

SH. NO. 602 OF 82072 JOB NO. 32068A CONTROL SECTION

RR X-ING GRATIOT S. OF CONNER

# MICHIGAN DEPARTMENT OF TRANSPORTATION

## PLANS OF PROPOSED

MICHIGAN PROJECT STP 2000(330)  
CONTROL SECTION STR 82072  
JOB NUMBER 32068A

M-3  
WAYNE CO.  
CITY OF DETROIT

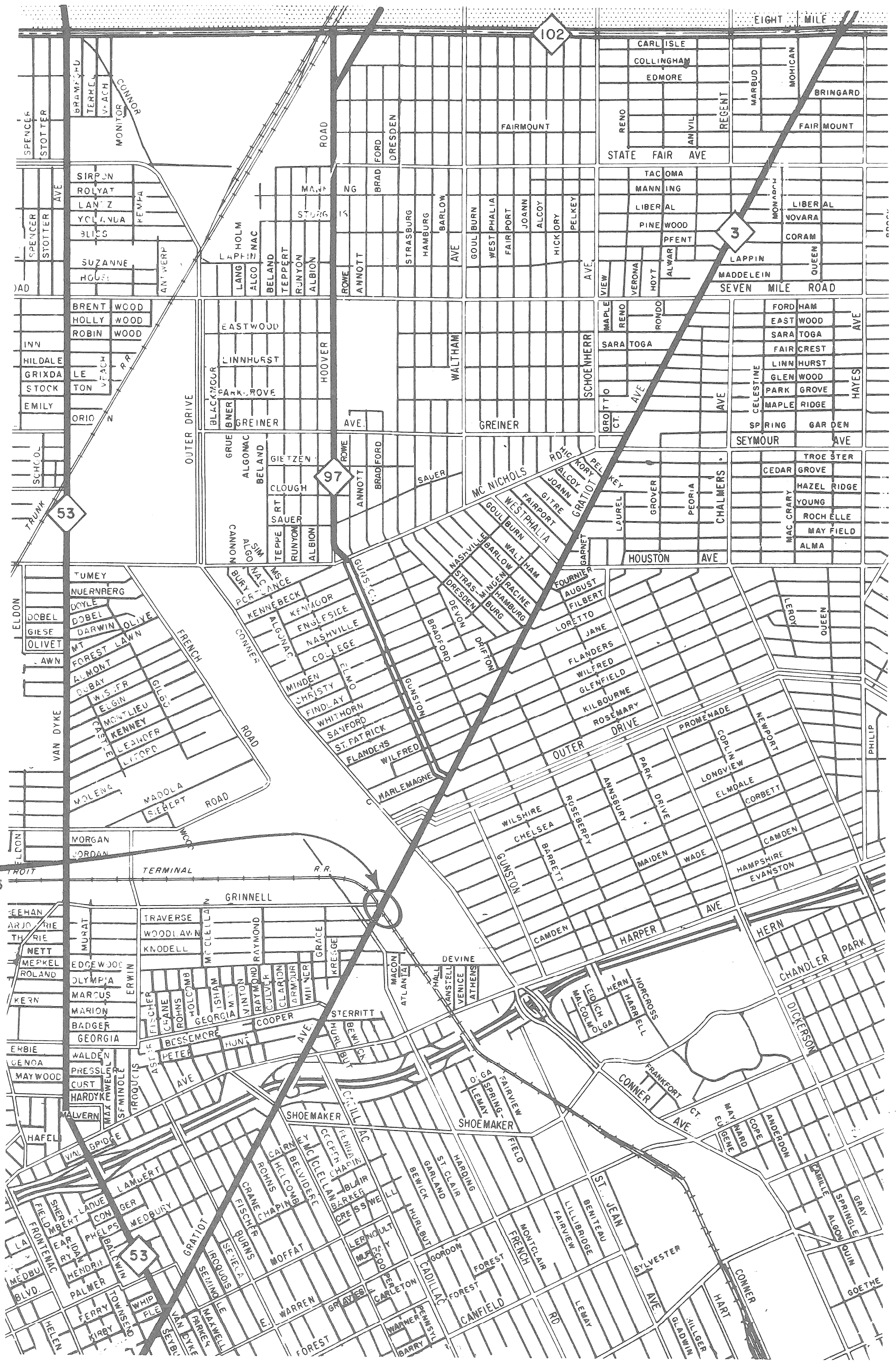
ROUTE	JOB NUMBER	FEDERAL NUMBER	SHT. NO.	TOTAL SHEETS
M-3	32068A	STP 2000(330)	1	

THE IMPROVEMENT COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF TRANSPORTATION 1990 STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS

YEAR 1990

A.D.T.	36,800
D.H.V.	3,600
COMM. %	3.0%
DESIGN SPEED	40 MPH
TRAINS PER DAY	5
TRAIN SPEED	15 MPH

ROAD PLANS	SHEET NOS.
TITLE .....	1
TYPICAL CROSS SECTION .....	2
NOTE SHEET .....	3
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DETAIL GRADES .....	5
MAINTAINING TRAFFIC .....	6-8
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SPECIAL DETAILS .....	9-13
<b>BRIDGE PLANS</b>	
<b>SIGNING PLANS</b>	



CS STR 82072  
JN 32068A  
G02 OF 82072  
REGINS STA 276+97.3  
MP 5.56  
ENDS STA 278+02.3  
MP 5.60

DEPT. OF TRANSPORTATION  
ENGINEERING  
PRINT UNIT  
JUN 18 1993

### TITLE SHEET LEGEND

PROPOSED PROJECT .....	=====
EXISTING ROADS	
PAVED .....	=====
BITUMINOUS .....	-----
GRAVEL .....	-----
UNIMPROVED OR CITY STREET .....	-----
SECTION LINE .....	=====
TOWNSHIP LINE .....	-----
COUNTY LINE .....	-----
CITY OR VILLAGE LIMITS .....	-----
RAILROADS .....	-----

### RAILROAD CROSSING RECONSTRUCTION & APPROACH WORK

#### APPROVALS

RECOMMENDED FOR APPROVAL	<i>W. C. Turner</i>	2/5/93
	ENGINEER ROAD DESIGN	DATE:
RECOMMENDED FOR APPROVAL	<i>John J. Reynolds</i>	2/5/93
	ENGINEER OF DESIGN	DATE:
RECOMMENDED FOR APPROVAL	<i>Robert E. White</i>	1/27/93
	ENGINEER OF TRAFFIC & SAFETY	DATE:
RECOMMENDED FOR APPROVAL	<i>William J. ...</i>	2/4/93
	ENGINEER OF CONSTRUCTION	DATE:

**MICHIGAN  
DEPARTMENT OF TRANSPORTATION**  
PATRICK M. NOWAK - DIRECTOR

APPROVED BY	DEPUTY DIRECTOR - HIGHWAYS	DATE
PLANS PREPARED BY	ERBEN	DESIGN UNIT
U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION		DATE
CONTROL SECTION	JOB NUMBER	FEDERAL NUMBER
G02 of 82072	32068A	STP 2000 (330)
		NA 911
		SHT. NO.
		1

G02 of 82072--32068A

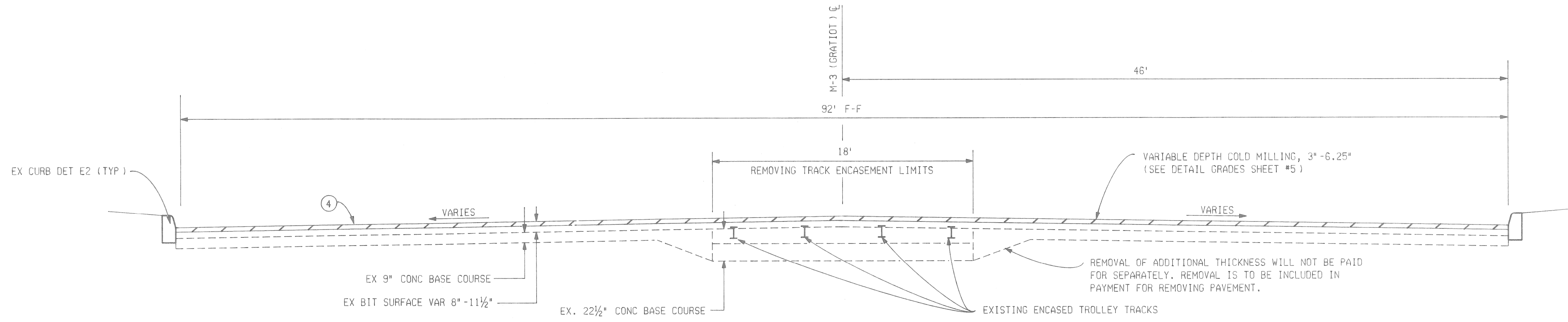
JOB NUMBER

FINAL R.O.W.			
AUTH	DATE	NO.	REVISION

SH. NO. 2  
JOB NO. 32068A  
CONTROL SECTION STR 82072

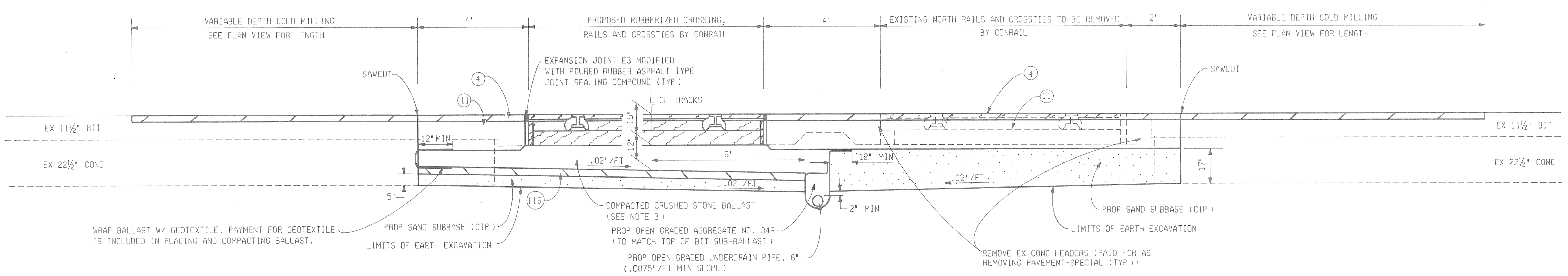
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PROPOSED BY: \_\_\_\_\_  
LAST CORRECTION BY: \_\_\_\_\_

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**TYPICAL CROSS SECTION**

TO APPLY: M=3 STA 276+97.3 TO STA 277+32.8 AND  
STA 277+64 TO STA 278+02.3

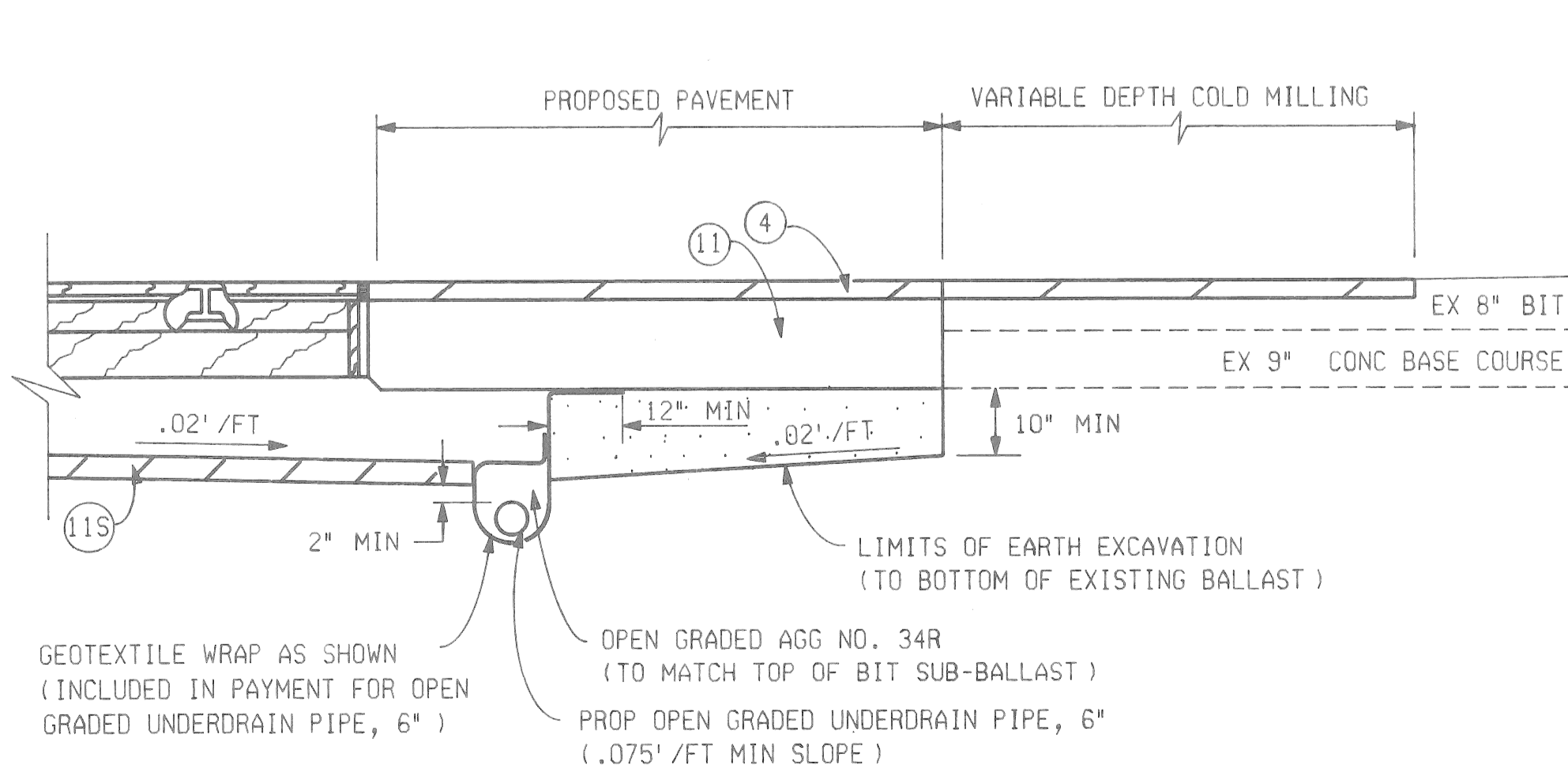


**TYPICAL CROSS SECTION**

PERPENDICULAR TO RR TRACK WITHIN TROLLEY AREA

**NOTES:**

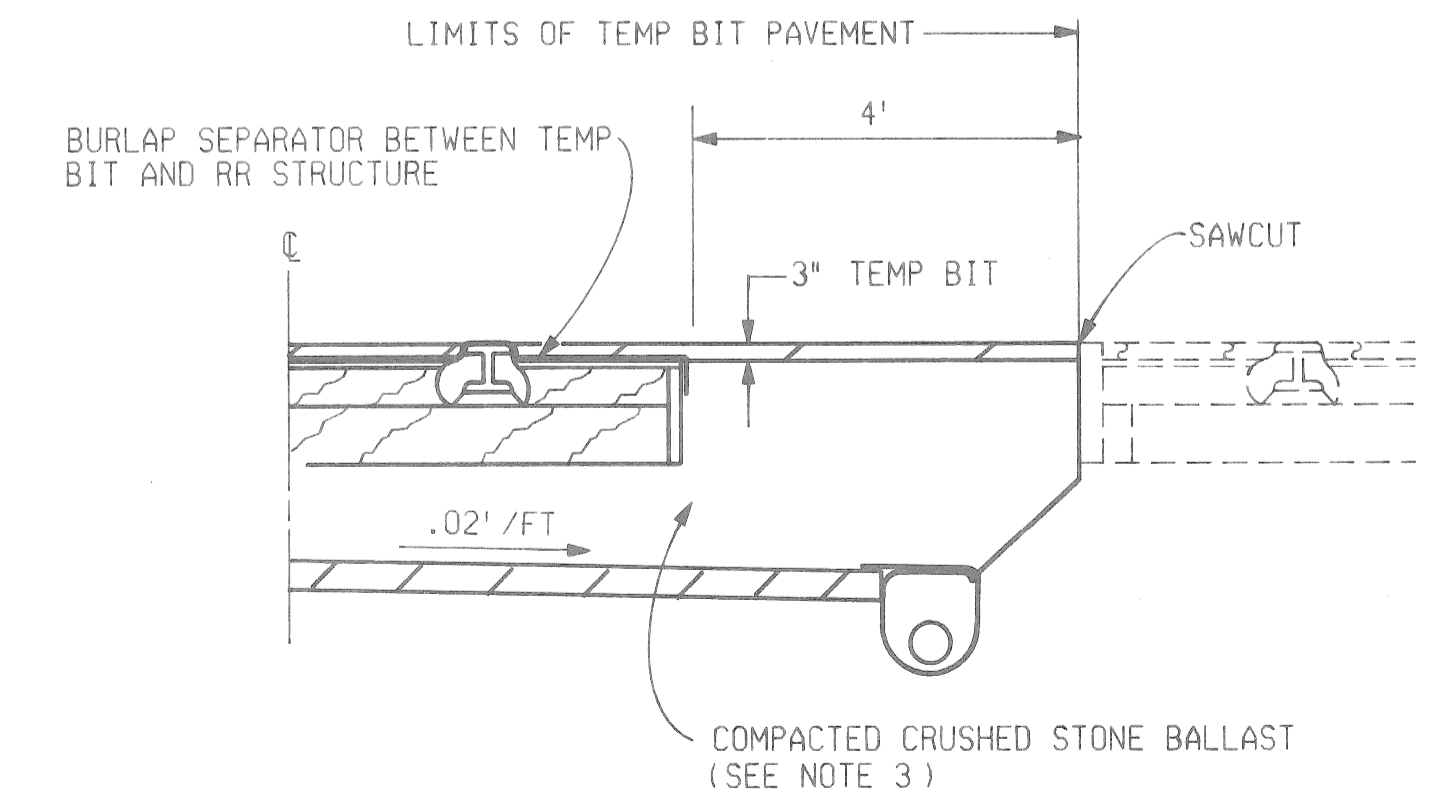
- REMOVAL AND DISPOSAL OF BALLAST WILL BE PAID FOR AS EARTH EXCAVATION.
- EXCAVATION, DISPOSAL OF EXCAVATED MATERIAL, AND GRADING NECESSARY TO PLACE PROPOSED BIT SUB-BALLAST AND PROPOSED SUBBASE, WILL BE PAID FOR AT THE CONTRACT PRICE FOR EARTH EXCAVATION.
- NEW BALLAST WILL BE SUPPLIED AND STOCKPILED AT THE SITE BY CONRAIL FOLLOWING PLACEMENT OF THE BIT SUB BALLAST, THE CONTRACTOR SHALL PLACE AND COMPACT 10" OF NEW BALLAST. (PAID FOR AS PLACING AND COMPACTING BALLAST.) THE REMAINING BALLAST REQUIRED FOR TRACK SUPPORT WILL BE PLACED AND COMPACTED BY CONRAIL. ANY ADDITIONAL BALLAST OUTSIDE THE 1 ON 1 INFLUENCE OF THE RR TRACK SHALL BE PLACED BY THE CONTRACTOR.
- PAYMENT FOR ALL SAWCUTS IS INCLUDED IN PAYMENT FOR REMOVING PAVEMENT-SPECIAL.
- THE WIDTH OF PAVEMENT REMOVAL ADJACENT TO THE 1' CONCRETE HEADERS IS 3' SOUTH OF THE SOUTH CROSSING AND 1' NORTH OF THE NORTH CROSSING. (MEASURED PARALLEL TO THE TRACKS)
- REMOVAL OF ALL CONCRETE, RAILS AND APPURTENANCES WITHIN THE TROLLEY TRACK AREA WILL BE PAID FOR AS REMOVING TRACK ENCASEMENT. REMOVAL OF BITUMINOUS PAVEMENT ABOVE SHALL BE PAID FOR AS REMOVING PAVEMENT.



**TYPICAL HALF SECTION**

PERPENDICULAR TO RR TRACK OUTSIDE OF TROLLEY AREA

NOTE:  
ADDITIONAL EXCAVATION AND OPEN GRADED AGGREGATE NEEDED TO MAINTAIN REQUIRED DEPTH OF UNDERDRAIN IS INCLUDED IN PAYMENT FOR OPEN GRADED UNDERDRAIN PIPE, 6".



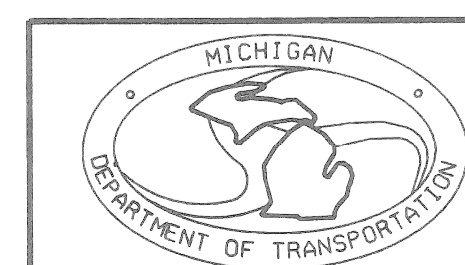
**TYPICAL HALF SECTION (TEMPORARY PAVEMENT-NB LANES)**

PERPENDICULAR TO RR TRACK  
(SEE TYPICAL CROSS SECTION FOR DETAILS NOT SHOWN)

**BITUMINOUS APPLICATION TABLE**

IDENT NO.	ITEM	RATE PER SYD	ASPHALT PENETRATION	REMARKS
4	BIT MIX 4B	330	85-100	PAID FOR AS BIT APPROACH
11	BIT MIX 11A	1540	85-100	BIT BASE COURSE
11S	BIT MIX 11A	440	85-100	BIT SUB BALLAST

0.00 TO 0.10 GAL/SYD BITUMINOUS BOND COAT AS DIRECTED BY ENGINEER



**CONRAIL CROSSING M-3 TYPICAL CROSS SECTION**

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
12/29/92	NTS	STR 82072	32068 A	ERBEN	R.O.W CONST. 2

ENGINEERING REPORT NO	ENVIRON IMPACT STMT	
METHOD OF SURVEY	YEAR	
SURVEY ORDER	SURVEY CHIEF G. HOBGOOD	YEAR 1991
AERIAL SURVEY NO		YEAR
HORIZ DATUM	VERT DATUM	
ROAD DESIGN INITIATED	COMPLETED	
PRELIMINARY PLANS BY W. ERBEN	FINAL PLANS BY W. ERBEN	
FIELD INSPECTION (GI) BY H. J. NYQUIST	DATE 3/30/92	
PLANS-IN-HAND BY (FHWA)	AND (MDOT)	DATE

**GENERAL PLAN NOTES**

**UNDERGROUND UTILITIES**

FOR PROTECTION OF UNDERGROUND UTILITIES AND IN CONFORMANCE WITH PUBLIC ACT 53, 1974, THE CONTRACTOR SHALL DIAL 1-800-462-7171 A MINIMUM OF THREE FULL WORKING DAYS, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS PRIOR TO BEGINNING EACH EXCAVATION IN AREAS WHERE PUBLIC UTILITIES HAVE NOT BEEN PREVIOUSLY LOCATED. MEMBERS WILL THUS BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE "MISS DIG" ALERT SYSTEM.

