

PROPOSED IMPROVEMENTS COVERED BY THESE PLANS ARE IN ACCORDANCE WITH AASHTO : "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS, 1990".

CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERING DIVISION
 IN CO-OPERATION WITH
MICHIGAN DEPARTMENT OF TRANSPORTATION

ECONOMIC DEVELOPMENT - CATEGORY A

CONTROL SECTION EDA 82522 JOB NO. 31695 A
 EDAX 82522 JOB NO. 40790 A

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PAVING OF ST. JEAN
 FRED TO EDLIE

ITEM NO. _____

CONTRACT FOR G. S. D. S. P. & UTILITIES

LOCAL AUTHORITY APPROVAL
 CITY OF DETROIT
 CITY ENGINEERING DIVISION
 DEPARTMENT OF PUBLIC WORKS

APPROVED BY: *[Signature]* DATE: *[Date]*

APPROVED BY: *[Signature]* DATE: *[Date]*

PREPARED UNDER SUPERVISION OF
[Signature]
 REGISTERED PROFESSIONAL ENGINEER
 REGISTRATION NO. 35225

CITY OF DETROIT
 ORGANIZATION
 DETROIT, MICHIGAN
 ADDRESS

SEAL:

TRAFFIC VOLUME	1996	2016
ADT - ST. JEAN	972	2580
DESIGN SPEED-----	35MPH	

FHWA REGION	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	SHEET TOTAL
5	MICH				
CITY	COUNTY	TWP	SHEET NO.	SHEET TOTAL	

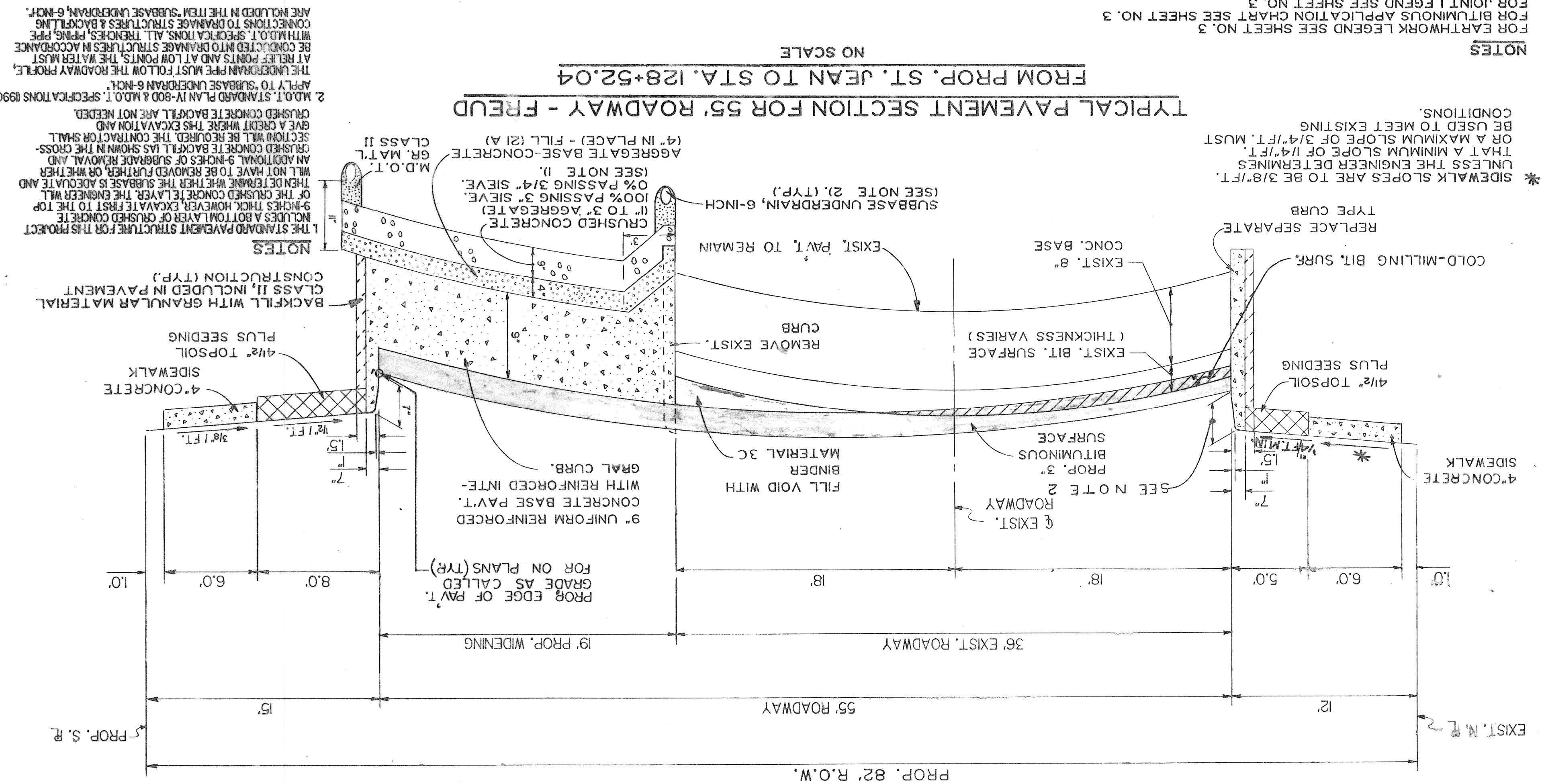
THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN DEPARTMENT OF TRANSPORTATION CURRENT STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS

