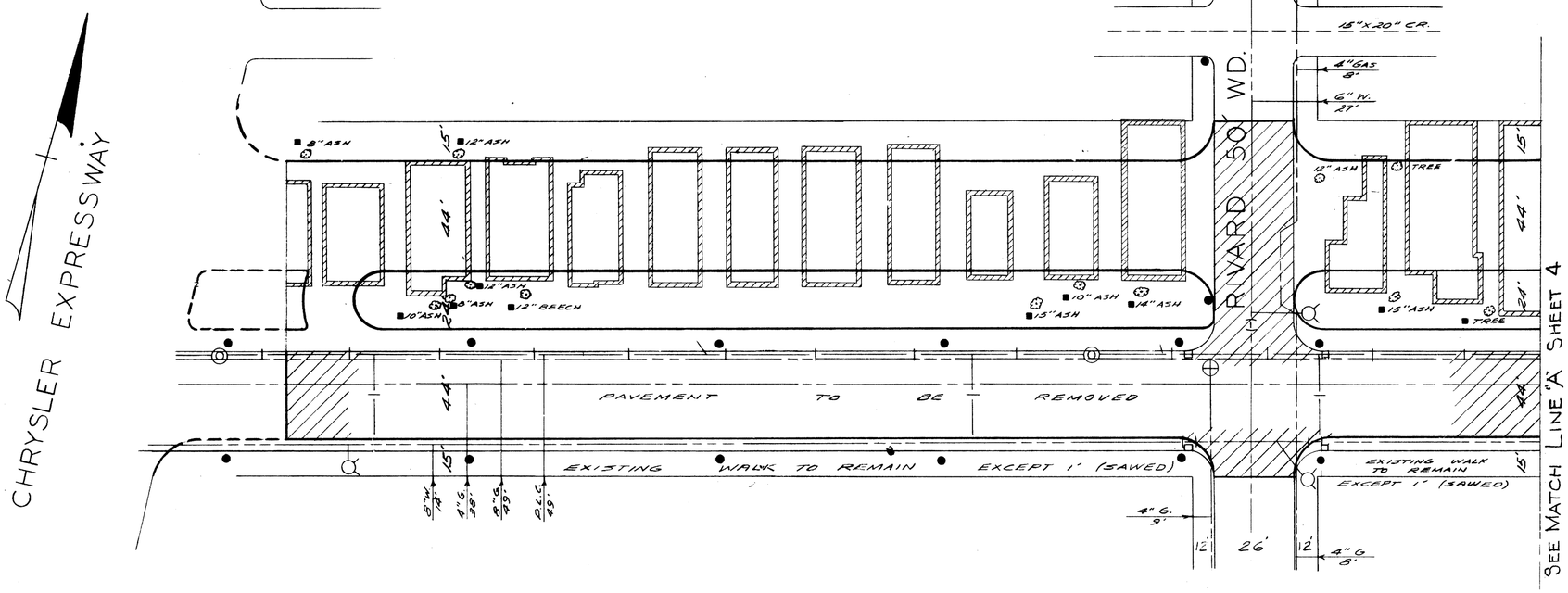
DEPARTMENT OF PUBLIC WORKS – CITY ENGINEERS OFFICE
FOREST – WARREN REROUTING

NAME OF STREET WARREN
FROM UTILITY LOCATION AND
TO REMOVAL SHEET

SHEET 3 OF 14 SHEETS

~~TYPE OF PAVEMENT 8-6-8 ONE COURSE CONCRETE INTEGRAL CURB FT. WD.~~
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~

BENCH MARKS ELEV.



LEGEND FOR UTILITIES

- GAS LINES
- WATER LINES
- P.L.C. CONDUIT
- SEWER LINES
- GAS MANHOLE OR GRATE
- FIRE DEPT. STD. HYDRANT
- WATER SHUT OFF
- P.L.C. MANHOLE
- POLE (ALL TYPES)
- CATCH BASIN

NOTE REMOVE EXISTING PAVEMENT AND WALKS IN PROPOSED R.O.W. EXCEPT AS NOTED.

