

| | | | |
|---------------|--------------|-----------|--------------|
| STATE PROJECT | FEDERAL NO. | SHEET NO. | TOTAL SHEETS |
| 82251 | I-75-(168)53 | 1 | 1 |

THE IMPROVEMENTS COVERED BY THESE PLANS SHALL BE DONE IN ACCORDANCE WITH THE MICHIGAN STATE HIGHWAY DEPARTMENT CURRENT STANDARD SPECIFICATIONS AND SUPPLEMENTAL SPECIFICATIONS

MICHIGAN
STATE HIGHWAY DEPARTMENT
JOHN C. MACKIE
 STATE HIGHWAY COMMISSIONER

PLANS OF PROPOSED

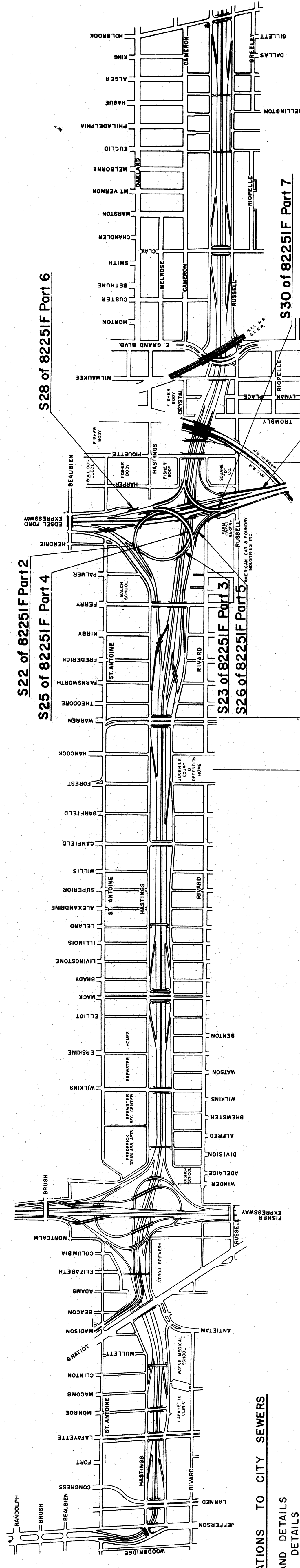
MICHIGAN PROJECT I-75-(156)-53
CONTROL SECTION BI 82251E C32 & BI 82251F C21
THE WALTER P. CHRYSLER EXPRESSWAY

WAYNE COUNTY
 CITY OF DETROIT

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CONTRACT FOR G&DS PAVING & UTILITY ALTERATIONS
 ITEM NO. 538

DIVISION APPROVAL

CHECKED _____ ENGINEER OF DESIGN-CONSULTANTS _____ DATE _____
 RECOMMENDED _____ ENGINEER _____ DATE _____
 APPROVAL _____ TRAFFIC ENGINEER _____ DATE _____
 FOR APPROVAL _____ ENGINEER OF BRIDGE AND ROAD DESIGN _____ DATE _____

OFFICES OF DESIGN AND CONSTRUCTION

APPROVED _____ CONSTRUCTION ENGINEER _____ DATE _____
 APPROVED _____ CHIEF DESIGN ENGINEER _____ DATE _____

STATE HIGHWAY DEPARTMENT APPROVAL

APPROVED JOHN C. MACKIE
 STATE HIGHWAY COMMISSIONER

BY _____ DIRECTOR OF ENGINEERING-CHIEF ENGINEER _____ DATE _____

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

APPROVED _____ ENGINEER OF EXPRESSWAYS
 APPROVED _____ ASSISTANT CITY ENGINEER-HIGHWAYS AND EXPRESSWAYS
 APPROVED _____ CITY ENGINEER
 APPROVED _____ COMMISSIONER OF PUBLIC WORKS AND EXPRESSWAYS COORDINATOR

| | | |
|-------------|---------------------|-----------|
| PROJECT NO. | FEDERAL PROJECT NO. | SHEET NO. |
| 82251 | I-75-(156)53 | 1 |

82251E C32

QUANTITY SHEET - E
SURFACING (SQ.YDS.) - CURB & GUTTER (LIN.FT.)

STATE: MICH. PROJECT NO.: 82251
 COUNTY: Wayne
 CITY: Detroit
 ROUTE: 82251

| SHEET NO. | LOCATION | AS CONSTRUCTED | | | | | | | | | | | | | | | EXPLANATION OF CHANGES | | | | | | | | | | | | |
|-----------|--|---|--|---|---|-------------------------------------|------------------------------------|--|--|-----------------------------------|--------------------------------------|---------------------------|----------------------------------|-----------------------------------|----------------------------------|----------------------------------|------------------------|------------------------------|------------------------------|---------------------------------------|--|--|--------------------------------------|---------------------------------|---------------------------|-------------------------------|-----|----|--|
| | | Concrete Pavement 10" Unif. Sp. Yds. | Concrete Pavement 8" Unif. Sp. Yds. | Concrete Base Course 9" Unif. Sp. Yds. | Concrete Base Course 9" Unif. Sp. Yds. | Concrete Ramp 10" Unif. Sp. Yds. | Concrete Ramp 9" Unif. Sp. Yds. | Bituminous Conc. Wearing Course Tons | Bituminous Conc. Levelling Course Tons | Aggreg. Shoulders Special Tons | Aggreg. Shoulders 4.11 MC-0 Gals. | Bitum. Bond Coat Gals. | Concrete Curb Det. 2 Lin. Ft. | Concrete Curb Det. 2A Lin. Ft. | Concrete Curb Det. A Lin. Ft. | Concrete Curb Det. B Lin. Ft. | | 4" Conc. Sidewalk Sq. Ft. | 7" Conc. Sidewalk Sq. Ft. | Conc. End Headers Det. 20 Lin. Ft. | Conc. Driveway Returns Det. 15 Lin. Ft. | Conc. Driveway Returns Det. 17 Lin. Ft. | Removing Asphalt Surface Sq. Yds. | Asphalt Replacement Sq. Yds. | Crushed Limestone Tons | Conc. Curb Det. 9 Lin. Ft. | | | |
| SH #13 | Construction Sequence for Russell Exit Ramp | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #22 | Grading Plans | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #23 | Palmer to Harper & Rivard to Hastings Medbury to Harper & Russell to Rivard | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #14 | Removal Items | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #15 | Warren to Kirby & E.S.D. to W.S.D. | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #16 | Kirby to Palmer & E.S.D. to W.S.D. | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #17 | Palmer to Harper & Hastings to St. Antoine | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #18 | Palmer to Harper & Rivard to Hastings Medbury to Harper & Russell to Rivard | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #35 | Expressway Items | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #37 | Sta. 137+10 to 145+50 | 6.914 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #38 | Sta. 145+50 to 156+00 | 8.418 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #39 | Sta. 156+00 to 166+50 | 8.412 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #41 | Turning Roadways | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #42 | E.B.D. to N.B.D. | 2.242 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #43 | E.B.D. to S.B.D. | 882 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #44 | E.B.D. to W.B.D. | 1,072 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #45 | E.B.D. & W.B.D. to S.B.D. | 2,321 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #46 | E.B.D. & W.B.D. to S.B.D. | 1,707 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #47 | W.B.D. to S.B.D. | 1,712 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #48 | N.B.D. to E.B.D. & W.B.D. | 2,684 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #49 | N.B.D. to E.B.D. & W.B.D. | 1,831 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #50 | N.B.D. & S.B.D. to W.B.D. | 1,260 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #51 | Ramps | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #52 | Warren Entrance | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #53 | Warren Exit | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #54 | Federick Entrance | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #55 | Boaubien Exit | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #56 | Russell Exit | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #57 | Russell Exit Defour | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #58 | Service Drives | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #59 | East Service Drive - Rivard to Russell | 3,709 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SH #60 | West Service Drive - Ferry to Hendrie | 2,744 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| TOTALS | | 23,744 | 3,709 | 287 | 17,570 | 1,481 | 58,585 | 35,382 | 364.5 | 478 | 1,224 | 65,775 | 2,222 | 7,825 | 2,745 | 122 | 9,988 | 20.5 | 22,376 | 8,771 | 590 | 97 | 39 | 82 | 242 | 68 | 978 | 49 | |

CONTROL SECTION I 82251 F. C. 21
 AUTHORIZATION 16
 CHECKED BY: J.V. WICKERY, R.A. COMLY, F.A. CHATTABIANO
 DATE: 12/22/57
 FILE NO. 82251E C32
 SHEET 135

QUANTITY SHEET-B

FENCING, GUARD RAIL & GUARD POSTS 18225IF C21

| AS PER PLANS | | AS CONSTRUCTED | | AS PER PLANS | | AS CONSTRUCTED | | EXTRAS TO PLAN | | AS CONSTRUCTED | |
|------------------|----------------------|--|-----------------------|--------------------------|--|-----------------------|------------------------------|----------------|---|----------------|------------------------|
| SHEET NO. | 48" Chain Link Fence | 6" Walk Gater for Chain Link Fence Ea. | Steel Beam Guard Rail | 48" Chain Link Fence Ea. | 6" Walk Gater for Chain Link Fence Ea. | Steel Beam Guard Rail | Double Steel Beam Guard Rail | STA. TO STA. | EXPLANATION OF CHANGES | STA. TO STA. | EXPLANATION OF CHANGES |
| Sheet #35 | 1275 | 2 | 164 | 1,253 | 2 | 164 | | #2001 | Remove & Reinstall U.G. Street Light Unit @ Fanny Russell Drive Const | | |
| Sheet #37 | 610 | 1 | 41 | 610 | 1 | 41 | | #2002 | 8" Sewer - Transferred from Bt. 5710E 8225IF | | |
| Turning Roadways | | | | | | | | #2003 | 8" Sewer - Deleted | | |
| Sheet #43 | 1100 | 2 | 120 | 1,080 | 2 | 120 | | #2004 | P.C. - Revising with 2003 | | |
| Sheet #47 | 545 | 1 | 82 | 545 | 1 | 82 | | #2005 | 15" CTE P.C. II Encased Sewer 6.1 T.M.A. - Deleted | | |
| Sheet #48 | 1040 | 1 | 41 | 1,040 | 1 | 41 | | #2006 | 15" CTE P.C. II Encased Sewer 6.1 T.M.A. - Deleted | | |
| Sheet #49 | 710 | 1 | 45 | 710 | 1 | 45 | | #2007 | 18" Trap Trap Detroit Edison Duct Change | | |
| Sheet #50 | 975 | 1 | 54 | 975 | 1 | 54 | | #2008 | 6" Sewer - 1" Deep on Less, Parky Drive Abundant | | |
| Service Drives | | | | | | | | #2009 | P.C. - Remove & Replace P.C. w/ Foundation | | |
| Sheet #56 | 950 | 1 | - | 950 | 1 | - | | #2010 | Add Additional Supp. Spec. Cold Weather Protection of Concrete Pavement | | |
| TOTALS | | 75 | 2265 | 10 | 2777 | 2905 | 75 | 7,023 | 10 | 4,372.4 | 2,336.1 |

REMOVING MASONRY & CONCRETE 18225IF C21

| AS PER PLANS | | AS CONSTRUCTED | | AS PER PLANS | | AS CONSTRUCTED | | EXTRAS TO PLAN | | AS CONSTRUCTED | |
|--------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|----------------|------------------------|----------------|------------------------|
| SHEET NO. | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | STA. TO STA. | EXPLANATION OF CHANGES | STA. TO STA. | EXPLANATION OF CHANGES |
| Sheet #18 | 4557 | 82 | 333 | 4557 | 82 | 333 | | #2011 | Balance with 2010 | | |
| Sheet #19 | 618 | 36 | 1087 | 618 | 36 | 1087 | | #2012 | Balance with 2011 | | |
| Sheet #15 | 6331 | 45 | 145 | 6331 | 45 | 145 | | #2013 | Balance with 2012 | | |
| Sheet #17 | 9253 | 40 | 2233 | 9253 | 40 | 2233 | | #2014 | Balance with 2013 | | |
| Sheet #19 | 2225 | - | 271 | 2225 | - | 271 | | #2015 | Balance with 2014 | | |
| TOTALS | | 23,045 | 203 | 4069 | 65 | 21,502 | 203 | 4,384 | 55 | | |

SODDING 18225IF C21

| AS PER PLANS | | AS CONSTRUCTED | | AS PER PLANS | | AS CONSTRUCTED | | EXTRAS TO PLAN | | AS CONSTRUCTED | |
|--------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|----------------|---------------------------|----------------|------------------------|
| SHEET NO. | Class A Sodding | Class A Sodding | Class A Sodding | Class A Sodding | Class A Sodding | Class A Sodding | Class A Sodding | STA. TO STA. | EXPLANATION OF CHANGES | STA. TO STA. | EXPLANATION OF CHANGES |
| Sheet #19 | 15,605 | 15,605 | | 15,605 | 15,605 | | | #2016 | Remove Trees 8"-12" Dia. | | |
| Sheet #20 | 20,366 | 20,366 | | 20,366 | 20,366 | | | #2017 | Remove Trees 13"-24" Dia. | | |
| Sheet #21 | 16,293 | 16,293 | | 16,293 | 16,293 | | | #2018 | Remove Trees 25"-36" Dia. | | |
| Sheet #23 | 12,983 | 12,983 | | 12,983 | 12,983 | | | #2019 | Remove D.F.R. Tract Strip | | |
| Sheet #23 | 363 | 363 | | 363 | 363 | | | #2020 | Fine Grading & Finishing | | |
| TOTALS | | 69,610 | 69,610 | 68,956 | 68,956 | | | #2021 | Earth Grading & Cleanup | | |

MISCELLANEOUS 18225IF C21

| AS PER PLANS | | AS CONSTRUCTED | | AS PER PLANS | | AS CONSTRUCTED | | EXTRAS TO PLAN | | AS CONSTRUCTED | |
|--------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|------------------------------------|----------------|------------------------|----------------|------------------------|
| SHEET NO. | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | Removing Old Masonry & Conc. Curbs | STA. TO STA. | EXPLANATION OF CHANGES | STA. TO STA. | EXPLANATION OF CHANGES |
| Sheet #18 | 4557 | 82 | 333 | 4557 | 82 | 333 | | #2022 | Balance with 2021 | | |
| Sheet #19 | 618 | 36 | 1087 | 618 | 36 | 1087 | | #2023 | Balance with 2022 | | |
| Sheet #15 | 6331 | 45 | 145 | 6331 | 45 | 145 | | #2024 | Balance with 2023 | | |
| Sheet #17 | 9253 | 40 | 2233 | 9253 | 40 | 2233 | | #2025 | Balance with 2024 | | |
| Sheet #19 | 2225 | - | 271 | 2225 | - | 271 | | #2026 | Balance with 2025 | | |
| TOTALS | | 23,045 | 203 | 4069 | 65 | 21,502 | 203 | 4,384 | 55 | | |