EDA 82522

JOB NO. 31695A

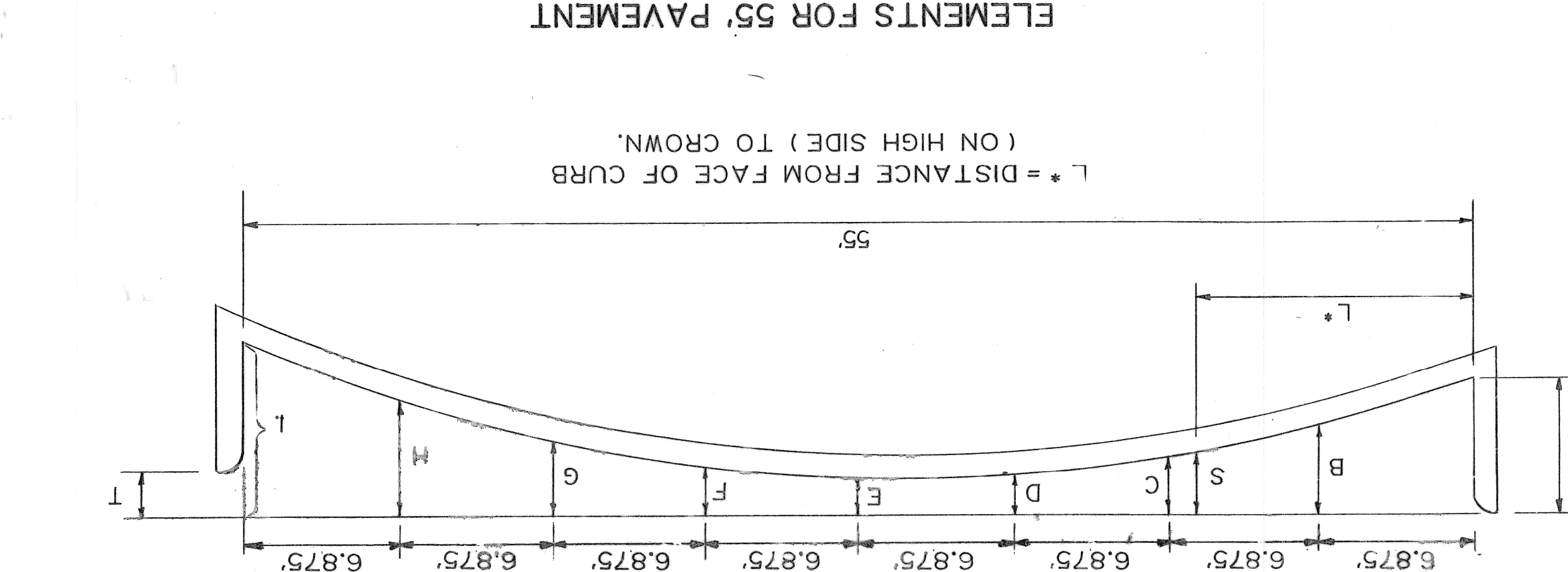
EDAX 82522 JOB NO. 40790

NOTES
 FOR EARTHWORK LEGEND SEE SHEET NO. 3
 FOR BITUMINOUS APPLICATION CHART SEE SHEET NO. 3
 FOR JOINT LEGEND SEE SHEET NO. 3



ELEMENTS FOR 55' PAVEMENT

PAVEMENT WIDTH	CURB FACE	T	A	B	C	D	E	F	G	H	I	L*	S
1.0	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.9	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.8	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.7	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.6	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.5	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.4	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.3	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.2	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.1	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.125	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.173	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.219	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.262	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.303	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.341	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.376	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.408	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.437	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.464	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376
0.488	0.583	0.491	0.465	0.495	0.470	0.415	0.416	0.425	0.390	0.379	0.365	0.341	0.376