**UNDERGROUND ELECTRICAL CONDUIT**

CONRAIL WILL REMOVE AND REPLACE AN EXISTING UNDERGROUND ELECTRICAL CONDUIT FOR THE WEST CROSSING SIGNAL WHICH IS LOCATED WITHIN THE LIMITS OF THE EXCAVATION. PRIOR TO EXCAVATION, CONRAIL WILL LOCATE THE EXISTING CONDUIT WHICH IS NOT INCLUDED IN THE MISS DIG SYSTEM.

**OLD PLANS**

THE FOLLOWING OLD PLANS WERE REFERRED TO IN THE DESIGN OF THIS PROJECT.

C.S. RRS 82072 11623  
PROJ. 82-11 U-180(27)

IN ADDITION, OTHER OLD PLANS THAT PREDATE THIS PROJECT MAY BE AVAILABLE. THESE PLANS MAY BE REVIEWED IN THE LANSING DESIGN OFFICE DURING NORMAL WORKING HOURS.

**STATIONING**

STATIONING ON THIS PROJECT WAS TAKEN FROM OLD PLANS AND PAVEMENT STENCILED STATIONING AND IS NOT NECESSARILY CORRECT.

**SIGNS**

ANY MDOT SIGNS REQUIRING RELOCATION DUE TO CONSTRUCTION OPERATIONS SHALL BE SALVAGED AND RESET BY THE CONTRACTOR AT LOCATIONS DESIGNATED BY THE ENGINEER. THIS WORK WILL BE CONSIDERED INCLUDED IN PAYMENT FOR OTHER CONTRACT ITEMS.

**PAVEMENT REMOVAL QUANTITIES**

PAVEMENT REMOVAL AS SHOWN ON THE PLANS SHALL BE AS DIRECTED BY THE ENGINEER. ADJUSTMENTS CAN BE MADE IN THE QUANTITIES AS NECESSARY, TO PROVIDE THE PROPER CROSS SECTION AND BASE.

**PLAN ELEVATION**

ELEVATIONS SHOWN ON THESE PLANS ARE BASED ON CITY OF DETROIT DATUM.

**PUBLIC UTILITIES**

THE EXISTING UTILITIES LISTED BELOW AND SHOWN ON THESE PLANS REPRESENT THE BEST INFORMATION AVAILABLE. THIS INFORMATION DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY TO BE SATISFIED AS TO IT'S ACCURACY AND THE LOCATION OF EXISTING UTILITIES.

NAME OF OWNER	KIND OF UTILITY
DETROIT EDISON COMPANY ATTN: GLENN SPENCE 2000 SECOND AVENUE - RM. 660 SB DETROIT, MI 48226 PHONE: 237-6447 FAX: 237-6495	ELECTRIC
MICHIGAN BELL TELEPHONE COMPANY ATTN: GARY SAMKOWIAK 11780 MERRIMAN ROOM 635 LIVONIA, MI 48150 PHONE: 523-7544	TELEPHONE
MICHIGAN CONSOLIDATED GAS COMPANY ATTN: CRAIG STROUD 3200 HOBSON DETROIT, MI 48201 PHONE: 577-7150	GAS
DETROIT WATER AND SEWERAGE DEPARTMENT 735 RANDOLPH ST. WATER BOARD BUILDING DETROIT, MI. 48226 PHONE: 224-4800	WATER MAINS AND SEWERS
DETROIT PUBLIC LIGHTING DEPARTMENT 9449 GRINNELL DETROIT, MI 48213 PHONE: 267-7226	STREET LIGHTING

**NOTES APPLYING TO STANDARD PLANS**

WHERE THE FOLLOWING ITEMS ARE CALLED FOR ON PLANS, THEY ARE TO BE CONSTRUCTED ACCORDING TO THE STANDARD PLAN GIVEN BELOW OPPOSITE EACH ITEM UNLESS OTHERWISE INDICATED.

DRIVEWAY OPENINGS & APPROACHES, AND CONCRETE SIDEWALKS	II-29E
CONCRETE CURB AND CONCRETE CURB & CUTTER	II-30D
TRACK CROSSINGS	VI-121D (SPECIAL DETAIL)
RAILROAD CROSSING SIGNALS	VI-122D
GRANULAR BLANKET, UNDERDRAINS, AND UNDERDRAIN END SECTIONS	IV-80D
LIGHTED ARROWS AND BARRICADES	VI-125H
TEMPORARY CONCRETE BARRIER	II-52F (SPECIAL DETAIL)
PLACEMENT OF TEMPORARY CONCRETE BARRIER	VI-126E

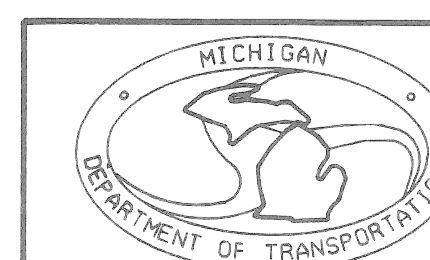
**MISCELLANEOUS ESTIMATES**

THE FOLLOWING ITEMS OF WORK SHALL BE DONE AS THEY APPLY THROUGHOUT THE PROJECT. THESE ITEMS ARE NOT DETAILED OR INCLUDED ON THE PLAN AND PROFILE SHEETS:

MOBILIZATION	1	LSUM
REMOVING PAVEMENT MARKING, LONGITUDINAL	4500	LFT
LIGHTED ARROW, TYPE A, FURNISHED	6	EACH
LIGHTED ARROW, TYPE A, OPERATED	6	EACH
BARRICADE, TYPE II LIGHTED, FURNISHED	150	EACH
BARRICADE, TYPE II LIGHTED, OPERATED	150	EACH
BARRICADE, TYPE III LIGHTED, FURNISHED	20	EACH
BARRICADE, TYPE III LIGHTED, OPERATED	20	EACH
MINOR TRAFFIC DEVICES	1	LSUM
SIGN, TYPE B TEMPORARY-FURNISHED	600	SFT
TEMPORARY PAVEMENT MARKING, TYPE R, 4" WHITE	10000	LFT
TEMPORARY PAVEMENT MARKING, TYPE R, 4" YELLOW	10000	LFT
FAST-CURE POLYESTER PAVEMENT MARKING, 4" WHITE.	1500	LFT
FAST-CURE POLYESTER PAVEMENT MARKING, 4", YELLOW.	2500	LFT
TEMPORARY CONCRETE BARRIER-FURNISHED	90	LFT
SIGN, TYPE A TEMPORARY-FURNISHED	700	SFT
SIGN, TYPE D TEMPORARY-FURNISHED	70	SFT
CLEANING DRAINAGE STRUCTURE	1	EACH
CLEANING DRAINAGE STRUCTURE LEADS	75	LFT
TEMPORARY BITUMINOUS	16	TON
REMOVING CURB	4	LFT
SIGN, TYPE B TEMPORARY-OPERATED	600	SFT
SIGN, TYPE A TEMPORARY-OPERATED	700	SFT
SIGN, TYPE D TEMPORARY-OPERATED	70	SFT
TEMPORARY CONCRETE BARRIER-OPERATED	90	LFT
TEMPORARY CONCRETE BARRIER-RELOCATED	90	LFT
NON-HAZARDOUS CONTAMINATED MATERIAL HANDLING AND DISPOSAL	155	CYD

THE FOLLOWING ITEMS ARE ESTIMATED FOR THE ENTIRE PROJECT TO CORRECT POSSIBLE UNSUITABLE SUBGRADE CONDITIONS WHERE DESIGNATED BY THE ENGINEER.

SUBGRADE UNDERCUTTING, TYPE II	10	CYD
SUBGRADE UNDERDRAIN, 6"	150	LFT



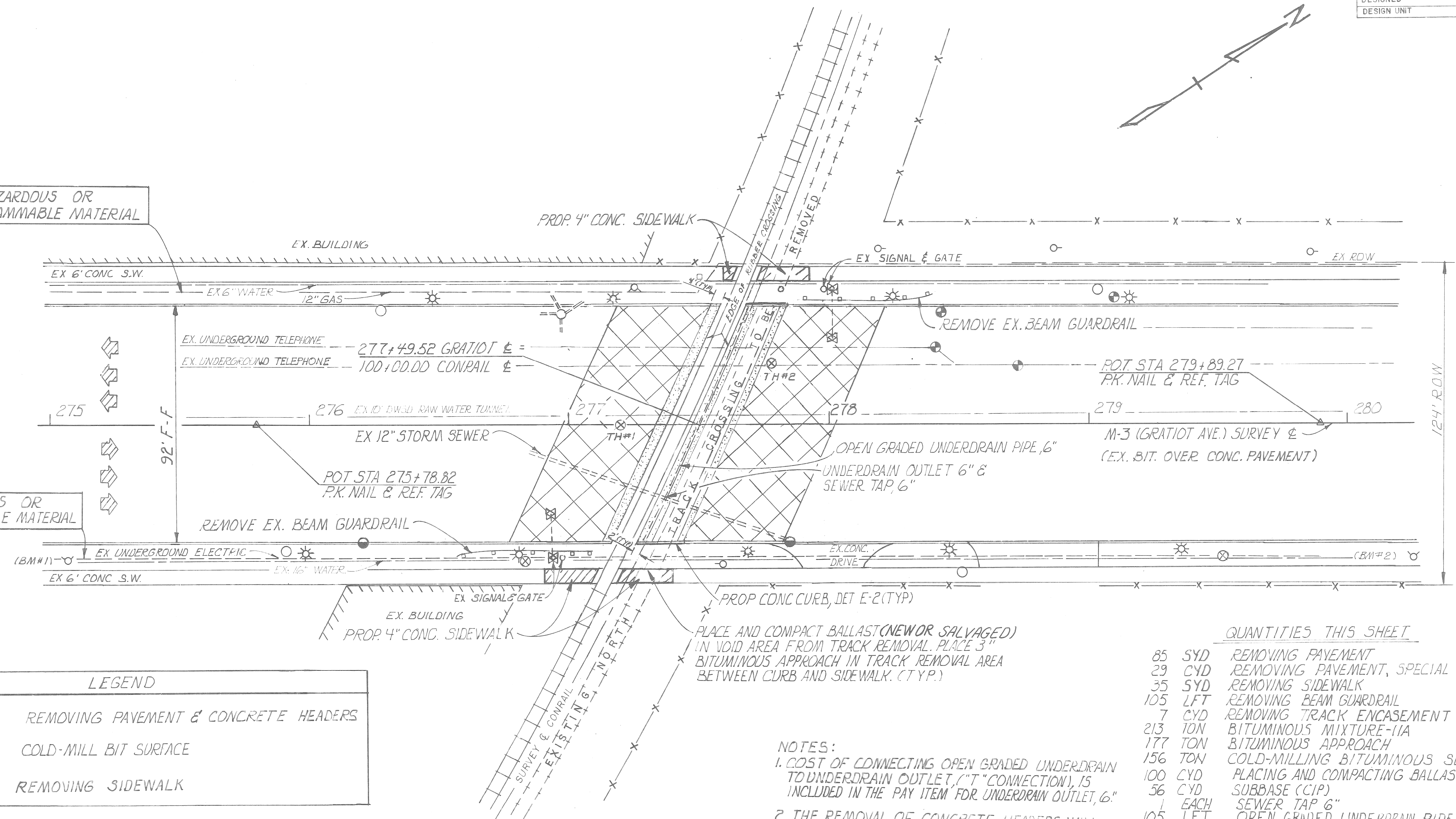
**NOTE SHEET**

DATE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
12/29/92	STR 82072	32068 A	ERBEN	R.O.W CONST. 3

FINAL R.O.W.		
AUTH.	DATE	NO. REVISION
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DESIGNED		START
DESIGN UNIT		COMP

HAZARDOUS OR FLAMMABLE MATERIAL

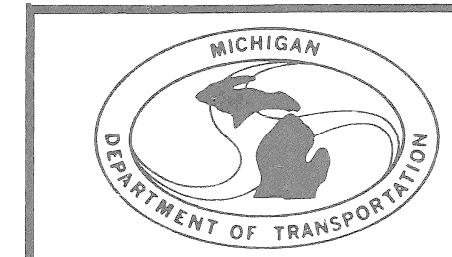
HAZARDOUS OR FLAMMABLE MATERIAL



LEGEND	
	REMOVING PAVEMENT & CONCRETE HEADERS
	COLD-MILL BIT SURFACE
	REMOVING SIDEWALK

NOTES:  
 1. COST OF CONNECTING OPEN GRADED UNDERDRAIN TO UNDERDRAIN OUTLET, ("T" CONNECTION), IS INCLUDED IN THE PAY ITEM FOR UNDERDRAIN OUTLET, 6".  
 2. THE REMOVAL OF CONCRETE HEADERS WILL BE PAID FOR AS REMOVING PAVEMENT, SPECIAL.

QUANTITIES THIS SHEET		
85	SYD	REMOVING PAVEMENT
29	CYD	REMOVING PAVEMENT, SPECIAL
35	SYD	REMOVING SIDEWALK
105	LFT	REMOVING BEAM GUARDRAIL
7	CYD	REMOVING TRACK ENCASEMENT
213	TON	BITUMINOUS MIXTURE-11A
177	TON	BITUMINOUS APPROACH
156	TON	COLD-MILLING BITUMINOUS SURFACE
100	CYD	PLACING AND COMPACTING BALLAST (CIP)
56	CYD	SUBBASE (CIP)
1	EACH	SEWER TAP 6"
105	LFT	OPEN GRADED UNDERDRAIN PIPE, 6"
202	LFT	EXPANSION JT E3-MODIFIED
115	SYD	SLOPE RESTORATION
430	SFT	CONC. SIDEWALK, 4"
43	LFT	CONC. CURB, DET E-2
6	LFT	UNDERDRAIN OUTLET, 6"
155	CYD	EARTH EXCAVATION



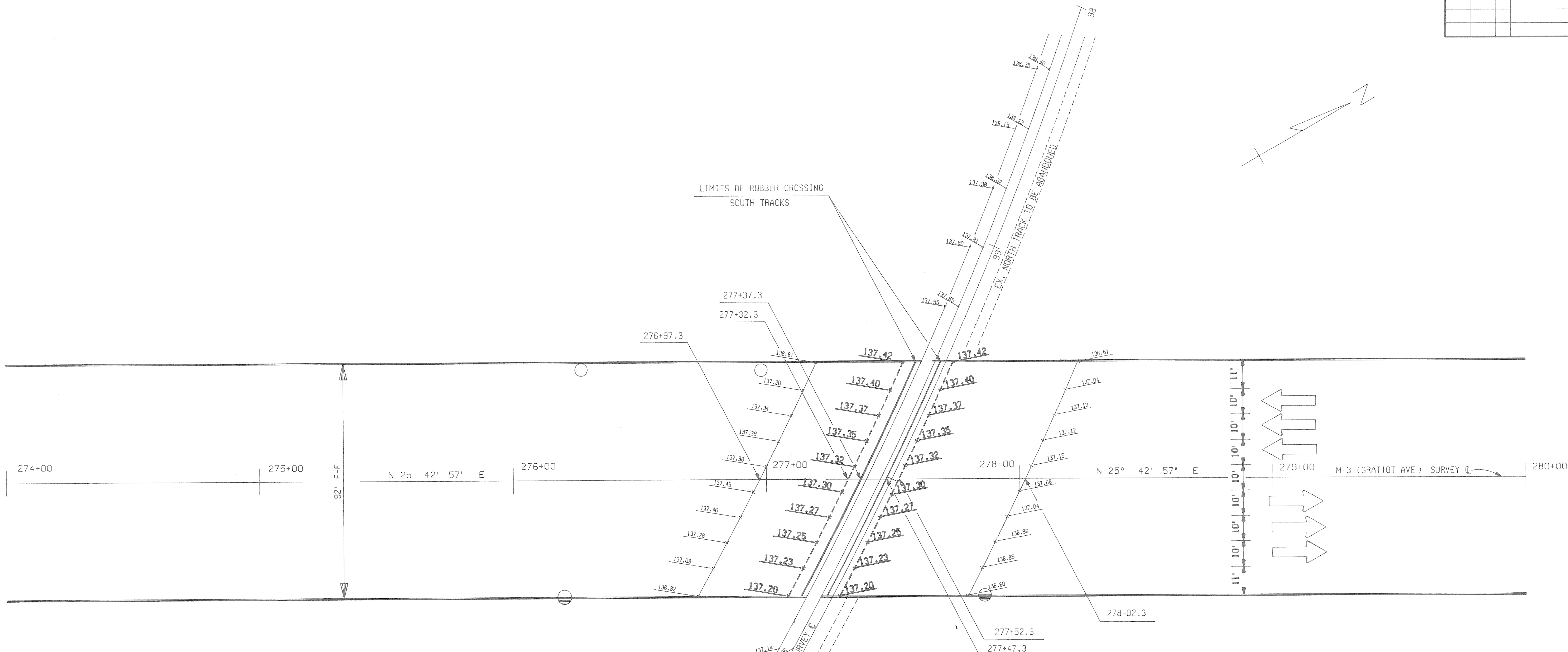
**APPROACH RECONSTRUCTION - CONRAIL AT M-3**

DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
12-28-92	1" = 20'	STR 82072	32068A	ERBEN	4

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PROPOSED BY:  
LAST CORRECTION BY:

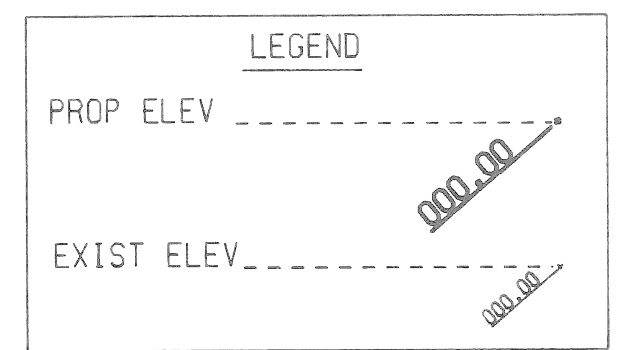
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AUTH	DATE	NO. REVISION

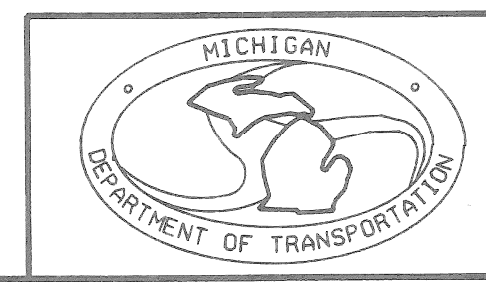


**BENCH MARK LIST**

- CBM # 57-253A CITY OF DETROIT BM DISK IN HANDHOLE 4 FT WEST OF THE EAST LINE OF FRENCH ROAD AND 4 FT SOUTH OF THE NORTH LINE OF DIVINE STREET. ELEV. 132.298
- CBM # 57-253 CITY OF DETROIT BM DISK IN HANDHOLE 4 FT WEST OF THE EAST LINE OF GRATIOT AVENUE AND 4 FT SOUTH OF THE NORTH LINE OF DIVINE STREET. ELEV. 135.455
- CBM # 59-254 CITY OF DETROIT BM DISK IN HANDHOLE 3 FT WEST OF THE EAST LINE OF CONNER STREET AND 3 FT SOUTH OF THE NORTH LINE OF GRATIOT AVENUE. ELEV. 135.225
- BM # 1 ARROW ON FIRE HYDRANT SOUTH OF RR TRACKS 53 FT RIGHT OF STATION 275+06. ELEV. 140.260
- BM # 2 ARROW ON THE FIRST FIRE HYDRANT NORTH OF RR TRACKS 51 FT RIGHT OF STATION 280+25. ELEV. 137.717



- NOTE:
- 1) STA 277+49.52 GRATIOT AVE C = STA 100+00 RAIL SURVEY C
  - 2) PROPOSED ELEVATIONS OF PAVEMENT, RUBBER CROSSING, AND RAILS ARE THE SAME BETWEEN A POINT FIVE FEET SOUTH OF THE CROSSING AND FIVE FEET NORTH OF THE CROSSING.
  - 3) THE FIRST RAIL ELEVATION SHOWN IMMEDIATELY EAST AND WEST OF THE CURB LINE IS LOCATED 75' FROM THE CENTERLINE OF M-3 (MEASURED ALONG THE RAILS). ADDITIONAL RAIL ELEVATIONS ARE LOCATED AT 25' INTERVALS.



