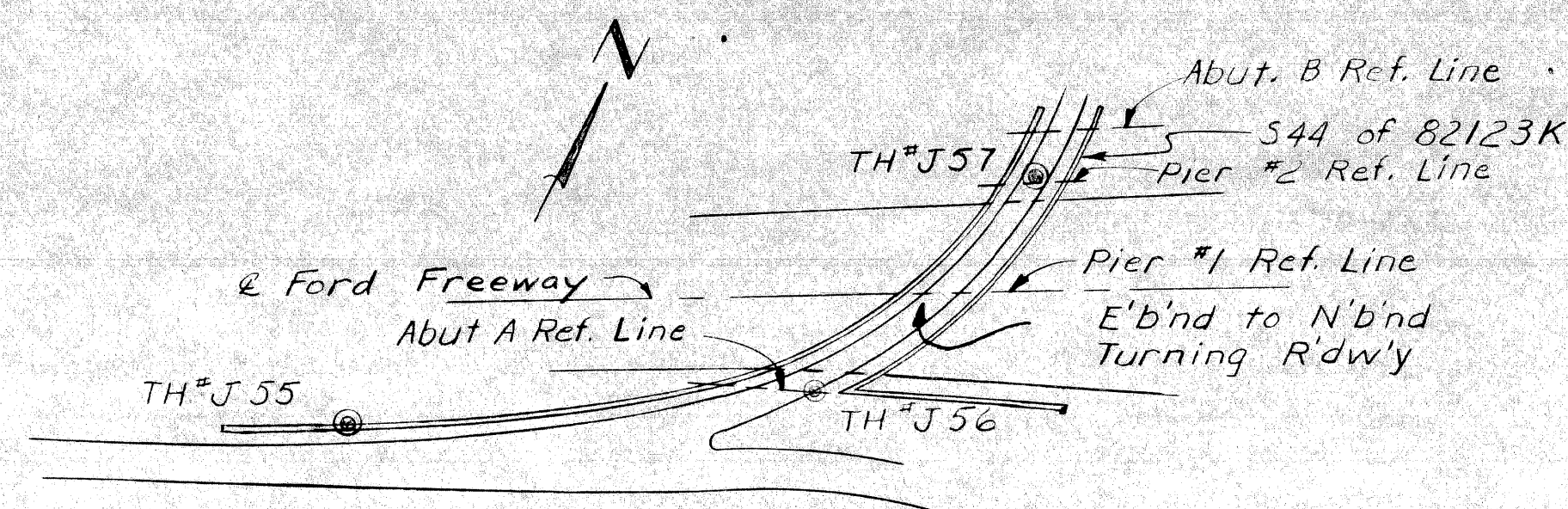
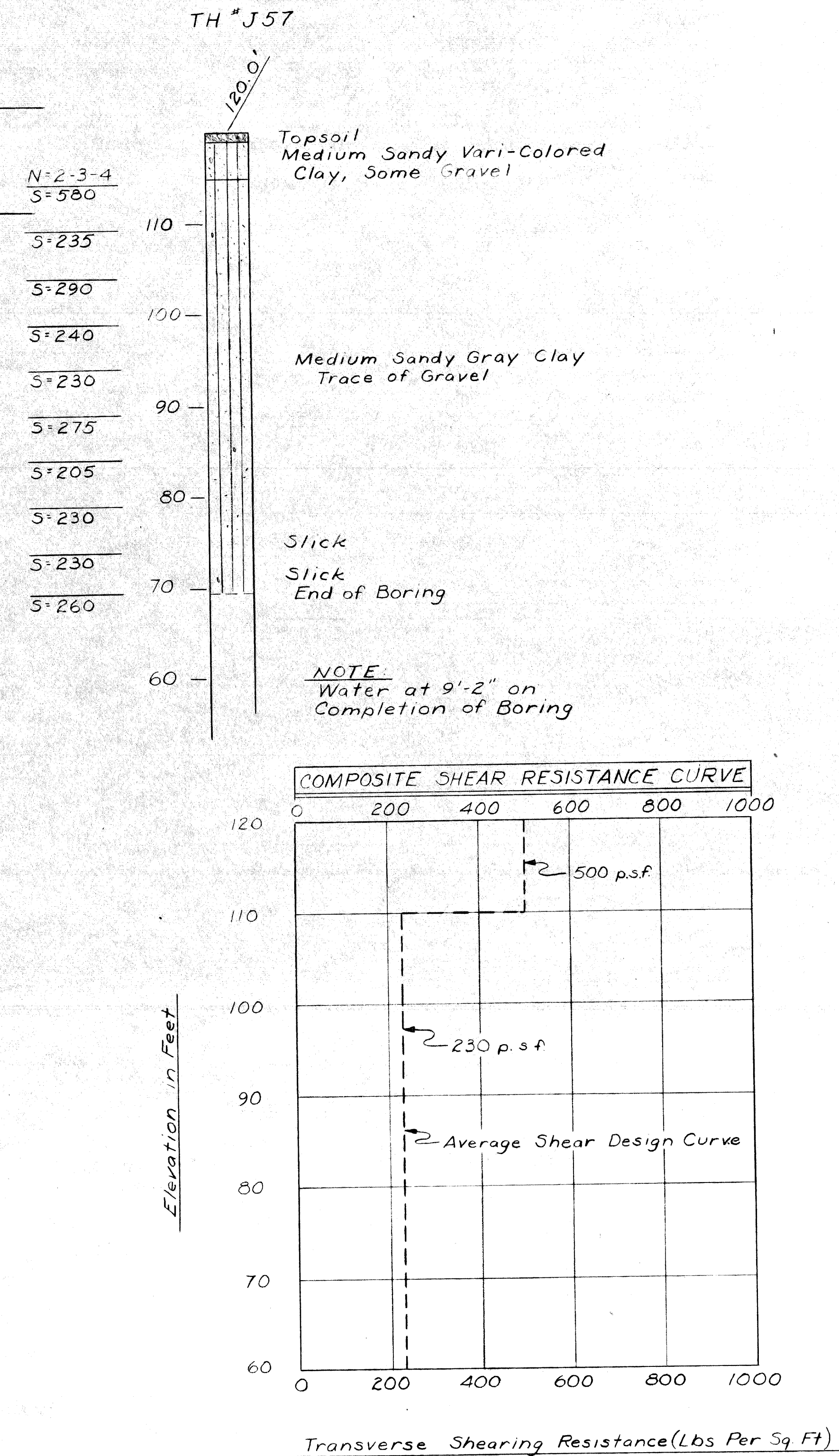