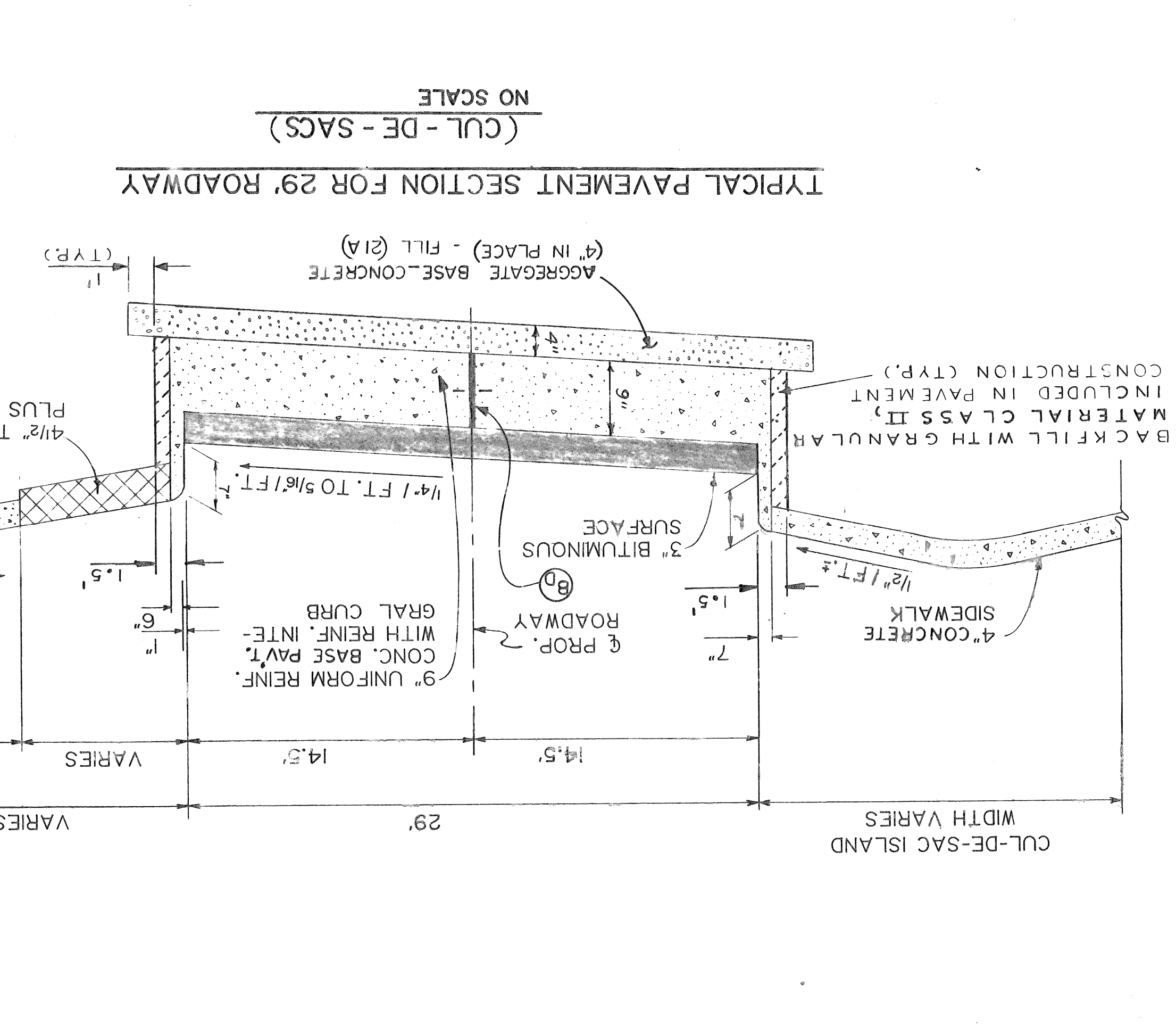
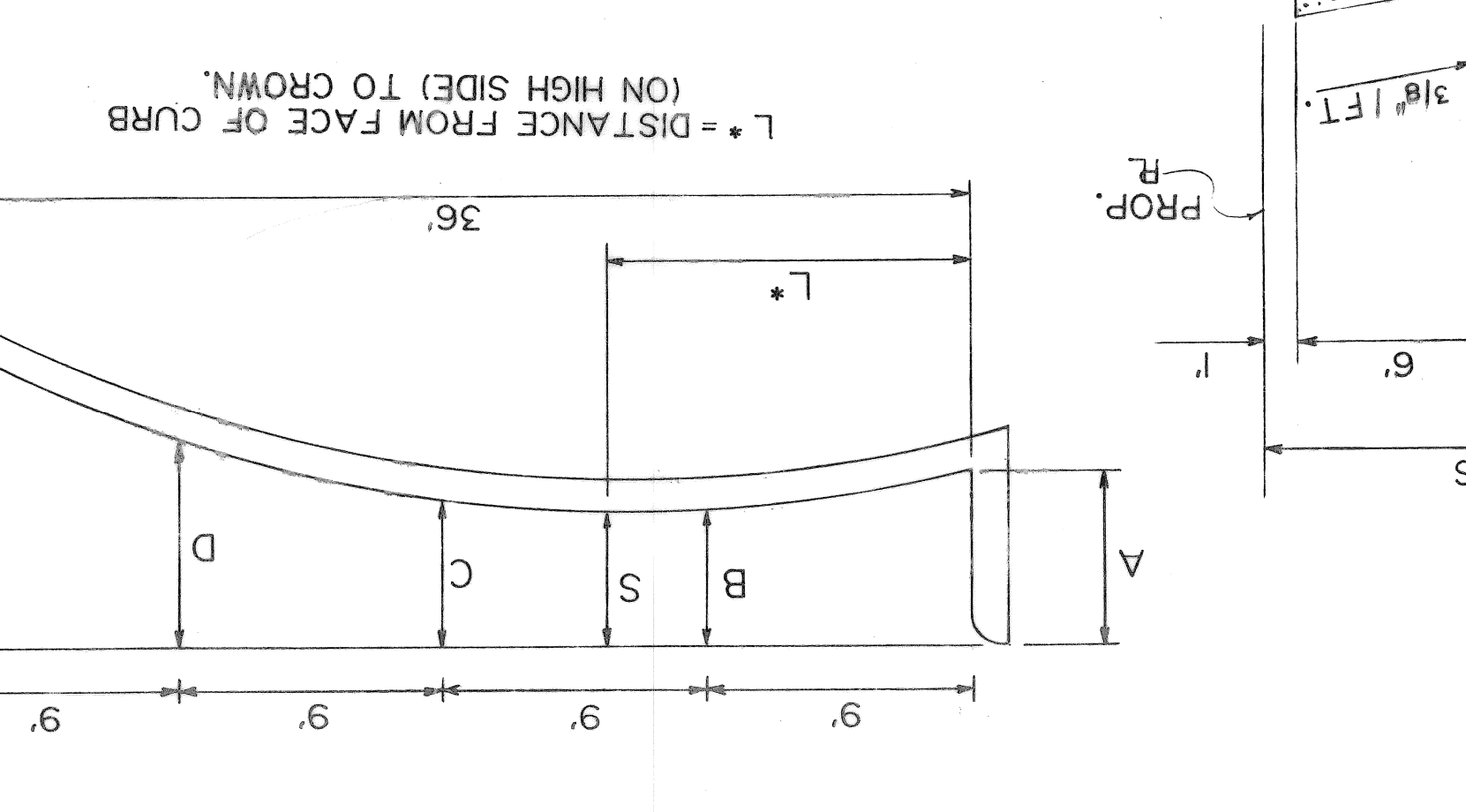
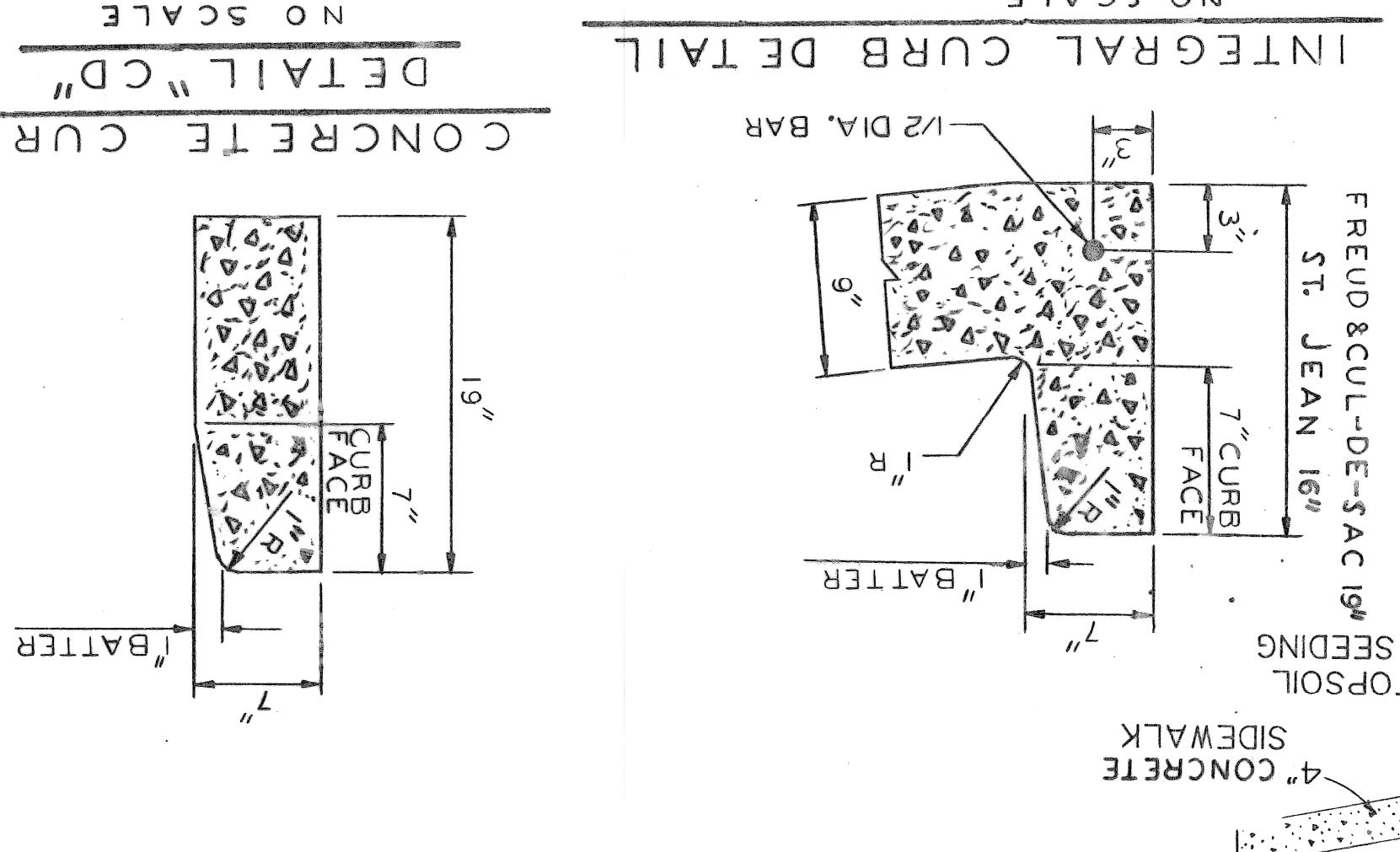