**DETAIL GRADES CONRAIL AT M-3**

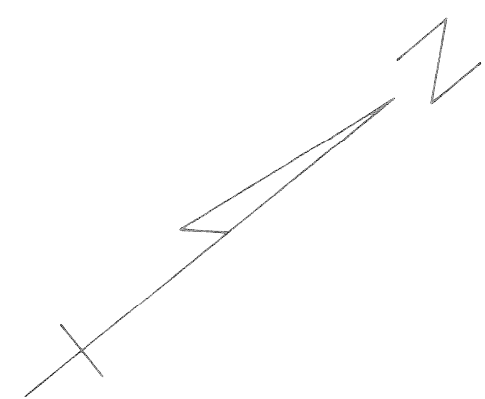
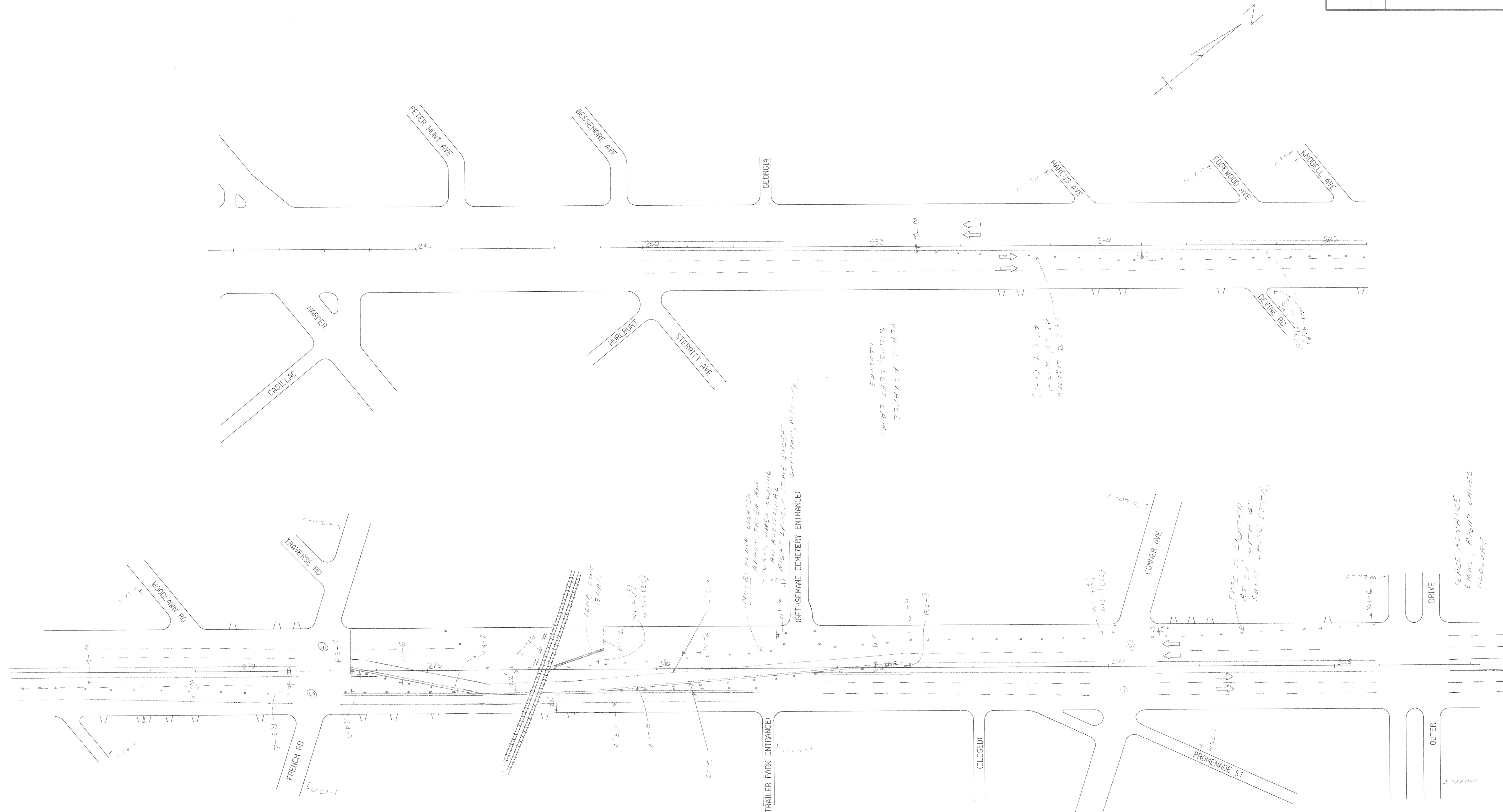
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12/ 1/92	1" = 20'	STR 82072	32068 A	ERBEN	R.O.W CONST. 5

EXISTING BY: DATE: DATE:  
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 LAST CORRECTION BY: DATE: DATE:

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FINAL R.O.W.		
AUTH	DATE	NO.



NOTE:  
 PLACE R7-14  
 SIGNS FROM STA. 265+00  
 TO STA 277+00 (EAST SIDE)  
 AT 50' SPACING.

NOTE: PLEASE LIGHTED  
 ARROWS, TRAILER AND  
 ALL WHEN GETTING  
 IN ADDITIONAL  
 RIGHT LANE. - TRAFFIC EXCEPT  
 GETHSEMANE CEMETERY ENTRANCE

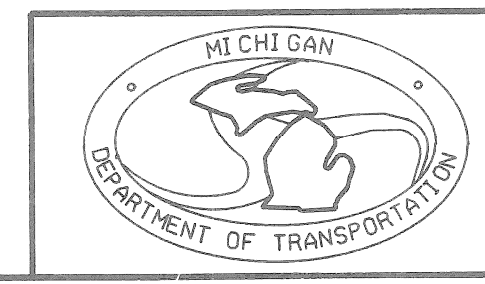
PLEASE ADVANCE  
 SIGNS, LEFT LANES  
 CLOSURE

TYPE II LIGHTED  
 AT 50' WITH  
 4" S.Y. (CTM)

TYPE II LIGHTED  
 AT 50' WITH 4"  
 SOLID WHITE (CTM)

PLEASE ADVANCE  
 SIGNS, RIGHT LANES  
 CLOSURE

- STAGE I
- ▲ LIGHTED ARROW
  - TYPE II LIGHTED BARR.
  - ≡ TYPE III LIGHTED BARR.
  - TEMP CONC. BARR WITH TRAFFIC LIGHTS



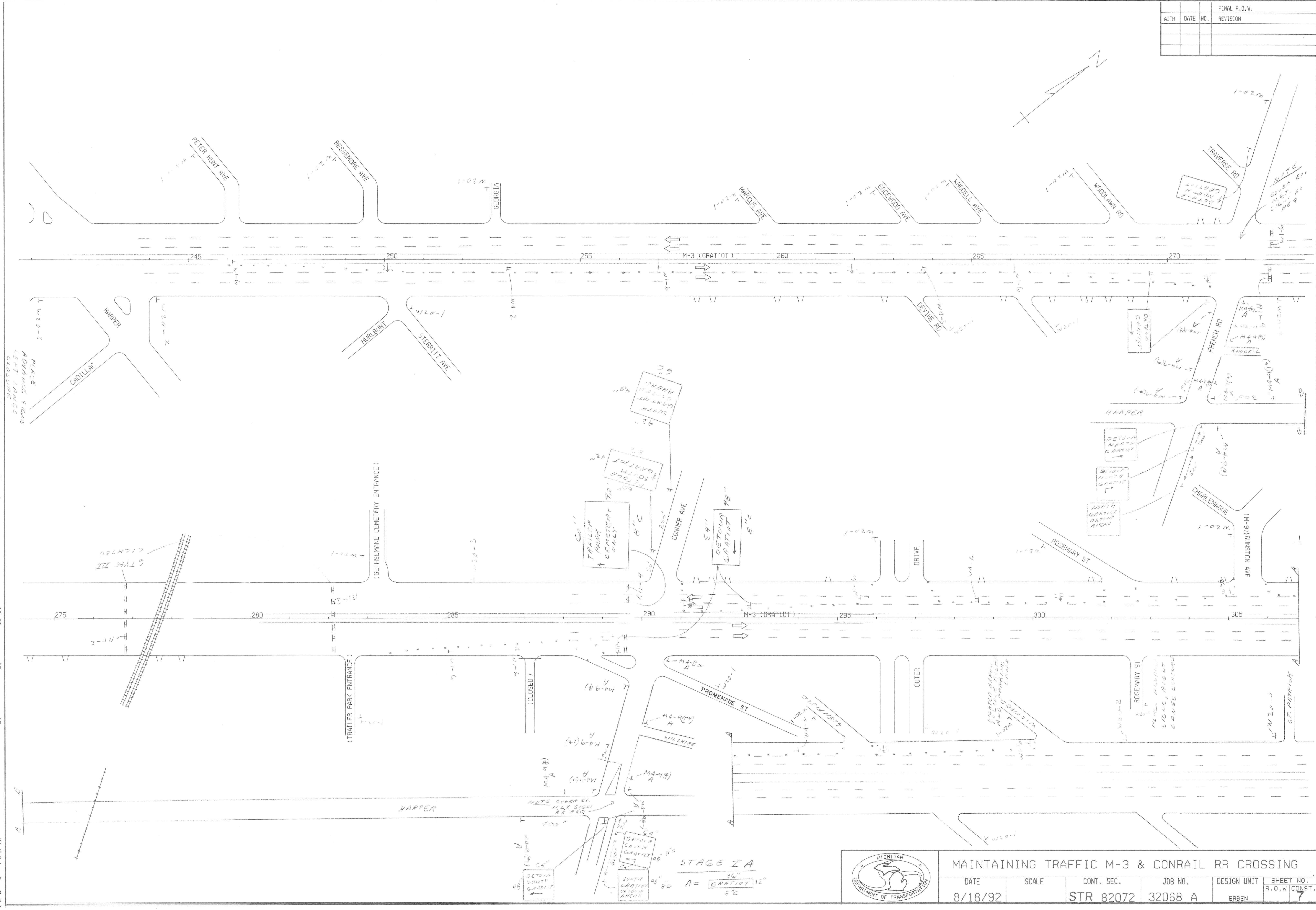
MAINTAINING TRAFFIC M-3 & CONRAIL RR CROSSING					
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
10/26/92		STR 82072	32068 A	ERBEN	R.O.W. CONST. 6

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LAST CORRECTION BY:

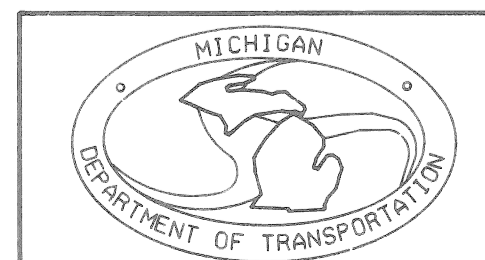
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FINAL R.O.W.			
AUTH	DATE	NO.	REVISION



STAGE I A  
A = GRATIOT 12"  
5'C



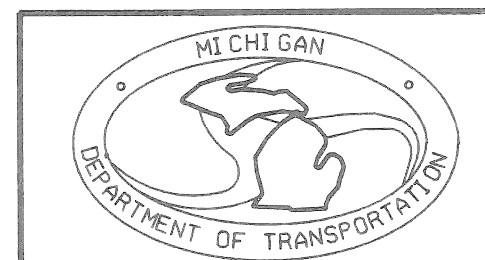
MAINTAINING TRAFFIC M-3 & CONRAIL RR CROSSING					
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT	SHEET NO.
8/18/92			32068 A	ERBEN	7

EXISTING BY: DATE: DATE:  
 PROPOSED BY: DATE: DATE:  
 LAST CORRECTION BY: DATE: DATE:

AUTH	DATE	NO.	REVISION	FINAL R.O.W.

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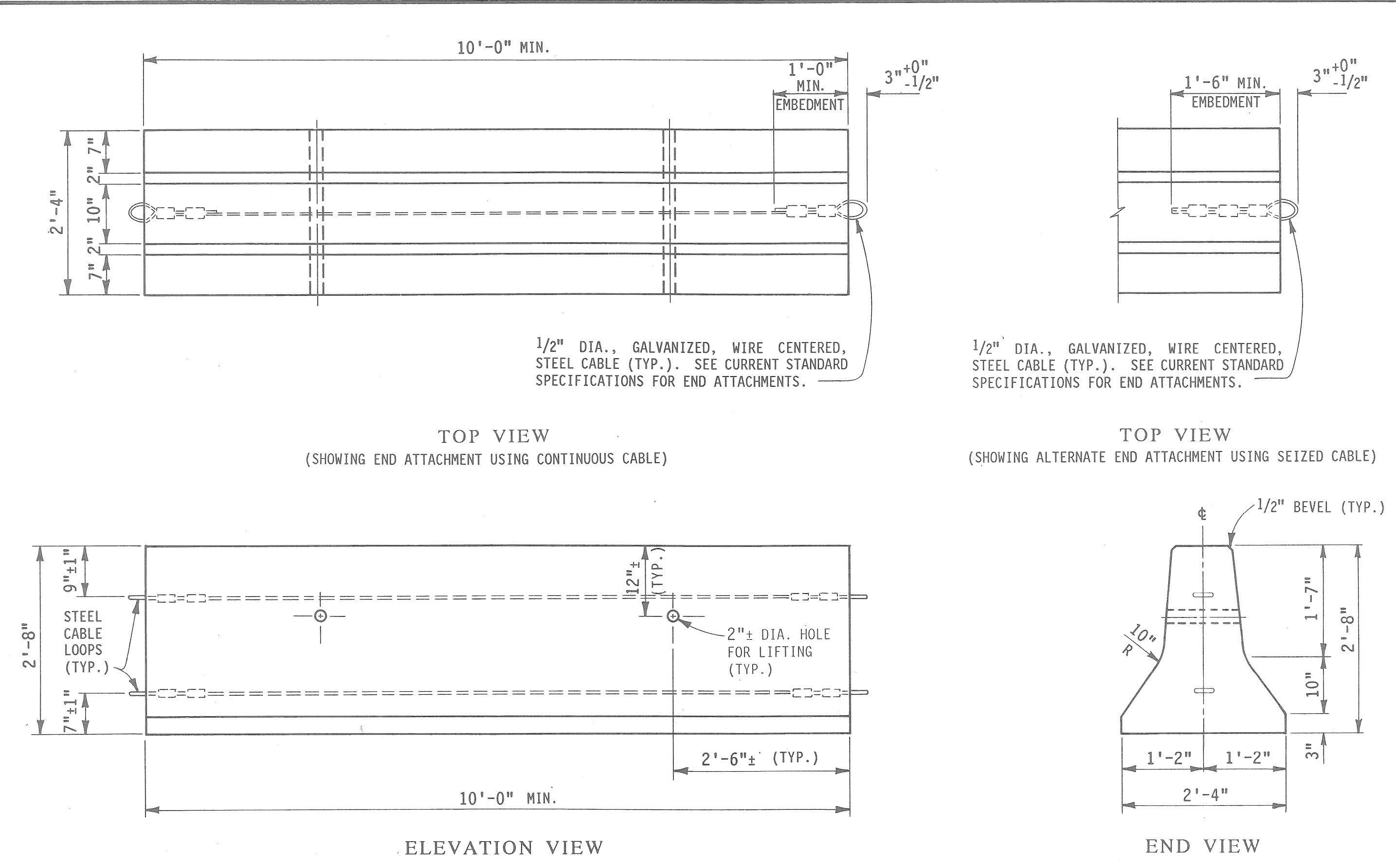
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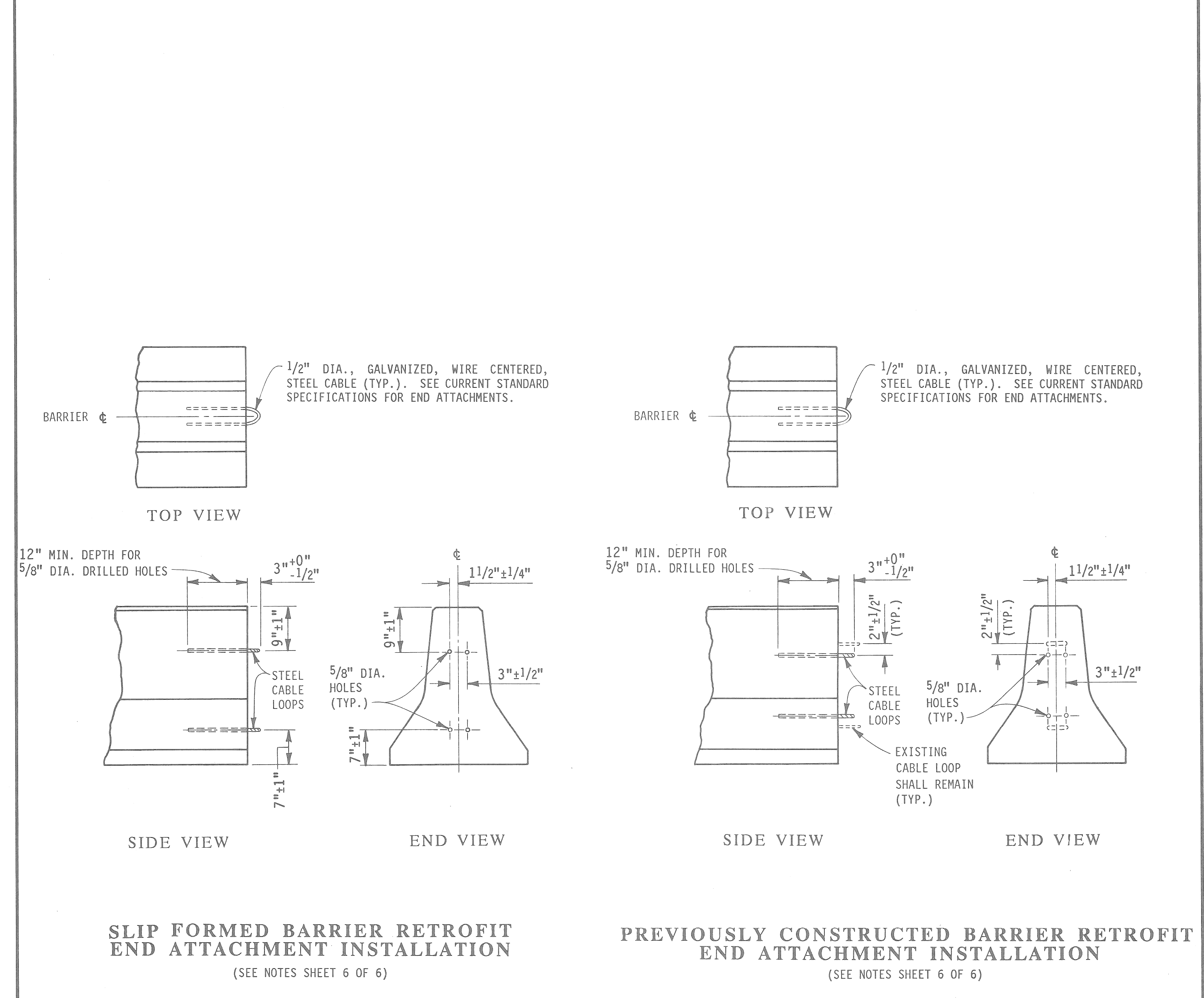
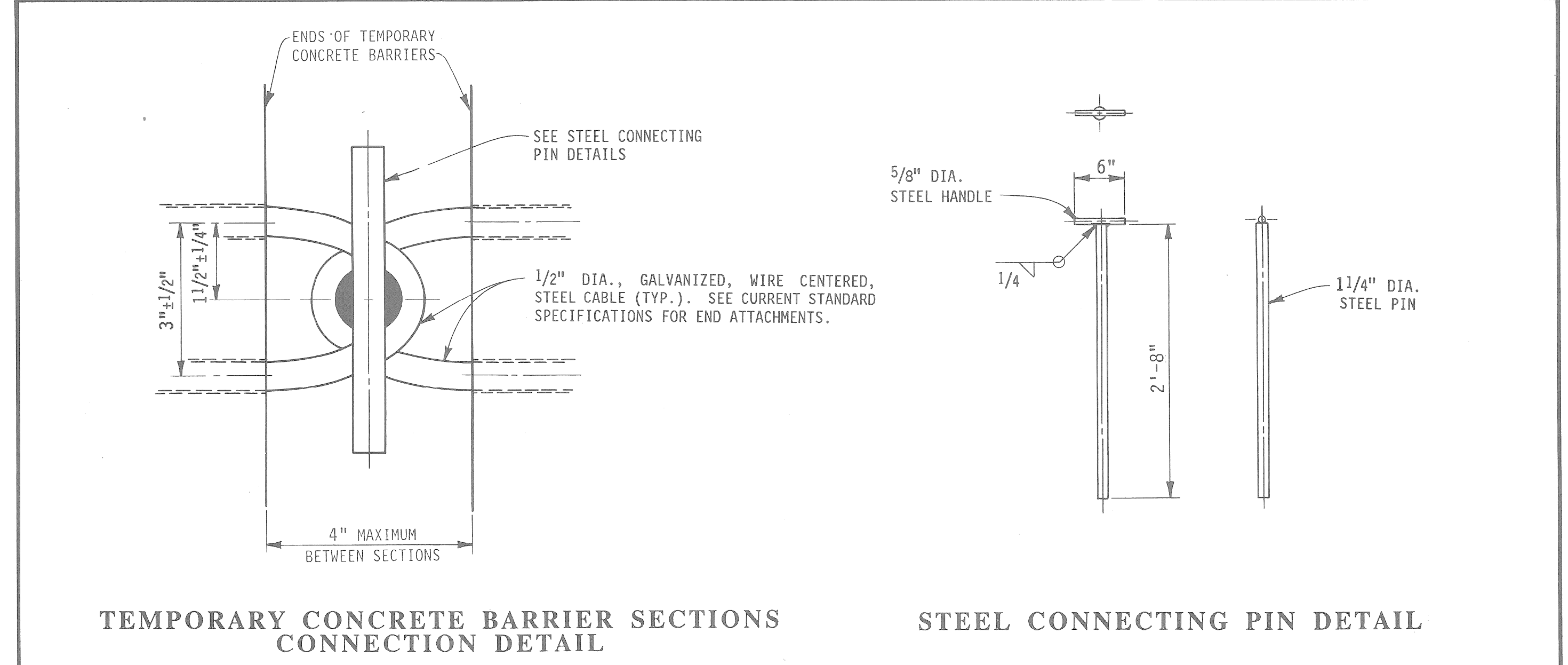
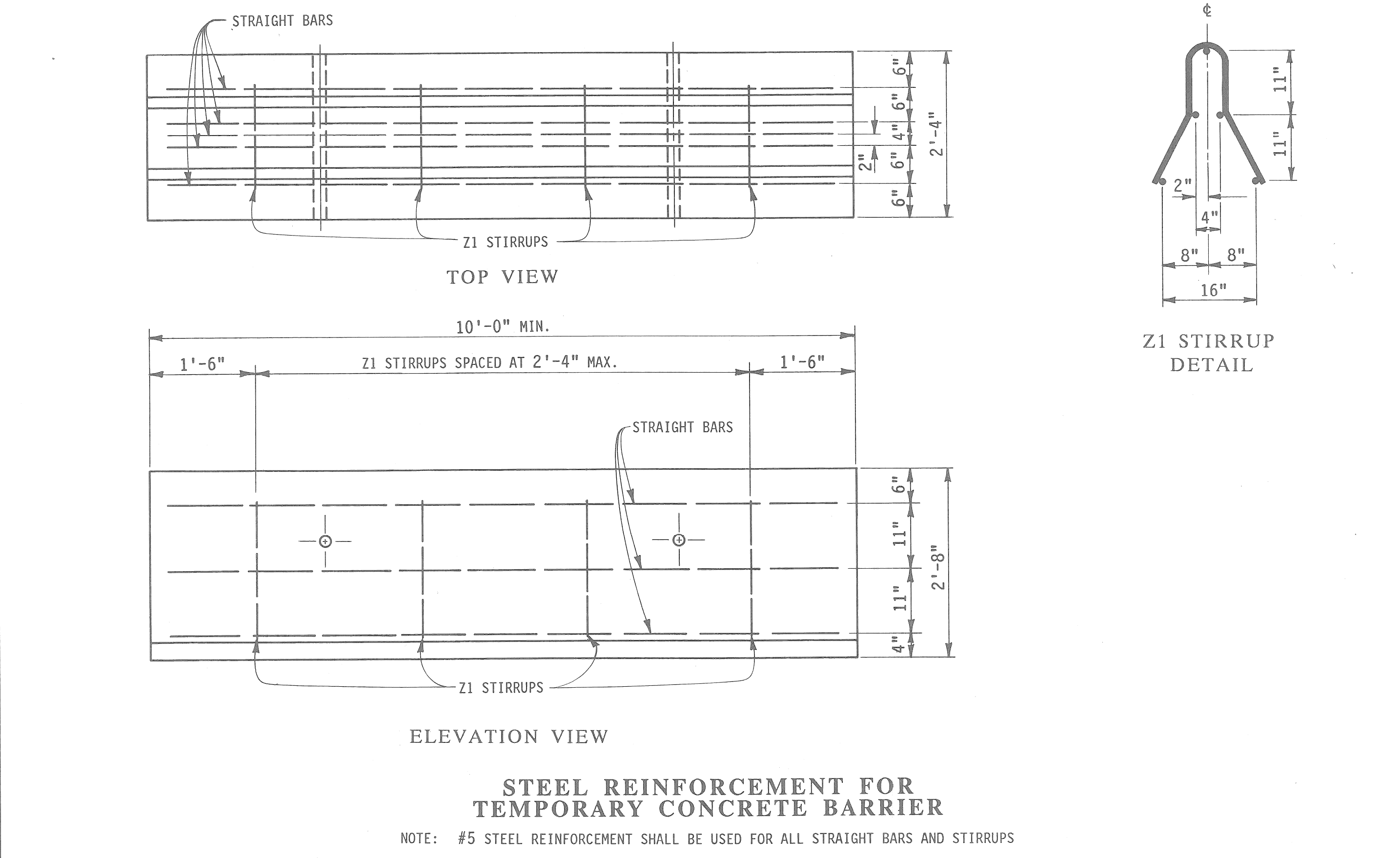
MAINTAINING TRAFFIC M-3 & CONRAIL RR CROSSING				
DATE	SCALE	CONT. SEC.	JOB NO.	DESIGN UNIT
10/26/92		STR 82072	32068 A	ERBEN
SHEET NO. 8				CONST.