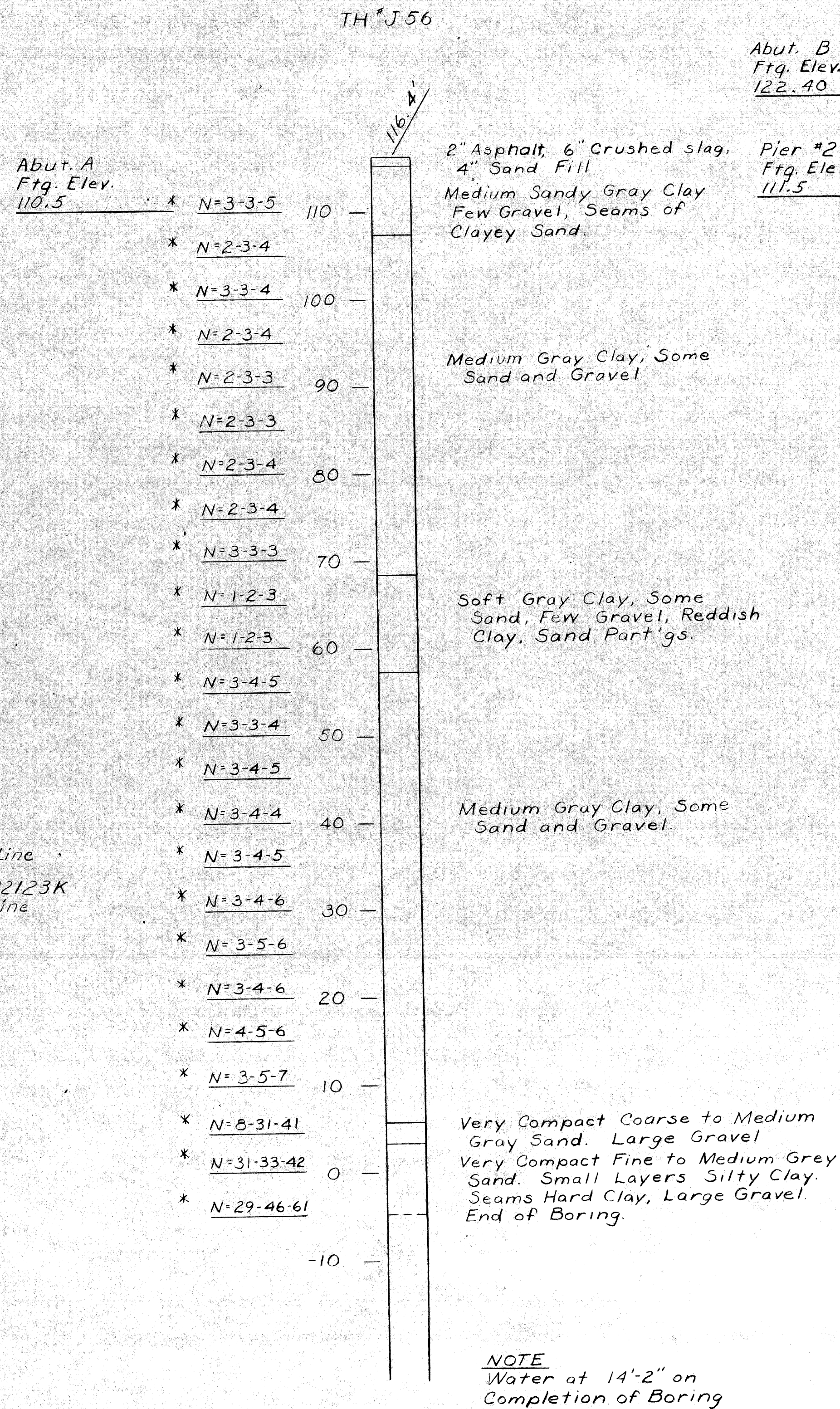