SEE SHEET 8 OF 14 FOR PAVING PLAN.

■ REMOVE TREES IF NECESSARY

92124

DATE	REVISIONS

SUPPLEMENTARY DETAILS			
~~C-325~~	C-524	C-733	~~C-888~~
C-326A	~~C-525~~	~~C-734~~	C-1608
C-327	~~C-526~~	~~C-734A~~	C-1783
~~C-328~~	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER - 12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A.O. 1025	GROUP NO.	
BOOK 1911	A	LEVEL	9-8-60
1909	37		9-27-60
NO. 1842		DATE	FEB. 1955
1834	PG. 13		FEB. 1955
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAW G.	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	T. MIRIANI	
PROFILE		
GRADE		
ESTIMATE		
SEWER APPROVED		
FINAL CHECK		G.E.B.

APPROVED Erickson
Behm
ENGINEER OF STREETS
Clyde L. Palmer
CITY ENGINEER

INDEX NO.
6
TOPO NO.
40
INDEXED

CONTRACT NO. P.W. 3935

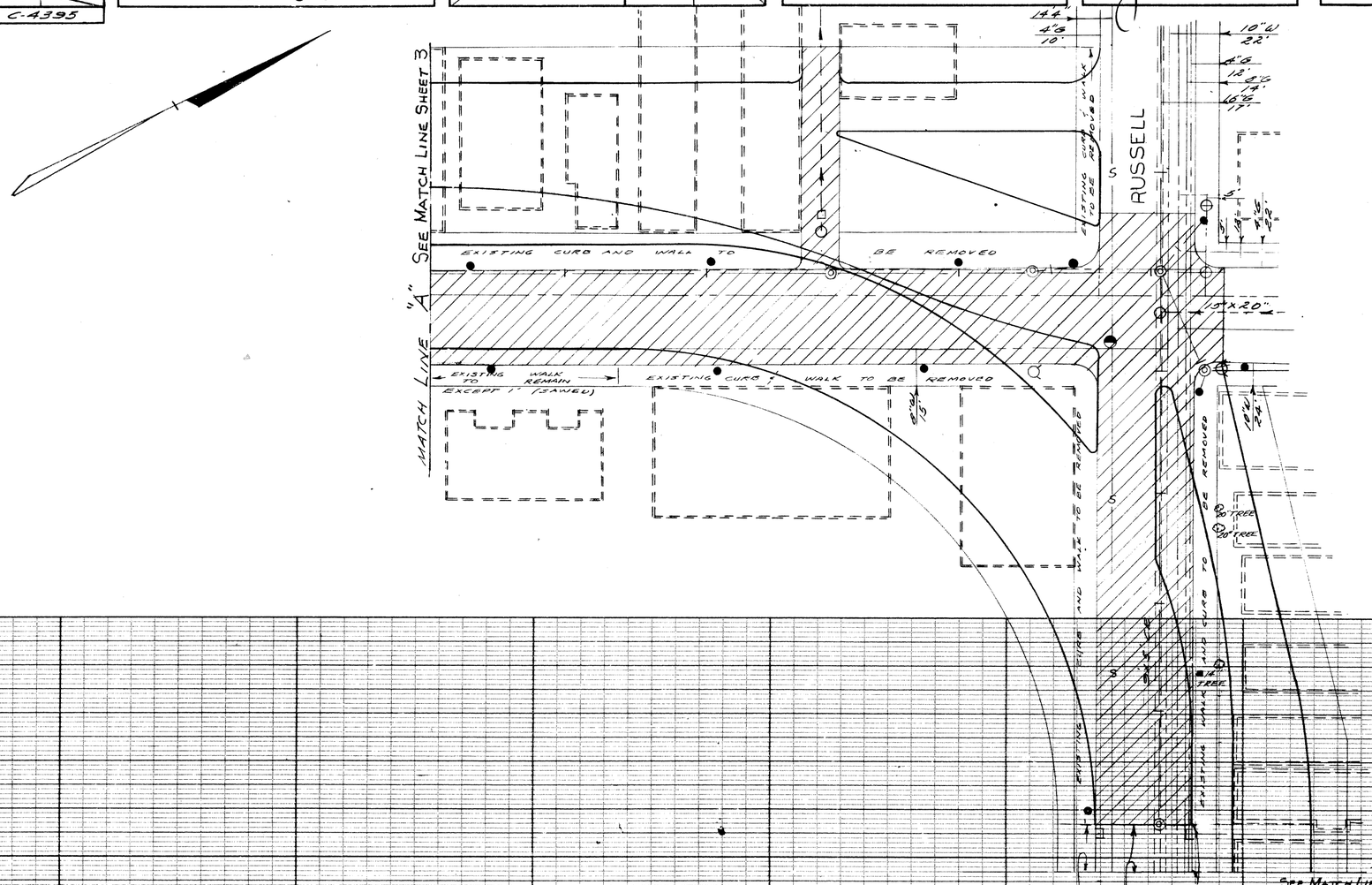
STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE
FOREST - WARREN REROUTING