GENERAL NOTES

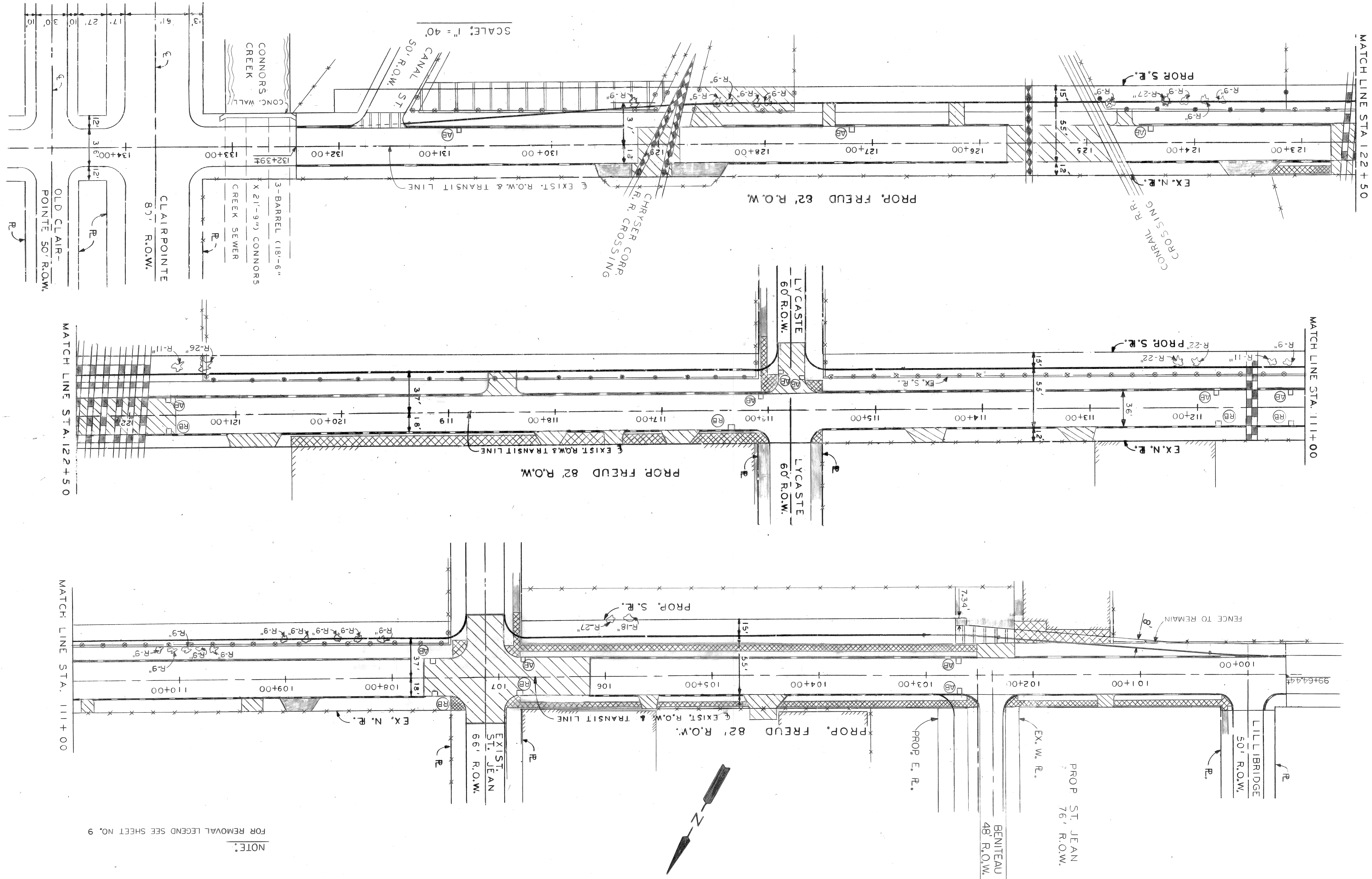
1. THE PROPOSED CURB SHALL BE INTEGRAL CURB (SEE DETAIL THIS SHEET) WITH CONCRETE CURB DETAIL "CD" ON NORTH SIDE OF FRED. REGARDLESS OF CONSTRUCTION METHOD, THE PROPOSED CURB SHALL BE PAID FOR AS "CONC. PAVT. (OR CONC. BASE PAVEMENT) REIN. - 9" UNIFORM WITH INTEGRAL CURB" EXCEPT THAT CONCRETE CURB DETAIL "CD" SHALL BE PAID SEPARATELY.
2. THE CURB FACE HEIGHT SHALL BE 7" IN THE PROPOSED PAVEMENT AREAS AND SHALL NOT BE LESS THAN 4" WHERE RECURFACING IS CALLED FOR ON THE PLANS, OR WHERE IT IS REQUIRED TO MEET EXISTING CONDITIONS AT THE PROPERTY LINE.
3. PLACE 4" CONCRETE SIDEWALK TO FACE OF BUILDING WHERE BUILDING FRONTS ON PROPERTY LINE WHEN SHOWN ON THE PLANS OR WHEN DIRECTED BY THE ENGINEER.
4. BACKFILL, IF REQUIRED, UNDER PROP. SIDEWALKS SHALL BE GRANULAR MATERIAL CLASS II (SIDEWALK CONSTRUCTION) INCLUDED IN PROPOSED PAVEMENT.
5. GRANULAR MATERIAL CLASS II WHICH IS USED FOR BACKFILL OTHER THAN THAT REQUIRED FOR "BASEMENT CLEANOUT" SHALL BE PAID FOR AS "EMBANKMENT - GRANULAR MATERIAL CLASS II".
6. SAW PLANE OF WEAKNESS JOINTS OVER THE EXISTING OR PROPOSED JOINT AS DIRECTED BY THE ENGINEER. THIS SAW PLANE SHALL BE PAID FOR AS "SAWED LONGITUDINAL PLANE OF WEAKNESS JOINT".