NOTE
Water at 14'-3" on
Completion of Boring



- N Indicates the number of blows required to drive the 2" Sampler 6" (or as noted) using a 140 lb. hammer falling 30". Where blow count is not shown, Sampler was levered, pushed or hand driven.
- S Indicates Transverse Shearing Resistance in lbs. per sq. ft. as determined by M. S. H. D. Standard Test.
- * Indicates no sample taken.

Boring J 56 is from Field report only.
Borings J 55 and J 57 are from Lab reports.
All elevations based on City of Detroit Datum. See
Sheet #3 for list of Construction Bench Marks.



PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

APPROVED *J. J. Conant*
STRUCTURAL ENGINEER

JOB No.
PW990121

NO.	DESCRIPTION	DATE	BY

MICHIGAN DEPARTMENT OF STATE HIGHWAYS

JEFFRIES-FORD INTERCHANGE
EAST BND. TO NORTH BND. TURNING ROADWAY CROSSING
THE EDSSEL FORD FREEWAY IN DETROIT

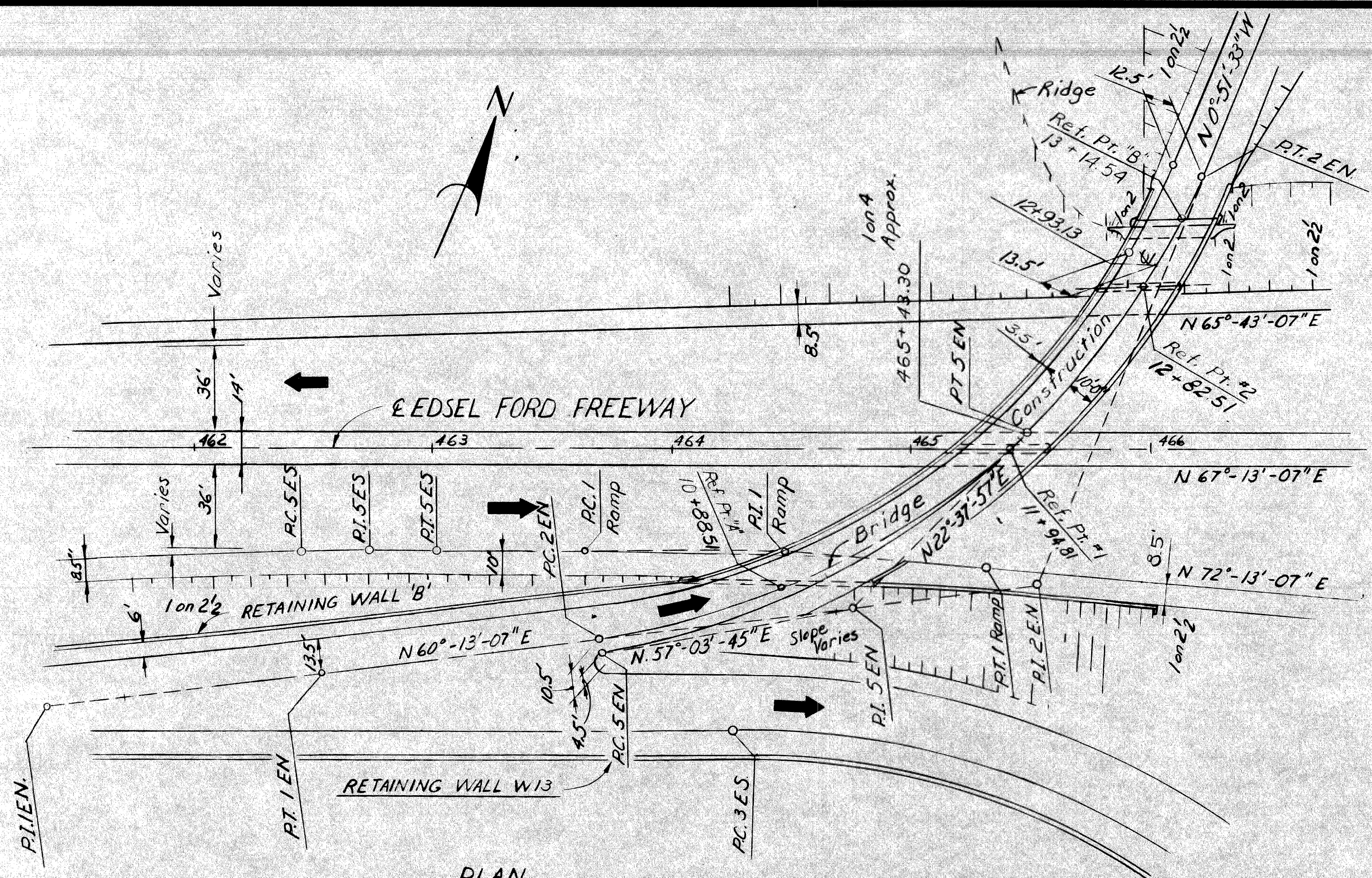
LOG OF SOIL BORINGS

CITY OF DETROIT

SQUAD BOSS *R. J. ...* 12-67
DRAWN BY *R. G.* 9-66
CHECKED BY *L. B. J.* 1-67
SHEET 3 OF 30

APPROVED _____ DESIGN SUPERVISING ENGINEER
APPROVED _____ ENGINEER OF DESIGN - CONSULTANTS

S 44 of 82123 K



PLAN
Scale: 1" = 40'

BENCH MARKS

C.B.M. 26 Arrow on Hydrant N.E. Corner Merrick and Tillman Elev. 134.43
 C.B.M. 35 Arrow on Hydrant S.E. Corner 23rd and Ford S. Service Dr. Elev. 134.47
 C.B.M. 57 Arrow on Hydrant N.E. Corner 23rd and Hudson Elev. 137.72
 C.B.M. denotes Construction Bench Mark

GRAND RIVER EXIT RAMP

CURVE DATA 1
 $\Delta = 5^{\circ}-00'-00''$
 $D = 3^{\circ}-00'-00''$
 $R = 1909.859'$
 $T = 83.386'$
 $L = 166.667'$
 $E = 1.819'$
 $PC = 463+63.549$
 $PI = 464+46.935$
 $PT = 465+30.216$