NAME OF STREET WARREN
FROM UTILITY LOCATION AND
TO REMOVAL SHEET

SHEET 4 OF 14 SHEETS

~~TYPE OF PAVEMENT 8-6-8 ONE COURSE CONCRETE INTEGRAL CURB~~ FT. WD.
~~8" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED~~



NOTE REMOVE EXISTING CURBS, WALKS, AND PAVEMENT AS NOTED.

LEGEND

- GAS LINES
- WATER LINES
- P.L.C. CONDUIT
- SEWER LINES
- DETROIT EDISON ELECTRIC CONDUIT
- SEWER MANHOLE
- P.L.C. MANHOLE
- FIRE DEPT. STD. MANHOLE
- EDISON ELECTRIC MANHOLE
- GAS MANHOLE OR GRATE
- POLE (ALL TYPES)
- CATCH BASIN

SEE SHEET 9 OF 14 FOR PAVING PLAN

■ REMOVE TREES IF NECESSARY

92125

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-585	C-737	C-1608
C-327	C-586	C-739A	C-1783
C-328	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	1" = 2'
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"			
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A0.1025	GROUP NO.	
BOOK 1909 NO. 1911	PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	

DATE OF DRAWING MARCH 29, 1961

	MADE BY	CHECKED BY
PLAN	CHAPMAN	—
PROFILE	CHAPMAN	—
GRADE	W. OWENS	BRANTIGAN
ESTIMATE		
SEWER APPROVED 3/20/61		
FINAL CHECK		G.E.B.