ELEMENTS FOR 36' PAVEMENT

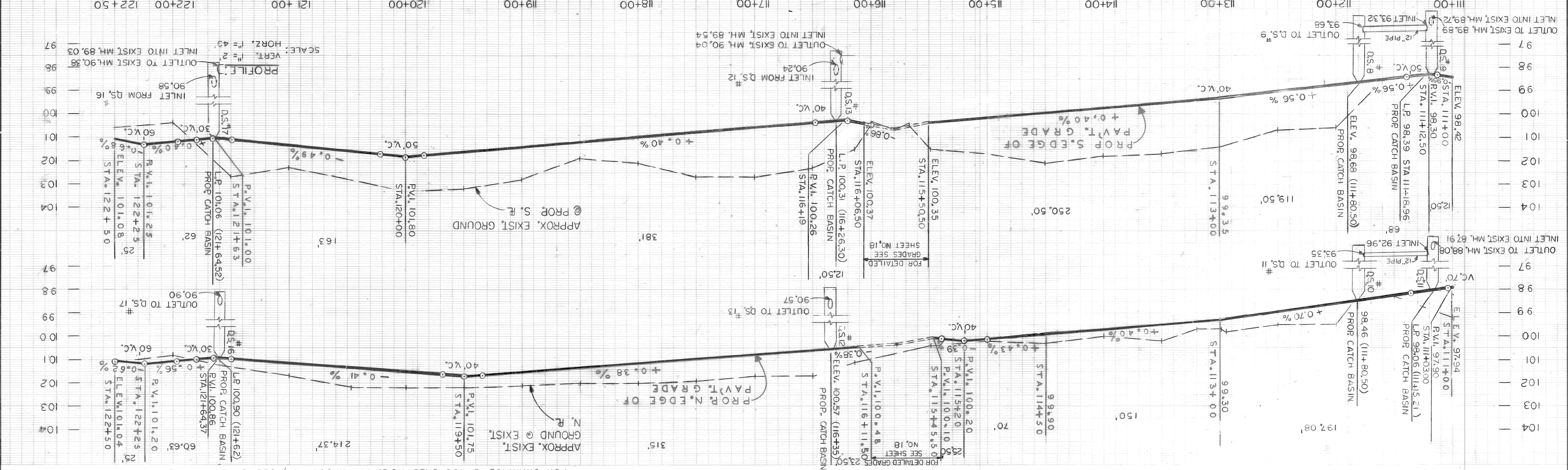
PAVEMENT WIDTH	CURB FACE	T	A	B	C	D	E	L*	S
0.8	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.7	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.6	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.5	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.4	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.3	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.2	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.1	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.208	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.256	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.301	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.343	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.381	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.416	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.448	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.476	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501
0.501	0.583	0.477	0.558	0.427	0.508	0.452	0.583	0.502	0.501



DATE MAR 1996		REVISIONS		DESCRIPTION	
ASSIGNMENT 92-14-02		FINAL		DATE	
CONTRACT NO.		ESTIMATE		BY DATE	
SHEET 8 OF 57 SHEETS		GRADE		BY DATE	
WIDENING OF FREUD FROM ST. JEAN TO CLAIRPONT, PAVING OF ST. JEAN FROM FREUD TO EDLIE, AND MISCELLANEOUS CONSTRUCTION		PLAN		BY DATE	
REMOVALS - FREUD		BY		BY DATE	
BUREAU OF STREETS AND HIGHWAYS		CHECKED BY		BY DATE	
CITY OF DETROIT		APPROVED		BY DATE	
CITY ENGINEERING DIVISION, D.P.W.		ENGINEER OF STREETS		BY DATE	
		NW		N.H.	
		A.P.		E.R.	