E'BND TO S'BND TURNING ROADWAY

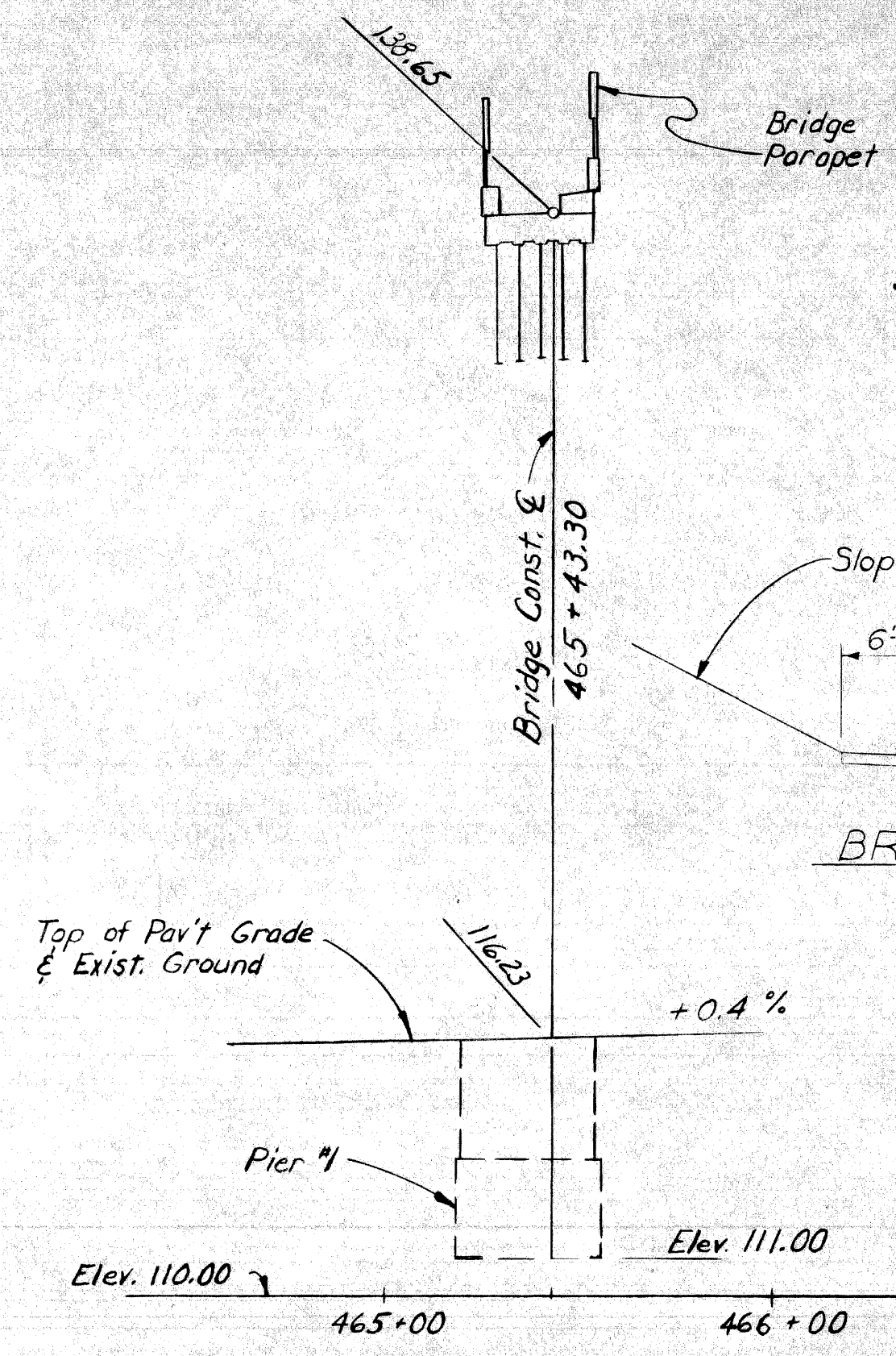
CURVE DATA 5 ES
 $\Delta = 1^{\circ}-37'-02''$
 $D = 3^{\circ}-00'-00''$
 $R = 1909.859'$
 $T = 26.955'$
 $L = 53.907'$
 $E = 0.190'$
 $PC = -$
 $PI = 462+74.635$
 $PT = 463+01.590$
 * E. Ford Frwy. Stationing Super El. $r = 0.06\%$ max.

CURVE DATA 2 EN
 $\Delta = 61^{\circ}-04'-40''$
 $D = 18^{\circ}-20'-04.74''$
 $R = 312.500'$
 $T = 184.362'$
 $L = 333.128'$
 $E = 50.330'$
 $PC = 10+00.000$
 $PI = 11+84.362$
 $PT = 13+33.128$