QUANTITY SHEET - E

CONSTRUCTION AND RELOCATION OF CITY LIGHTING, POWER, COMMUNICATIONS AND SIGNAL FACILITIES

AS PER PLANS

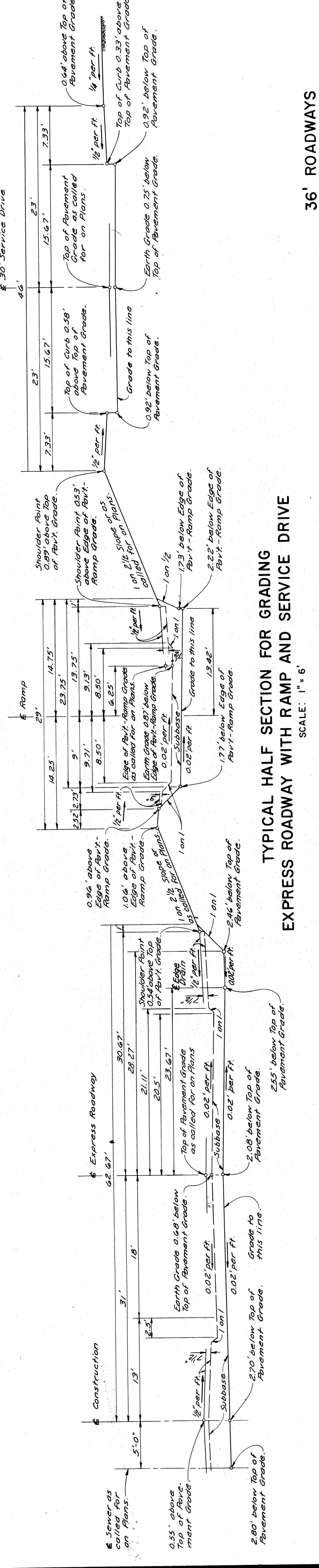
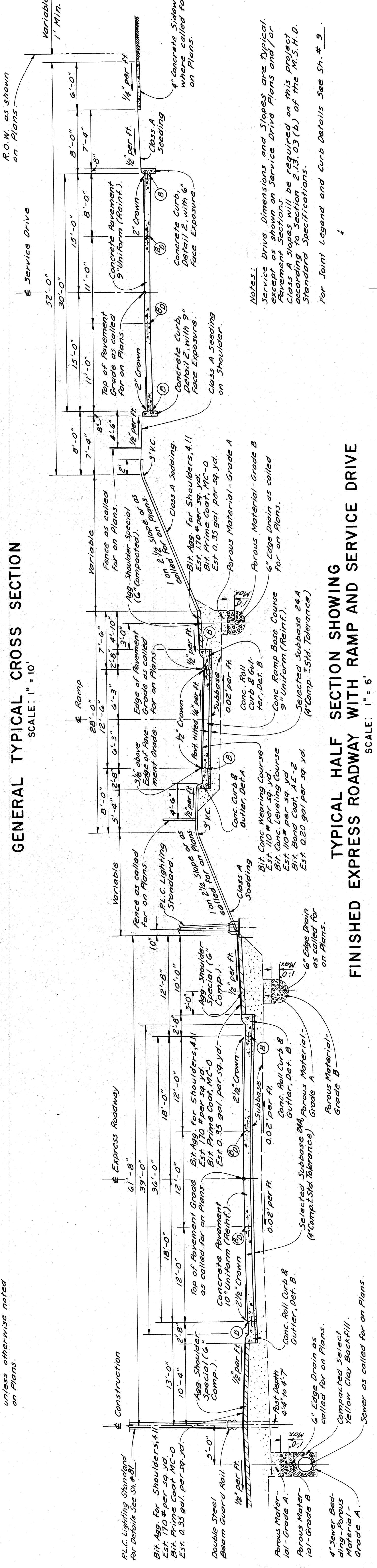
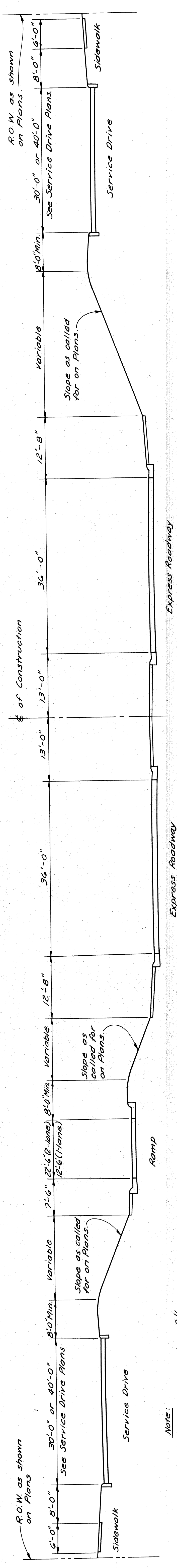
AS CONSTRUCTED

| | | |
|-------|-----------|-----------|
| STATE | PROJ. NO. | SHEET NO. |
| MICH. | 157/5025 | 137 |
| CITY | WAYNE | DETROIT |

| DESCRIPTION | UNIT | QUANTITY | CONTROL SECTION BI 82251E C2 | CONTROL SECTION BI 82251E C1 |
|--|----------|-----------|------------------------------|------------------------------|
| Flasher Signal Installation | Lump | 100% | | |
| Warden Ave Bridge Underbridge Wiring System | Lump | 100% | | |
| Underbridge Lighting Unit Mounted Between Bridge Beams | Each | 32 | | |
| 600 V, 2-1/2" #6 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Conduit | Lin. Ft. | 850 | | AUTH. (1065) |
| 600 V, 1-1/2" #14 Lead Covered Cable | Lin. Ft. | 235 | | |
| Replace Existing 1.5 KVA Transformer with New 10 KVA Transformer | Each | 1 | | |
| CONTROL SECTION BI 82251F C2 | | | | |
| 6" Sewer | Lin. Ft. | 25 | | 35 AUTH. (1065, 37) |
| Removing Wood Poles | Each | 49 | | 49 AUTH. (1062) |
| Removing Post Foundation | Each | 5 | | 5 |
| Removing O.H. St. Ltg. Unit's | Each | 45 | | 46 AUTH. (1001, 62) |
| Removing 2" #6 O.H. Lines | Lin. Ft. | 5080 | | 5085 AUTH. (1062) |
| Street Type 5" Handholes | Each | 1 | | 1 |
| 1-3" Encased Conduit | Lin. Ft. | 440 | | 420 AUTH. (1060) |
| 2-3" Encased Conduit | Lin. Ft. | 385 | | 405 " (1060) |
| 3-3" Encased Conduit | Lin. Ft. | 68 | | 69 " (1060) |
| 4-3" Encased Conduit | Lin. Ft. | 129 | | 133 " (1060) |
| 6-3" Encased Conduit | Lin. Ft. | 44 | | 44 |
| 1-4" Encased Conduit | Lin. Ft. | 13 | | 12 AUTH. (1060) |
| 1-4" & 1-3" Encased Conduit | Lin. Ft. | 35 | | 32 " " " " |
| 7500 V, 2-1/2" #8 Lead Covered Street Lighting Cables | Each | 3 | | 3 |
| Police Call Boxes - Furnish Only | Each | 65 | | 7688 " " " " |
| 600 V, 1-1/2" #6 Direct Burial Cable in Earth | Lin. Ft. | 65 | | 65 " " " " |
| 600 V, 2-1/2" #6 Direct Burial Cable in Earth | Lin. Ft. | 135 | | 135 " " " " |
| 600 V, 1-1/2" #2 Direct Burial Cable in Earth | Lin. Ft. | 115 | | 115 " " " " |
| 600 V, 2-1/2" #2 Direct Burial Cable in Earth | Lin. Ft. | 1105 | | 1101 " " " " |
| 600 V, 1-1/2" #6 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 1320 | | 1317 AUTH. (1065, 60) |
| 600 V, 2-1/2" #6 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 1320 | | 1319 AUTH. (1065, 60) |
| 600 V, 3-1/2" #6 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 14,035.74 | | 14,035.74 AUTH. (1065, 60) |
| 600 V, 3-1/2" #2 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 75 | | 75 AUTH. (1065, 60) |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 2,240.1 | | 2,240.1 AUTH. (1065, 60) |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 200 | | 200 " " " " |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 150.4 | | 150.4 " " " " |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 790 | | 780.28 " " " " |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 172 | | 184.08 " " " " |
| 600 V, 2-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 215 | | 242.34 " " " " |
| 600 V, 1-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 330 | | 495.7 " " " " |
| 600 V, 2-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Earth | Lin. Ft. | 3105 | | 3473.4 AUTH. (1065, 60) |
| 600 V, 4-1/2" #6 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Conduit | Lin. Ft. | 265 | | 272.2 " " " " |
| 600 V, 2-1/2" #2 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Conduit | Lin. Ft. | 135 | | 228.88 " " " " |
| 600 V, 3-1/2" #2 Direct Burial Cables & 1-1/2" Neoprene Covered Wire in Conduit | Lin. Ft. | 1713 | | 1815.53 " " " " |
| 600 V, 2-1/2" #2 Parkway Cables, 1-1/2" #2 Direct Burial Cable & 1-1/2" Neoprene Covered Wire in Conduit | Lin. Ft. | 34 | | 32 " " " " |
| 600 V, 4-1/2" #6 Lead Covered Cable | Lin. Ft. | 65 | | 51 " " " " |
| 600 V, 1-1/2" #14 Lead Covered Cable | Lin. Ft. | 1660 | | 1,587 " " " " |
| Communications Cable - 6/c Parkway | Lin. Ft. | 1015 | | 1,082.2 " " " " |
| Removing Flathead Signal Equipment | Lump | 1720 | | 1,451.1 " " " " |
| Removing Barricade Lighting System | Lump | 100% | | 100% |
| Relocating O.H. St. Ltg. Unit Less Coil | Each | 1 | | 1 |
| Relocating U.G. St. Ltg. Unit on New Foundation | Each | 4 | | 4 |
| Relocating U.G. St. Ltg. Unit on Existing Foundation | Each | 1 | | 2 AUTH. (1000) |
| Installing Salvaged Wood Pole | Each | 1 | | 1 |
| Installing Salvaged O.H. St. Ltg. Unit on New 6' Arm | Each | 1 | | 1 |

TYPICAL CROSS-SECTIONS

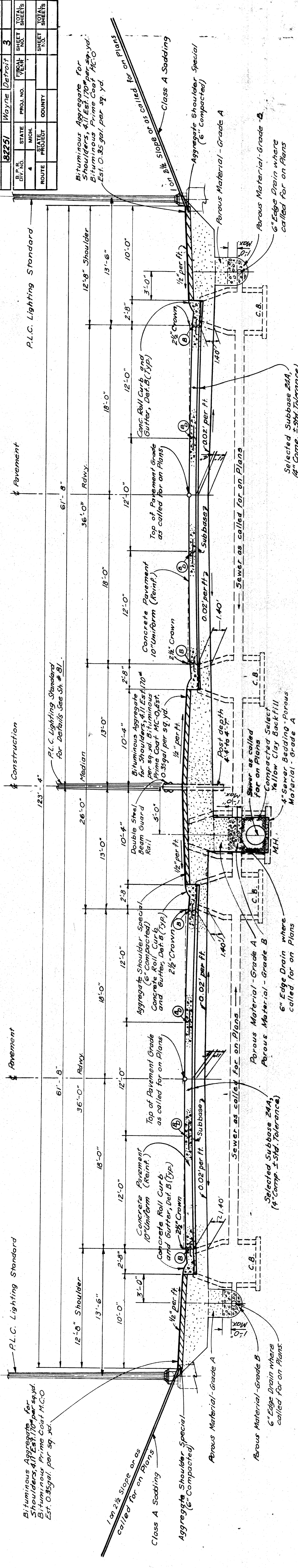
| | | | | |
|-----------|-----------|-----------|--------------|--------------|
| STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
| MICH. | 775/09957 | | | |
| COUNTY | CITY | | SHEET NO. | TOTAL SHEETS |
| Wayne | Detroit | | 2 | |
| PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS | |
| 82251 | | | | |
| COUNTY | | | SHEET NO. | TOTAL SHEETS |
| | | | | |



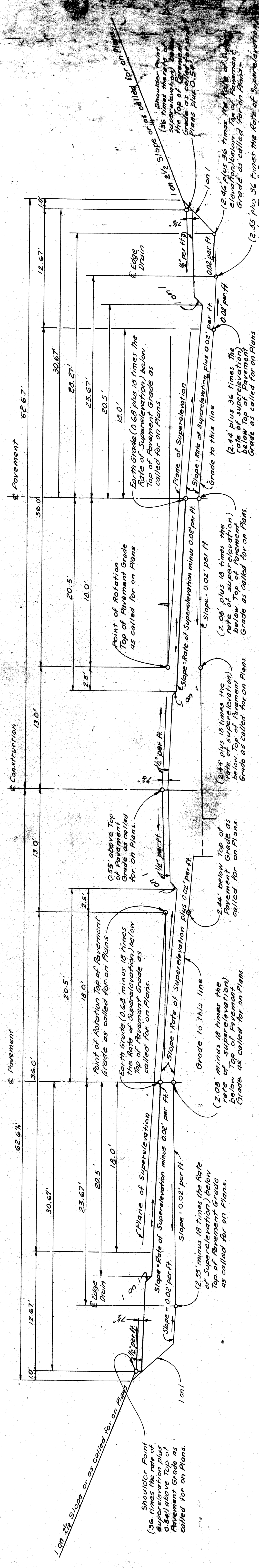
PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

TYPICAL CROSS SECTIONS

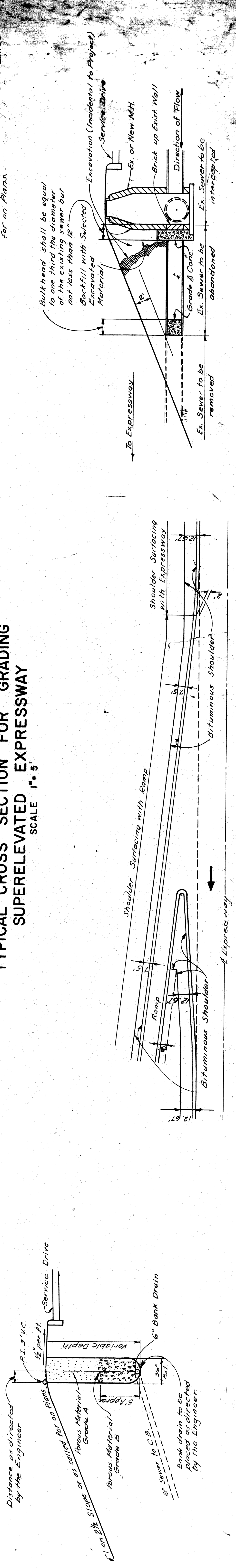
| | | | |
|--------------|---------|-------------|---------|
| STATE | MICH. | PROJECT NO. | 8225E |
| COUNTY | Wayne | CITY | Detroit |
| ROUTE | | | |
| DATE | 7-25-36 | SHEET NO. | 3 |
| TOTAL SHEETS | | 3 | |



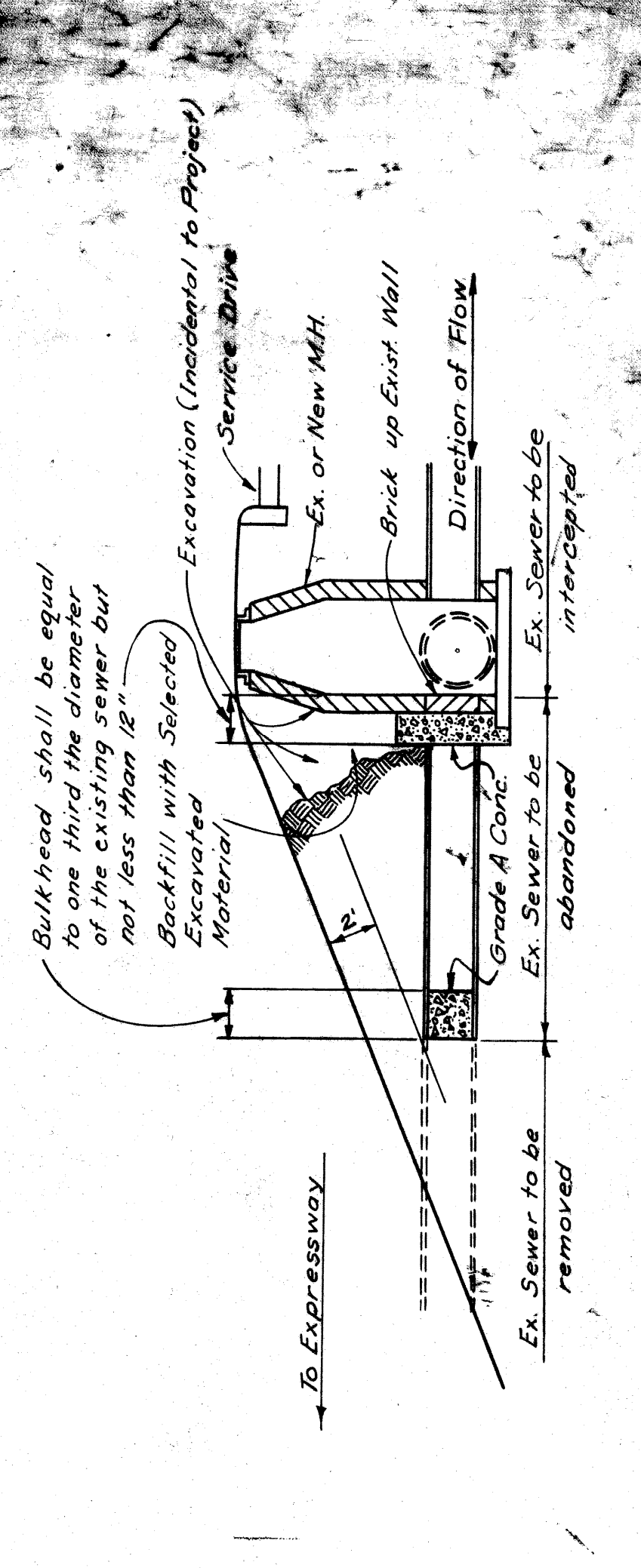
TYPICAL CROSS SECTION OF EXPRESSWAY SHOWING PAVEMENT, SUBBASE AND UNDERDRAINAGE
SCALE 1" = 5'