APPROVED Erickson

Behm

ENGINEER OF STREETS

CITY ENGINEER

INDEX NO. 6

TOPO NO. 30

INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE

FOREST — WARREN REROUTING

NAME OF STREET WARREN

FROM 16' W. OF E.P.L. OF ST. ANTOINE

TO 5' W. OF W.P.L. OF CHRYSLER EX-WAY

SHEET 7 OF 14 SHEETS

TYPE OF PAVEMENT 9" UNIFORM

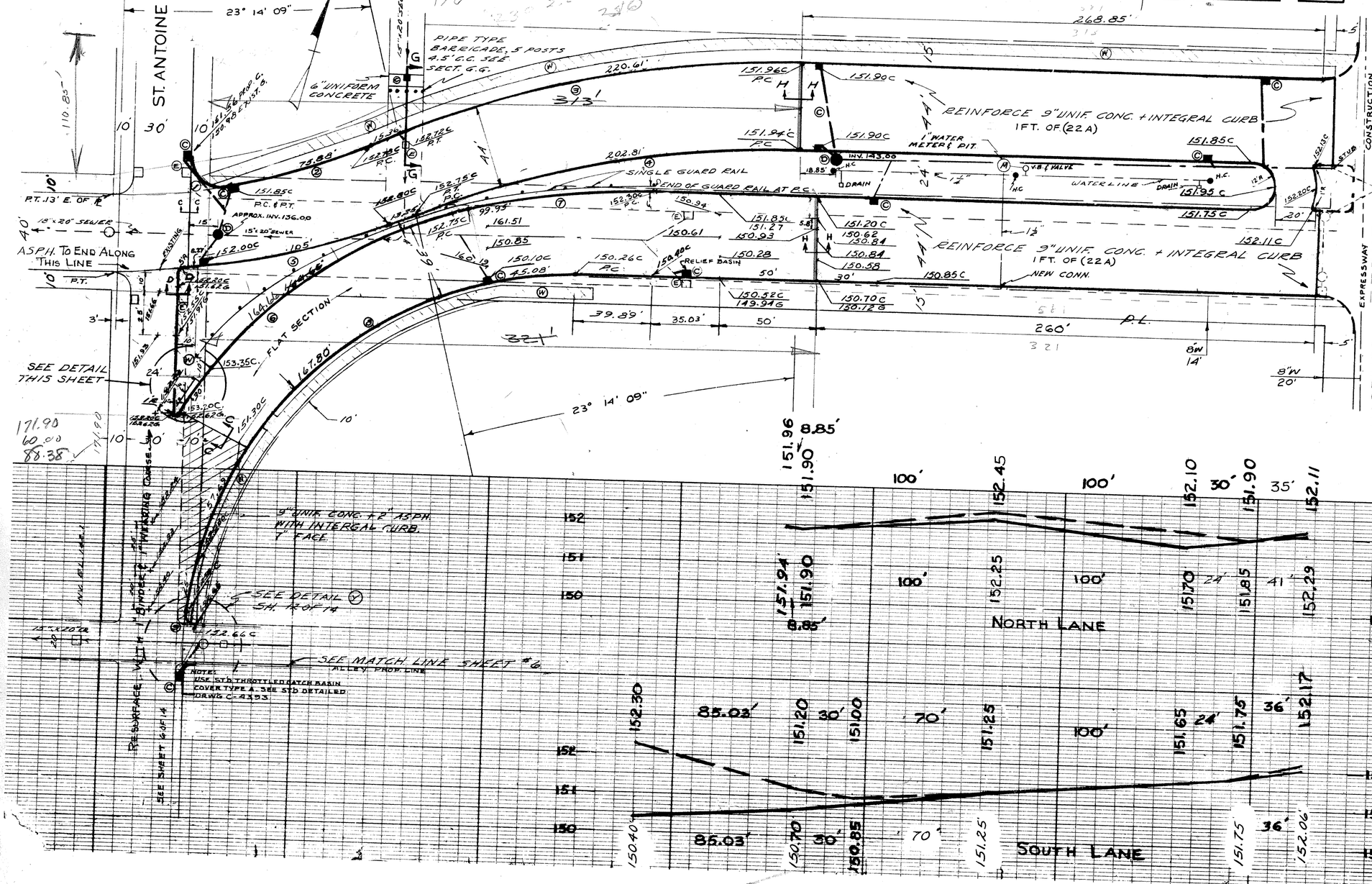
9" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED

CONCRETE-INTEGRAL CURB EXCEPT AS NOTED

BENCH MARKS	ELEV.
P.L.B.M. # 40-251 N.E. COR. HASTINGS & WARREN	151.840
P.L.B.M. # 40-251A N.E. COR. RINARD & WARREN	151.405
C.B.M. ON HYD. S.E. COR. OF ST. ANTOINE & FOREST	153
C.B.M. ON HYD. S.E. COR. OF RUSSEL & WARREN	156.1

NOTE:
FOR SYMBOLS (A) ETC. SEE STD. DET. DWG. C-4378
FOR WATER SUPPLY SYSTEM SYMBOLS SEE SHEET
HORIZONTAL CONTROL
SEE BUREAU OF SURVEYS DWG. S-17
FOR SECTIONS C-C, D-D SEE DETAIL SH. # 11 OF 14
FOR SECTIONS G-G, H-H SEE DETAILS SHEETS # 12 & 13 OF 14