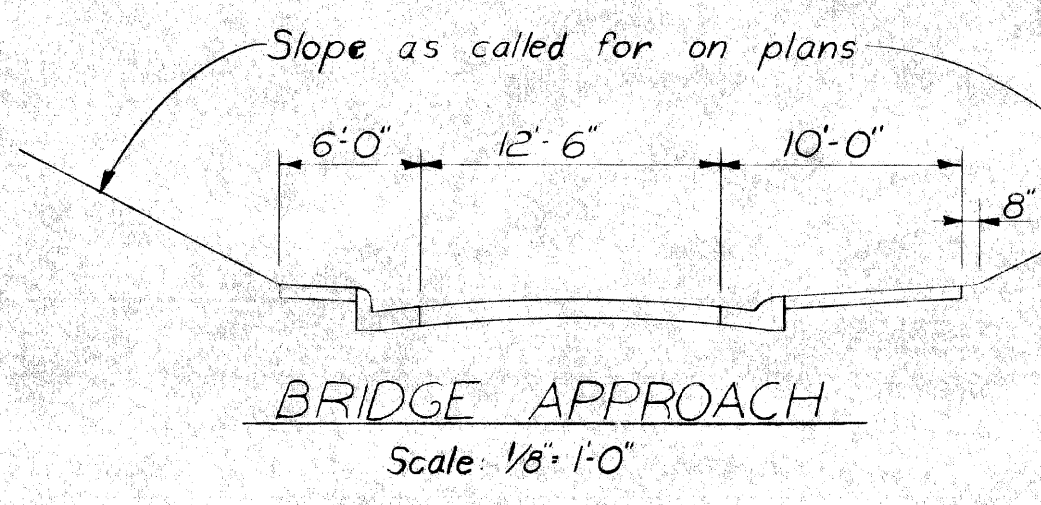
E'BND TO N'BND TURNING ROADWAY

CURVE DATA 5 EN
 $\Delta = 34^{\circ}-25'-48''$
 $D = 16^{\circ}-39'-29.42''$
 $R = 343.950'$
 $T = 106.569'$
 $L = 206.685'$
 $E = 16.131'$
 $PC = -$
 $PI = -$