SHEET NO. 32068A  
 JOB NO. 82072  
 CONTROL SECTION



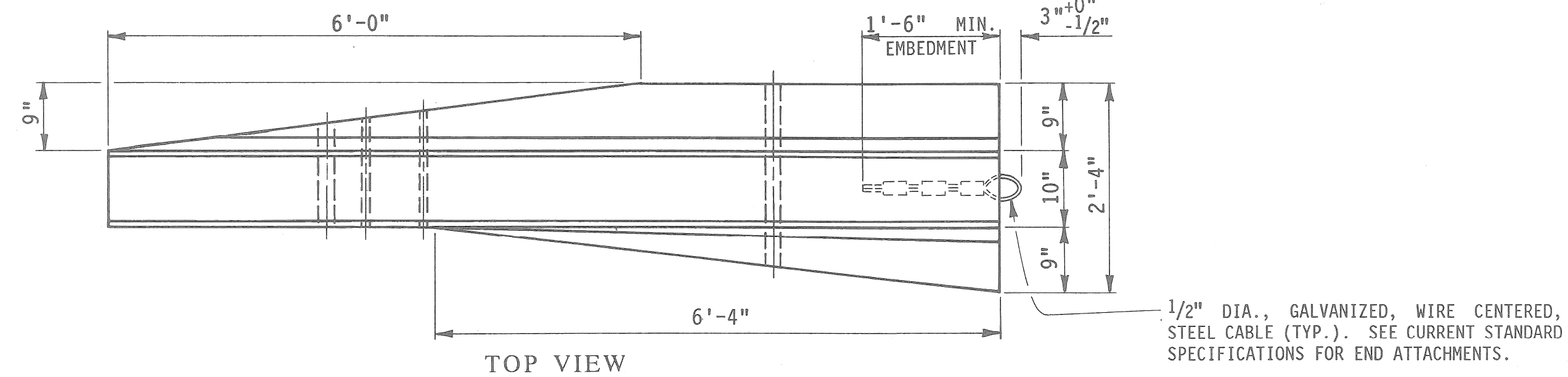
**TEMPORARY CONCRETE BARRIER AND END ATTACHMENTS**



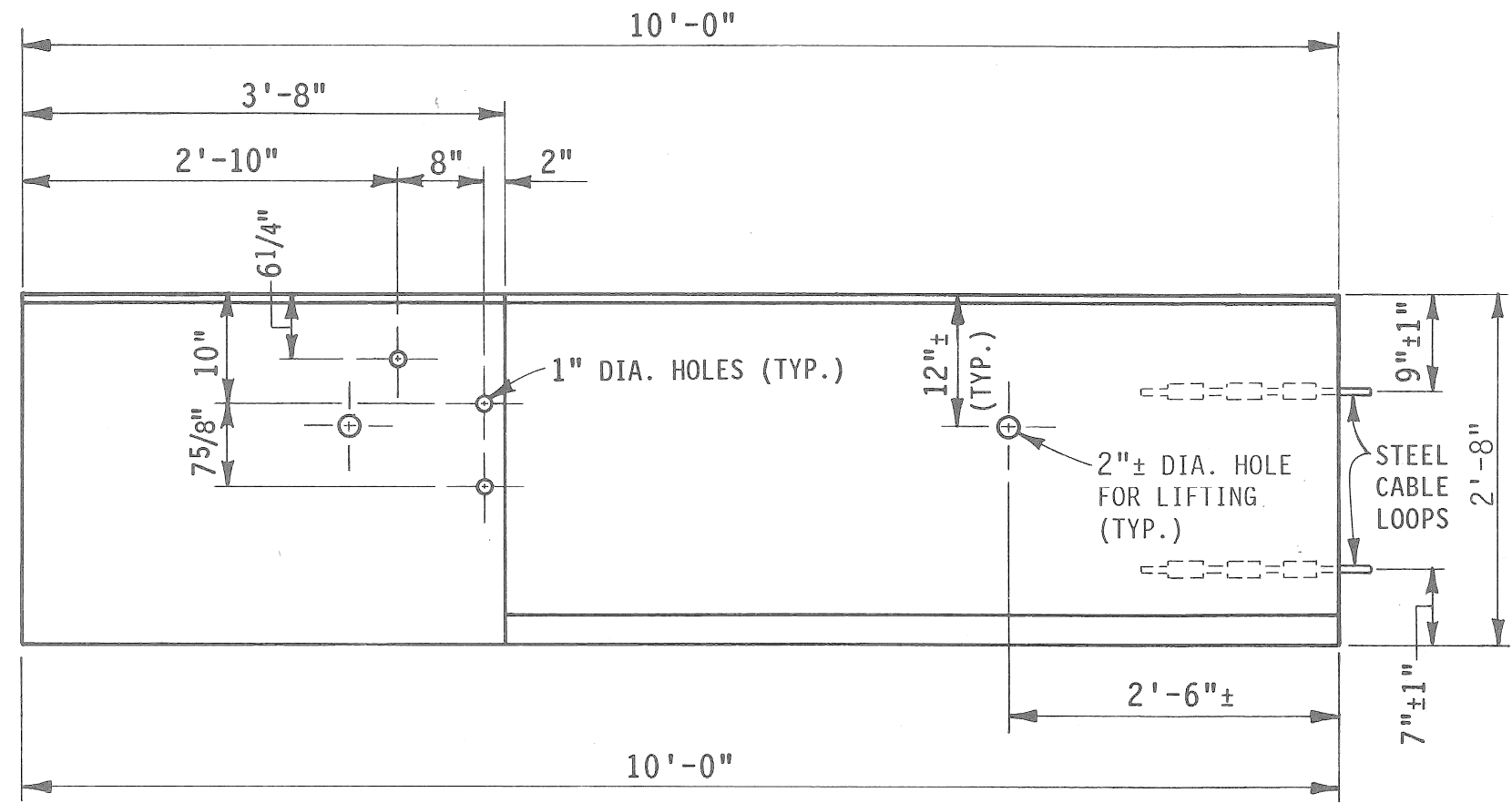
<p>PREPARED BY DESIGN DIVISION</p> <p>DRAWN BY: D.F.M.</p> <p>CHECKED BY: R.S.C.</p>	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b> II-52F SHEET 1 OF 6		
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN			
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK			
	ENGINEER OF TRAFFIC AND SAFETY	DEPUTY DIRECTOR-HIGHWAYS			
	F.H.W.A. APPROVAL	9-26-91 PLAN DATE			

<p>PREPARED BY DESIGN DIVISION</p> <p>DRAWN BY: D.F.M.</p> <p>CHECKED BY: R.S.C.</p>	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b> II-52F SHEET 2 OF 6		
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN			
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK			
	ENGINEER OF TRAFFIC AND SAFETY	DEPUTY DIRECTOR-HIGHWAYS			
	F.H.W.A. APPROVAL	9-26-91 PLAN DATE			

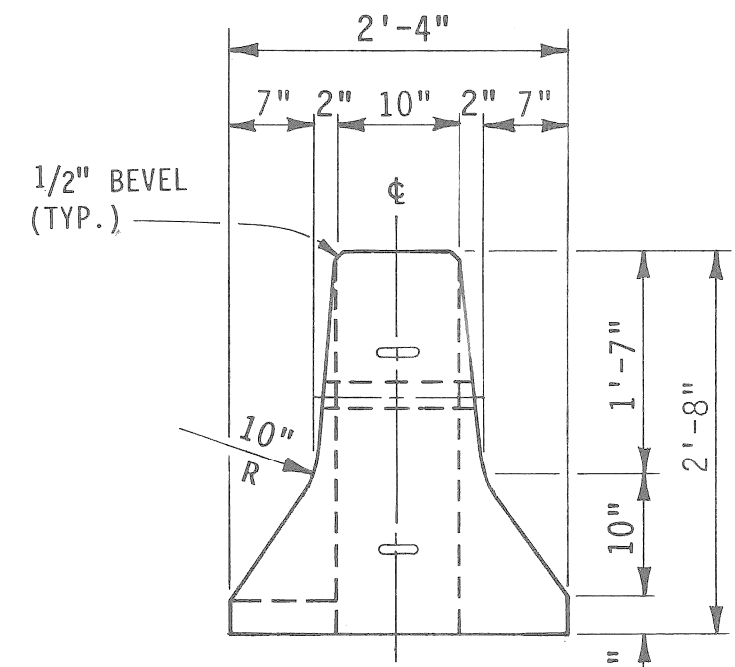
CONTROL SECTION 82072 JOB NO. 32068A SHEET NO. 9



1/2" DIA., GALVANIZED, WIRE CENTERED, STEEL CABLE (TYP.). SEE CURRENT STANDARD SPECIFICATIONS FOR END ATTACHMENTS.

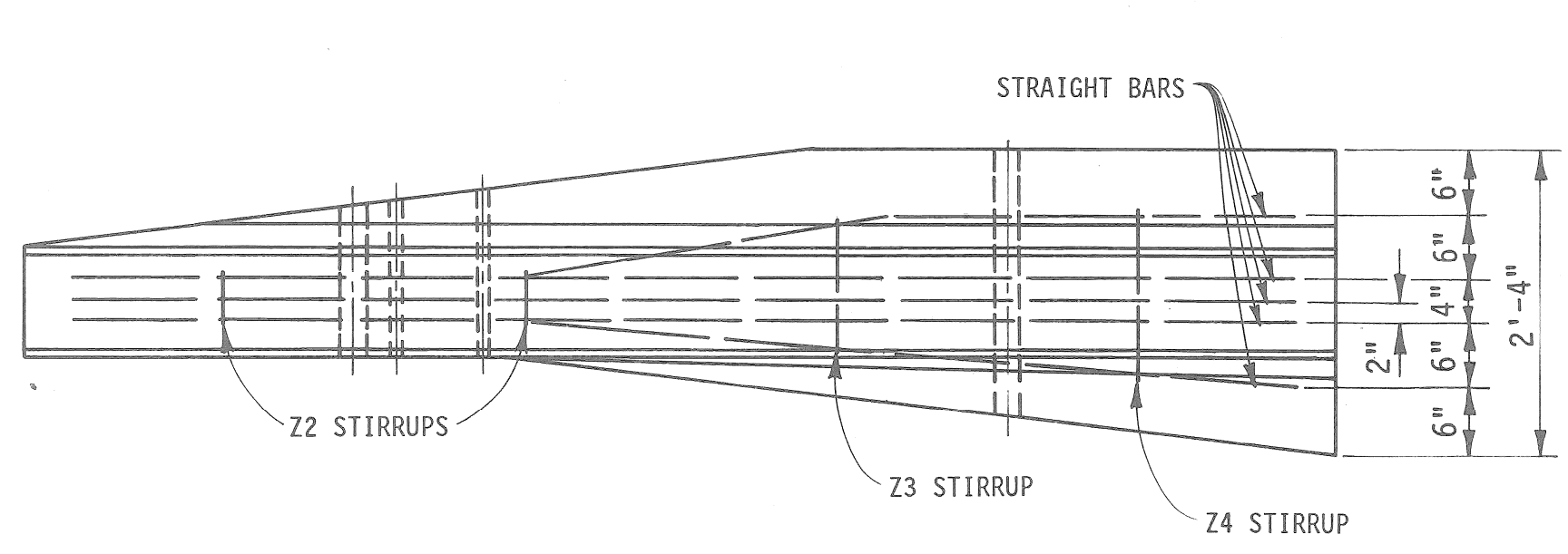


ELEVATION VIEW

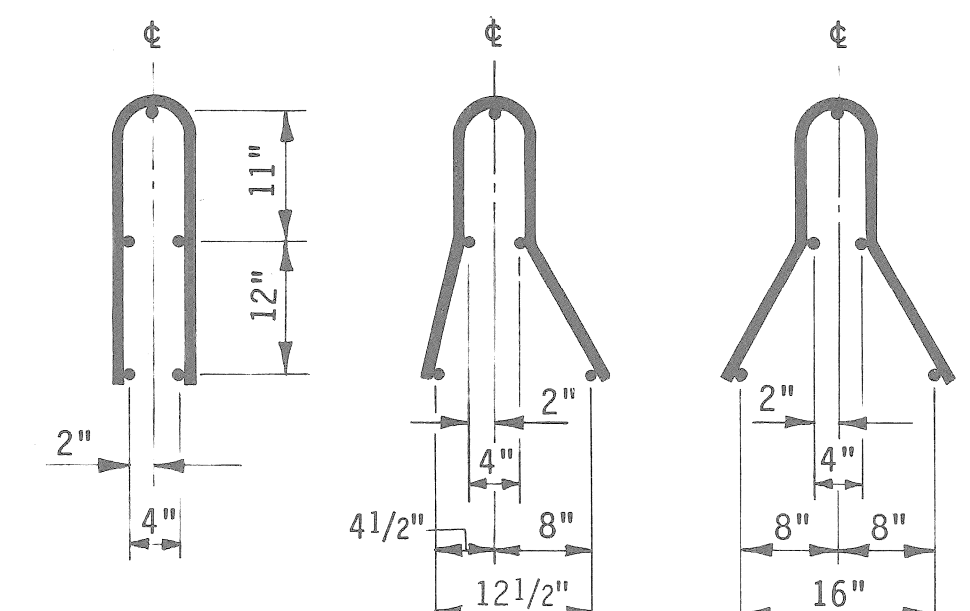


END VIEW

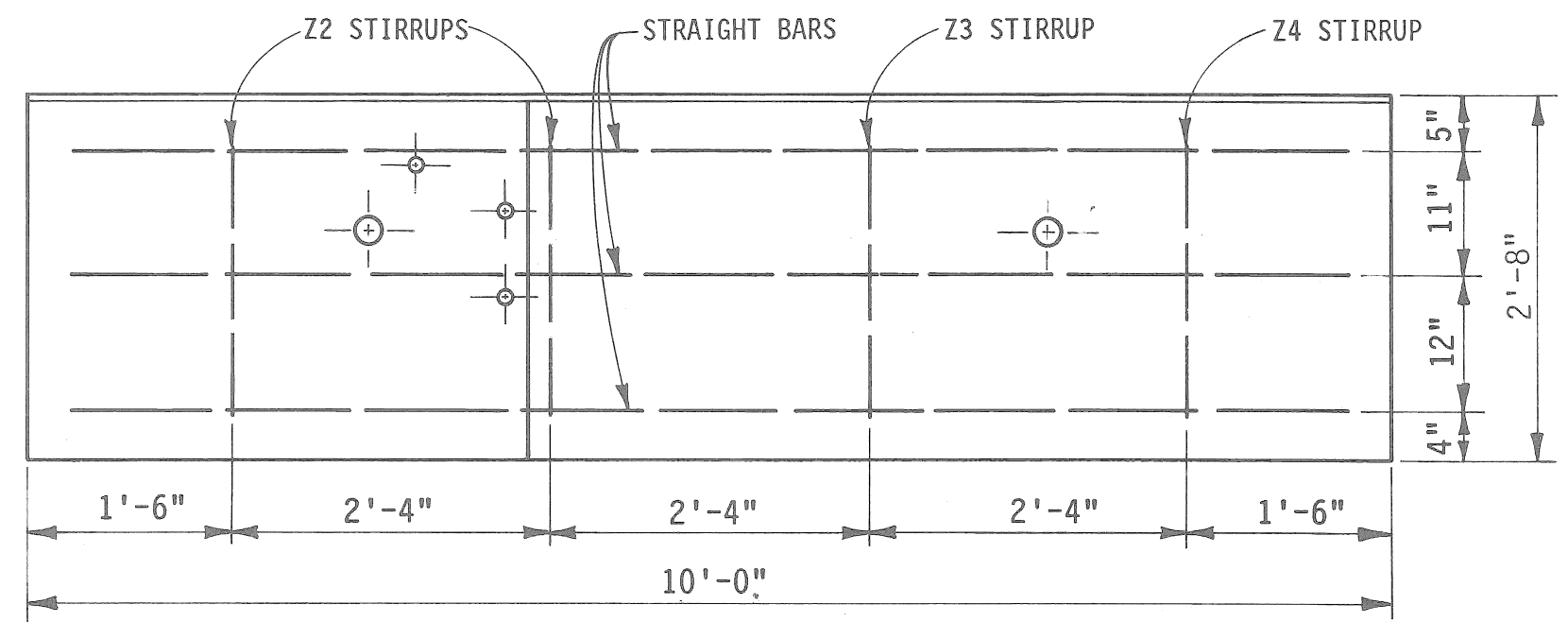
TEMPORARY CONCRETE BARRIER CONNECTING SECTION



TOP VIEW



Z2 STIRRUP DETAIL Z3 STIRRUP DETAIL Z4 STIRRUP DETAIL

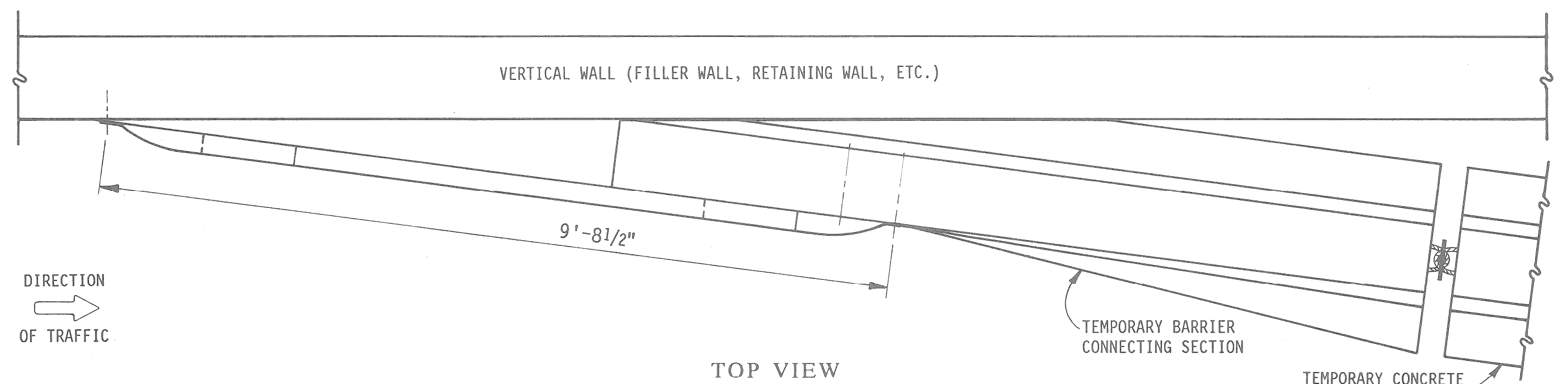


ELEVATION VIEW

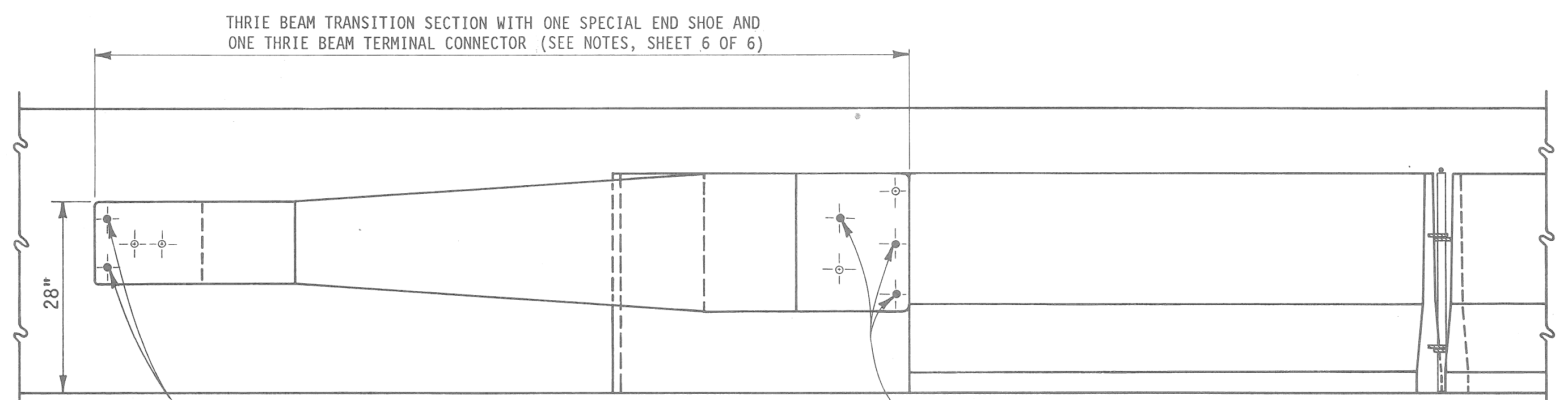
STEEL REINFORCEMENT FOR TEMPORARY CONCRETE BARRIER CONNECTING SECTION

NOTE: #5 STEEL REINFORCEMENT SHALL BE USED FOR ALL STRAIGHT BARS AND STIRRUPS

 PREPARED BY DESIGN DIVISION DRAWN BY: D.F.M. CHECKED BY: R.S.C.	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b>	SHEET 3 OF 6
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN		
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK		
	BY: _____	DEPUTY DIRECTOR-HIGHWAYS		
	ENGINEER OF TRAFFIC AND SAFETY	F.H.W.A. APPROVAL	9-26-91 PLAN DATE	II-52F