SEE SH. 2 OF 14 FOR REMOVALS & UTILITIES



92128

DATE	REVISIONS

SUPPLEMENTARY DETAILS			
~~C-825~~	C-524	C-733	~~C-738~~
C-326A	~~C-685~~	~~C-737~~	C-1608
C-327	~~C-586~~	~~C-739A~~	C-1783
~~C-190~~	C-587	C-739B	

C-4378 THRU C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
"	"		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER-12"	LIN. FT.		

PETITION NO.	PETITION DATE
ASSIGNMENT NO. A.O. 1025	GROUP NO.
BOOK 1909 NO. 1911 PG.	LEVEL DATE
LOTS 2L	LENGTH
CITY 2L	LENGTH
TOTAL	LENGTH
DATE OF DRAW'G. MARCH 29, 1961	

	MADE BY	CHECKED BY
PLAN	D. HAUER	—
PROFILE	D. HAUER	—
GRADE	W. OWENS	BRANTIGAN
ESTIMATE		
SEWER APPROVED 7/27/61		
FINAL CHECK		G.E.B.
APPROVED	Erickson	
	Behm	
	ENGINEER OF STREETS	
	CITY ENGINEER	

INDEX NO.
6
TOPO NO.
40
INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE

FOREST — WARREN REROUTING

NAME OF STREET WARREN

FROM 28.33 E. OF E.P.L. CHRYSLER EX-WAY S. SIDE

TO 92.91' E. OF E.P.L. OF RIVARD

SHEET 8 OF 14 SHEETS

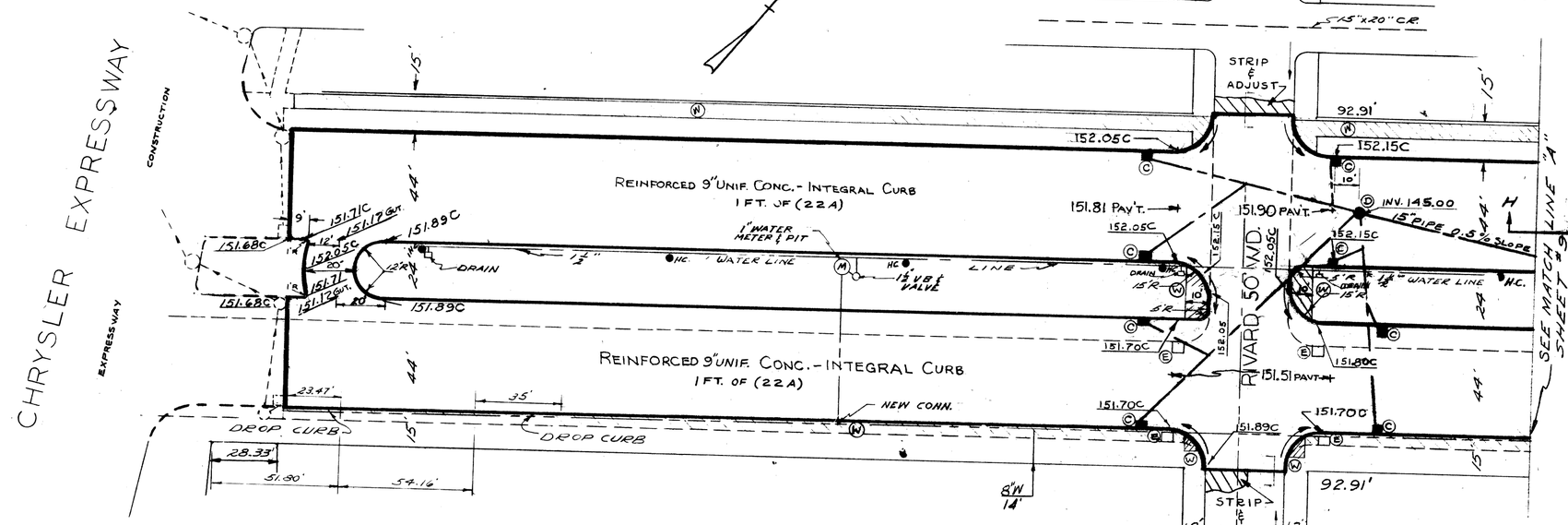
TYPE OF PAVEMENT 9" UNIFORM CONCRETE-INTEGRAL CURB REINFORCED
9" UNIFORM AT STREET INTERSECTION ~~EXCEPT AS NOTED~~ REINFORCED