TYPICAL CROSS SECTION FOR GRADING SUPER-ELEVATED EXPRESSWAY
SCALE 1" = 5'



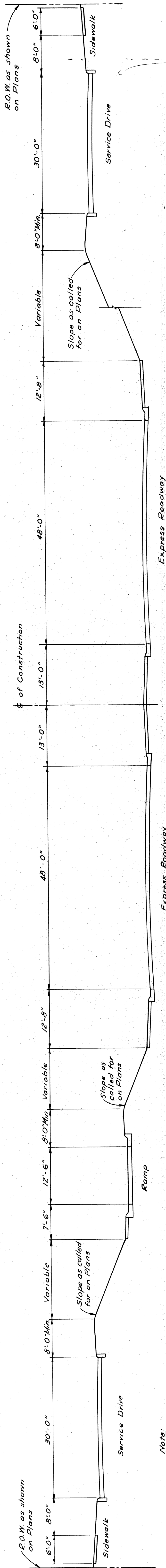
SKETCH SHOWING BANK DRAIN USED WHERE DIRECTED BY THE ENGINEER
SCALE 1" = 6'



SKETCH SHOWING METHOD FOR BULKHEADING SEVERED SEWERS

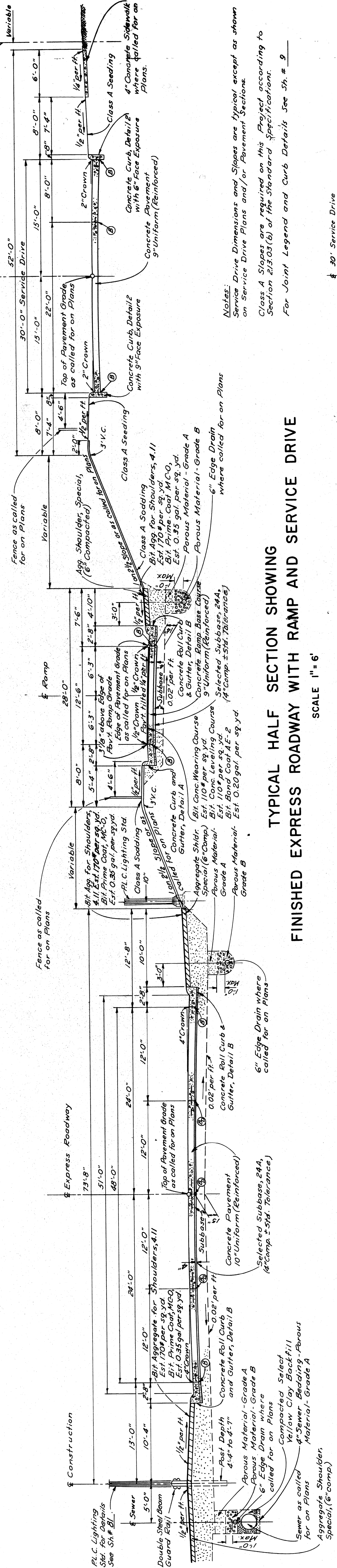
TYPICAL CROSS - SECTIONS

| PROJ. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-----------|---------------|------------|---------------|--------------|
| 4 | MI | 2-55(3945) | 5 | 5 |
| ROUTE | CITY | ROUTE | CITY | TOTAL SHEETS |
| 8225 | Wayne Detroit | 8225 | Wayne Detroit | 5 |
| PROJ. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 4 | MI | 2-55(3945) | 5 | 5 |
| ROUTE | CITY | ROUTE | CITY | TOTAL SHEETS |
| 8225 | Wayne Detroit | 8225 | Wayne Detroit | 5 |



GENERAL TYPICAL CROSS SECTION

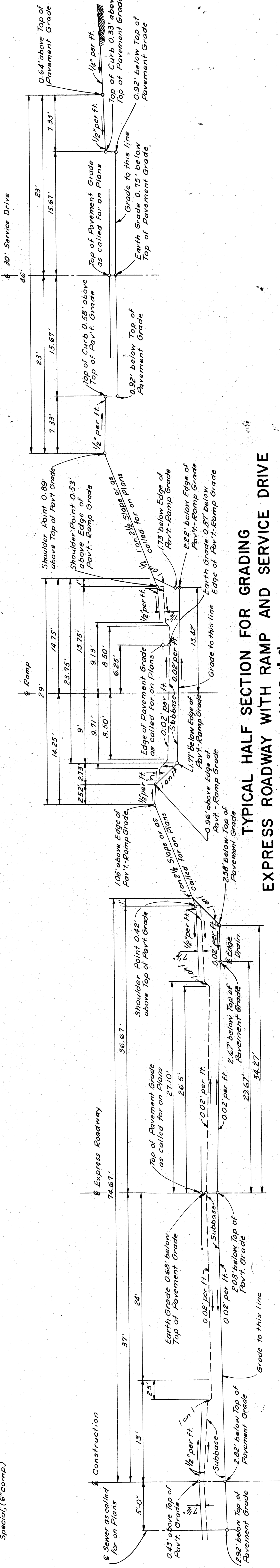
SCALE 1" = 10'



Note:
All Slopes are 1 on 2 1/2 unless otherwise noted on Plans.

TYPICAL HALF SECTION SHOWING FINISHED EXPRESS ROADWAY WITH RAMP AND SERVICE DRIVE

SCALE 1" = 6'



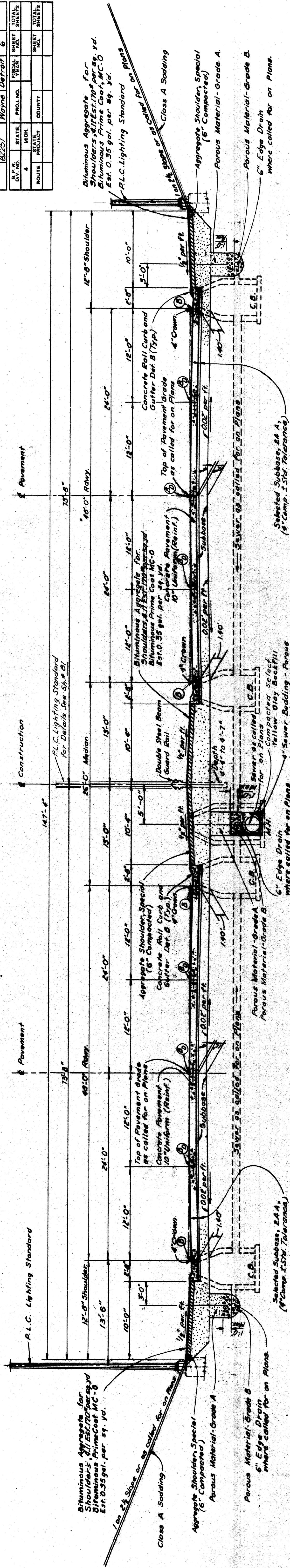
TYPICAL HALF SECTION FOR GRADING EXPRESS ROADWAY WITH RAMP AND SERVICE DRIVE

SCALE 1" = 6'

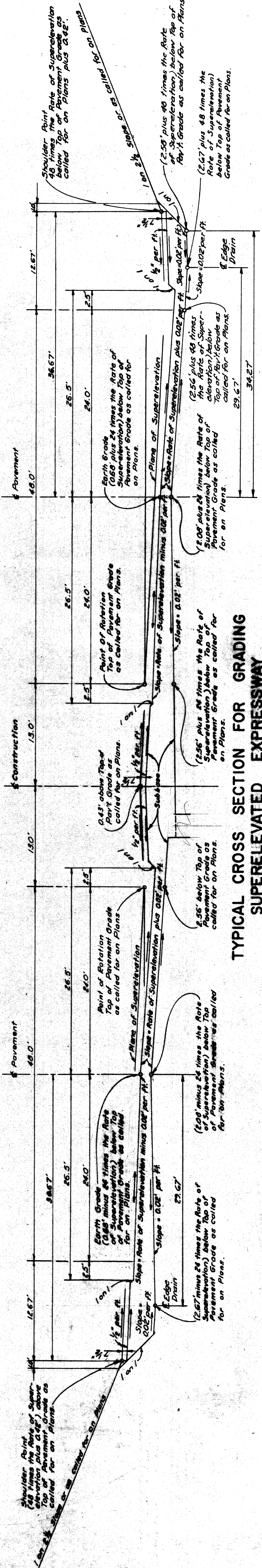
PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

TYPICAL CROSS SECTIONS

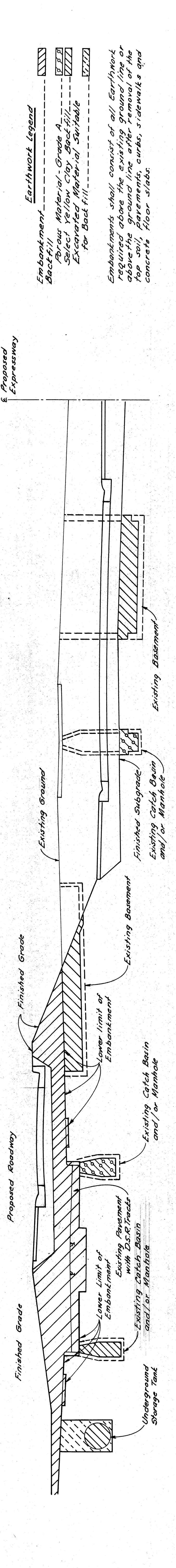
| PROJ. NO. | STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
|-----------|--------|-----------|-------|-----------|--------------|
| 4 | MICH. | 725/2503 | 53 | | |
| ROUTE | COUNTY | CITY | ROUTE | SHEET NO. | TOTAL SHEETS |
| 82251 | Wayne | Dearborn | 6 | | |
| PROJ. NO. | STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
| 4 | MICH. | 725/2503 | 53 | | |
| ROUTE | COUNTY | CITY | ROUTE | SHEET NO. | TOTAL SHEETS |
| 82251 | Wayne | Dearborn | 6 | | |



TYPICAL CROSS SECTION OF EXPRESSWAY SHOWING PAVEMENT, SUBBASE AND UNDERDRAINAGE SCALE 1"=6'



TYPICAL CROSS SECTION FOR GRADING SUPERELEVATED EXPRESSWAY SCALE 1"=6'



TYPICAL CROSS SECTION OF EMBANKMENT AND BACKFILL SCALE 1"=6'

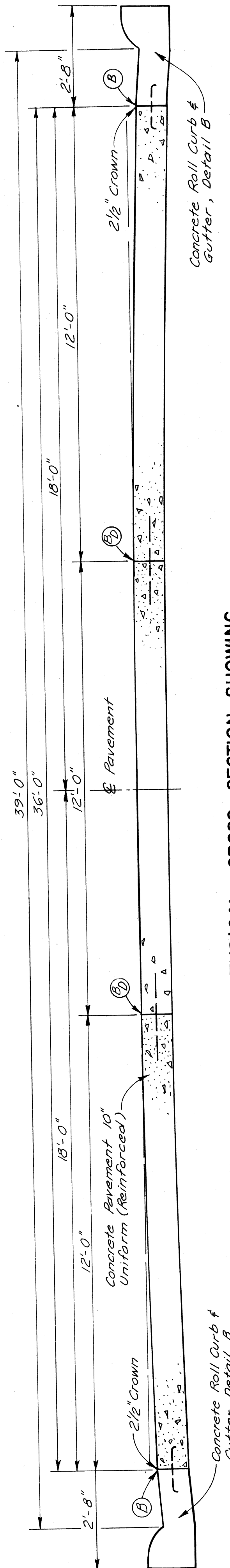
TYPICAL CROSS-SECTIONS

| B.V. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|----------|-------|--|---|----------------------------------|--------------|
| 4 | MICH. | 2757(50)33 | | | |
| | ROUTE | PROJ. NO. <td>FISCAL YEAR <td>SHEET NO. <td>TOTAL SHEETS </td></td></td> | FISCAL YEAR <td>SHEET NO. <td>TOTAL SHEETS </td></td> | SHEET NO. <td>TOTAL SHEETS </td> | TOTAL SHEETS |
| | 22231 | Wayne Detroit | | 9 | |
| B.V. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 4 | MICH. | | | | |
| | ROUTE | PROJ. NO. <td>FISCAL YEAR <td>SHEET NO. <td>TOTAL SHEETS </td></td></td> | FISCAL YEAR <td>SHEET NO. <td>TOTAL SHEETS </td></td> | SHEET NO. <td>TOTAL SHEETS </td> | TOTAL SHEETS |
| | | | | | |

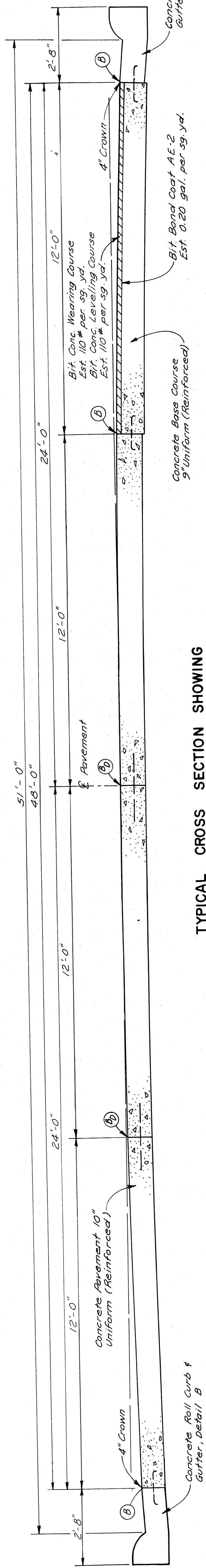
JOINT LEGEND

- B — Longitudinal Bulkhead Construction Joint according to M.S.H.D. Std. Plan E-4-A-33G, Detail 2
- D — Longitudinal Lane Tie Joint with Tie Bars, according to M.S.H.D. Std. Plan E-4-A-33G, Detail 1
- BD — Optional B or D

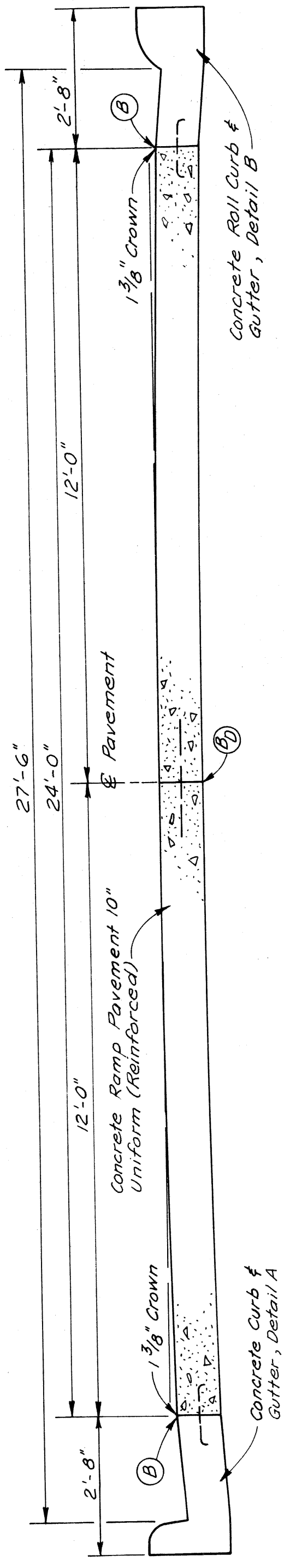
NOTE: The Concrete Base Course, Concrete Ramp Base Course, and Concrete Pavement shall be reinforced throughout in accordance with M.S.H.D. Std. Plan E-4-A-21F.