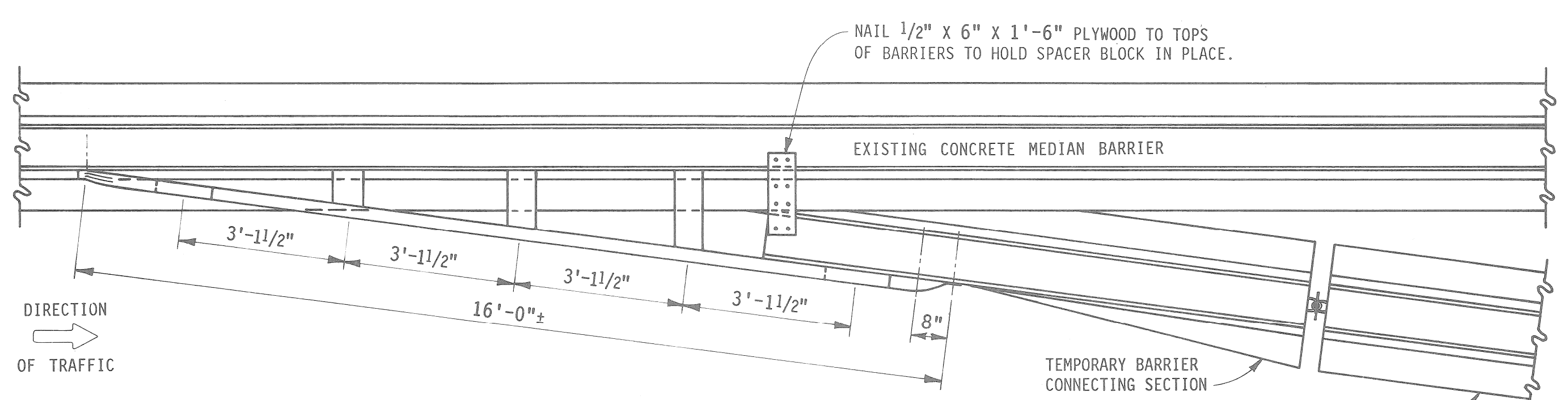


TOP VIEW



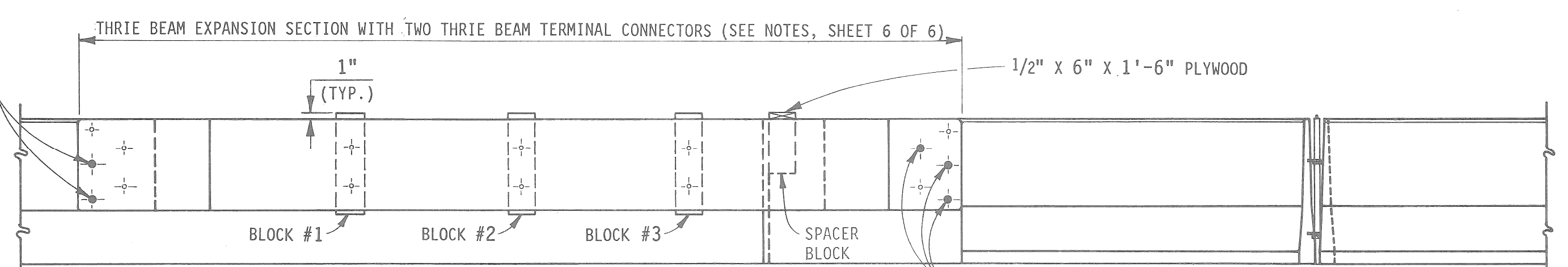
ELEVATION VIEW

DETAILS FOR CONNECTING TEMPORARY CONCRETE BARRIER CONNECTING SECTION TO VERTICAL WALL



DIRECTION OF TRAFFIC

TOP VIEW

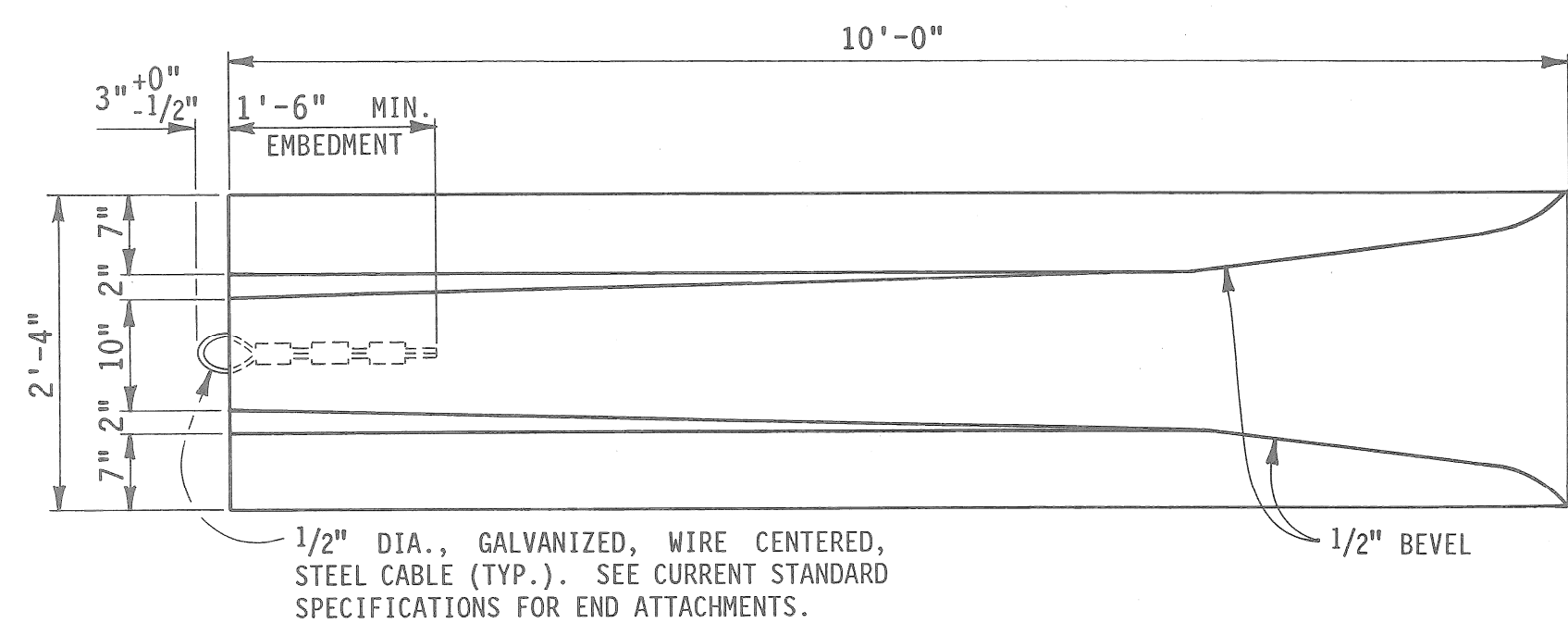


ELEVATION VIEW

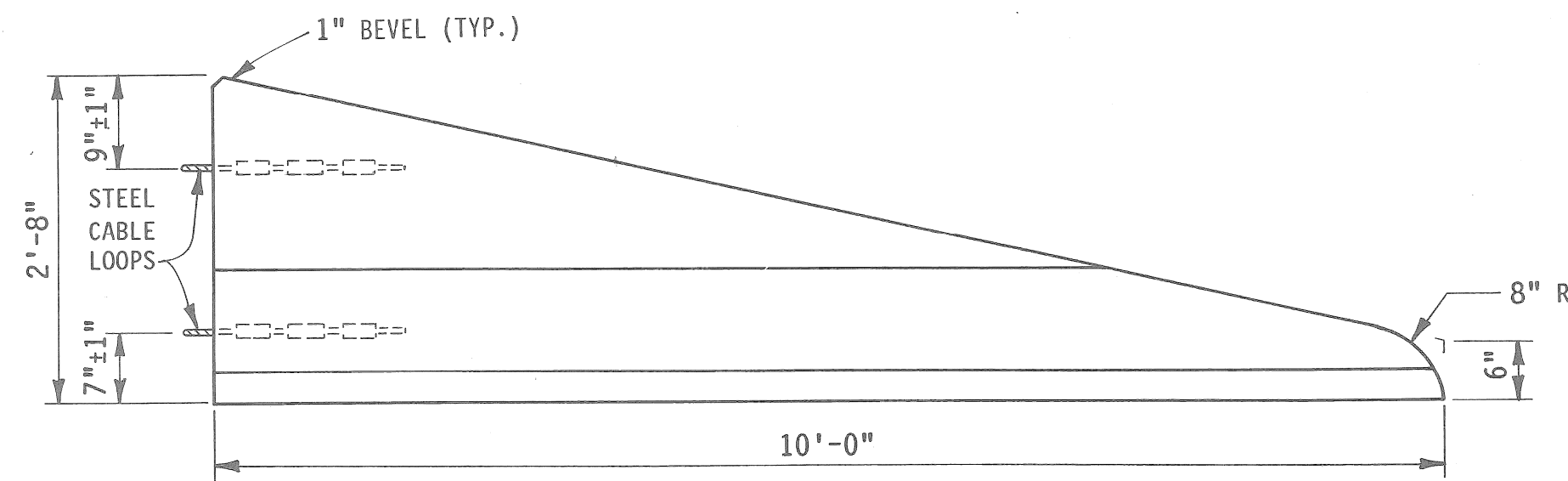
DETAILS FOR CONNECTING TEMPORARY CONCRETE BARRIER CONNECTING SECTION TO CONCRETE BARRIER

NOTE: WOOD BLOCKS #1, #2, AND #3 SHALL BE FASTENED TO THIRIE BEAM EXPANSION SECTION WITH 5/8" X 4" LAG SCREWS (TWO PER WOOD BLOCK)

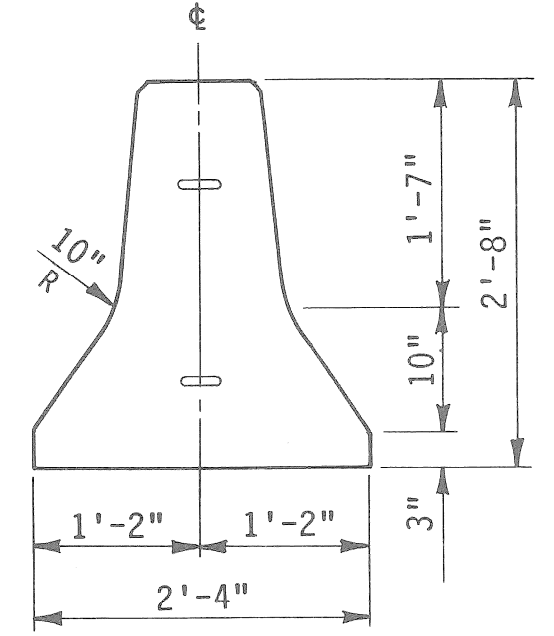
 PREPARED BY DESIGN DIVISION DRAWN BY: D.F.M. CHECKED BY: R.S.C.	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b>	SHEET 4 OF 6
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN		
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK		
	BY: _____	DEPUTY DIRECTOR-HIGHWAYS		
	ENGINEER OF TRAFFIC AND SAFETY	F.H.W.A. APPROVAL	9-26-91 PLAN DATE	II-52F



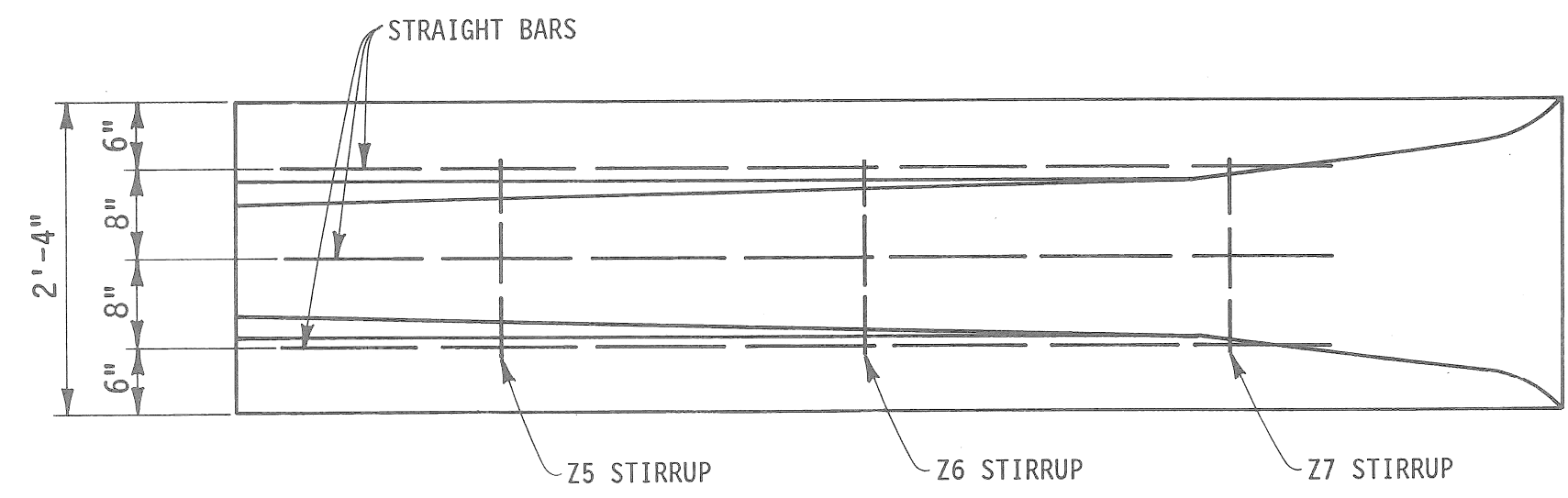
TOP VIEW



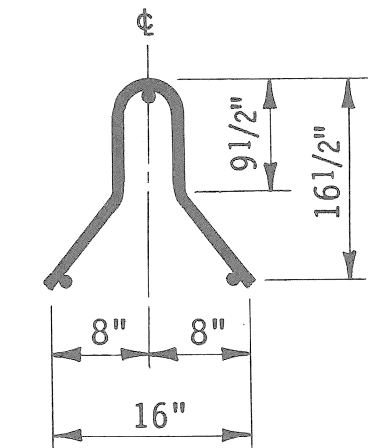
ELEVATION VIEW  
 TEMPORARY CONCRETE BARRIER ENDING SECTION