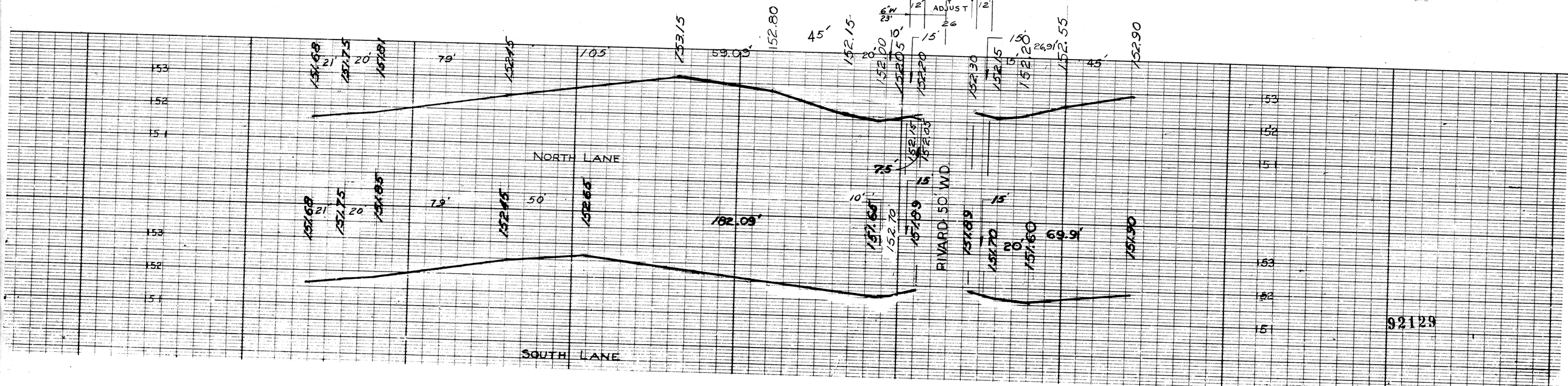
BENCH MARKS	ELEV.
P.L.B.M. #40-251 N.E. COR. HASTINGS & WARREN	151.840 R.
P.L.B.M. #40-251-A N.E. COR. RIVARD & WARREN	151.405 R.
ARROW ON HYD. S.E. COR. OF ST. ANTOINE & FOREST	153.52
ARROW ON HYD. S.E. COR. OF RUSSELL & WARREN	156.11

NOTE:
FOR SYMBOLS (A) ETC. SEE STD. DET. DWG. C-437
FOR DETAIL SECTIONS SEE DETAIL SHEETS 11 OF 14 & 12 OF 14
FOR WATER SUPPLY SYSTEM SYMBOLS SEE SHEET 12 OF 14
HORIZONTAL CONTROL FOR HORIZONTAL CONTROL SEE BUREAU OF SURVEYS DWG. S-170.

SEE SHEET 3 OF 14 FOR UTILITY LOCATION & REMOVAL

FOR SECTION H-H, W-W SEE DETAIL SH. 12 OF 14





DATE	REVISIONS
5-3-62	ALLEY RETURN ADDED SOUTH SIDE OF WARREN AVE

SUPPLEMENTARY DETAILS			
C-325	C-524	C-733	C-938
C-326A	C-565	C-739	C-1608
C-327	C-584	C-739A	C-1783
C-390	C-587	C-739B	

C-4378 THRU. C-4395

BEFORE STARTING CONSTRUCTION
CONTRACTOR MUST CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.