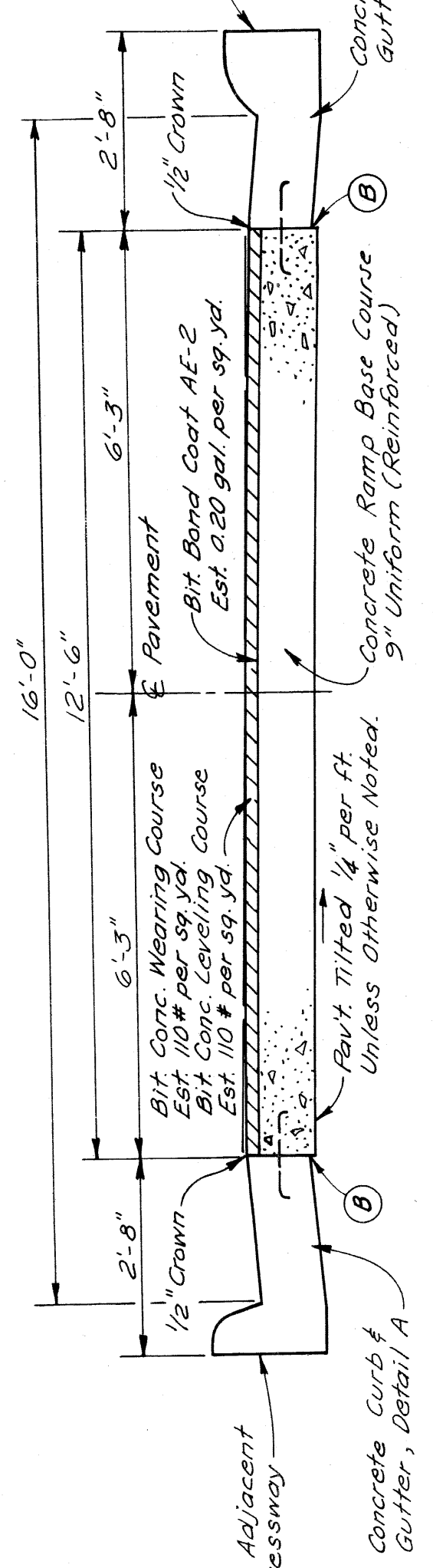
TYPICAL CROSS SECTION SHOWING
CONCRETE PAVEMENT 10" UNIFORM (REINFORCED)
36' CHRYSLER EXPRESS ROADWAY — SCALE: 1/2" = 1'



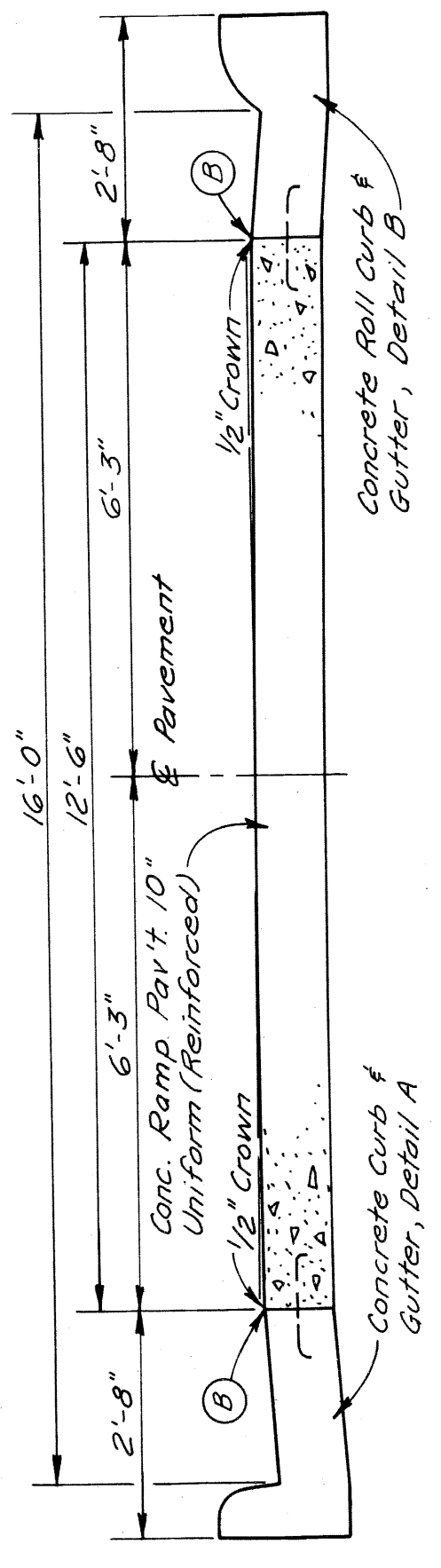
TYPICAL CROSS SECTION SHOWING
CONCRETE PAVEMENT 10" UNIFORM (REINFORCED)
48' CHRYSLER EXPRESS ROADWAY — SCALE: 1/2" = 1'



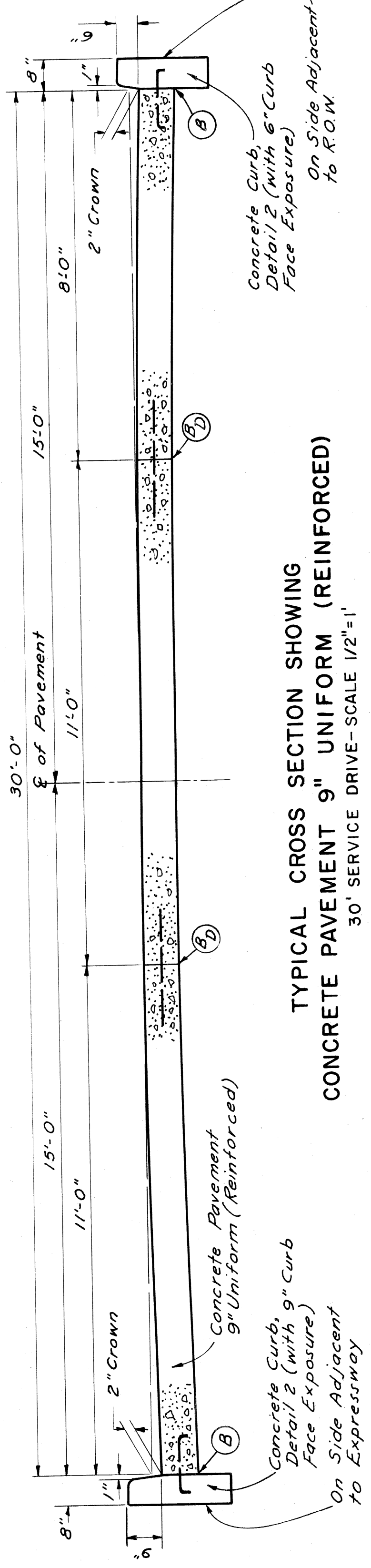
TYPICAL CROSS SECTION SHOWING
CONCRETE RAMP PAVEMENT 10" UNIFORM (REINFORCED)
24' TURNING ROADWAY — SCALE: 1/2" = 1'



TYPICAL CROSS SECTION SHOWING
CONCRETE RAMP BASE COURSE 9" UNIFORM (REINFORCED)
WITH BITUMINOUS CONCRETE PAVEMENT
12.5' RAMP — SCALE: 1/2" = 1'



TYPICAL CROSS SECTION SHOWING
CONCRETE RAMP PAVEMENT 10" UNIFORM (REINFORCED)
12.5' TURNING ROADWAY — SCALE: 1/2" = 1'



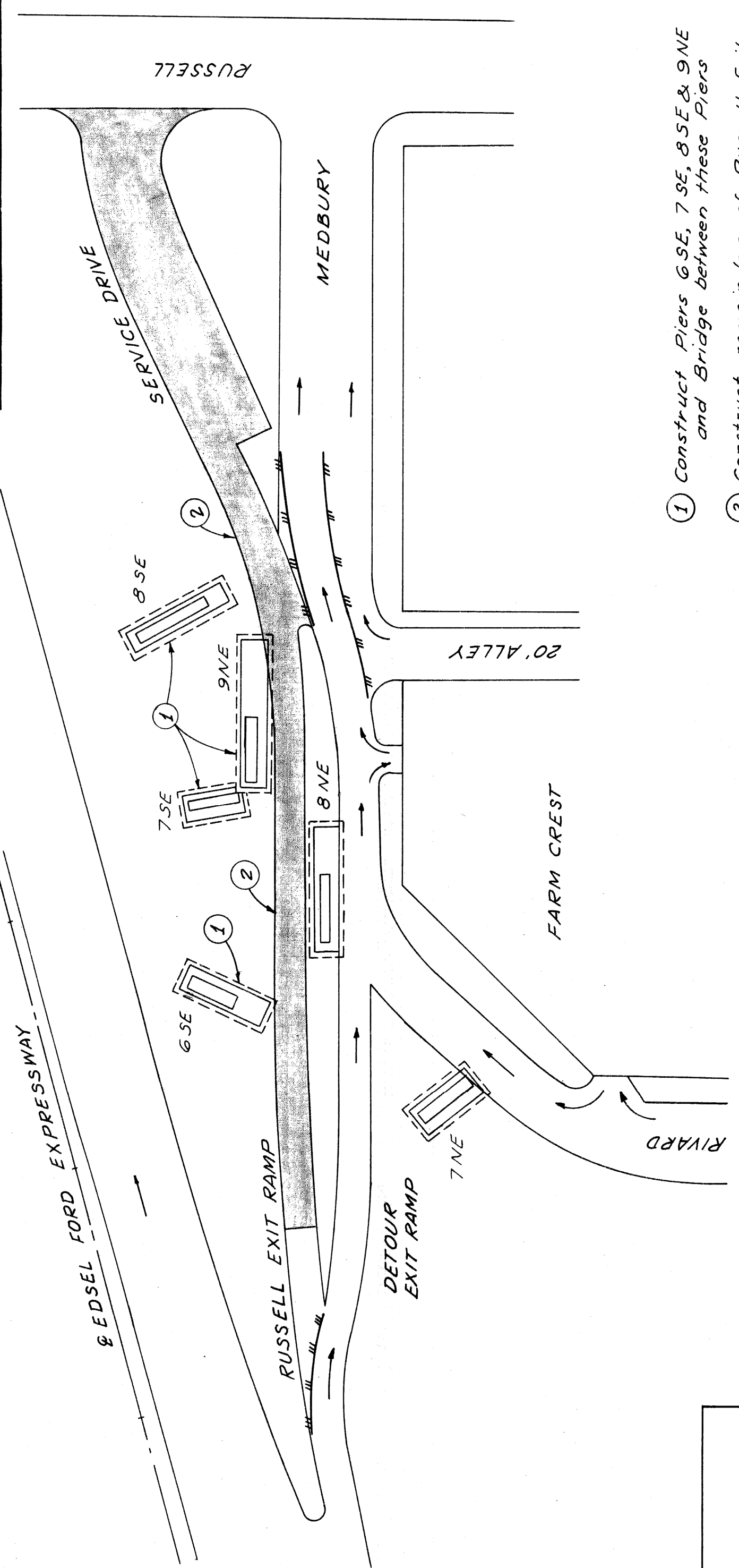
TYPICAL CROSS SECTION SHOWING
CONCRETE PAVEMENT 9" UNIFORM (REINFORCED)
30' SERVICE DRIVE — SCALE: 1/2" = 1'

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

PAVEMENT SECTIONS

FILE NO. 82251E C32
SHEET 9

| | | | | | | | |
|-------|---------|-----------|------------|-----------|----|--------------|----|
| STATE | MICH. | PROJ. NO. | 1725/69/53 | SHEET NO. | 13 | TOTAL SHEETS | 13 |
| CITY | DETROIT | COUNTY | WAYNE | SHEET NO. | 13 | TOTAL SHEETS | 13 |
| ROUTE | 82257 | CITY | DETROIT | SHEET NO. | 13 | TOTAL SHEETS | 13 |

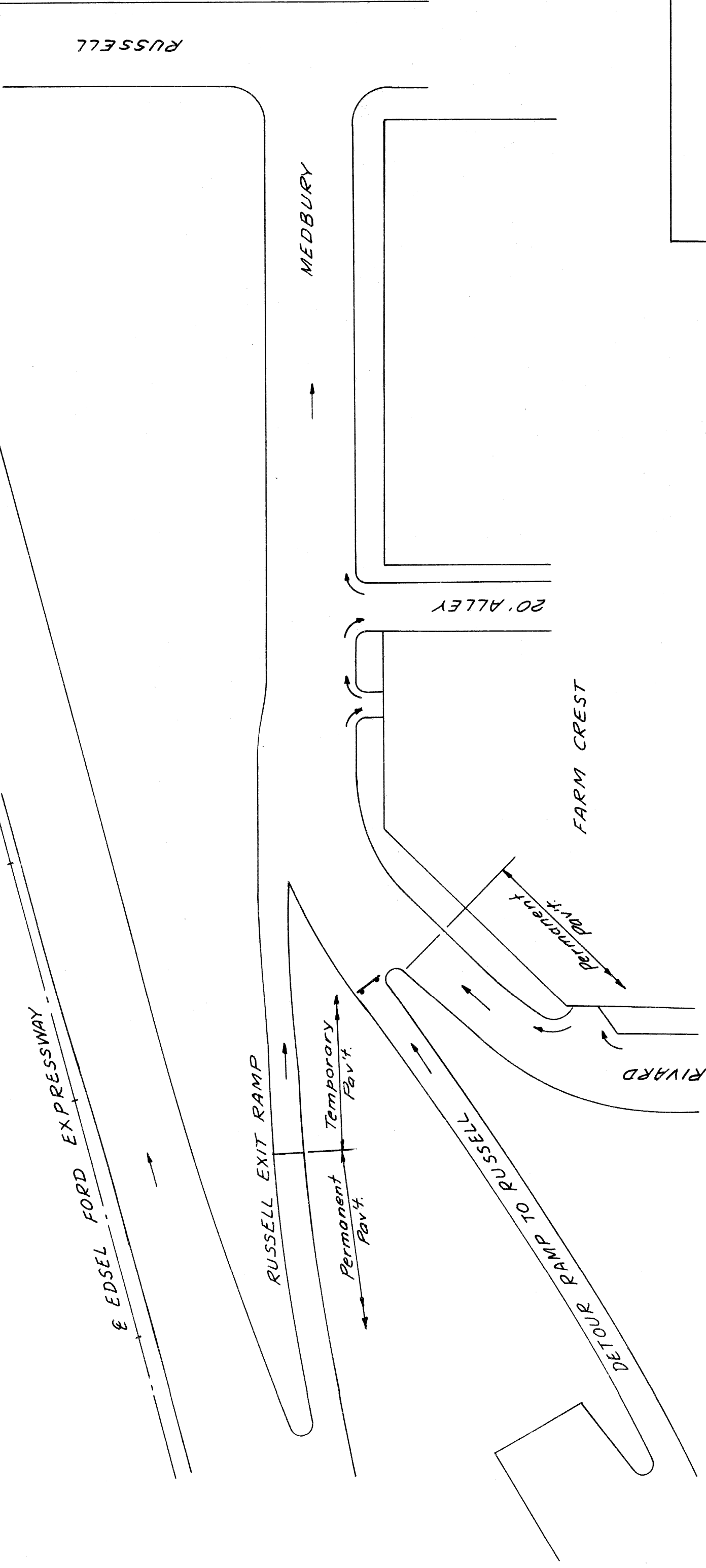


- ① Construct Piers 6 SE, 7 SE, 8 SE & 9 NE and Bridge between these Piers
- ② Construct remainder of Russell Exit Ramp and a portion of the Service Drive