 $PT = 12+05.000$

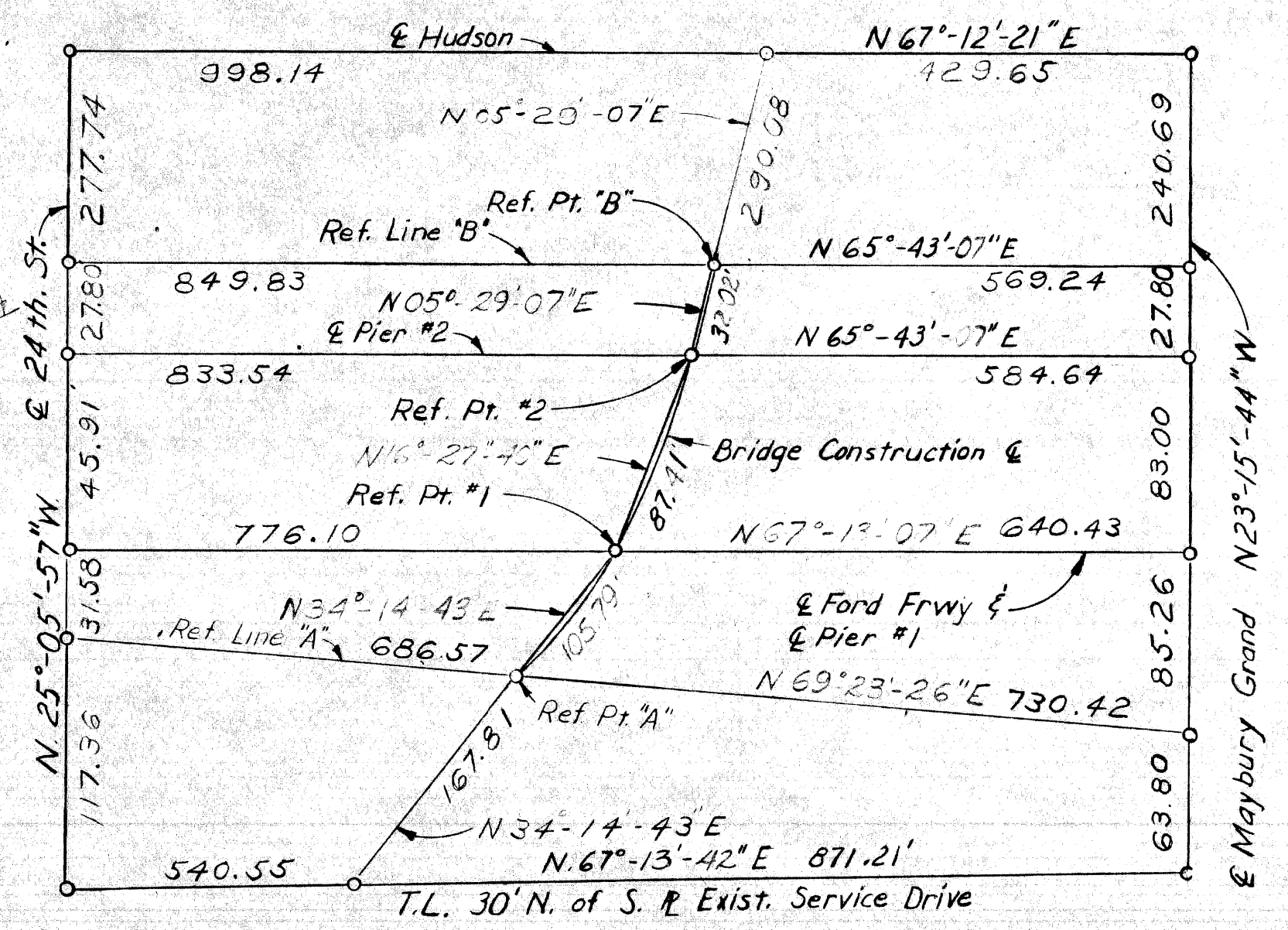
CURVE DATA 1 EN
 $\Delta = 7^{\circ}-00'-00''$
 $D = 3^{\circ}-00'-00''$
 $R = 1909.859'$
 $T = 116.812'$
 $L = 233.333'$
 $E = 3.569'$
 $PC = 6+50.263$
 $PI = 7+67.075$
 $PT = 8+83.596$