SCALE	
LOTS & ALLEYS	1" = 40'
STREET WIDTH	1" = 40'
VERTICAL	
DRAINAGE COMPLETED	
PAVING COMPLETED	C.C. 8-21-62

QUANTITIES		ESTIMATED LOTS	ESTIMATED CITY
EXCAVATION	CU. YDS.		
NEW CURB	LIN. FT.		
PAVING	SQ. YDS.		
TEST CORES			
SIDEWALK	SQ. FT.		
MANHOLES	ADJUST		
	CONSTRUCT		
BASINS	ADJUST		
	CONSTRUCT		
SEWER—12"	LIN. FT.		

PETITION NO.		PETITION DATE	
ASSIGNMENT NO.	A.O. 1025	GROUP NO.	
BOOK NO.	1909 / 1911 PG.	LEVEL DATE	
LOTS 2L		LENGTH	
CITY 2L		LENGTH	
TOTAL		LENGTH	
DATE OF DRAWG.	MARCH 29, 1961		

	MADE BY	CHECKED BY
PLAN	CHAPMAN	—
PROFILE	CHAPMAN	—
GRADE	W. OWENS	BRANTIGAN
ESTIMATE		
SEWER APPROVED	3/29/61	
FINAL CHECK		G.E.B.

APPROVED Erickson

Behm
ENGINEER OF STREETS

CITY ENGINEER

INDEX NO. 6

TOPO NO. 40

INDEXED

CONTRACT NO. P.W. 3935

STREET PAVING PLAN
CITY OF DETROIT

DEPARTMENT OF PUBLIC WORKS — CITY ENGINEERS OFFICE