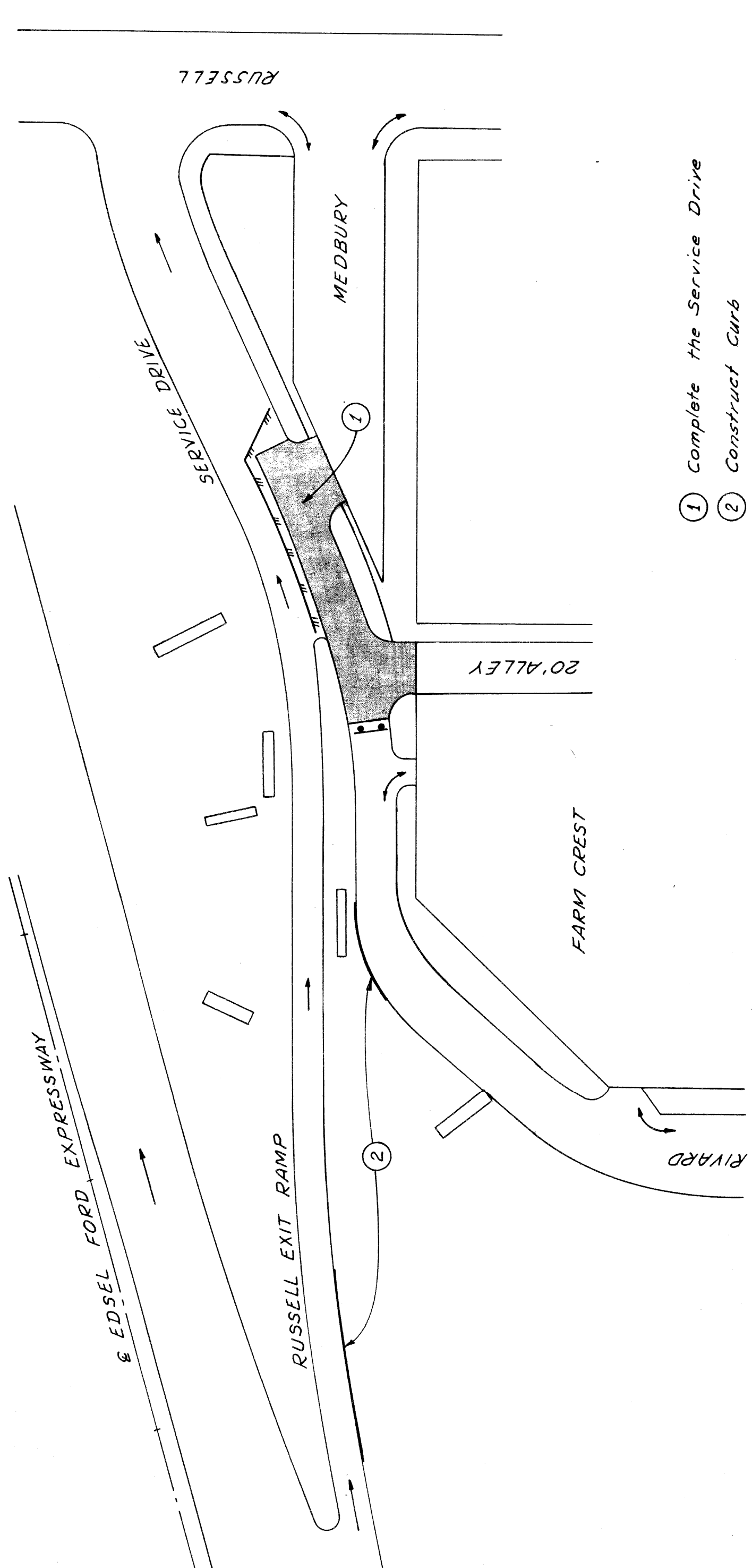
STAGE II

LEGEND

- Proposed Pavement
- Temp. Bit Conc. Pav't to provide a smooth Connection
- Timber Curb & Snow Fence
- Barricade

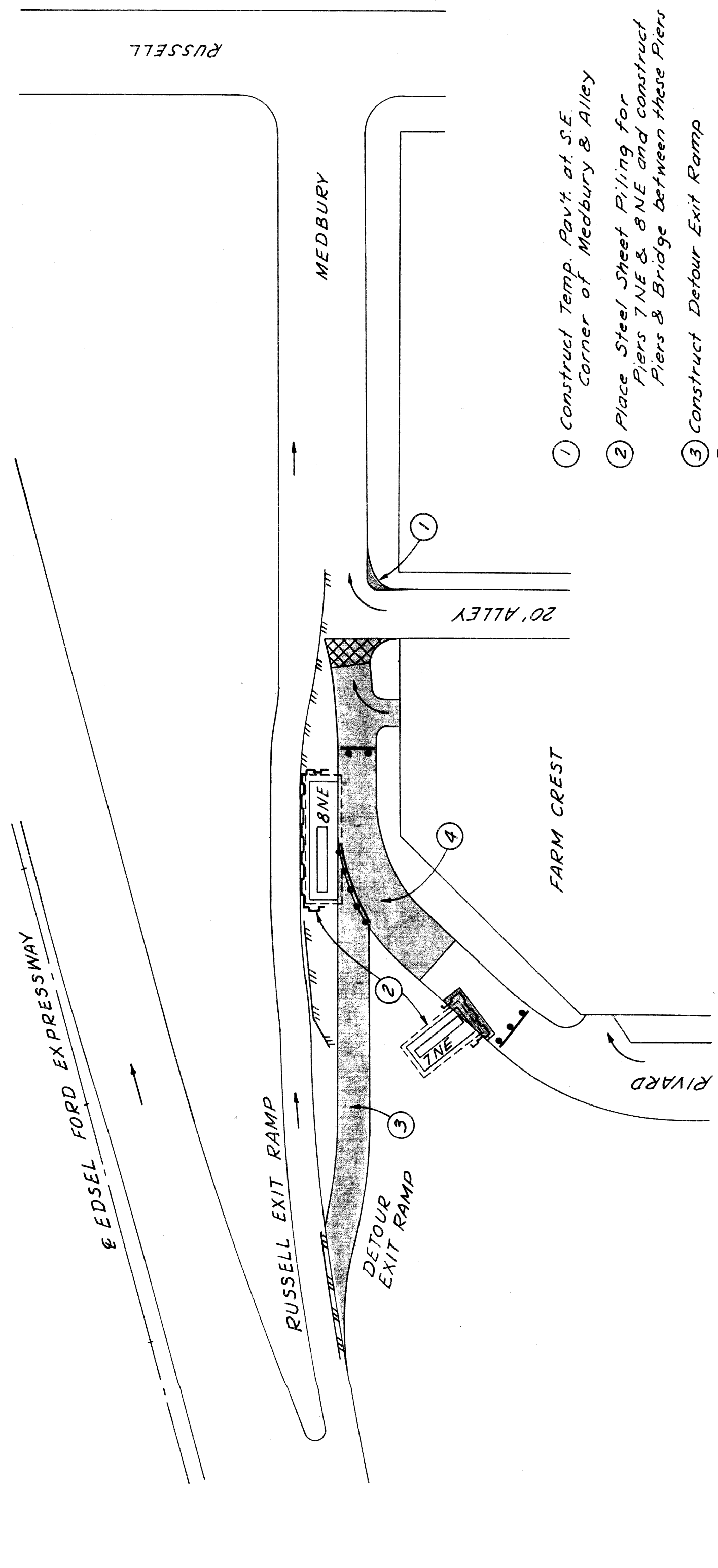


PRIOR TO CONTRACT 13



- ① Complete the Service Drive
- ② Construct Curb

STAGE III



- ① Construct Temp. Pav't at S.E. Corner of Medbury & Alley
- ② Place Steel Sheet Piling for Piers 7 NE & 8 NE and construct Piers & Bridge between these Piers
- ③ Construct Detour Exit Ramp
- ④ Construct a portion of the Service Drive & Temp. Connection to Medbury

STAGE I

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | | |
|------|------|----|------------------|
| DATE | 7/62 | BY | F. A. CALTABIANO |
| DATE | 7/62 | BY | R. A. COMY |
| DATE | 7/62 | BY | M. H. HARRIS |
| DATE | 7/62 | BY | U. L. WICKREY |

| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------|---------------|-----------|--------------|
| MI. | 1757/8253 | 7/2 | 12 |
| CITY | Wayne Detroit | | |
| ROUTE | 82251 | | |

| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------|---------------|-----------|--------------|
| MI. | 1757/8253 | 7/2 | 12 |
| CITY | Wayne Detroit | | |
| ROUTE | 82251 | | |

QUANTITIES

Removing Trees 8" x 18" --- 9 Each
 Removing Trees 13" x 24" --- 24 Each
 Removing Trees 24" x 36" --- 4 Each
 Abandoning Catch Basins --- 3 Each
 Abandoning Manholes --- 3 Each
 Removing Sidewalk --- 333 Sq Yds
 Removing Pavement --- 622.7 Sq Yds
 Removing D.S.P. Tract Strip --- 2 1/2 Sq Yds
 Removing Masonry and Concrete --- 82 Cu Yds

**EARTHWORK: For Basements and Abandoned
 Miscellaneous Structures**

Excavation (Including Removal of Debris
 from Basements) --- 3465 Cu Yds
 Backfill --- 3465 Cu Yds

Backfill

Porous Material - Grade A
 (Compacted in Place) --- 5 Cu Yds
 Select Yellow Clay Backfill
 (Compacted in Place) --- 3460 Cu Yds

Embankment (233 x 25%) --- 291 Cu Yds

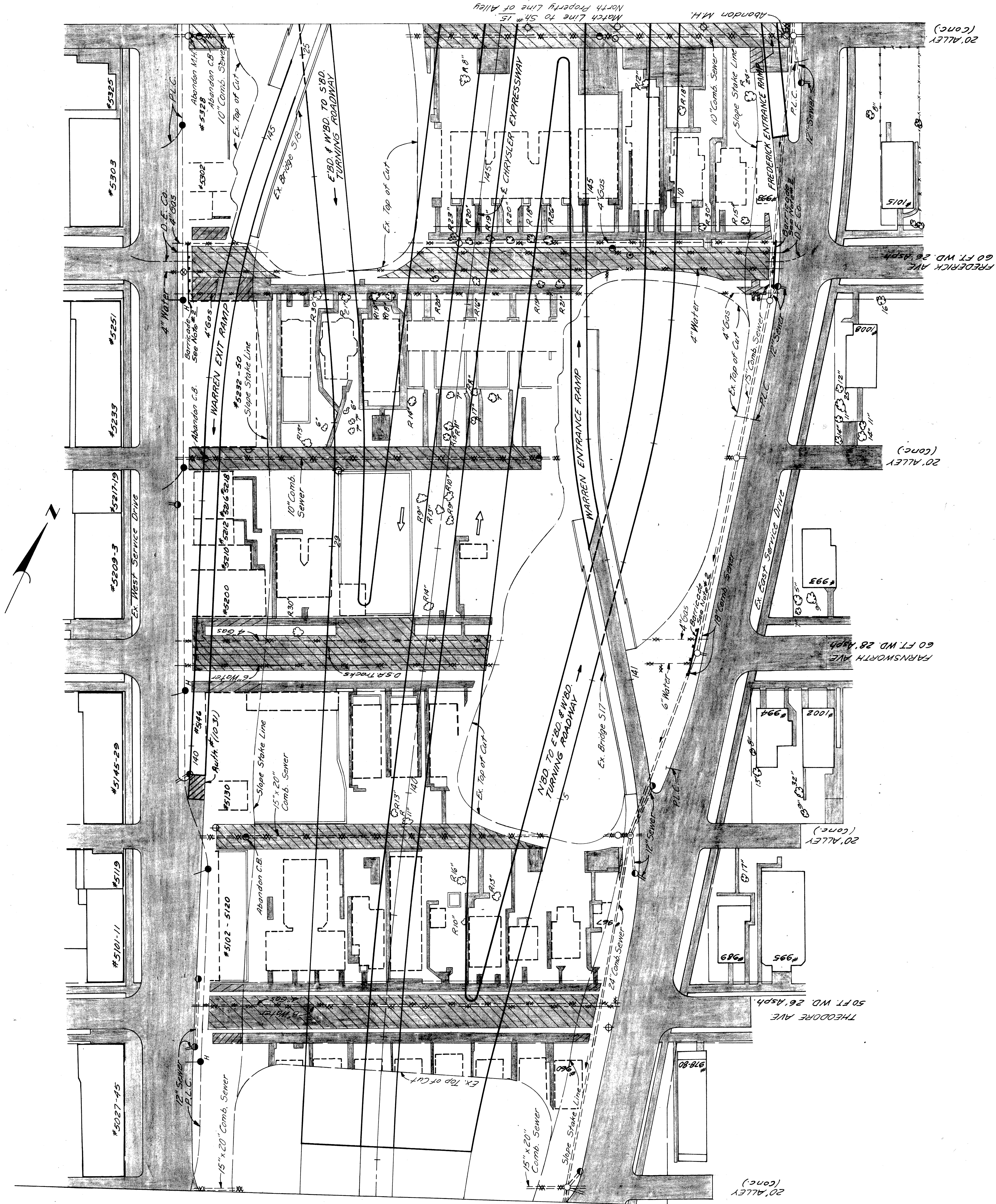
(*) To be obtained from Roadway
 Excavation Sh. # 36

LEGEND

- Removing Curb and Gutter
- Removing Sidewalk
- Removing Pavement
- Regrading
- Proposed Pavement
- Existing Foundation Wall
- Existing Basement
- Existing Concrete Slab
- Removing Trees
- Save Trees

NOTES:
 1. For Typical Cross Section of Embankment
 and Backfill See Sh. # 6

2. Remove Semi-Permanent Barricade.
 This work is incidental to Contract.



PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|-----|---------------|
| 7/2 | J. V. WICKREY |
| 7/2 | G. M. SMITHS |
| 7/2 | N. J. KANWAS |
| 7/2 | W. J. SMITH |
| 7/2 | R. M. KADLE |

REMOVAL PLAN
 BETWEEN ALLEY SOUTH OF THEODORE
 AND ALLEY NORTH OF FREDERICK

FILE NO. 82251E C32
 SHEET 7/2

| STATE | COUNTY | PROJECT | SHEET NO. | TOTAL SHEETS |
|-------|--------|---------|-----------|--------------|
| MI | Wayne | DeTour | 76 | 126 |

| STATE | COUNTY | PROJECT | SHEET NO. | TOTAL SHEETS |
|-------|--------|---------|-----------|--------------|
| MI | Wayne | DeTour | 76 | 126 |

ST. ANTOINE ST.
50' W.D.
24' ASPH.
6" WATER

QUANTITIES

Removing Trees 8"-12" - 212 (100%) - 1 Each
 Removing Trees 13"-24" - 2 (100%) - 2 Each
 Abandoning Catch Basins - 2 (100%) - 2 Each
 Abandoning Manholes - 2 (100%) - 2 Each
 Removing Catch Basins - 2 (100%) - 2 Each
 Removing Manholes - 2 (100%) - 2 Each
 Removing Sidewalk - 2 (100%) - 2 Sq. Yds.
 Removing Pavement - 2 (100%) - 2 Sq. Yds.
 Removing D.S.R. Track Strip - 2 (100%) - 2 Sq. Yds.
 Removing Masonry and Concrete - 1 - 45 Cu. Yds.