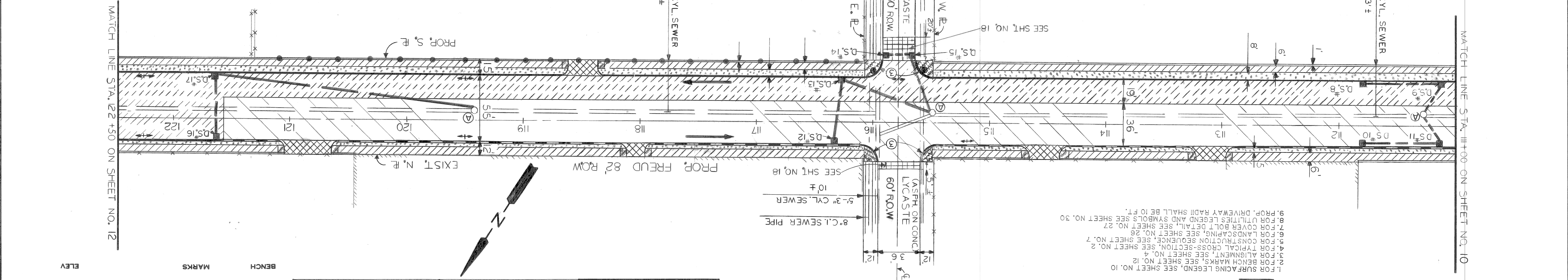


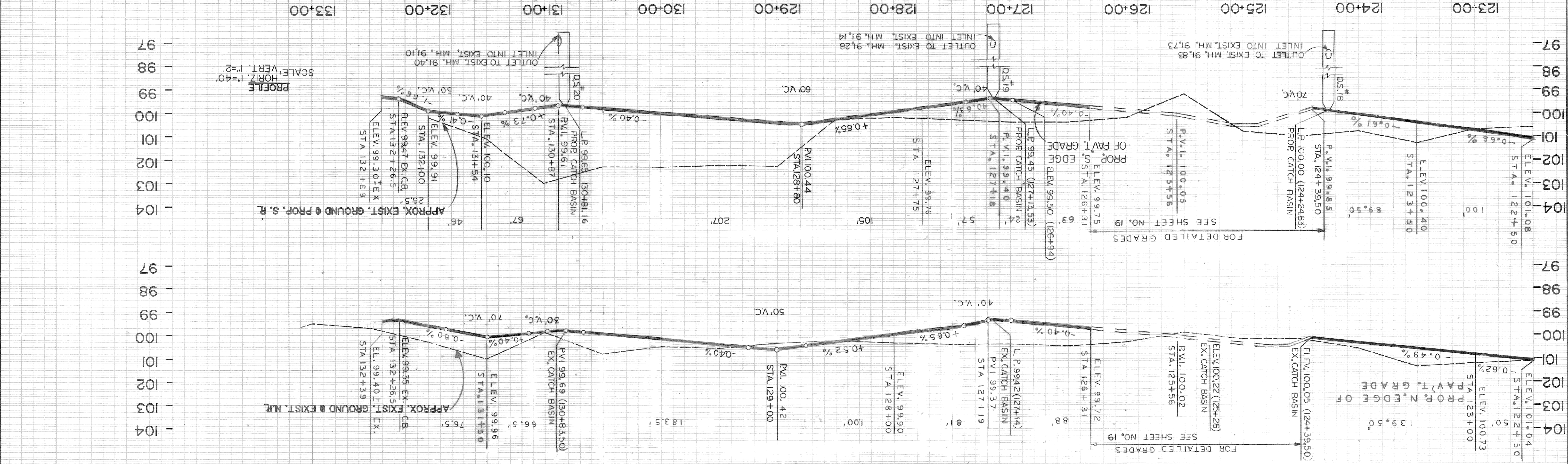
NOTE:
FOR REMOVAL LEGEND SEE SHEET NO. 9



FOR DRAINAGE STRUCTURES NOS. 14 THROUGH 17, SEE SHEET NO. 12.

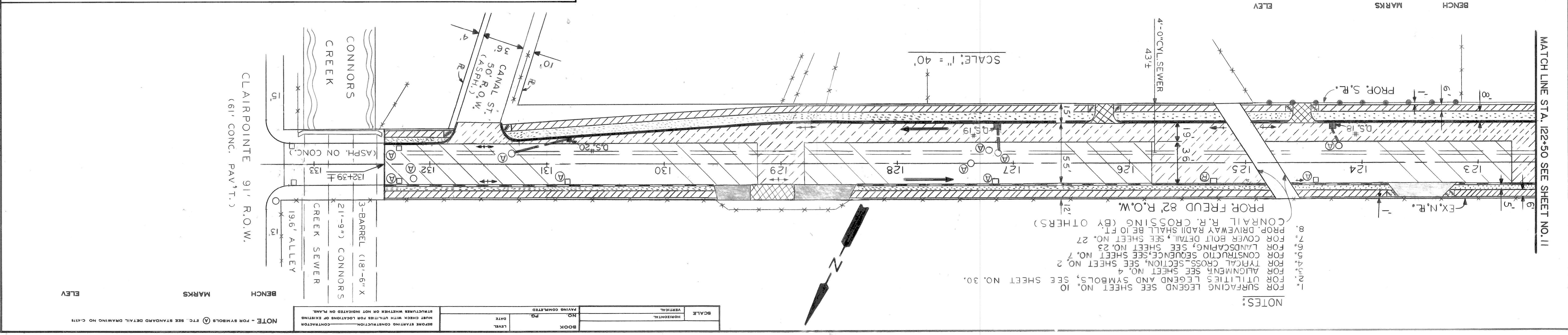
DRAINAGE STRUCTURE NO. (D.S.)	TYPE OF STRUCT.	WITH TRAP	LENGTH (FT.)	SEWER LENGTH & TYPE LEAVING (D.S.)		TAP SIZE (IN.)	TRENCH DETAIL (L.F.)	COMMENTS
				SIZE (IN.)	CLASS (C-7-6)			
13	C.B."B"	T	83	12	III	12	83	
12	C.B."B"	-	55	12	III	12	55	
11	C.B."B"	T	29	12	III	12	29	
10	C.B."A"	-	65	12	III	12	65	
9	C.B."B"	T	28	12	III	12	60	
8	C.B."A"	-	60	12	III	12	24	
7	C.B."B"	T	24	12	III	12	66	
6	C.B."B"	T	66	12	III	12	9	