FOREST-WARREN REROUTING

NAME OF STREET WARREN

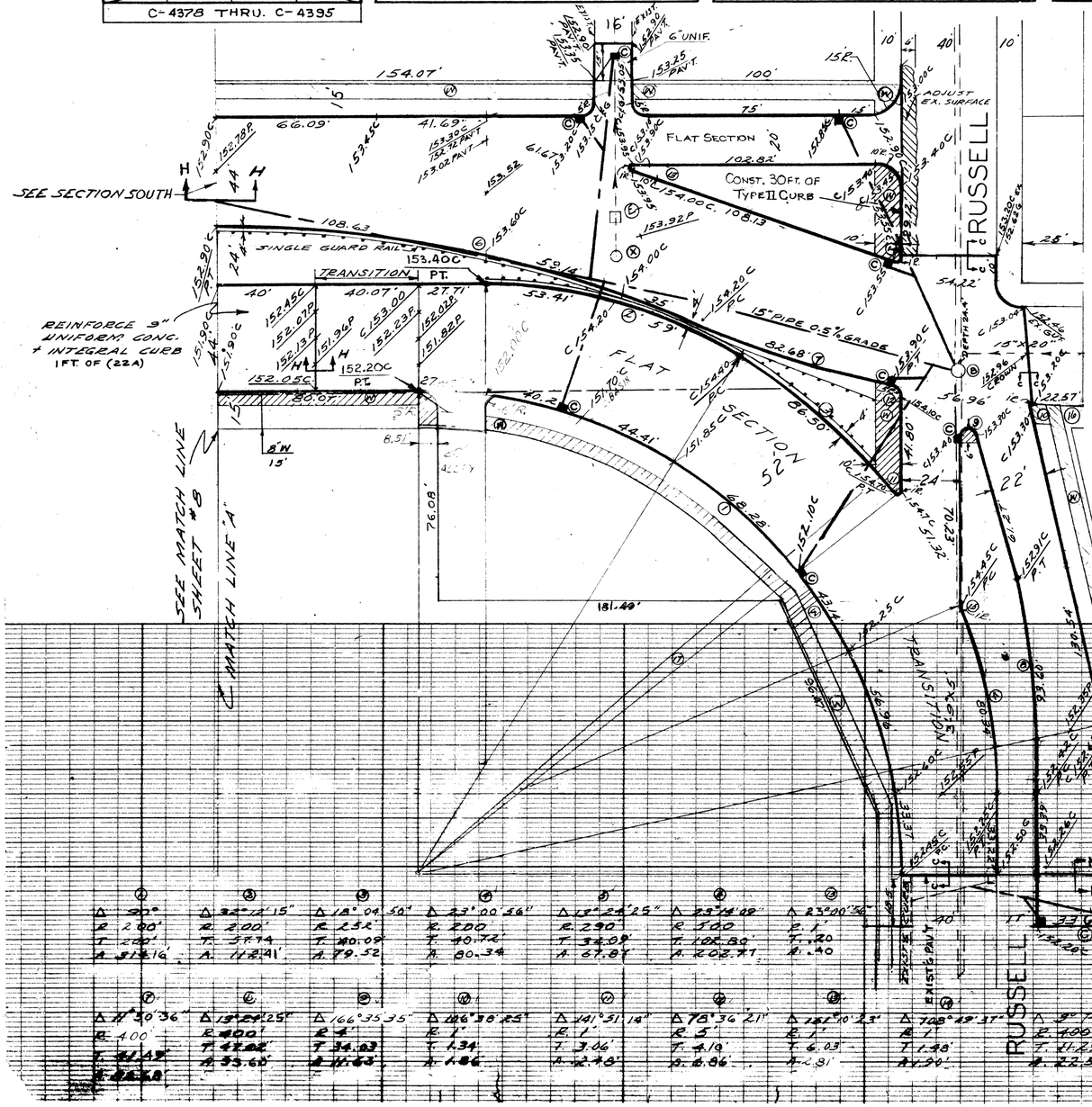
FROM 272.39' W. OF W.PL. OF RUSSELL

TO E. OF E.PL. OF RUSSELL

SHEET 9 OF 14 SHEETS

TYPE OF PAVEMENT 9" UNIFORM CONCRETE-INTEGRAL CURB EXCEPT AS NOTED
9" UNIFORM AT STREET INTERSECTION EXCEPT AS NOTED

BENCH MARKS	ELEV.
P.L.B.M. # 40-251 N.E. COR. HASTINGS & WARREN	151.840 R.
P.L.B.M. # 40-251 A N.E. COR. RIVARD & WARREN	151.405 R.
ARROW ON HYD. S.E. COR. OF ST. ANTOINE & FOREST	153.32
ARROW ON HYD. S.E. COR. OF RUSSELL & WARREN	156.11



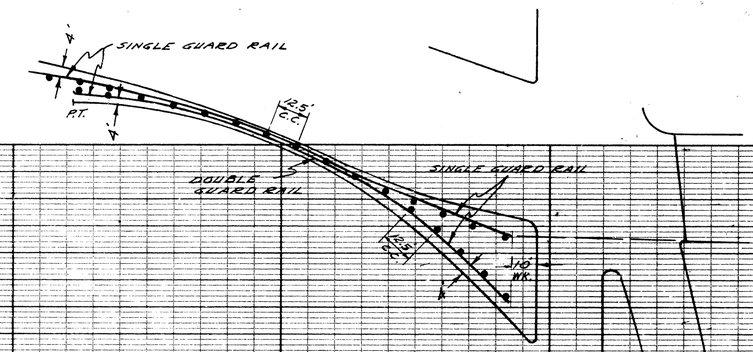
NOTE:
FOR SYMBOLS A ETC. SEE STD. DET. DWG. C-4378

⊗ ABANDON M.H

FOR SECTION C-C SEE DETAIL SH # 11 OF 14
FOR SECTION H-H, M-M SEE DETAIL SH # 12 OF 14

HORIZONTAL CONTROL
SEE BUREAU OF SURVEYS DWG. S-170

SEE SH. 4 OF 14 FOR REMOVALS AND UTILITIES



LOCATION OF GUARD RAIL

92130