EARTHWORK: For Basements and Abandoned Miscellaneous Structures
 Excavation (Including Removing of Debris from Basements) - 144 Cu. Yds.
 Backfill - 144 Cu. Yds.

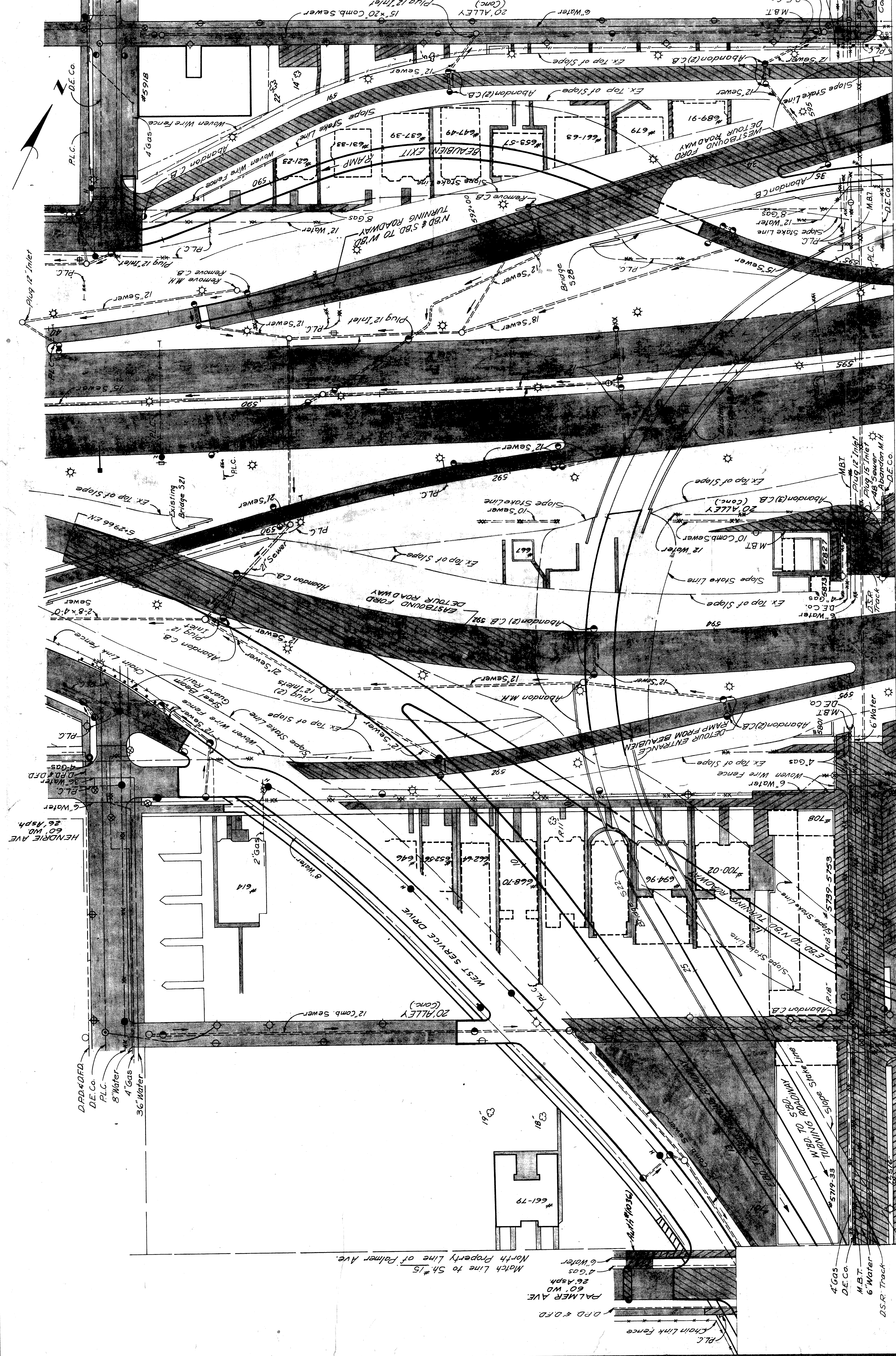
Porous Material - Grade A Auth. # (1012) (Compacted in Place) - 75 Cu. Yds.
 Select Yellow Clay Backfill - 75 Cu. Yds.
 (Compacted in Place) - 75 Cu. Yds.

Embankment (1924 + 25%) - 2405 Cu. Yds.
 (*) To be obtained from Roadway Excavation 514' x 39'

LEGEND

- Removing Curb - [Symbol]
- Removing Sidewalk - [Symbol]
- Replacing Pavement - [Symbol]
- Proposed Pavement - [Symbol]
- Existing Foundation Wall - [Symbol]
- Existing Concrete Slab - [Symbol]
- Removing Trees - [Symbol]
- Save Trees - [Symbol]

NOTES:
 1. For Typical Cross Section of Embankment and Backfill See S14.1E
 2. Plugging of Abandoned Sewers with Brick Bulkheads as shown on the Plan is Incidental to the Contract
 3. All Trees Noted S.(Size) shall be retained. These Trees shall be adequately marked and shall be protected from damage. This work is incidental to the Contract.



Match Line to S14.1E
 East Property Line of Hastings St.

**REMOVAL PLAN
 BETWEEN PALMER AND ALLEY SOUTH OF
 HARPER, AND ST. ANTOINE TO HASTINGS**

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| DATE | BY | REVISION |
|------|-------------|----------|
| 7/62 | J.V. WICKER | REVISED |
| 7/62 | J.V. WICKER | REVISED |
| 7/62 | J.V. WICKER | REVISED |
| 7/62 | J.V. WICKER | REVISED |
| 7/62 | J.V. WICKER | REVISED |

PROJECT NO. 182251E C32
 SHEET NO. 76

| STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
|-------|-----------|--------|-----------|--------------|
| MICH. | 7-5-18633 | | | |
| ROUTE | CITY | COUNTY | SHEET NO. | TOTAL SHEETS |
| 8225E | Detroit | Wayne | 77 | |

QUANTITIES

- Removing Trees 8" - 12" --- 17 Each
- Removing Trees 13" - 24" --- 34 Each
- Removing Trees 25" - 36" --- 5 Each
- Abandoning Catch Basins --- 15 Each
- Abandoning Manholes --- 2 Each
- Removing Sidewalk --- 2233 Sq. Yds.
- Removing Pavement --- 10433 Sq. Yds.
- Removing Underground Storage Tanks --- 2 Each
- Removing Masonry and Concrete --- 40 Cu. Yds.

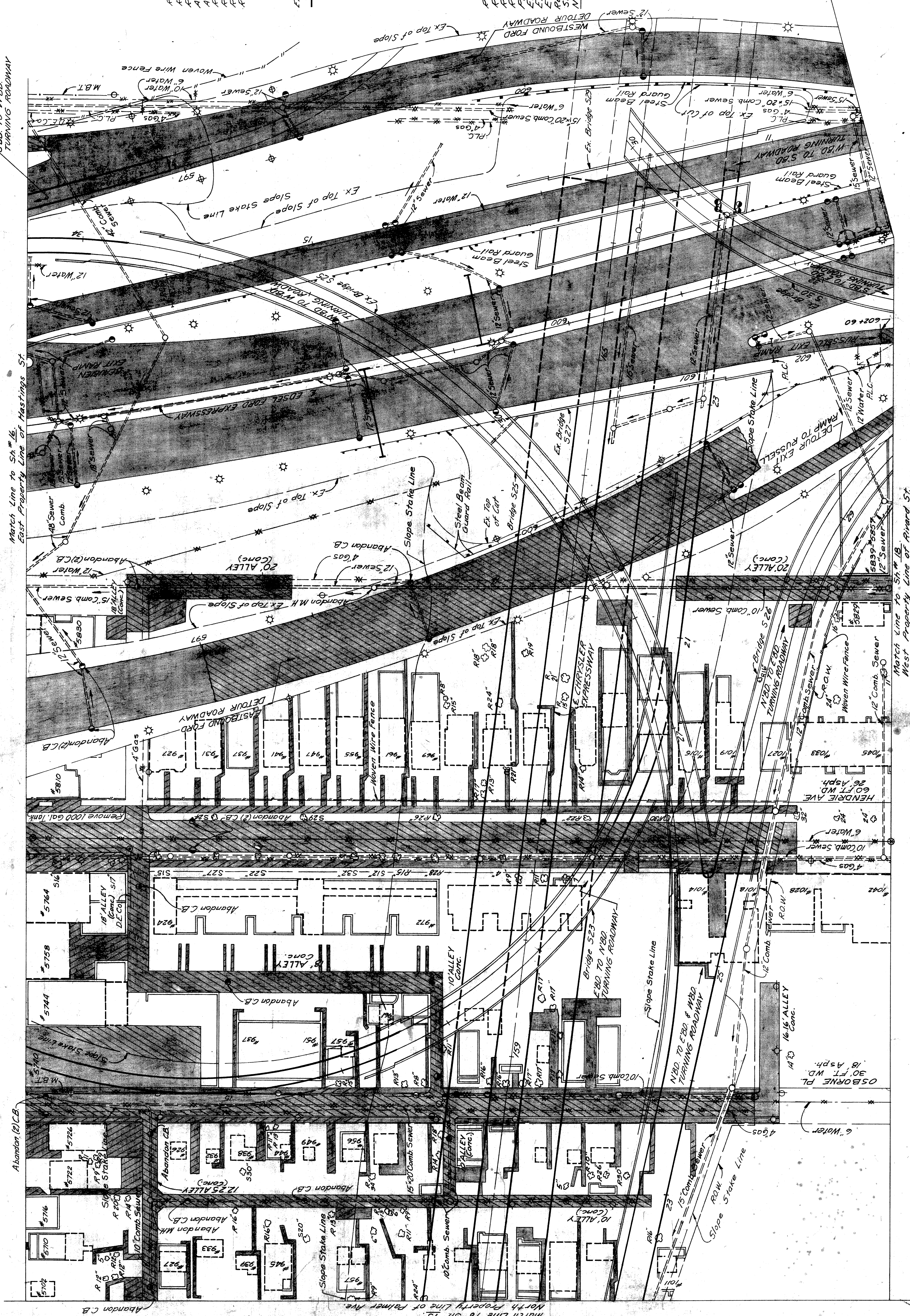
FARTHWORK: For Basements and Abandoned

- Excavation (Including Removal of Debris from Basements) --- 569 Cu. Yds.
- Backfill --- 10433 Cu. Yds.
- Porous Material - Grade A (Compacted in Place) --- None
- Select Yellow Clay Backfill (Compacted in Place) --- 5764 Cu. Yds.
- Embankment (73% + 25%) --- 2164 Cu. Yds.
- (*) To be obtained from Roadway Excavation Sta. # 40

LEGEND

- Removing Curb and Gutter
- Removing Sidewalk
- Replacing Pavement
- Proposed Pavement
- Existing Foundation Wall
- Existing Basement
- Removing Concrete Slab
- Save Trees

NOTES:
 1. For Typical Cross Section of Embankment and Backfill See Sta. 16.
 2. All Trees, Noted S-(Size) shall be retained. These Trees shall be adequately marked and shall be protected from damage. This work is incidental to the Contract.



Match Line to S4#16
 West Property Line of Rivard St.

**REMOVAL PLAN
 BETWEEN PALMER AND ALLEY SOUTH
 OF HARPER, AND HASTINGS TO RIVARD**

FILE NO. **8225E C32**
 SHEET 77 OF 77

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|---------|------------------------|
| DATE | BY |
| 7/12/22 | M. R. FOLLO |
| 7/12/22 | J. WICKREY |
| 7/12/22 | J. WICKREY & R. HARRIS |
| 7/12/22 | M. R. FOLLO |

| STATE | PROJECT | ROUTE | SECTION | SHEET NO. | TOTAL SHEETS |
|-------|---------|---------|---------|-----------|--------------|
| MICH. | 72251 | Medbury | Detour | 18 | 18 |

| STATE | PROJECT | ROUTE | SECTION | SHEET NO. | TOTAL SHEETS |
|-------|---------|---------|---------|-----------|--------------|
| MICH. | 72251 | Medbury | Detour | 18 | 18 |

QUANTITIES

Abandoning Catch Basins - 4 (12' x 20') - 2 Each
 Removing Sidewalk - 277 Sg. Yds.
 Replacing Pavement - 2225 Sg. Yds.
 Replacing Curb - 216 Sg. Yds.
 Replacing Sidewalk - 216 Sg. Yds.
 Replacing Pavement - 50 Sg. Yds.
 Replacing Curb - 15 Lin. Ft.

EARTHWORK: For Basements and Abandoned Backfill Miscellaneous Structures

Perous Material - Grade A
 (Compacted in Place) - 78 Cu. Yds.
 Embankment (147 + 25%) - 185 Cu. Yds. (*)

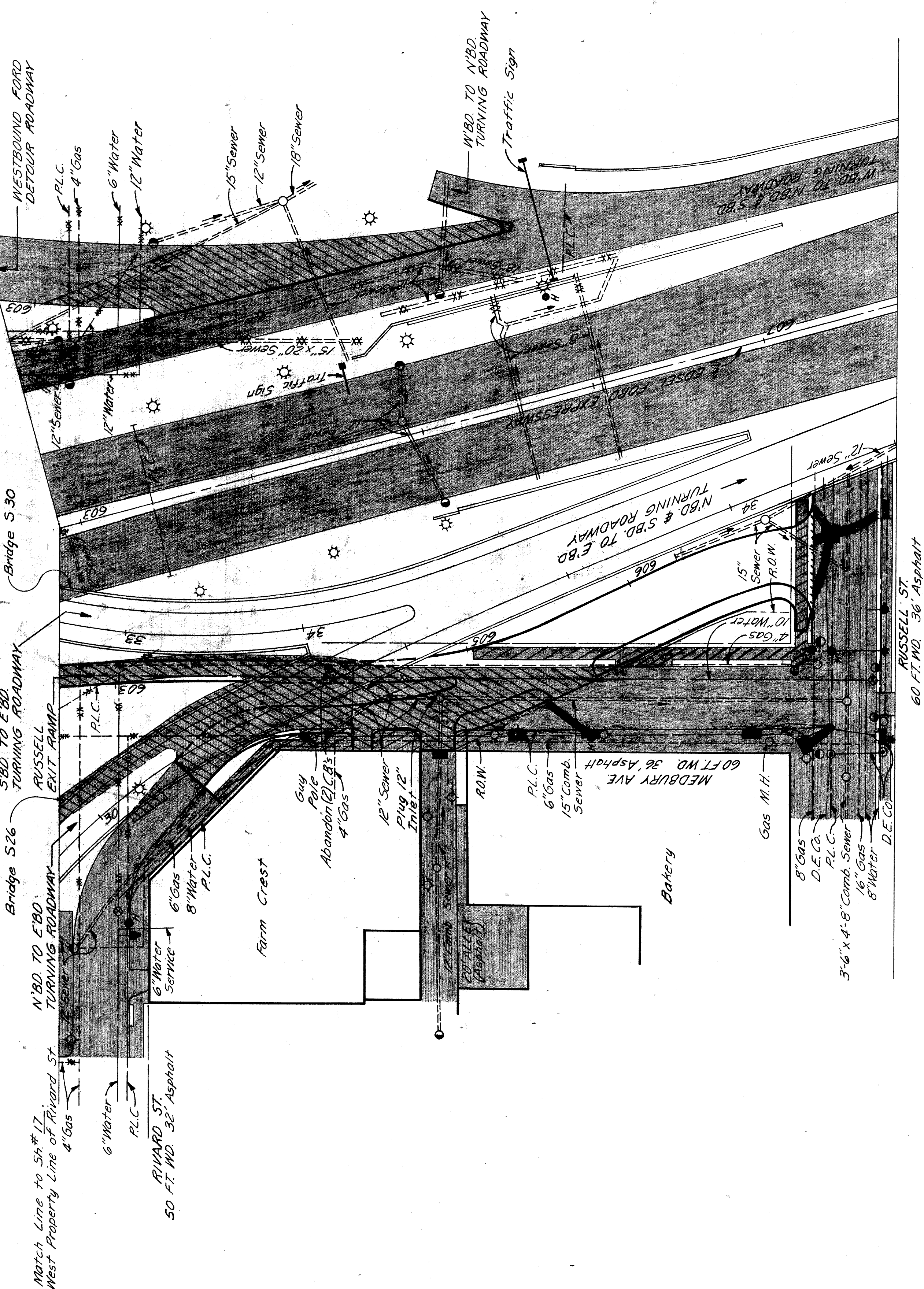
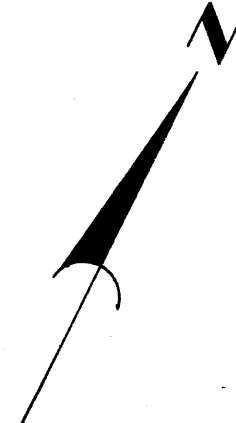
(*) To Be Obtained from Roadway Excavation See Sh#40
 (**) Includes Removal of Temporary Russell Detour Ramp.