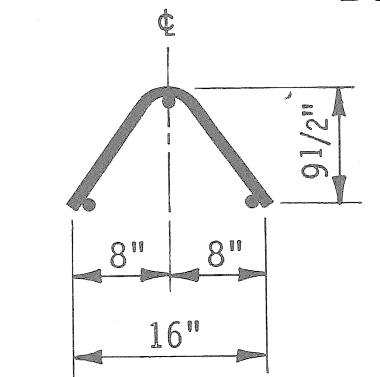
END VIEW



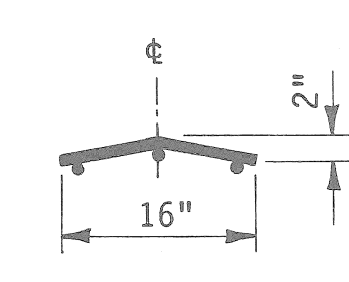
TOP VIEW



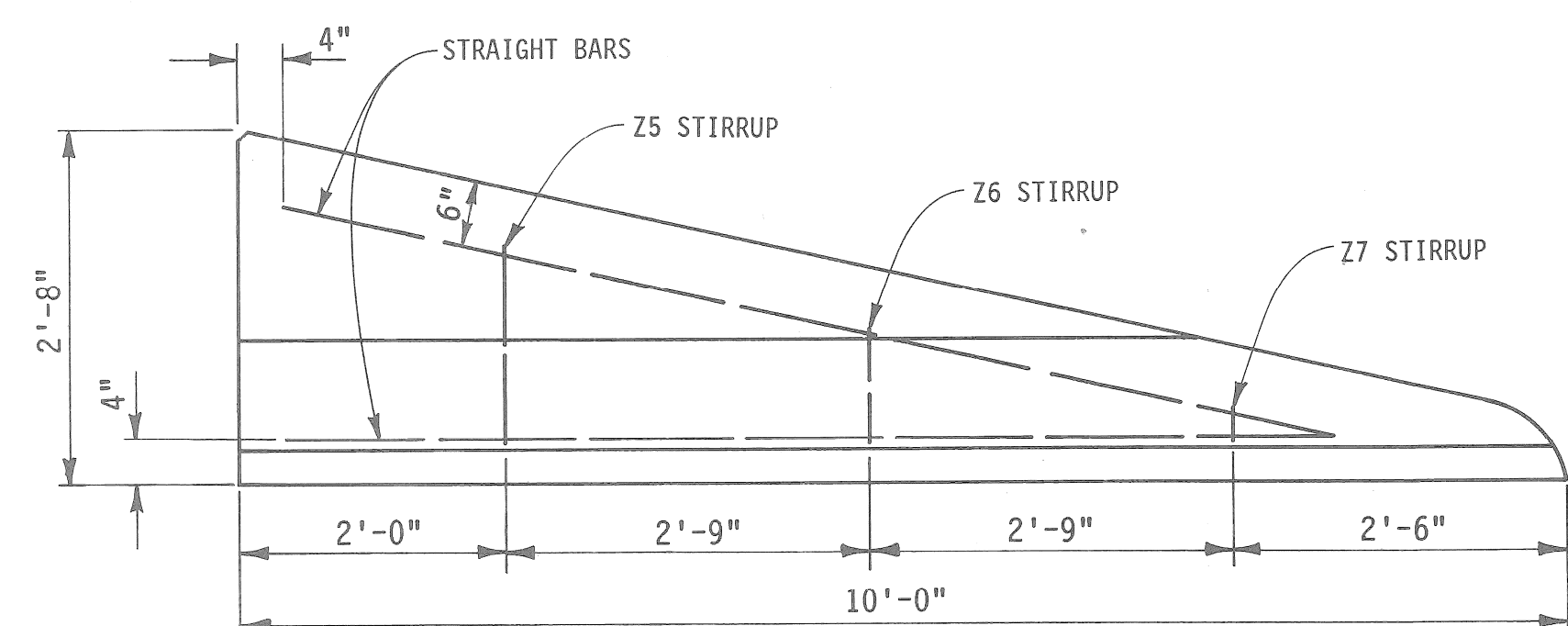
Z5 STIRRUP DETAIL



Z6 STIRRUP DETAIL



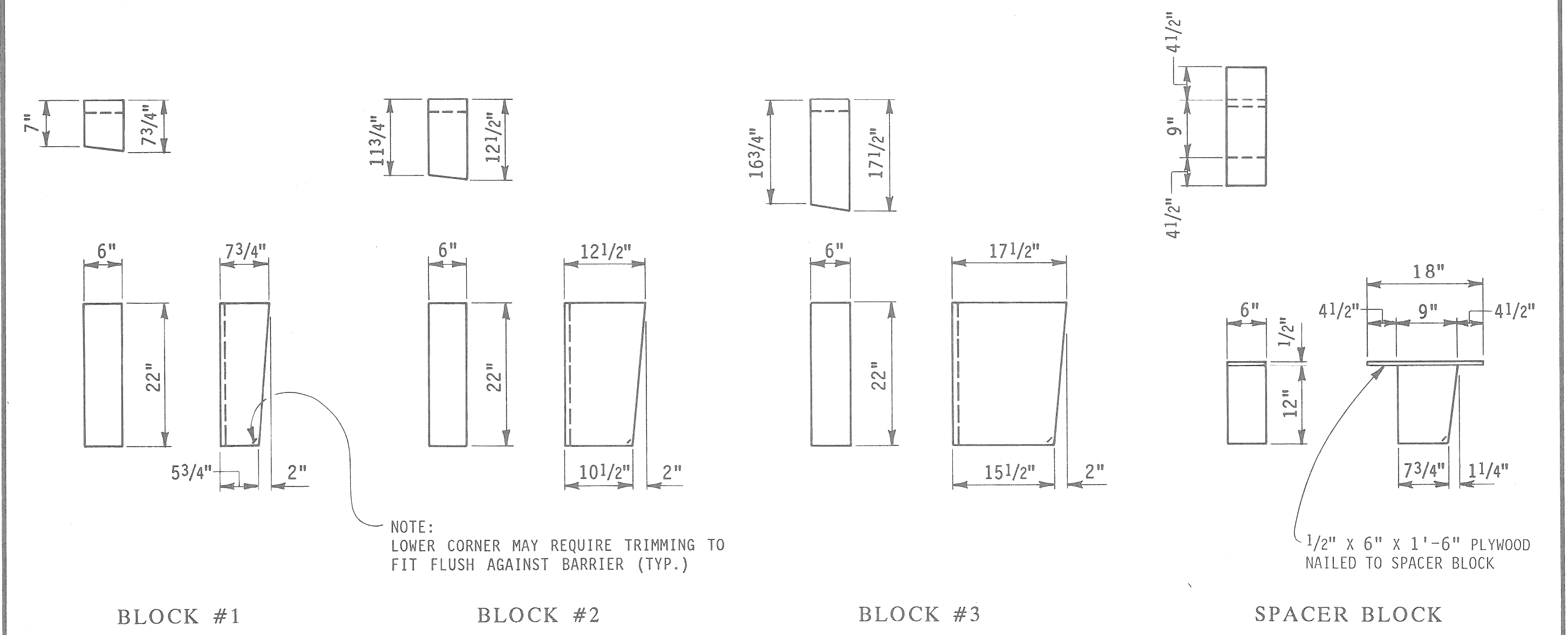
Z7 STIRRUP DETAIL



ELEVATION VIEW

STEEL REINFORCEMENT FOR  
 TEMPORARY CONCRETE BARRIER ENDING SECTION

NOTE: #5 STEEL REINFORCEMENT SHALL BE USED FOR ALL STRAIGHT BARS AND STIRRUPS



DETAILS FOR WOOD BLOCKS

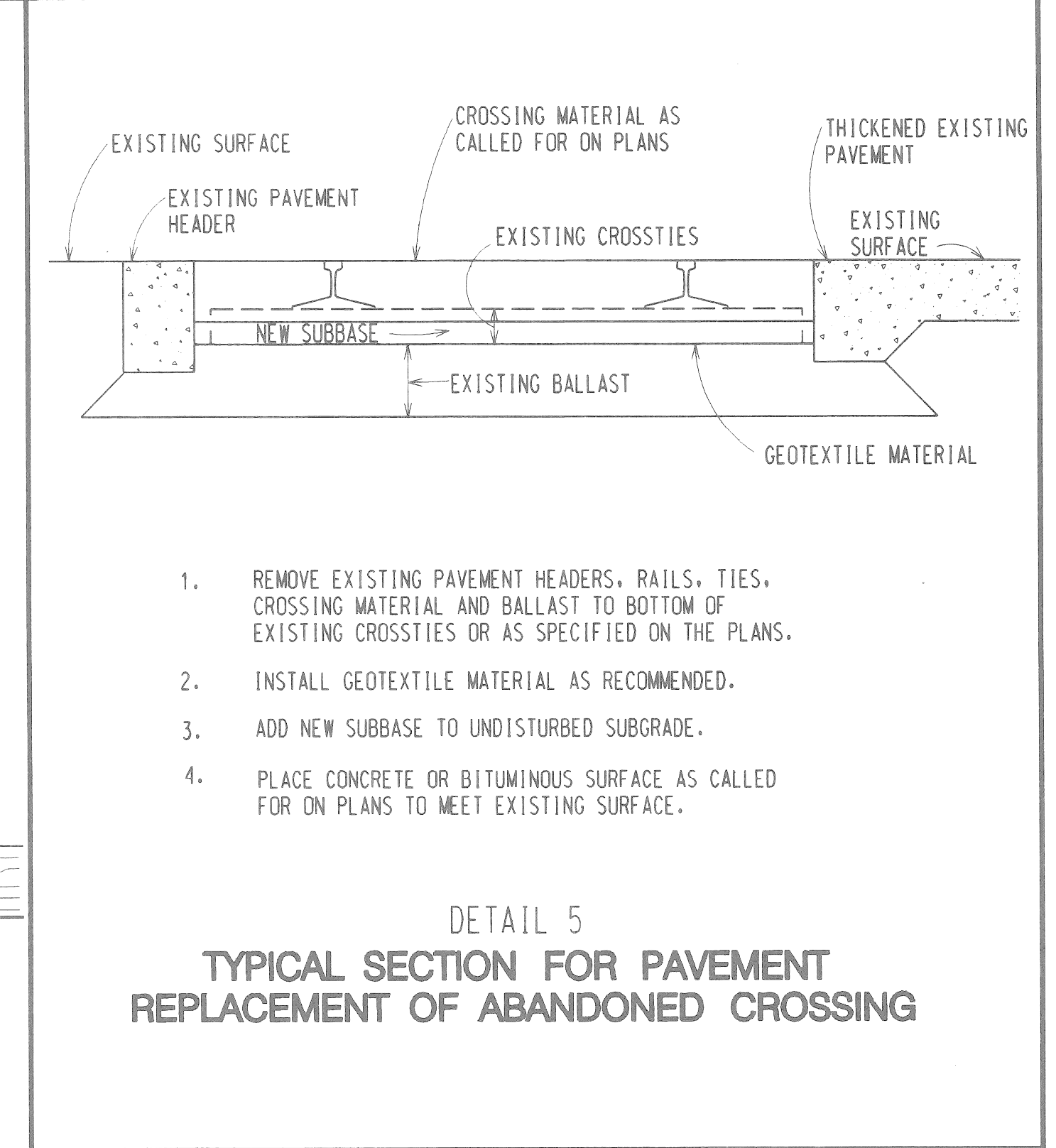
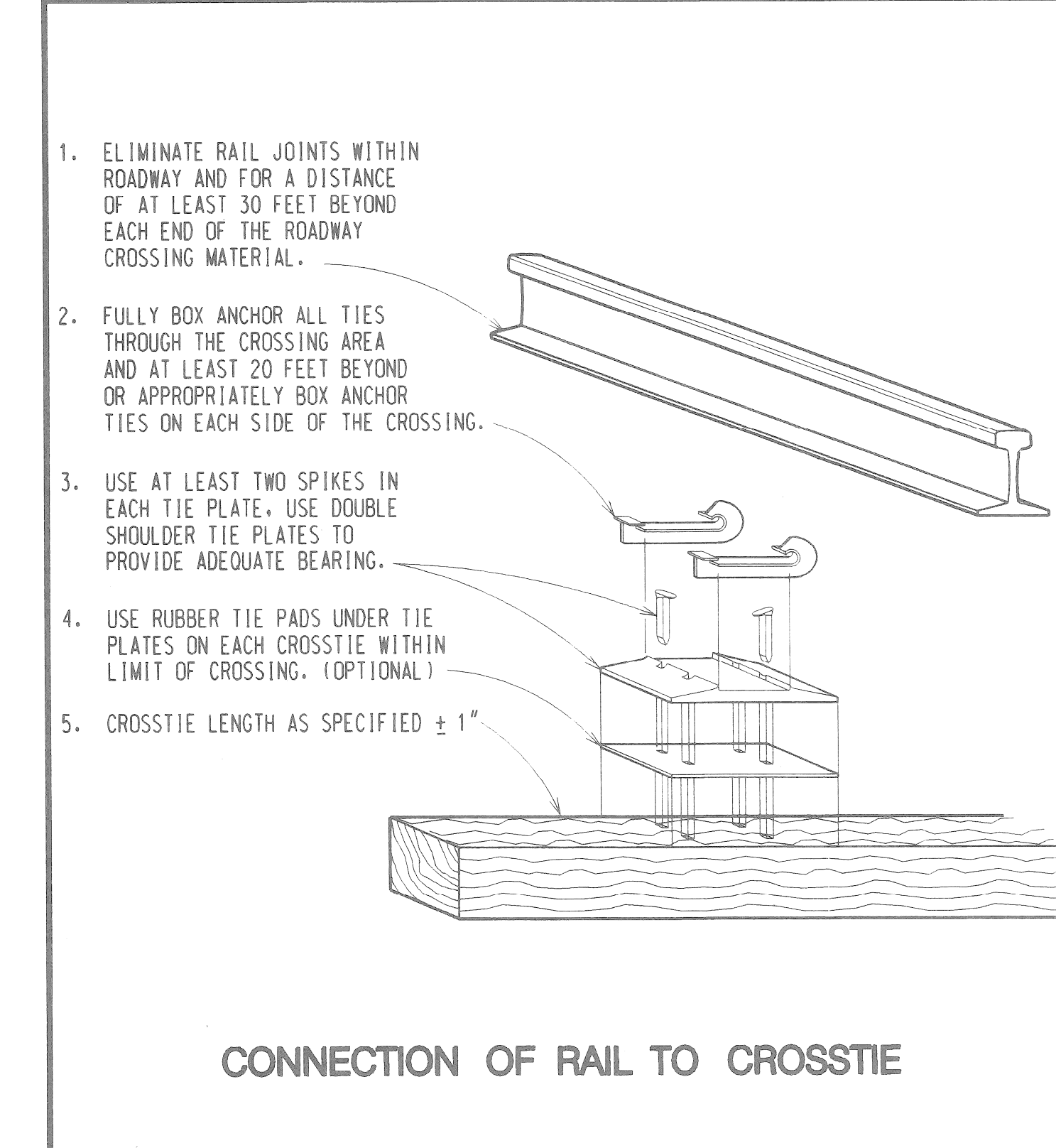
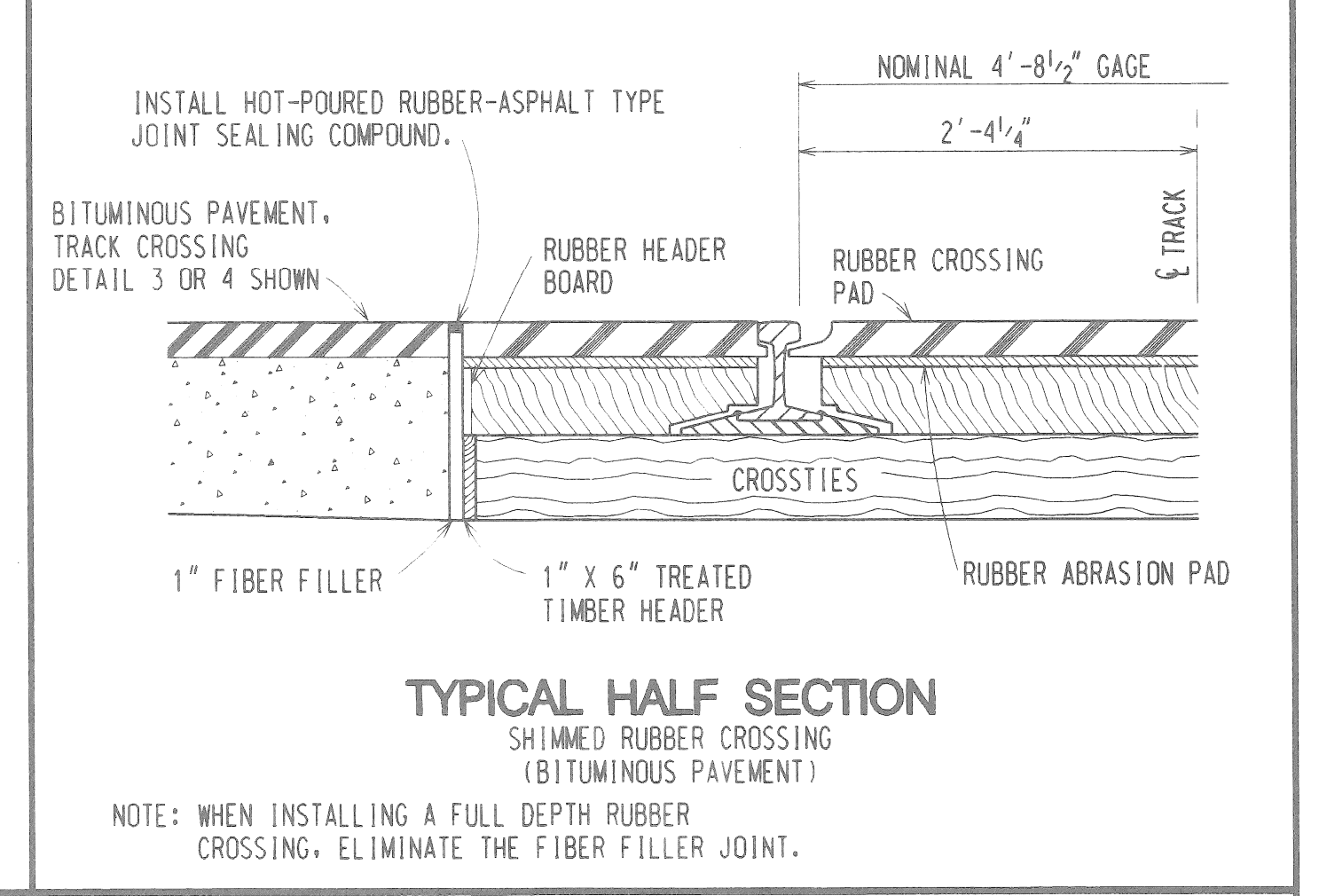
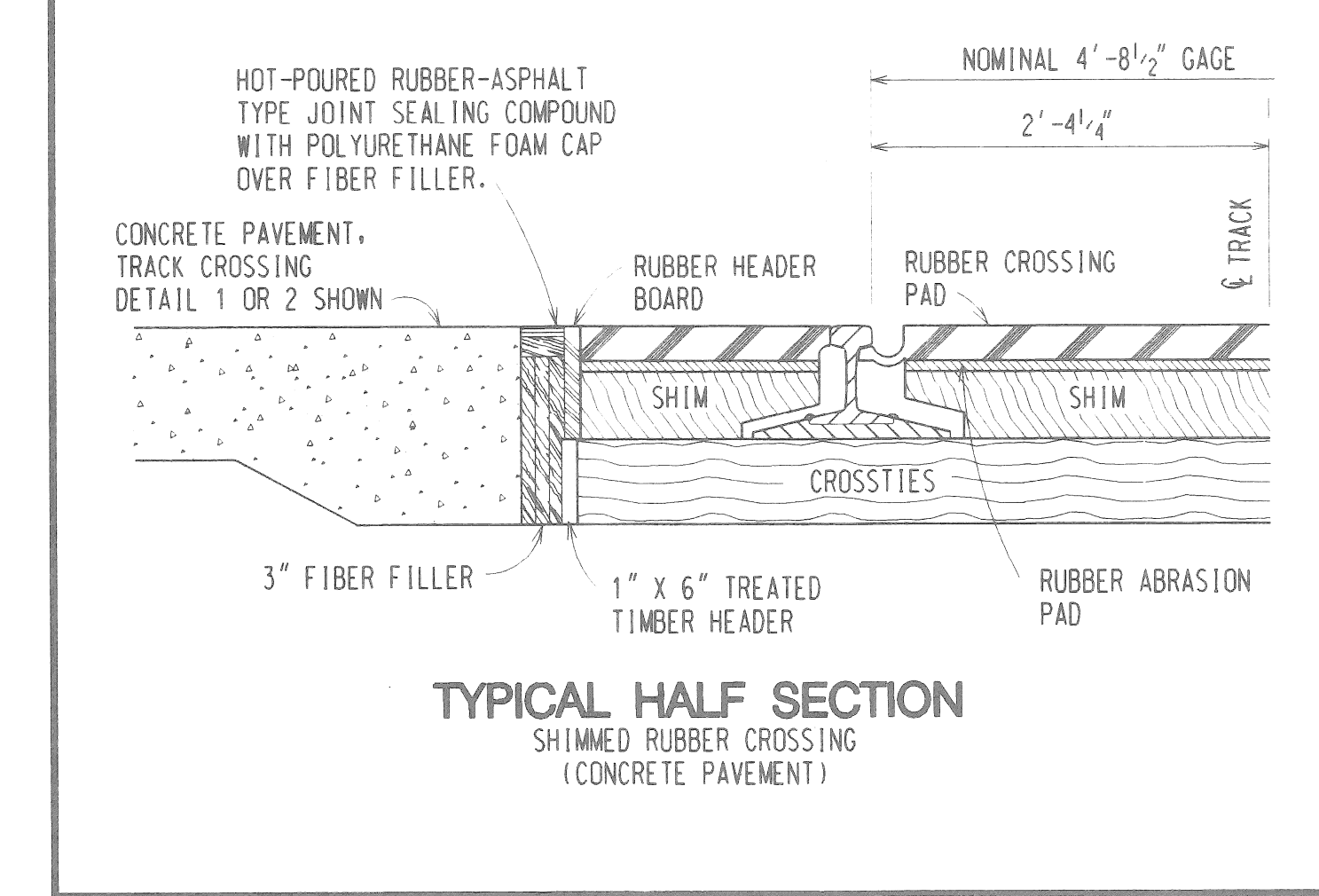
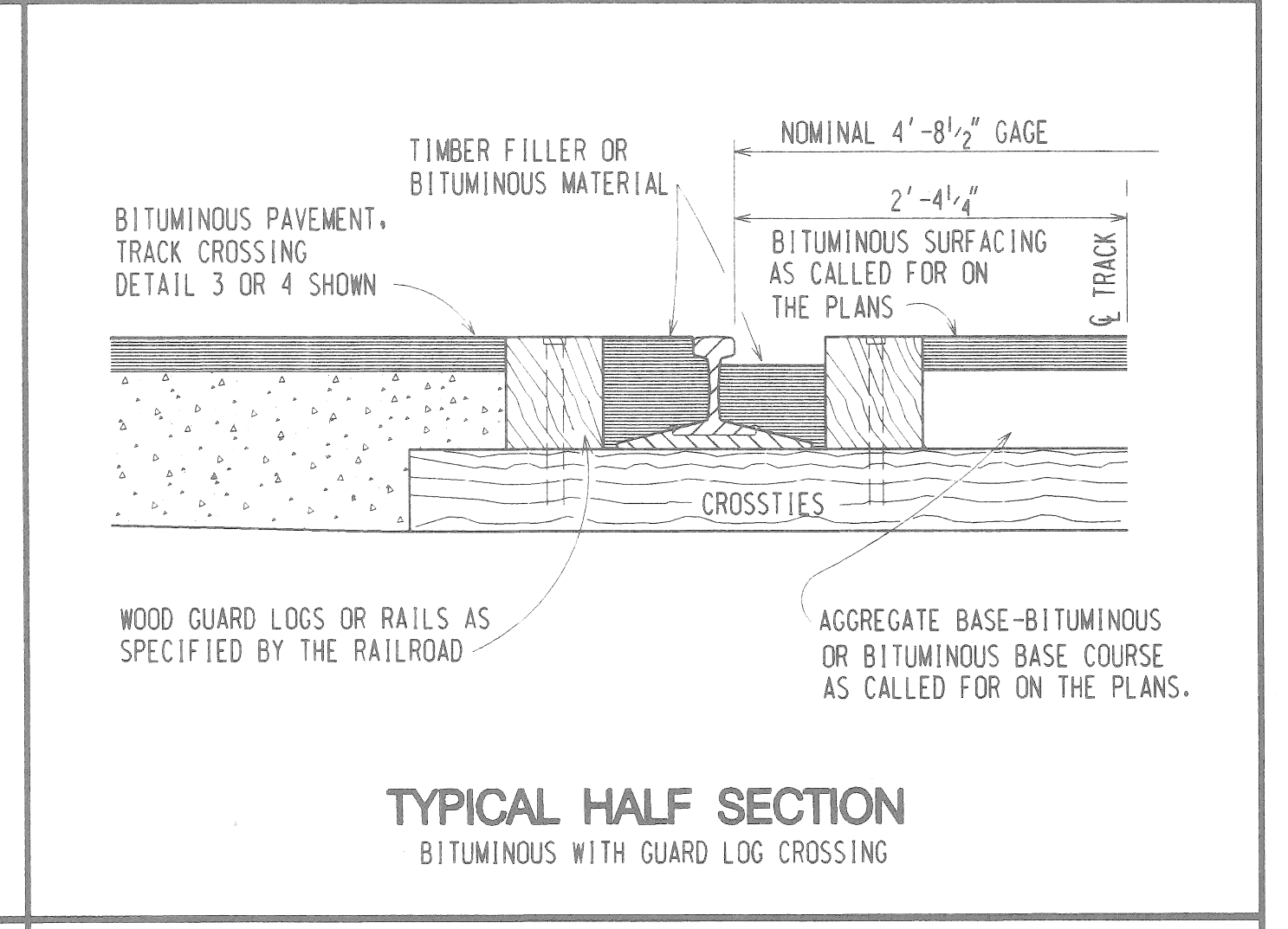
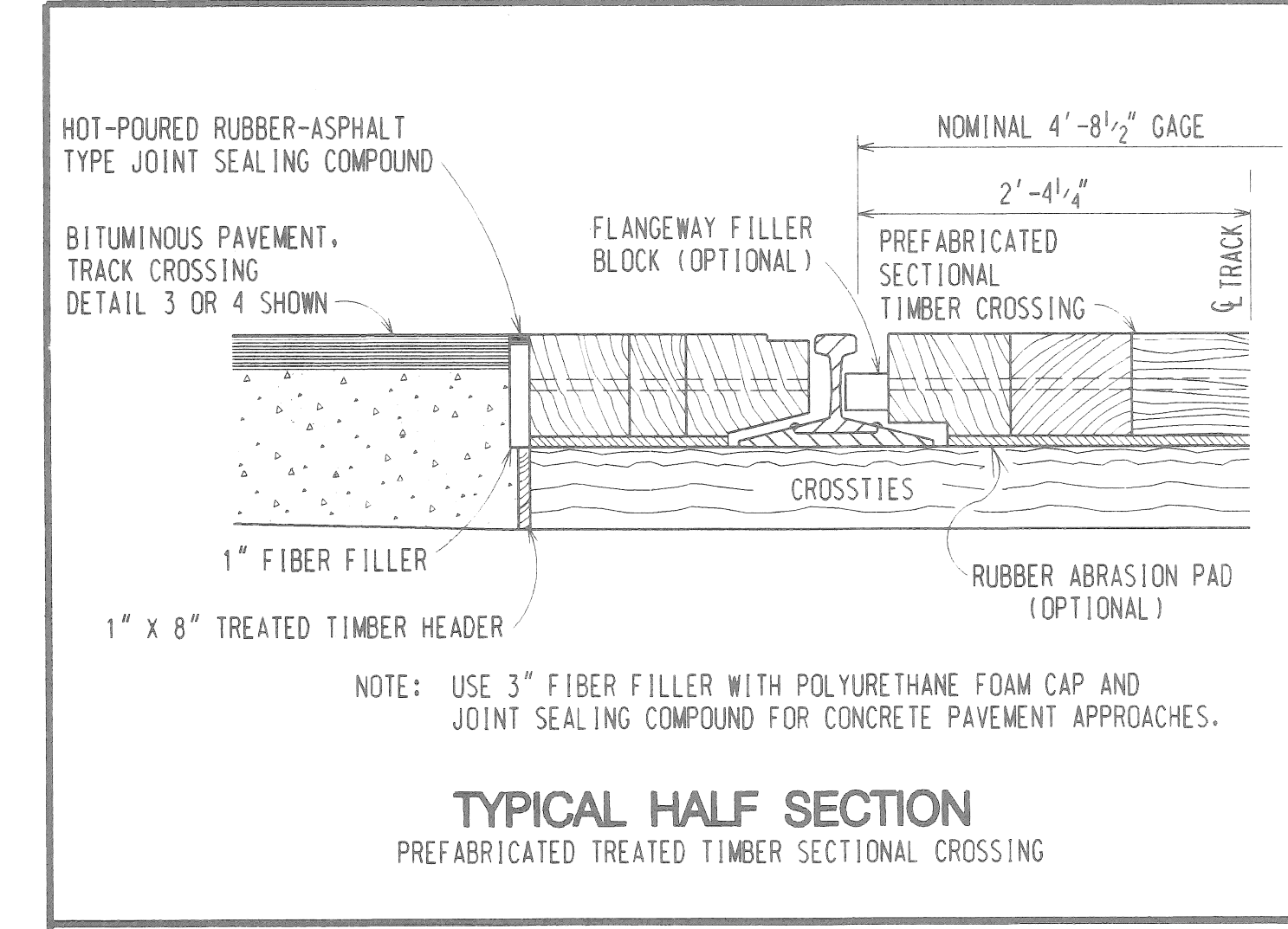
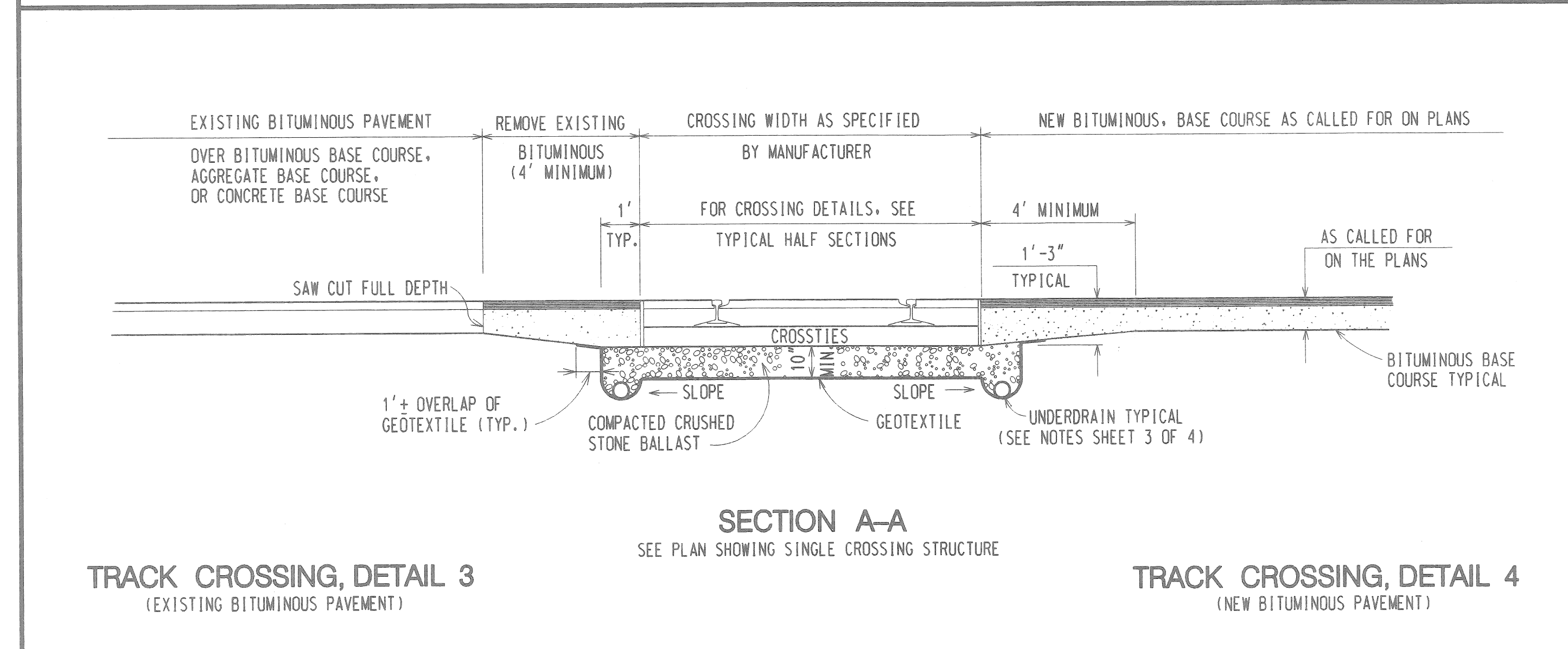
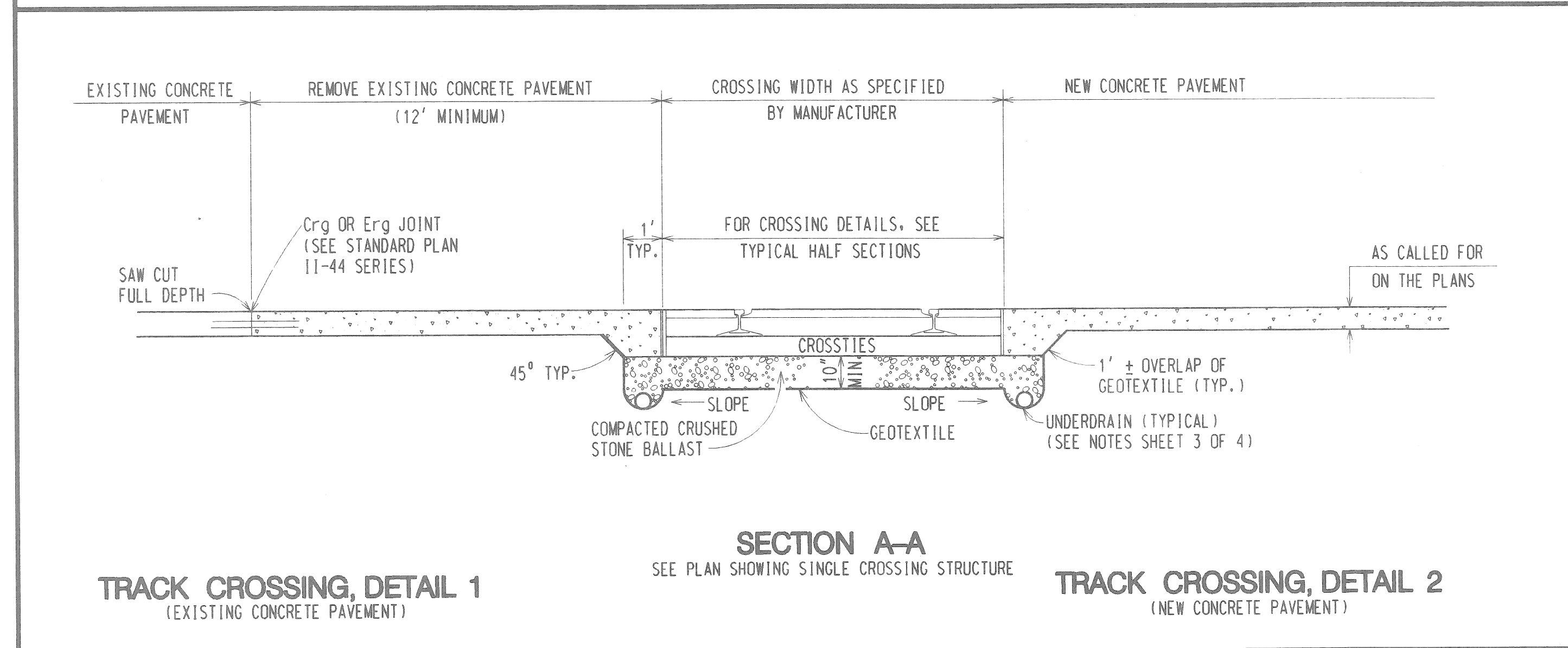
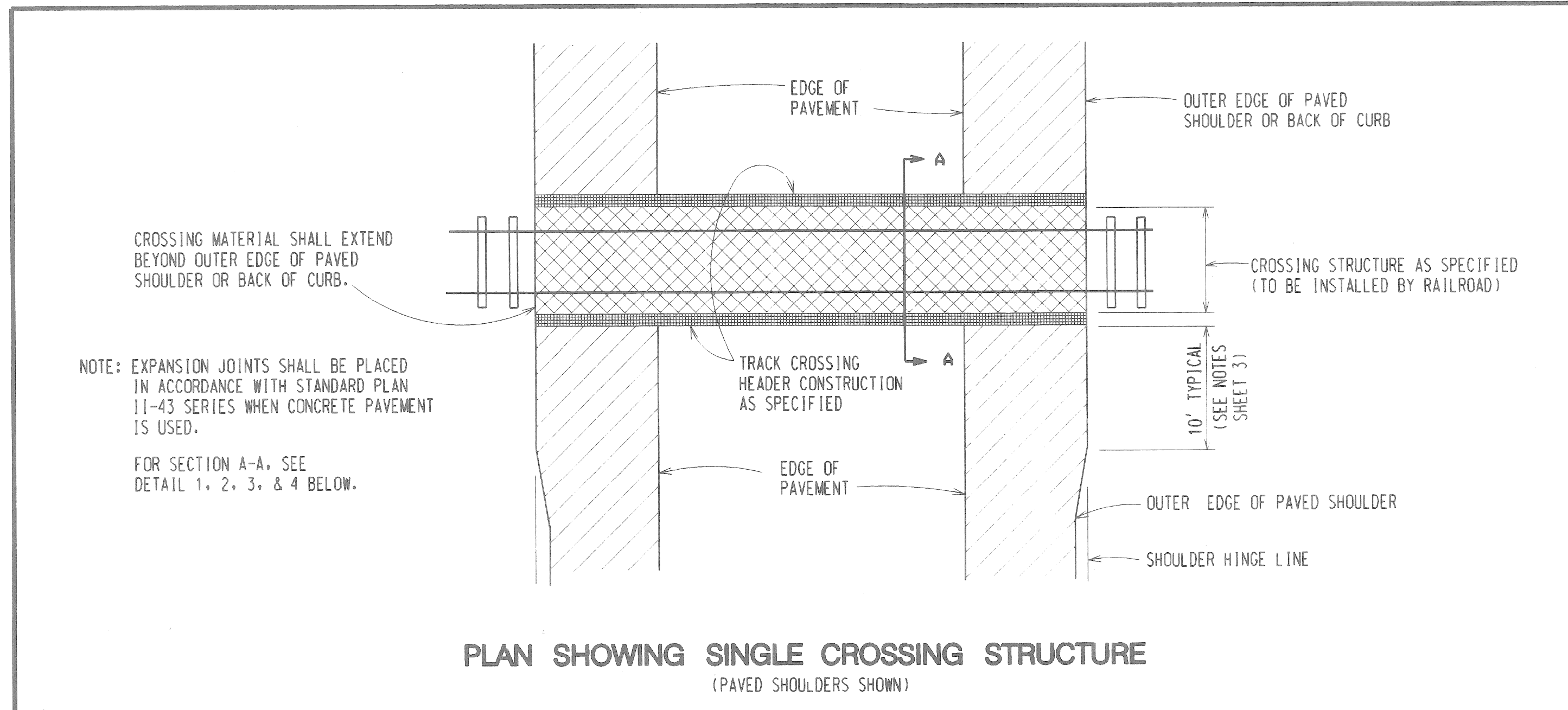
NOTES:

- THE TEMPORARY CONCRETE BARRIER SHOWN ON THIS PLAN CONFORMS TO THE "NEW JERSEY SHAPE".
- THE PLACEMENT OF 2"± DIAMETER HOLES, DRILLED OR PREFORMED, AND LOCATED AS SHOWN ON THIS PLAN MAY BE USED FOR THE PURPOSE OF LIFTING THE BARRIER UNITS. CARE SHALL BE TAKEN NOT TO SEVER THE REINFORCING STEEL WHEN DRILLING.
- PRECAST UNITS MUST BE CAST MONOLITHICALLY, THAT IS, A "COLD JOINT" RESULTING FROM SEPARATE POURING OPERATIONS WILL NOT BE PERMITTED.
- THE END ATTACHMENTS FOR PRECAST BARRIER SECTIONS SHALL MEET THE REQUIREMENTS OF THE CURRENT STANDARD SPECIFICATIONS.
- STIRRUPS MAY BE OMITTED DURING CASTING, IF A SATISFACTORY METHOD OF HOLDING LONGITUDINAL REINFORCING BARS IN POSITION IS USED.
- ALTERNATE METHODS OF REINFORCING THAT MAY BE USED ARE AS FOLLOWS: USE FIVE #5 "A" BARS, GRADE 40 STEEL AND POSITION THE STEEL AS SHOWN ON SHEET #1 OF THESE PLANS; OR USE THREE #6 "A" BARS, GRADE 60 STEEL AND POSITION THE TOP BAR SAME AS THE TOP BAR ON SHEET #1 AND POSITION THE TWO OTHER BARS THE SAME AS THE TWO BOTTOM BARS ON SHEET #1 OF THESE PLANS. THESE THREE #6 BARS MAY ALSO BE PLACED IN VERTICAL ALIGNMENT WITH THE TOP BAR 6" BELOW THE TOP OF THE BARRIER AND THE BOTTOM BAR 4" ABOVE THE BOTTOM OF THE BARRIER; THE MIDDLE BAR SHALL BE CENTERED BETWEEN THESE TWO BARS.
- CAST-IN-PLACE BARRIERS MAY BE USED IN LIEU OF PRECAST UNITS. THE UNITS SHALL BE REINFORCED AS SHOWN ON THESE PLANS. THE UNITS MAY BE CUT FOR MOVING TO ANOTHER LOCATION PROVIDED THE CUT ENDS ARE VERTICALLY STRAIGHT AND SMOOTH ENOUGH TO ALLOW INSTALLATION OF 1/2" DIAMETER GALVANIZED WIRE CENTERED STEEL CABLE. THE RETROFIT END ATTACHMENTS SHALL BE AS SHOWN ON SHEET 2 OF 6 OF THESE PLANS AND IN ACCORDANCE WITH THE CURRENT STANDARD SPECIFICATIONS.
- THREE BEAM EXPANSION ELEMENT, THREE BEAM TRANSITION ELEMENT, SPECIAL END SHOE, THREE BEAM TERMINAL CONNECTOR, BEAM ELEMENT SPLICES, USED TO CONNECT TEMPORARY BARRIER TO EXISTING CONCRETE BARRIER SHALL BE IN ACCORDANCE WITH CURRENT STANDARD PLANS III-60 SERIES AND III-67 SERIES. GUARDRAIL ELEMENTS, BOLTS, WASHERS, ETC., SHALL BE GALVANIZED.
- THE CONCRETE CONNECTING BLOCK AND WOOD BLOCKS ARE DESIGNED TO CONNECT TO THE LEFT SIDE OF "NEW JERSEY SHAPE" CONCRETE MEDIAN BARRIER. THE SHAPE OF THE CONCRETE CONNECTING BLOCK AND THE BEVELS ON THE WOOD BLOCKS MUST BE REVERSED TO FIT AGAINST BARRIER WALLS ON THE RIGHT. THE DESIGN OF THE WOOD BLOCKS MUST BE ALTERED WHEN CONNECTING TEMPORARY CONCRETE BARRIER HAVING ANOTHER SHAPE OR WHEN THE SKEW OF THE CONNECTIONS IS OTHER THAN 1 ON 8.
- ALTERNATE DESIGNS FOR 2'-8" CONNECTING PINS ("J" OR "L" SHAPE) WITH 6" MINIMUM LATERAL OFFSET MAY BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
- ANY ALTERATIONS TO THE DESIGN OF THE UNITS NOT SPECIFICALLY CALLED FOR ON THE PLANS ARE SUBJECT TO THE FINAL APPROVAL OF THE ENGINEER OF CONSTRUCTION.

 PREPARED BY DESIGN DIVISION DRAWN BY: D.F.M. CHECKED BY: R.S.C.	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b> II-52F SHEET 5 OF 6
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN	
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK	
	ENGINEER OF TRAFFIC AND SAFETY	DEPUTY DIRECTOR-HIGHWAYS	
	F.H.W.A. APPROVAL	9-26-91 PLAN DATE	

 PREPARED BY DESIGN DIVISION DRAWN BY: D.F.M. CHECKED BY: R.S.C.	ENGINEER OF CONSTRUCTION	ENGINEER-ROAD DESIGN	MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TEMPORARY CONCRETE BARRIER</b> II-52F SHEET 6 OF 6
	ENGINEER OF MAINTENANCE	ENGINEER OF DESIGN	
	ENGINEER OF MATERIALS & TECHNOLOGY	DEPARTMENT DIRECTOR PATRICK NOWAK	
	ENGINEER OF TRAFFIC AND SAFETY	DEPUTY DIRECTOR-HIGHWAYS	
	F.H.W.A. APPROVAL	9-26-91 PLAN DATE	

Sheet 12 of 12

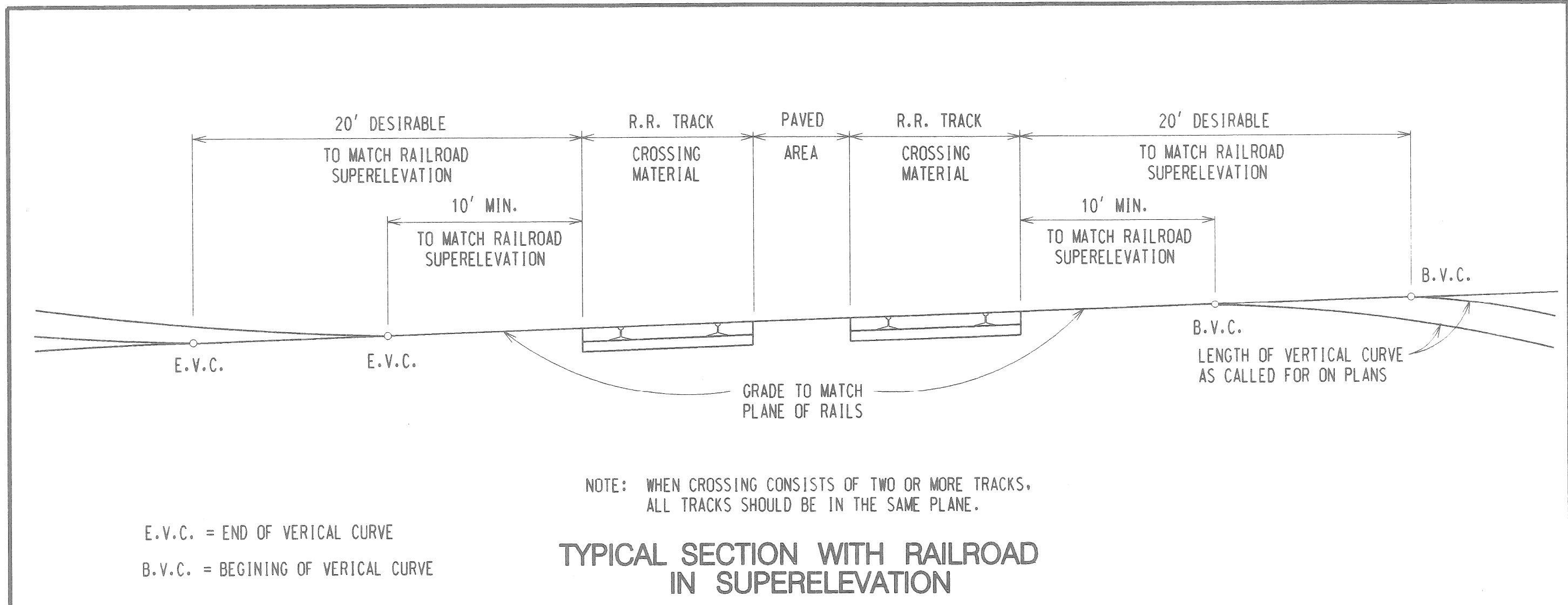


		<b>MICHIGAN DEPARTMENT OF TRANSPORTATION</b> BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TRACK CROSSING</b>	
RAILROAD COORDINATION ENGINEER <i>Samuel White</i>	ENGINEER OF CONSTRUCTION 	ENGINEER - ROAD DESIGN 	
ENGINEER OF MAINTENANCE 	ENGINEER OF DESIGN DEPARTMENT DIRECTOR <b>PATRICK M. NOWAK</b>		
DRAWN BY: <b>D.R.S.</b> CHECKED BY: <b>J.R.G.</b>	BY: <b>J.R.G.</b> DEPUTY DIRECTOR - HIGHWAYS	F.H.W.A. APPROVAL 	07/21/92 PLAN DATE <b>VI-121D</b> SHEET 1 OF 4

		<b>MICHIGAN DEPARTMENT OF TRANSPORTATION</b> BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TRACK CROSSINGS</b>	
RAILROAD COORDINATION ENGINEER <i>Samuel White</i>	ENGINEER OF CONSTRUCTION 	ENGINEER - ROAD DESIGN 	
ENGINEER OF MAINTENANCE 	ENGINEER OF DESIGN DEPARTMENT DIRECTOR <b>PATRICK M. NOWAK</b>		
DRAWN BY: <b>D.R.S.</b> CHECKED BY: <b>J.R.G.</b>	BY: <b>J.R.G.</b> DEPUTY DIRECTOR - HIGHWAYS	F.H.W.A. APPROVAL 	07/21/92 PLAN DATE <b>VI-121D</b> SHEET 2 OF 4

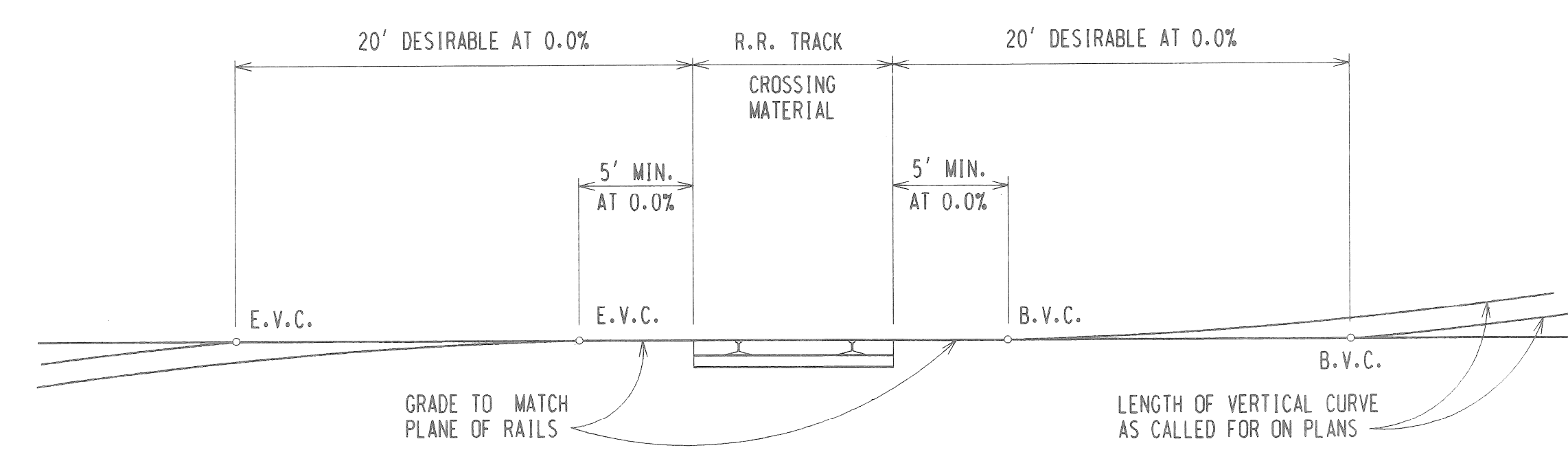
82072-32068A Sht. 12

Sht. 13  
32068A



**TYPICAL SECTION WITH RAILROAD IN SUPERELEVATION**

E.V.C. = END OF VERTICAL CURVE  
B.V.C. = BEGINNING OF VERTICAL CURVE



**TYPICAL SECTION WITH RAILROAD ON LEVEL PLANE**

**NOTES:**

THE RAILROAD COMPANY WILL FURNISH AND INSTALL THE CROSSING STRUCTURE INCLUDING UNDERDRAIN, GEOTEXTILE MATERIAL, BALLAST, WOOD TIES, RAILS, CROSSING SURFACE, AND HEADERS.