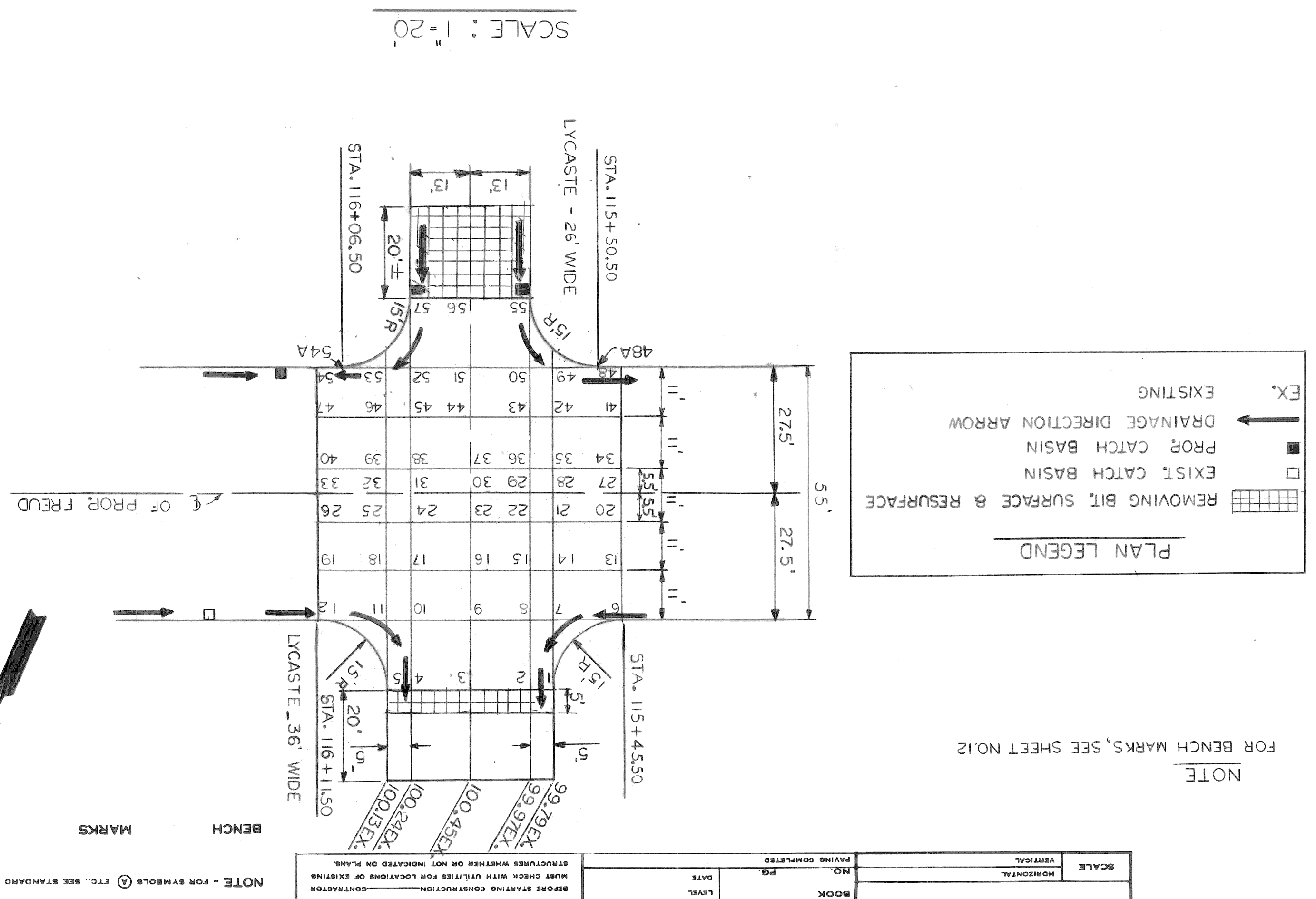
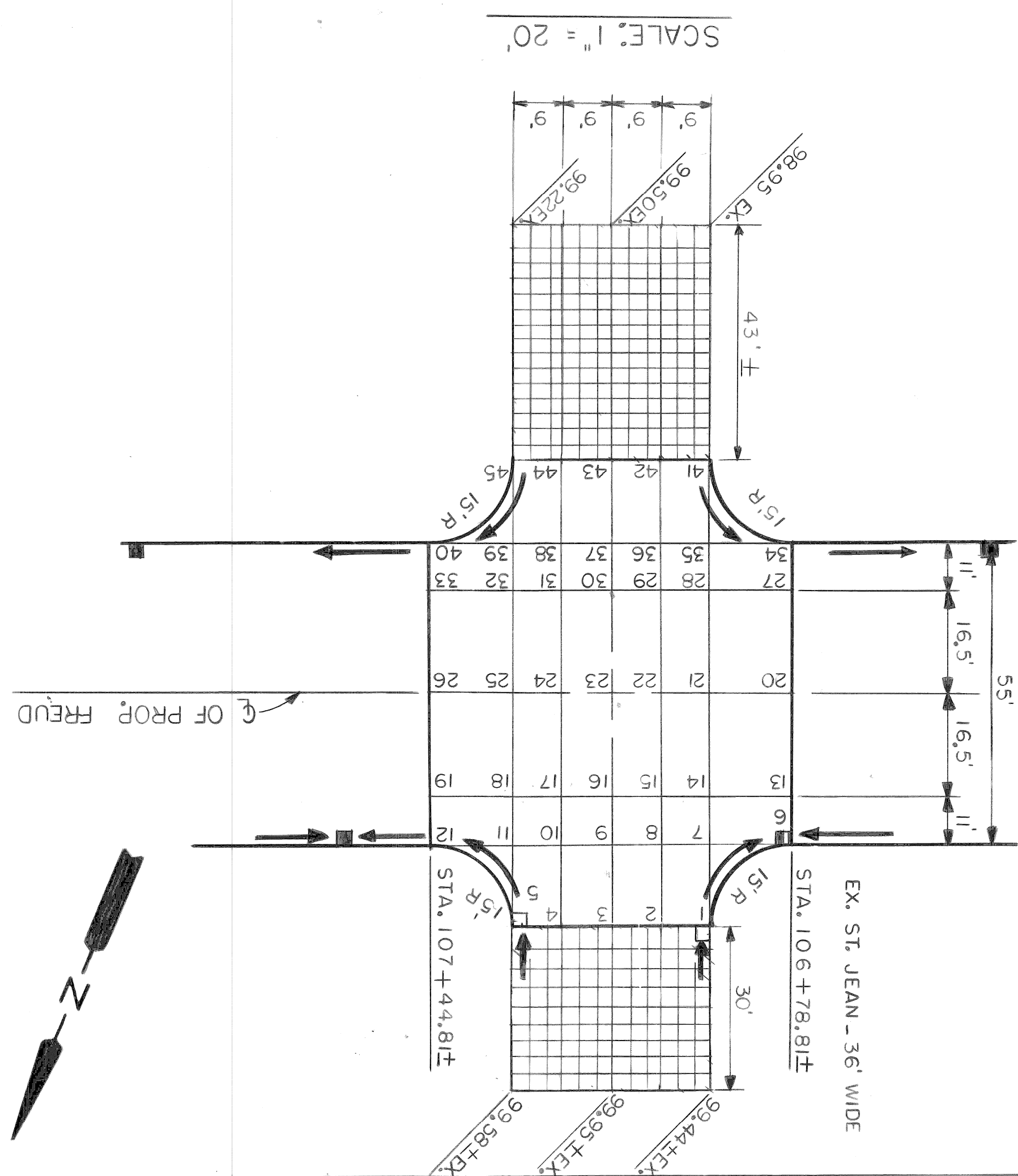
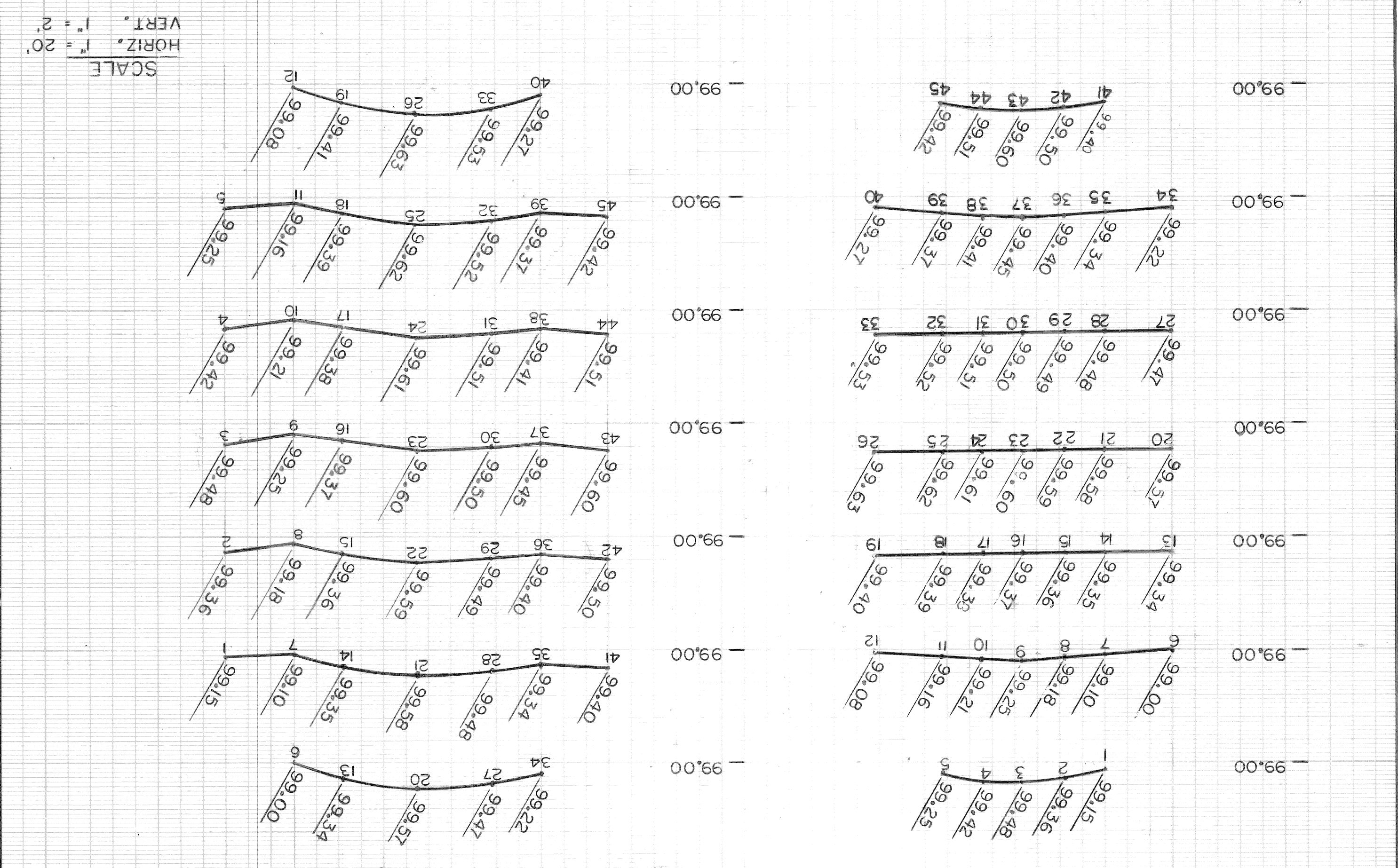