LEGEND

Removing Curb
 Removing Sidewalk
 Replacing Pavement
 Replacing Curb
 Proposed Pavement
 Existing Foundation Wall
 Existing Concrete Slab
 Replacing Sidewalk

NOTES:

1. For Typical Cross Section of Embankment and Backfill See Sh#16.
 2. The plugging of Abandoned Sewer with Brick Bulkhead as shown on the Plan is Incidental to the Contract.



PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

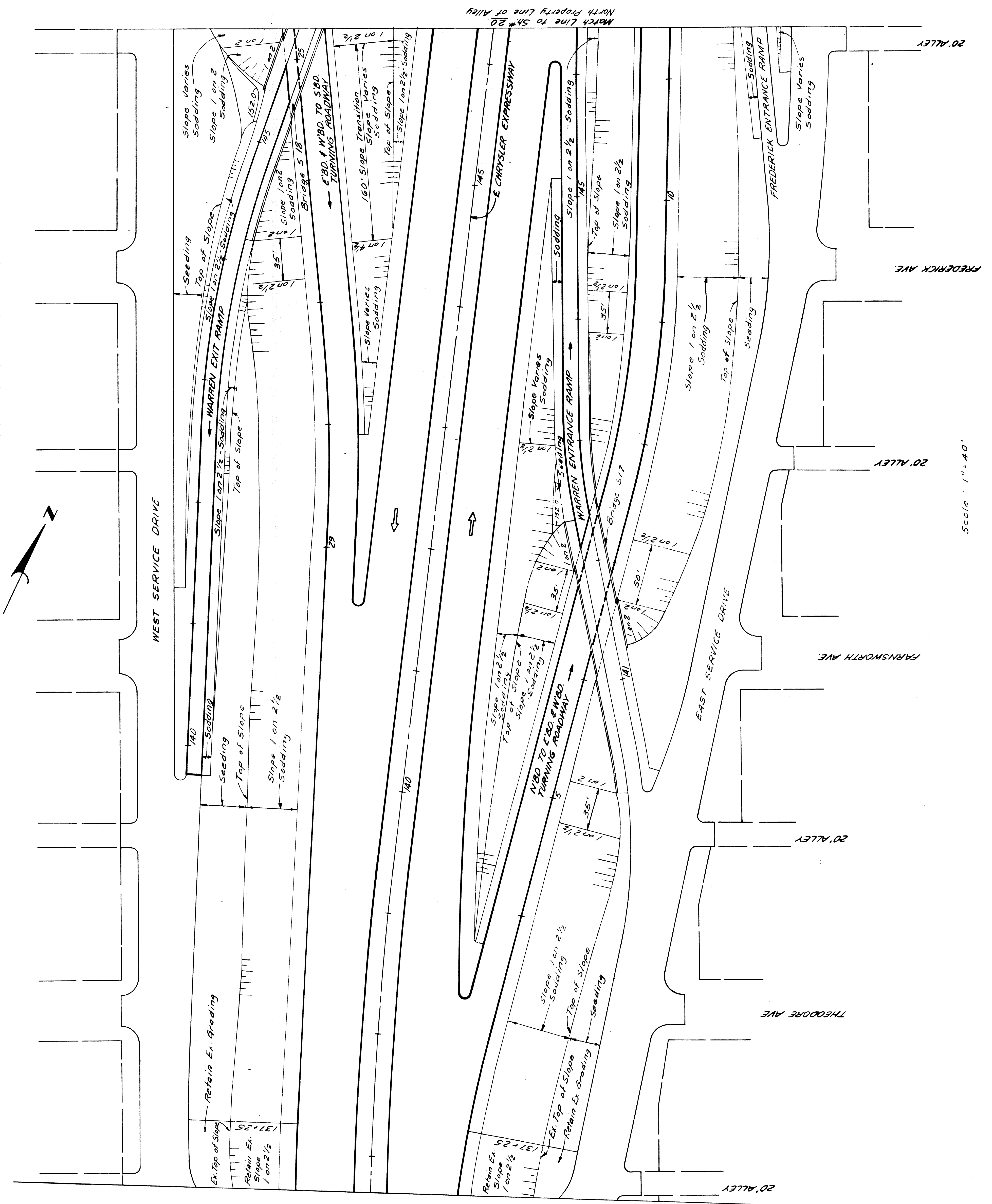
| | |
|------|---------------|
| 7/62 | D. V. WICKLEY |
| 7/62 | G. M. SMITS |
| 7/62 | C. R. HOPKINS |
| 7/62 | W. R. HILL |
| 7/62 | W. R. HILL |

REMOVAL PLAN
 BETWEEN MEDBURY AND EDEL FORD
 EXPRESSWAY, AND RUSSELL TO RIVARD

FILE NO. PROJECT SHEET
 82251E C32 18

| | | | | | |
|----------|--------------|------------|---------|-----------|--------------|
| DIV. NO. | STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
| 4 | MD | 175-100-20 | 1958 | 19 | 20 |
| ROUTE | CITY | COUNTY | PROJECT | SHEET NO. | TOTAL SHEETS |
| 0225 | Wayne Detour | 19 | | | |
| DIV. NO. | STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
| 4 | MD | 175-100-20 | 1958 | 19 | 20 |
| ROUTE | CITY | COUNTY | PROJECT | SHEET NO. | TOTAL SHEETS |
| 0225 | Wayne Detour | 19 | | | |

QUANTITIES
 Class A Sodding 15,005.59 Yds. Auth. # (1067)
 Class A Seeding 11 Acres " (1058)



North Property Line to SH # 20

Scale: 1" = 40'

GRADING PLAN
 BETWEEN ALLEY SOUTH OF THEODORE
 AND ALLEY NORTH OF FREDERICK

2251E C32
 15-05-59
 11 Acres " (1058)

PLANS PREPARED BY
 CIVIL ENGINEER
 STATE OF MARYLAND
 BUREAU OF HIGHWAYS AND TRANSPORTATION

7/62
 J. V. WICKERY
 C. R. HOPKINS
 M. E. ROYER
 R. H. HARRIS

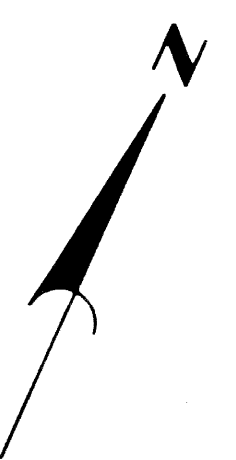
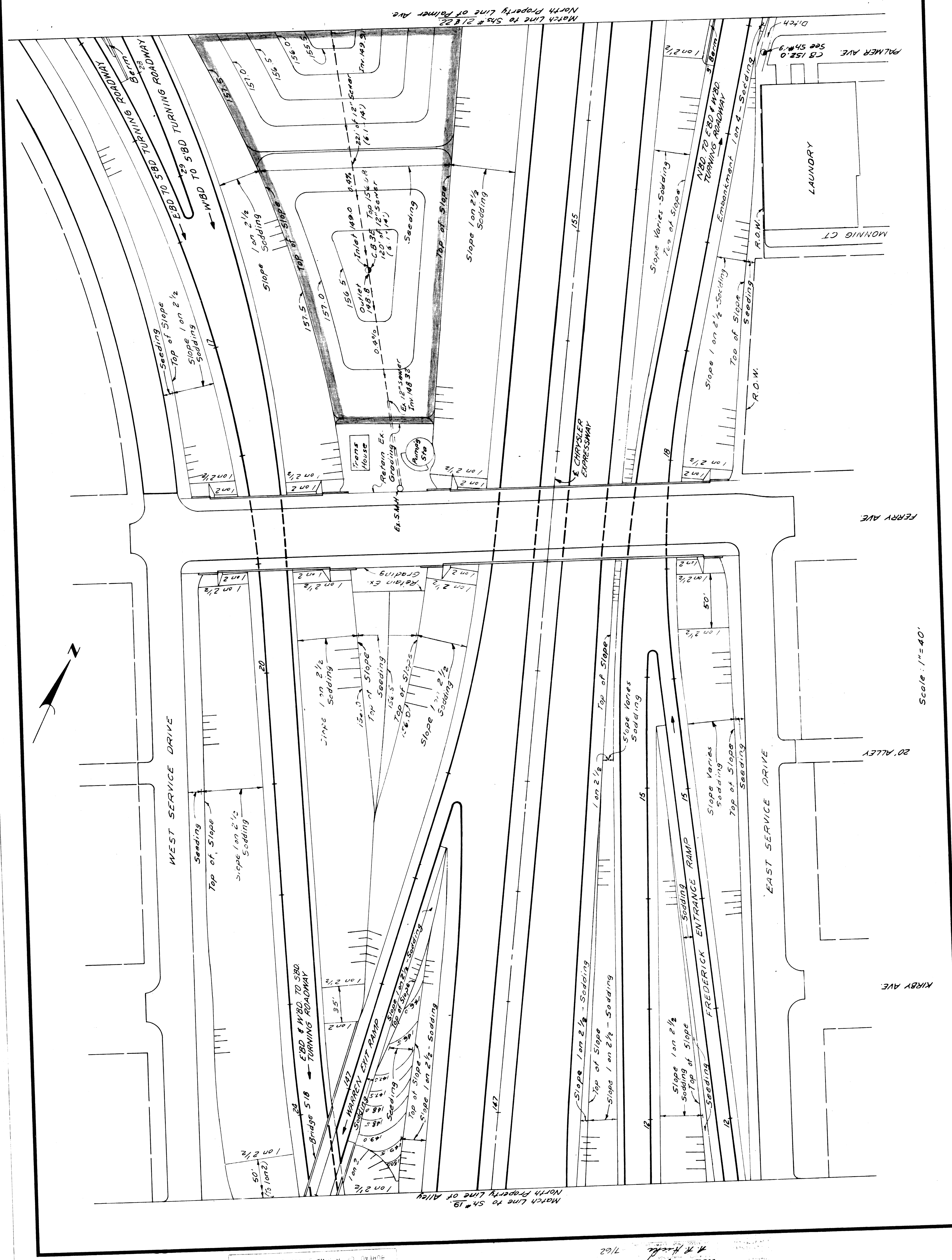
| | | | |
|---------|-----------|-------------|----------|
| STATE | MASS. | PROJECT NO. | 225 |
| CITY | Worcester | SHEET NO. | 20 |
| COUNTY | Worcester | DATE | 10/25/20 |
| PROJECT | GRADING | SCALE | 1" = 40' |

QUANTITIES
 Class A Sodding 24,168 Sq. Yds. (1067)
 Class A Seeding 24,366 Sq. Yds.
 Earthwork 1.9 Acres
 Embankment to be obtained From Sta. #38, 4,089 + 25% = 6236 Cu. Yds.

Note
 The embankment quantity listed on this sheet is required to provide adequate drainage of areas occurring outside the limits of roadway excavations and/or embankment. These areas are outlined by a shaded line.

**GRADING PLAN
 BETWEEN ALLEY SOUTH OF KIRBY
 AND NORTH PROPERTY LINE OF PALMER**

PROJECT NO. 225 SHEET NO. 20
 DATE 10/25/20



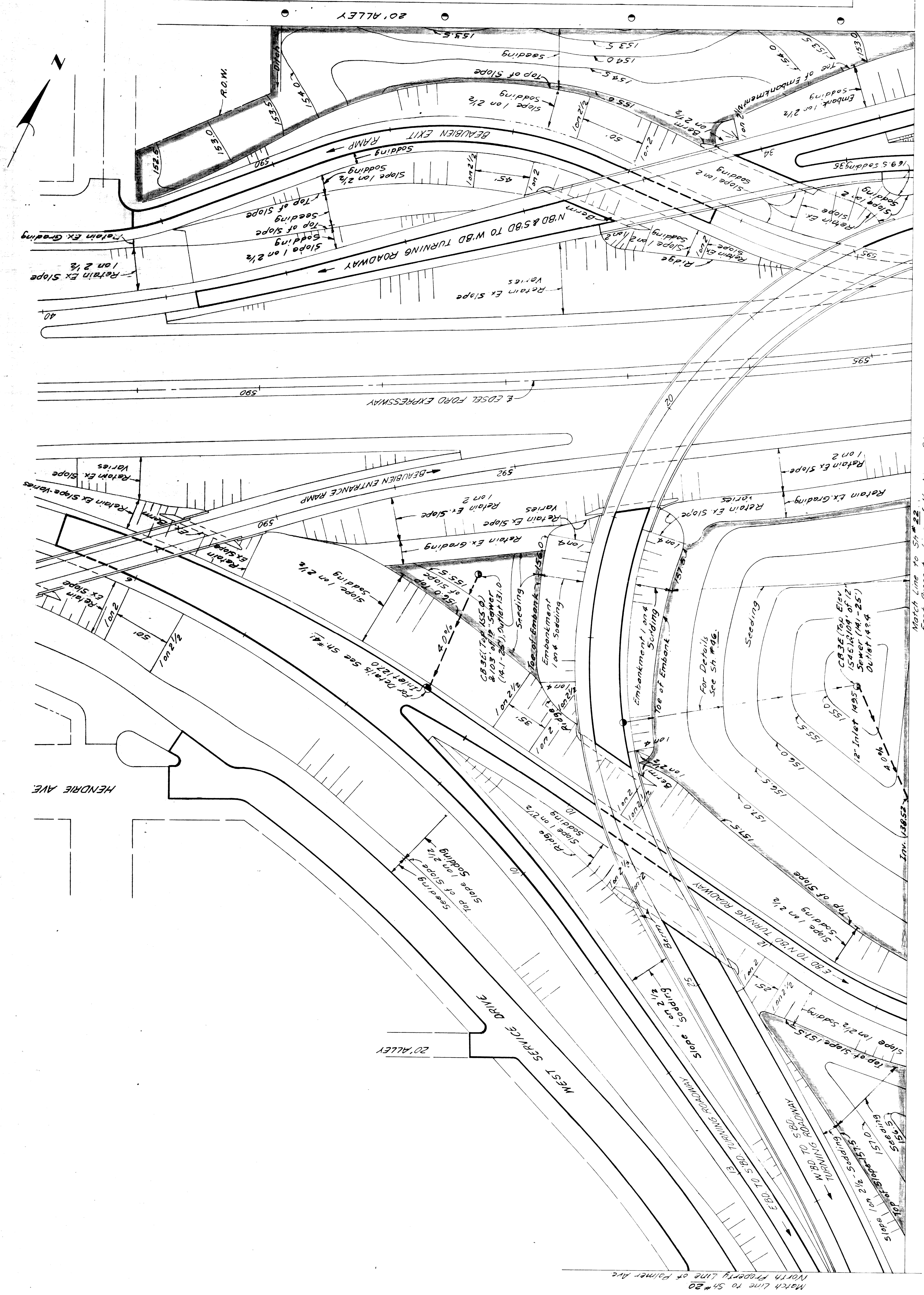
Scale: 1" = 40'

PLANS PREPARED BY
 CIVIL ENGINEERS
 100 STATE STREET
 WASHINGTON, D.C. 20001

J.V. WICKERY
 L. KNOX
 C.R. HOPKINS
 M. R. HUBBARD
 7/62 7/62 7/62 7/62

| | | | |
|--------|------------|-------|-------|
| STATE | PROJ. NO. | YEAR | SHEET |
| MICH. | 7225(69)53 | | 21 |
| COUNTY | CITY | SHEET | 21 |
| Wayne | Detroit | | |
| ROUTE | PROJ. NO. | YEAR | SHEET |
| 8225 | | | 21 |
| STATE | PROJ. NO. | YEAR | SHEET |
| MICH. | 7225(69)53 | | 21 |
| COUNTY | CITY | SHEET | 21 |
| Wayne | Detroit | | |
| ROUTE | PROJ. NO. | YEAR | SHEET |
| 8225 | | | 21 |

ST. ANTOINE ST.