THE EDGES OF THE PAVEMENT GUTTERS, AND THE CROWN OF EITHER SIDE OF THE CROSSING, SHALL BE SMOOTHLY TRANSITIONED TO MEET THE PROPOSED GRADE OF THE RAILROAD TRACK. THE CROSSING SHOULD BE INSTALLED APPROXIMATELY 1/2" ABOVE THE PROPOSED PLAN GRADE TO ALLOW FOR SETTLEMENT AT ACTIVE MAIN LINE TRACKS. TEMPORARY BITUMINOUS WEDGING MAY BE REQUIRED.

THE HEIGHT OF ANY CURB ADJACENT TO THE RAILROAD TRACKS SHALL BE REDUCED TO ONE INCH AT A POINT 8'-6" FROM THE CENTERLINE OF THE TRACK, NORMAL TO THE TRACK, BY STANDARD TRANSITIONS SHOWN ON STANDARD PLAN II-30 SERIES, UNLESS OTHERWISE SHOWN ON THE PLANS.

UNDERDRAINS WRAPPED IN GEOTEXTILE SHALL BE PLACED WHERE DRAINAGE IS NEEDED AND A POSITIVE OUTLET CAN BE PROVIDED.

PAVEMENT HEADERS FORMED BY THICKENING THE ENDS OF THE CONCRETE PAVEMENT OR THE BASE COURSE FOR THE BITUMINOUS OVERLAY WILL BE INCLUDED IN THE ITEMS OF CONCRETE PAVEMENT, CONCRETE BASE COURSE, BITUMINOUS BASE COURSE, OR AGGREGATE BASE COURSE-BITUMINOUS.

EXPANSION JOINTS AT THE RAILROAD CROSSINGS SHALL BE CONSTRUCTED AS SHOWN ON THIS PLAN. THE ADDITIONAL JOINTS IN THE PAVEMENT SHALL BE LOCATED AS SHOWN ON STANDARD PLAN II-43 SERIES AND CONSTRUCTED IN ACCORDANCE WITH STANDARD PLAN II-39 SERIES.

PAVEMENTS ILLUSTRATED ON THIS PLAN ARE TYPICAL TREATMENTS ONLY, AND THE TYPE OF PAVEMENT USED WILL BE AS CALLED FOR ON THE PLANS.

THE APPROACHES TO RAILROAD CROSSINGS SHOULD PROVIDE A SMOOTH TRANSITION FOR VEHICULAR TRAFFIC. THE APPROACH ADJACENT TO THE CROSSING SHOULD MATCH THE PLANE OF THE RAILS AND HAVE LIBERAL VERTICAL CURVES TO THE APPROACH GRADE. THE APPROACH GRADE SHOULD BLEND INTO THE EXISTING PAVEMENT TO ELIMINATE UNNECESSARY UNDULATION IN THE VERTICAL PROFILE.

THE FULL WIDTH OF SHOULDERS SHOULD BE PAVED WITH BITUMINOUS AT THE CROSSING TO MEET THE CROSSING SURFACE MATERIAL. SHORT TAPERS WILL EXTEND BACK TO THE PAVED PORTION OF THE SHOULDER.

WHEN A RAILROAD CROSSING STRUCTURE IS RAISED, THE PAVEMENT TAPER TO MEET THE RAISED CROSSING STRUCTURE SHOULD BE 3/4" IN 25', 1 1/2" IN 50', 2 1/2" IN 75', OR AS DETERMINED BY THE ENGINEER. THE PROPOSED TRANSITION LENGTH SHOULD BE SHOWN ON THE PLANS. THE EXISTING PAVEMENT SHOULD BE REMOVED OR MILLED TO PROVIDE A STRAIGHT AND VERTICAL BUTT JOINT.

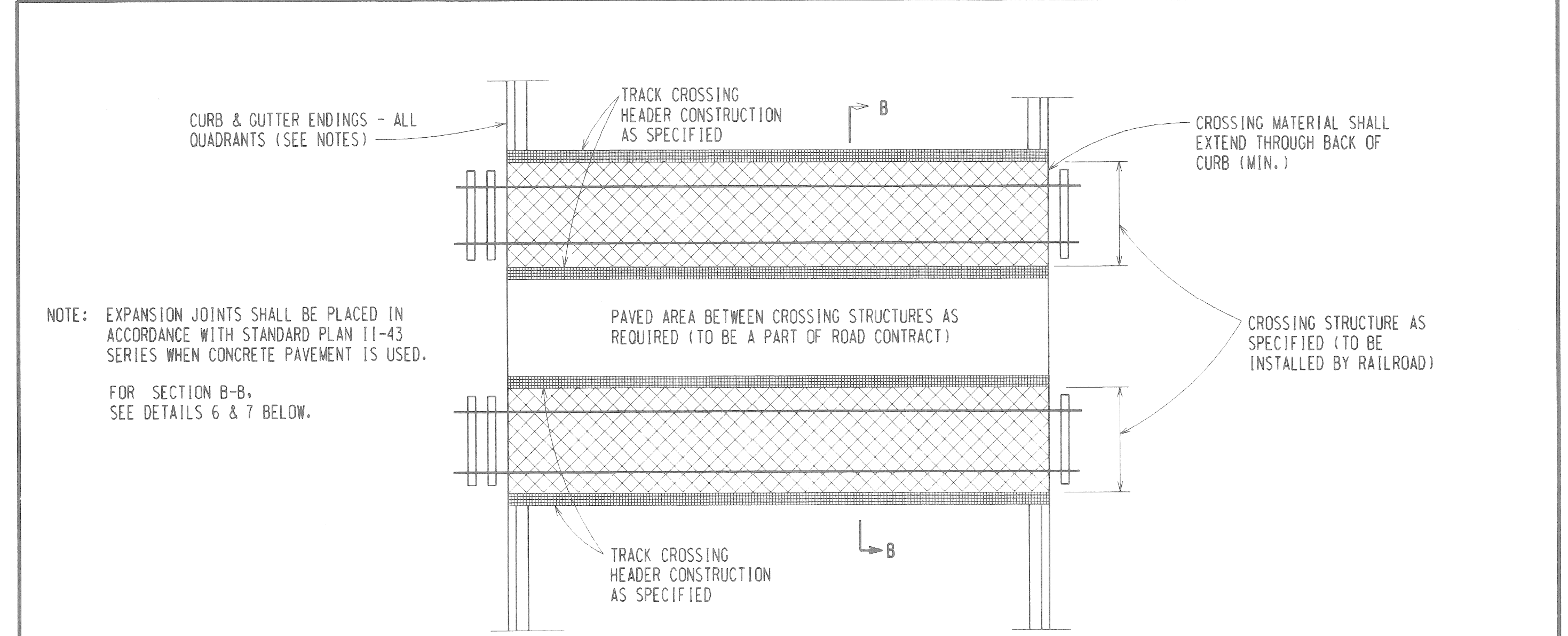
TREATED TIMBER AND/OR RUBBER HEADER BOARDS WILL BE USED WHEN SPECIFIED AND SHALL BE INSTALLED BY THE RAILROAD COMPANY.

IF COLD PATCH MATERIAL OR GRAVEL IS USED AS TEMPORARY FILL IN THE GAP BETWEEN THE CROSSING AND THE PAVEMENT, IT SHALL BE REMOVED PRIOR TO REPLACEMENT WITH A PLANT MIX. THE BITUMINOUS MATERIAL ADJACENT TO THE CROSSING SHALL BE COMPACTED WITH A ROLLER ACCORDING TO THE STANDARD SPECIFICATIONS.

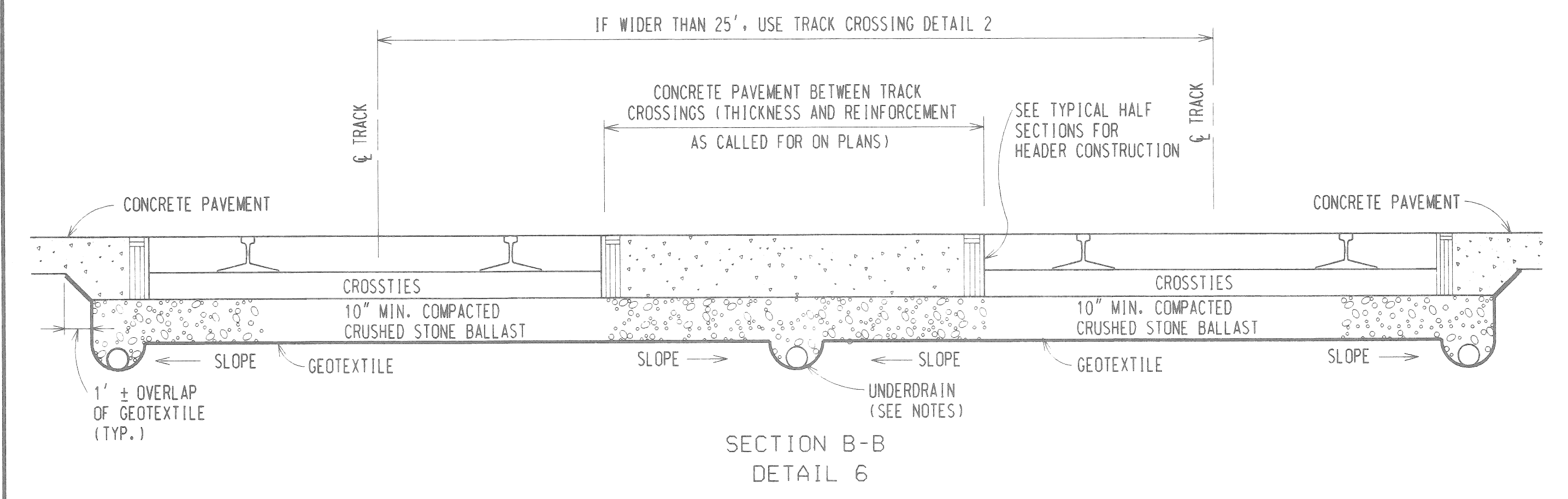
SIDEWALK CROSSINGS WILL NORMALLY BE CONSTRUCTED OF BITUMINOUS, TIMBER, OR RUBBER PADS SIMILAR TO THE CROSSING, EXCEPT THAT THE FIBER FILLER AND TREATED TIMBER HEADER MAY BE OMITTED.

THE OPEN ROADWAY AREA BETWEEN THE SAWED PAVEMENT EDGE AND THE NEWLY INSTALLED CROSSING SHALL BE ROLLER COMPACTED PRIOR TO THE PAVEMENT CONTRACTOR FILLING THIS AREA WITH CONCRETE OR BITUMINOUS.

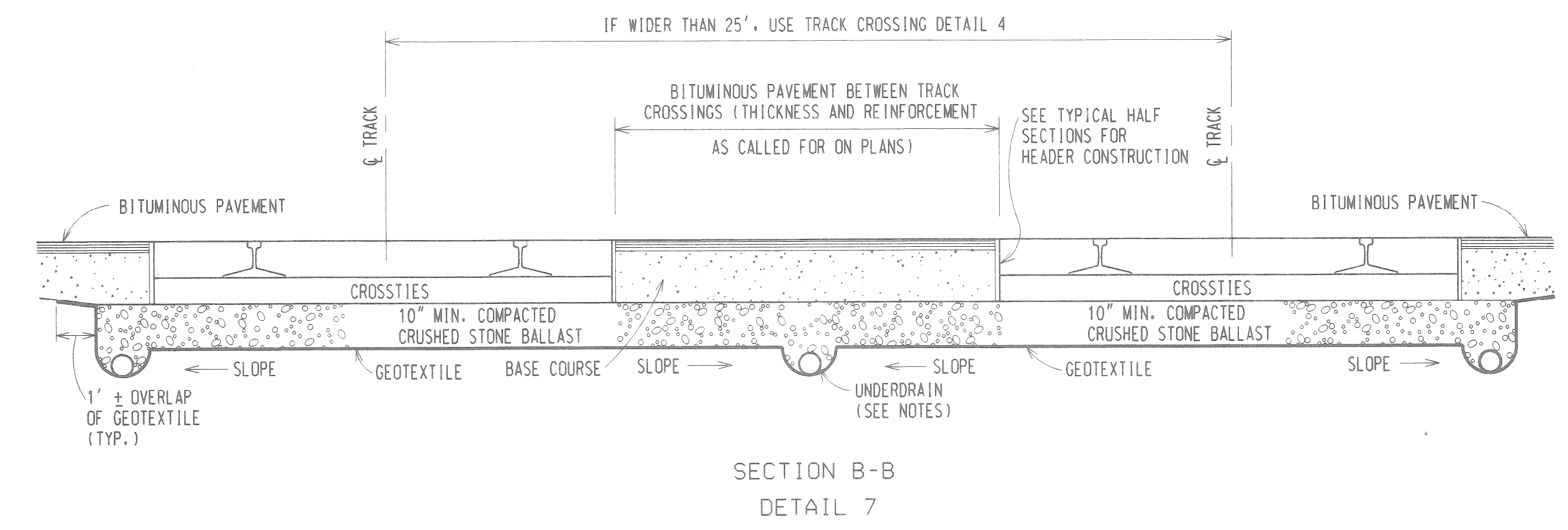
PROPRIETARY CROSSING SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.



**PLAN SHOWING PAVED AREA BETWEEN CROSSING STRUCTURES**



**SECTION B-B DETAIL 6 SHOWING CONCRETE PAVEMENT BETWEEN CROSSING STRUCTURES**



**SECTION B-B DETAIL 7 SHOWING BITUMINOUS PAVEMENT BETWEEN CROSSING STRUCTURES**

 RAILROAD COORDINATION ENGINEER ENGINEER OF CONSTRUCTION ENGINEER - ROAD DESIGN		MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TRACK CROSSINGS</b>	
ENGINEER OF MAINTENANCE ENGINEER OF DESIGN DEPARTMENT DIRECTOR PATRICK M. NOWAK		SHEET 3 OF 4	
DRAWN BY: D.R.S. CHECKED BY: J.R.G.		07/21/92 PLAN DATE VI-121D	
ENGINEER OF MATERIALS & TECHNOLOGY ENGINEER OF TRAFFIC AND SAFETY DEPUTY DIRECTOR - HIGHWAYS		F.H.W.A. APPROVAL	

 RAILROAD COORDINATION ENGINEER ENGINEER OF CONSTRUCTION ENGINEER - ROAD DESIGN		MICHIGAN DEPARTMENT OF TRANSPORTATION BUREAU OF HIGHWAYS STANDARD PLAN FOR <b>TRACK CROSSINGS</b>	
ENGINEER OF MAINTENANCE ENGINEER OF DESIGN DEPARTMENT DIRECTOR PATRICK M. NOWAK		SHEET 4 OF 4	
DRAWN BY: D.R.S. CHECKED BY: J.R.G.		07/21/92 PLAN DATE VI-121D	
ENGINEER OF MATERIALS & TECHNOLOGY ENGINEER OF TRAFFIC AND SAFETY DEPUTY DIRECTOR - HIGHWAYS		F.H.W.A. APPROVAL	

32068A Sht. 13

SH. NO. 14  
 JOB NO. 32068A  
 CONTROL SECTION 82072

032068A		ROAD TOTAL		SUMMARY OF QUANTITIES																
ITEM DESCRIPTION	ITEM CODE	UNIT	PROJECT TOTAL	JOB SUBTOTAL	SHEET 003	SHEET 004														
REMOVING PAVEMENT	2070003	SYD	85	85		85														
REMOVING CURB	2070003	LFT	4	4	4															
REMOVING SIDEWALK	2070006	SYD	35	35		35														
REMOVING BEAM GUARDRAIL	2070009	LFT	105	105		105														
REMOVING TRACK ENGAGEMENT	2070011	CYD	7	7		7														
REMOVING PAVEMENT, SPECIAL	2077003	CYD	29	29		29														
EARTH EXCAVATION	2080001	CYD	155	155		155														
SUBGRADE UNDERCUTTING TYPE II	2080031	CYD	10	10	10															
SUBBASE (CIP)	2110002	CYD	56	56		56														
DRUM AND CUTTER INLET FILTER	2137002	EACH	1	1		1														
COLD-MILLING BITUMINOUS SURFACE	4000004	TON	156	156		156														
BITUMINOUS MIXTURE - 11A	4000041	TON	213	213		213														
BITUMINOUS APPROACH	4000120	TON	177	177		177														
TEMPORARY BITUMINOUS	4037001	TON	16	16	16															
PLACING AND COMPACTING BALLAST (CIP)	4037003	CYD	100	100		100														
EXPANSION JOINT F3 - MODIFIED	4507277	LFT	202	202		202														
SEWER TAP, 6"	5130621	EACH	1	1		1														
CLEANING DRAINAGE STRUCTURE	5140014	EACH	1	1	1															
CLEANING DRAINAGE STRUCTURE LEADS	5140015	LFT	75	75	75															
OPEN-GRADED UNDERDRAIN PIPE, 6"	6020061	LFT	105	105		105														
SUBGRADE UNDERDRAIN, 4"	6020101	LFT	150	150	150															
UNDERDRAIN OUTLET, 6"	6020137	LFT	6	6		6														
CONCRETE CURB, DETAIL F2	6030002	LFT	43	43		43														
CONCRETE SIDEWALK, 4"	6110002	SFT	430	430		430														
MOBILIZATION	6230001	LSUM	1	1	1															
FAST-CURE POLYESTER PAVEMENT MARKING, 4", WHITE	6290138	LFT	1500	1500	1500	1500														
FAST-CURE POLYESTER PAVEMENT MARKING, 4", YELLOW	6290159	LFT	2500	2500	2500	2500														
LIGHTED ARROW, TYPE A - FURNISHED	6310011	EACH	6	6	6	6														
LIGHTED ARROW, TYPE A - OPERATED	6310012	EACH	6	6	6	6														
BARRICADE, TYPE II, LIGHTED - FURNISHED	6310026	EACH	150	150	150	150														
<b>SIGN, TYPE A TEMPORARY-OPERATED</b>	<b>6310061</b>	<b>SFT</b>	<b>700</b>	<b>700</b>	<b>700</b>	<b>700</b>														
BARRICADE, TYPE II, LIGHTED - OPERATED	6310027	EACH	150	150	150	150														
BARRICADE, TYPE III, LIGHTED - FURNISHED	6310036	EACH	20	20	20	20														
BARRICADE, TYPE III, LIGHTED - OPERATED	6310037	EACH	20	20	20	20														
TEMPORARY CONCRETE BARRIER-FURNISHED	6310040	LFT	90	90	90	90														
MINOR TRAFFIC DEVICES	6310054	LSUM	1	1	1	1														
<b>SIGN, TYPE B TEMPORARY-OPERATED</b>	<b>6310066</b>	<b>SFT</b>	<b>600</b>	<b>600</b>	<b>600</b>	<b>600</b>														
SIGN, TYPE A TEMPORARY-FURNISHED	6310060	SFT	700	700	700	700														
SIGN, TYPE B TEMPORARY-FURNISHED	6310065	SFT	600	600	600	600														
SIGN, TYPE D TEMPORARY-FURNISHED	6310075	SFT	70	70	70	70														
TEMPORARY PAVEMENT MARKING, TYPE R, 4", WHITE	6310085	LFT	10000	10000	10000	10000														
TEMPORARY PAVEMENT MARKING, TYPE R, 4", YELLOW	6310086	LFT	10000	10000	10000	10000														
<b>SIGN, TYPE D TEMPORARY-OPERATED</b>	<b>6310076</b>	<b>SFT</b>	<b>70</b>	<b>70</b>	<b>70</b>	<b>70</b>														
REMOVING PAVEMENT MARKING, LONGITUDINAL	6510139	LFT	4500	4500	4500	4500														
SLOPE RESTORATION	6530036	SYD	115	115		115														
TEMPORARY CONCRETE BARRIER-OPERATED	6310041	LFT	90	90	90	90														
TEMPORARY CONCRETE BARRIER-RELOCATED	6310043	LFT	90	90	90	90														
NON-HAZARDOUS CONTAMINATED MATERIAL HANDLING AND DISPOSAL	2087000	CYD	155	155	155	155														

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