DRAINAGE STRUCTURE TABLE

DRAINAGE STRUCTURE NO. (D.S.)	TYPE	WTH TRAP	SEWER LENGTH (D.S.)			TRENCH DETAIL	COMMENTS
			LENGTH (FT.)	SIZE (IN.)	CLASS (C-76)		
20	C.B.'B	T	50	12	III		
19	C.B.'B	T	23	12	III		
18	C.B.'B	T	17	12	III		
17	C.B.'B	T	225	12	III		
16	C.B.'B	-	53	12	III		
15	C.B.'B	T	50	12	III		
14	C.B.'A	-	24	12	III		



CITY OF DETROIT ENGINEERING DIVISION, D.P.W. BUREAU OF STREETS AND HIGHWAYS		CITY OF DETROIT ENGINEERING DIVISION, D.P.W. BUREAU OF STREETS AND HIGHWAYS		APPROVED BY	CHECKED BY	BY	PLAN	GRADE	ESTIMATE	FINAL	REVISIONS
WIDENING OF FREUD FROM ST. JEAN TO CLAIRPONT, AND MISCELLANEOUS CONSTRUCTION		WIDENING OF FREUD FROM ST. JEAN TO CLAIRPONT, AND MISCELLANEOUS CONSTRUCTION									
DETAILED GRADES FREUD AT ST. JEAN - FREUD AT LYCASTE		DETAILED GRADES FREUD AT ST. JEAN - FREUD AT LYCASTE									
DATE	ASSIGNMENT	INDEXED	CONTRACT NO.	NO. OF SHEETS							
MAR, 1996	NO. 92-14-02			18 OF 57							



PLAN LEGEND

- EX. EXISTING
- ← DRAINAGE DIRECTION ARROW
- PROP. CATCH BASIN
- EXIST. CATCH BASIN
- ▨ REMOVING BIT SURFACE & RESURFACE

NOTE
FOR BENCH MARKS, SEE SHEET NO. 12

BOOK	DATE	SCALE	LEVEL
BEFORE STARTING CONSTRUCTION, CONTRACTOR SHALL CHECK WITH UTILITIES FOR LOCATIONS OF EXISTING STRUCTURES WHETHER OR NOT INDICATED ON PLANS.			
NOTE - FOR SYMBOLS & ETC. SEE STANDARD DETAIL DRAWING NO. C-177E			