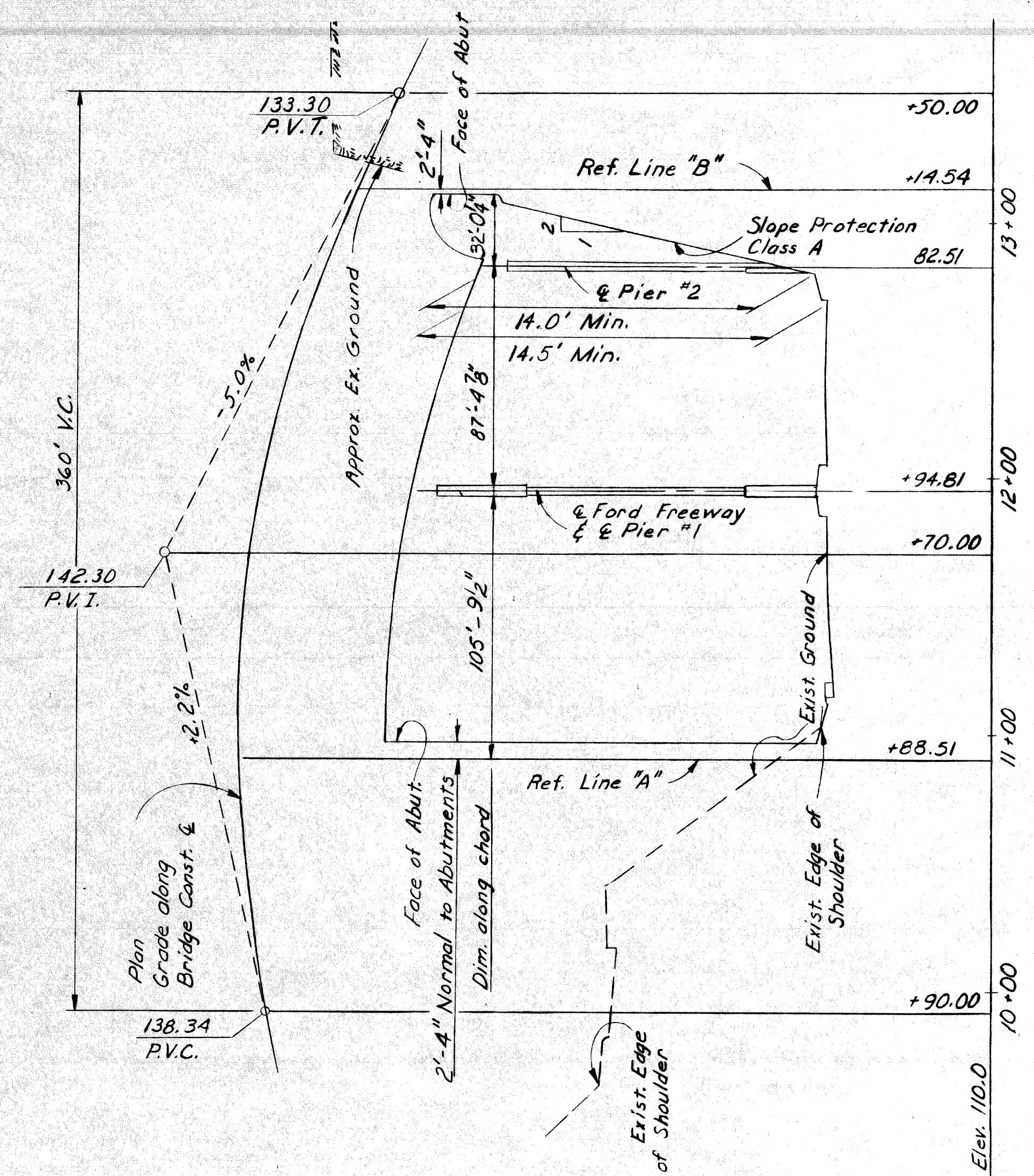
PROFILE ALONG FREEWAY
Hor. 1" = 40'
Vert. 1" = 4'



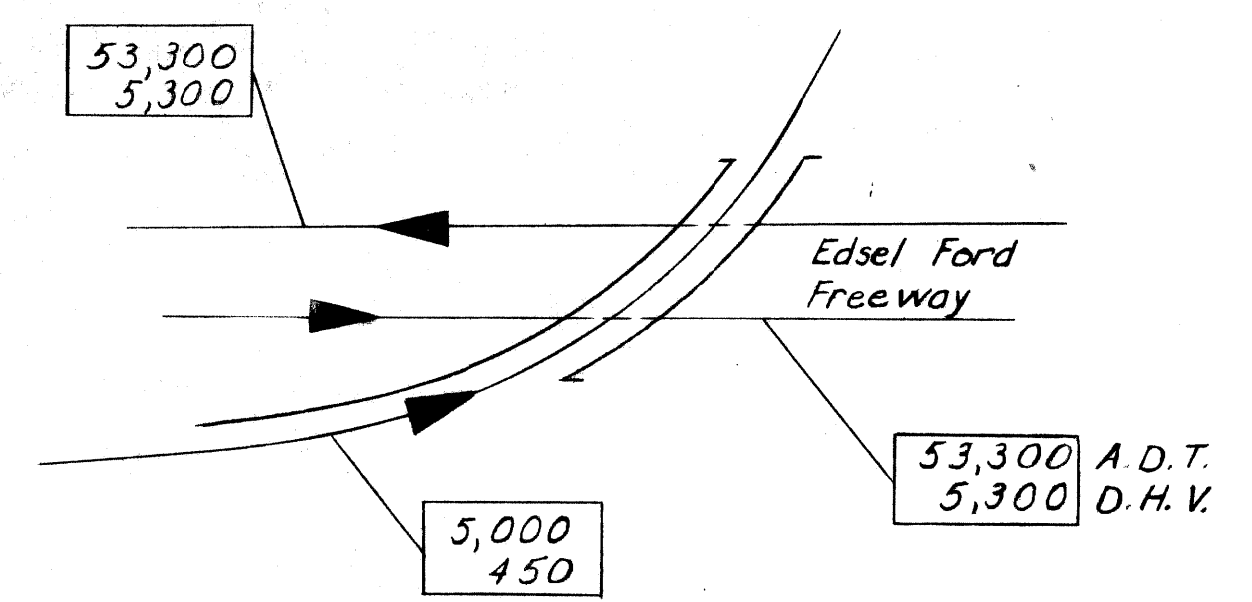
BRIDGE APPROACH
Scale: 1/8" = 1'-0"



ALIGNMENT DIAGRAM
No Scale



PROFILE ALONG BRIDGE CONSTRUCTION
Hor. 1" = 40'
Vert. 1" = 4'



A.D.T. denotes Average Daily Traffic
 D.H.V. denotes Design Hourly Volume

TRAFFIC COUNT
Estimated Traffic 1990

PLANS PREPARED BY
CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

APPROVED: *J. J. Conroy* STRUCTURAL ENGINEER
 JOB No. PW990(2)

NO.	DESCRIPTION	DATE	BY

MICHIGAN DEPARTMENT OF STATE HIGHWAYS

JEFFRIES-FORD INTERCHANGE
 EAST BND. TO NORTH BND TURNING ROADWAY
 CROSSING THE EDESEL FORD FREEWAY IN DETROIT

GENERAL DRAWING

APPROVED: _____ DESIGN SUPERVISING ENGINEER
 APPROVED: _____ ENGINEER OF DESIGN - CONSULTANTS

SQUAD BOSS	DATE
<i>R. James</i>	<i>12-57</i>
<i>A. Morton</i>	<i>12-15-66</i>
<i>L.B. J. R.</i>	<i>1-67</i>

SHEET 4 OF 30
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