QUANTITIES
 Class A Sodding 18,103 Cu. Yds. (1067)
 Class A Seeding 16,203 Sq. Yds.
 Earthwork 2.7 Acres
 Auth. (1058)
 Embankment to be obtained from Sh #40, 15,624 + 25% = 19,792 #
 from Sh #21, 5,209 + 25% = 6,761
 Total Embankment = 26,553 Cu. Yds.
 Select Yellow Clay to be obtained from Sh #41 123 Cu. Yds.

Note
 The embankment quantity listed on this sheet is required to provide adequate drainage of areas occurring outside the limits of roadway excavations and/or embankments. These areas are outlined by a shaded line.

Scale: 1"=40'

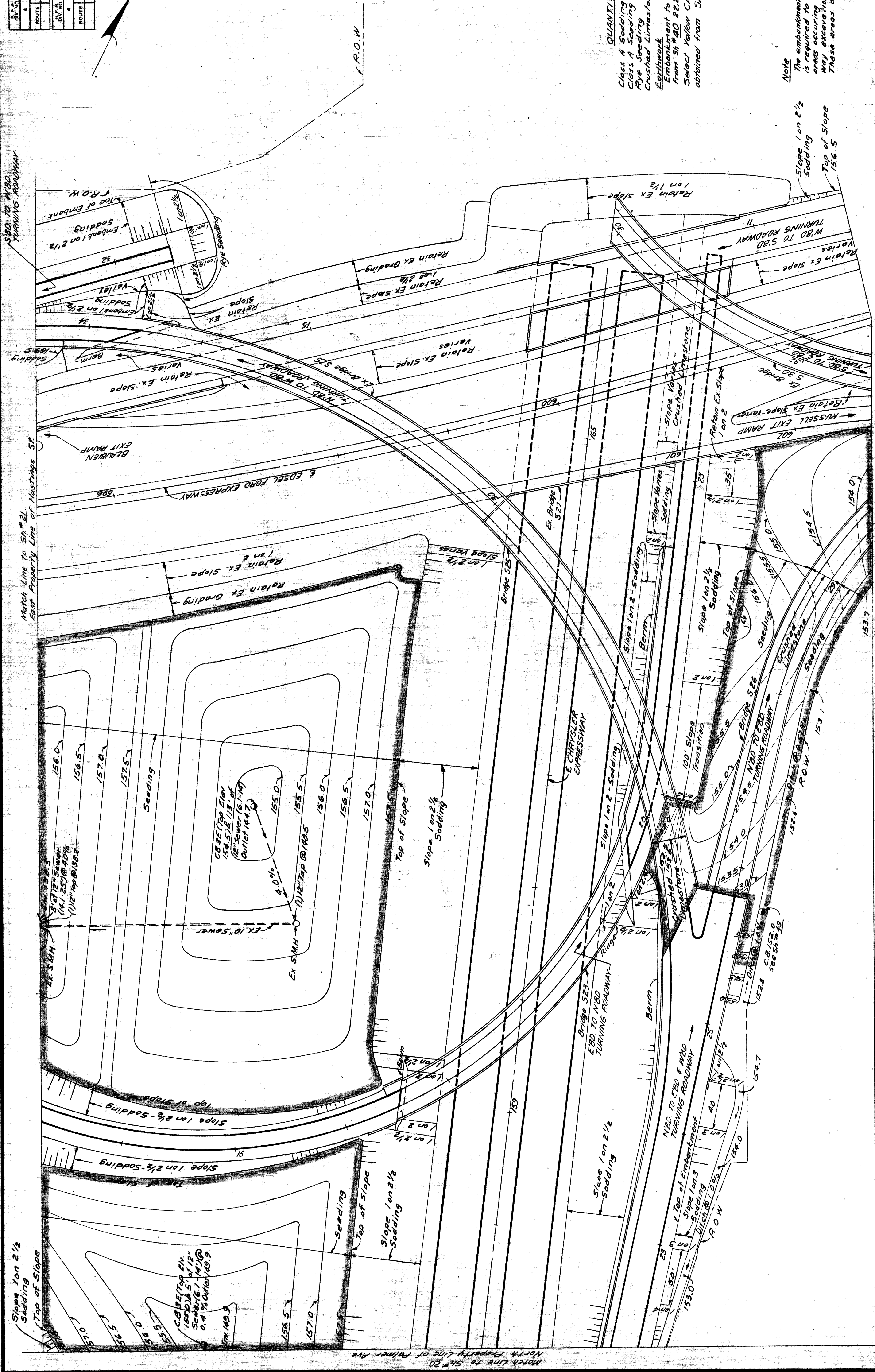
GRADING PLAN
BETWEEN PALMER AND ALLEY SOUTH OF HARPER, AND ST. ANTOINE TO HASTINGS

PLAN NO. 82251E C32
 SHEET 21 OF 21

PLANS PREPARED BY
 CITY OF DETROIT
 DIVISION OF PUBLIC WORKS
 CIVIL ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

7/62
 L.V. WICKERY
 7/62
 J. J. KNOX
 7/62
 C.R. HORNBY
 7/62
 7/62
 7/62

| | | | |
|-----------|-----------|-------|--------------|
| STATE | PROJ. NO. | SHEET | TOTAL SHEETS |
| MICH. | 725/6033 | 22 | 22 |
| CITY | COUNTY | ROUTE | PROJECT |
| WASHTENAW | WASHTENAW | 8225E | GRADING PLAN |



QUANTITIES
 Class A Sodding 12,883 Sq. Yds.
 Class B Sodding 15,982 Sq. Yds.
 Eye Sodding 87 1/2 Acres (1052)
 Crushed Limestone 490.3 Tons
 Earthwork
 Embankment to be obtained from Sh #40 22,285 ± 25% = 27,869 Cu. Yds.
 Select Yellow Clay to be obtained from Sh #40 1 Cu. Yd.

Note
 The embankment quantity listed on this sheet is required to provide adequate drainage of areas occurring outside the limits of road-way excavations and/or embankments. These areas are outlined by a shaded line.

**GRADING PLAN
 BETWEEN PALMER AND ALLEY SOUTH
 OF HARPER, AND HASTINGS TO RIVARD**

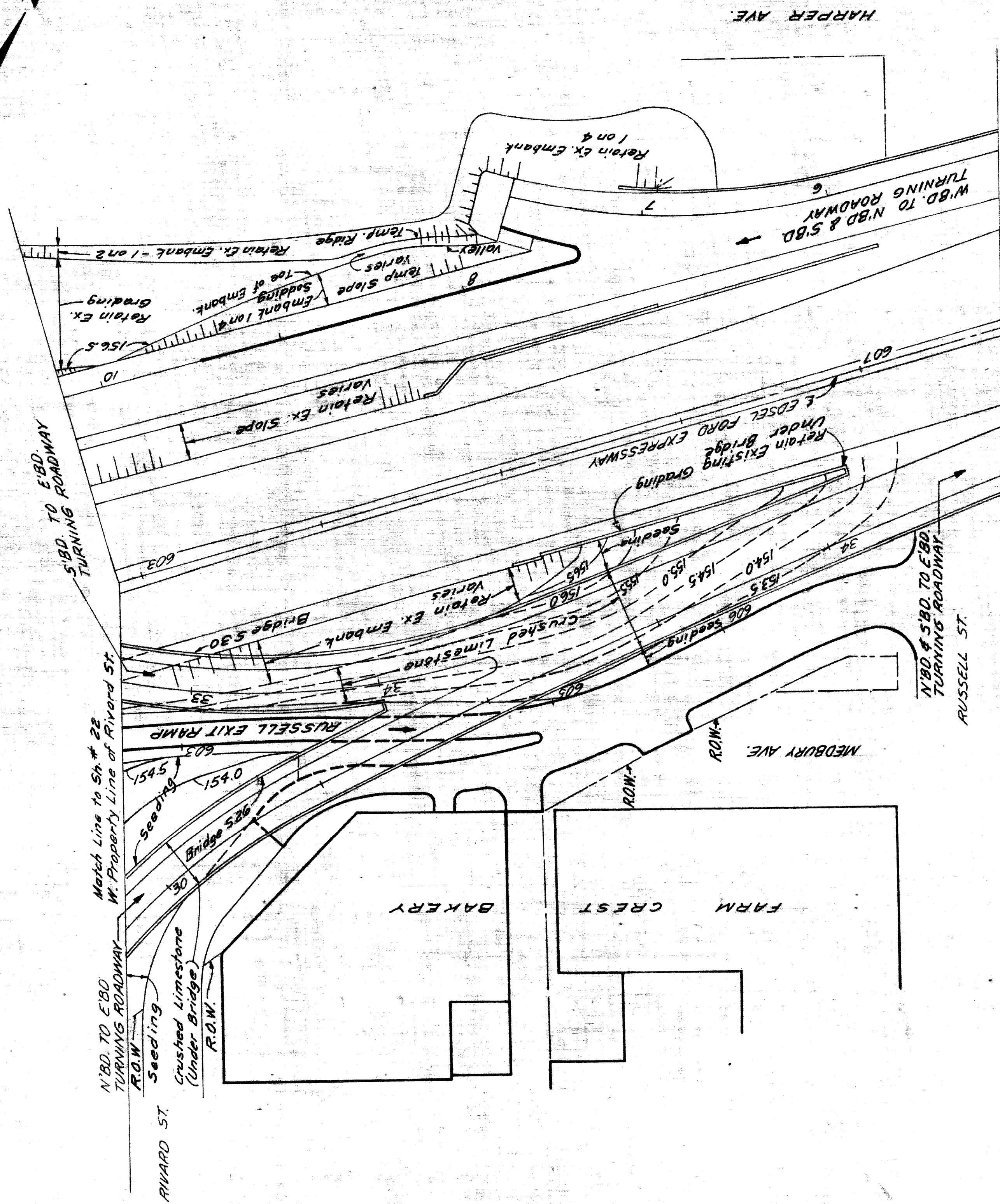
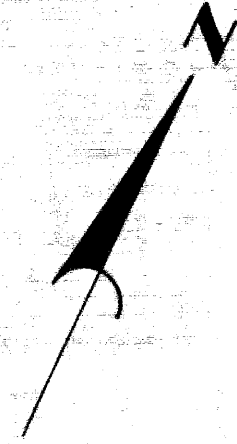
8225E C32
 SHEET NO. 22
 TOTAL SHEETS 22

Scale: 1" = 40'

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS' OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | |
|------|--------------|-------------|
| 7/62 | U.V. WICKER | DESIGNED BY |
| 7/62 | J. KNOX | CHECKED BY |
| 7/62 | C.R. HOPKINS | APPROVED |
| 7/62 | | RECOMMENDED |

| | | | | | |
|--------|--------|-----------|----------|-------------|----|
| STATE | MICH. | PROJ. NO. | 725/6053 | SHEET NO. | 23 |
| COUNTY | Wayne | CITY | Detroit | SHEET TOTAL | 23 |
| ROUTE | 82251E | PROJECT | | | |



QUANTITIES
 Class A Seeding 363 Sq. Yds. (1067)
 Class A Seeding .2 Acres (1050)
 Crushed Limestone 426.2 Tons

Scale: 1" = 40'

**GRADING PLAN
 BETWEEN MEDBURY AND HARPER,
 AND RUSSELL TO RIVARD**

FILE NO. 82251E C32
 SHEET NO. 23

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CIVIL ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | |
|------|---------------|---------------|
| 7/62 | U.P. MICKLEY | EQUIP. LEADER |
| 7/62 | V. KNOX | DRAWN BY |
| 7/62 | C. R. HOPKINS | CHECKED BY |
| 7/62 | W. H. HOPKINS | DESIGNED BY |
| 7/62 | R. H. WILSON | IN CHARGE |

| ROUTE | PROJ. NO. | STATE | MICH. | PROJ. NO. | STATE | MICH. | PROJ. NO. | STATE | MICH. | PROJ. NO. | STATE | MICH. | PROJ. NO. | STATE | MICH. |
|-------|-----------|-------|-------|-----------|-------|-------|-----------|-------|-------|-----------|-------|-------|-----------|-------|-------|
| 82251 | 125 | MI | 125 | 125 | MI | 125 | 125 | MI | 125 | 125 | MI | 125 | 125 | MI | 125 |

CURVE DATA F.B.D. TO S.B.D. TURNING ROADWAY

CURVE 3 E.S. Curve 4.65
 $\Delta = 33^{\circ}55'55''$ D = 57.19-27.7
 $\Delta = 8^{\circ}34'49''$
 $D = 5^{\circ}06'56.50''$ D = 57.00-00.00
 $R = 1120'$ R = 145.92
 $T = 34.69'$ T = 33.25'
 $L = 66.82'$ L = 66.48'
 $E = 50.96'$ E = 0.48'
 $PC = 12+81.45$ PC = 23+22.17
 $PI = 16+23.14$ PI = 23+53.43
 $PT = 19+44.73$ PT = 28+10.49
 $PT = 19+44.73$ PT = 28+10.49

CURVE 4 E.S. Curve 5.57
 $\Delta = 16^{\circ}49'45''$ D = 72.26-42.2
 $D = 7^{\circ}50'00''$ D = 57.00-00.00
 $R = 763.94'$ R = 145.92
 $T = 123.01'$ T = 124.94
 $L = 224.39'$ L = 249.90
 $E = 8.71'$ E = 6.79'
 $PC = 142+61.94$ PC = 149+83.27
 $PI = 143+74.35$ PI = 151+08.21
 $PT = 144+65.73$ PT = 152+32.17

CURVE DATA EXIT RAMP

$\Delta = 16^{\circ}49'45''$ D = 72.26-42.2
 $D = 7^{\circ}50'00''$ D = 57.00-00.00
 $R = 763.94'$ R = 145.92
 $T = 123.01'$ T = 124.94
 $L = 224.39'$ L = 249.90
 $E = 8.71'$ E = 6.79'
 $PC = 142+61.94$ PC = 149+83.27
 $PI = 143+74.35$ PI = 151+08.21
 $PT = 144+65.73$ PT = 152+32.17

CURVE DATA ENTRANCE RAMP TO EXPRESSWAY

$\Delta = 10^{\circ}33'25''$ D = 57.57-34.8
 $\Delta = 1^{\circ}36'45''$
 $D = 14^{\circ}19'26.00''$ D = 57.00-00.00
 $R = 400'$ R = 145.92
 $T = 37.07'$ T = 58.65'
 $L = 73.93'$ L = 177.19'
 $E = 1.71'$ E = 0.19'
 $PC = 142+45.22$ PC = 145+96.38
 $PI = 142+95.29$ PI = 146+55.03
 $PT = 143+19.16$ PT = 147+13.57
 $PT = 143+19.16$ PT = 147+13.57

CURVE DATA N.B.D. TO E.B.D. & W.B.D. TURNING ROADWAY

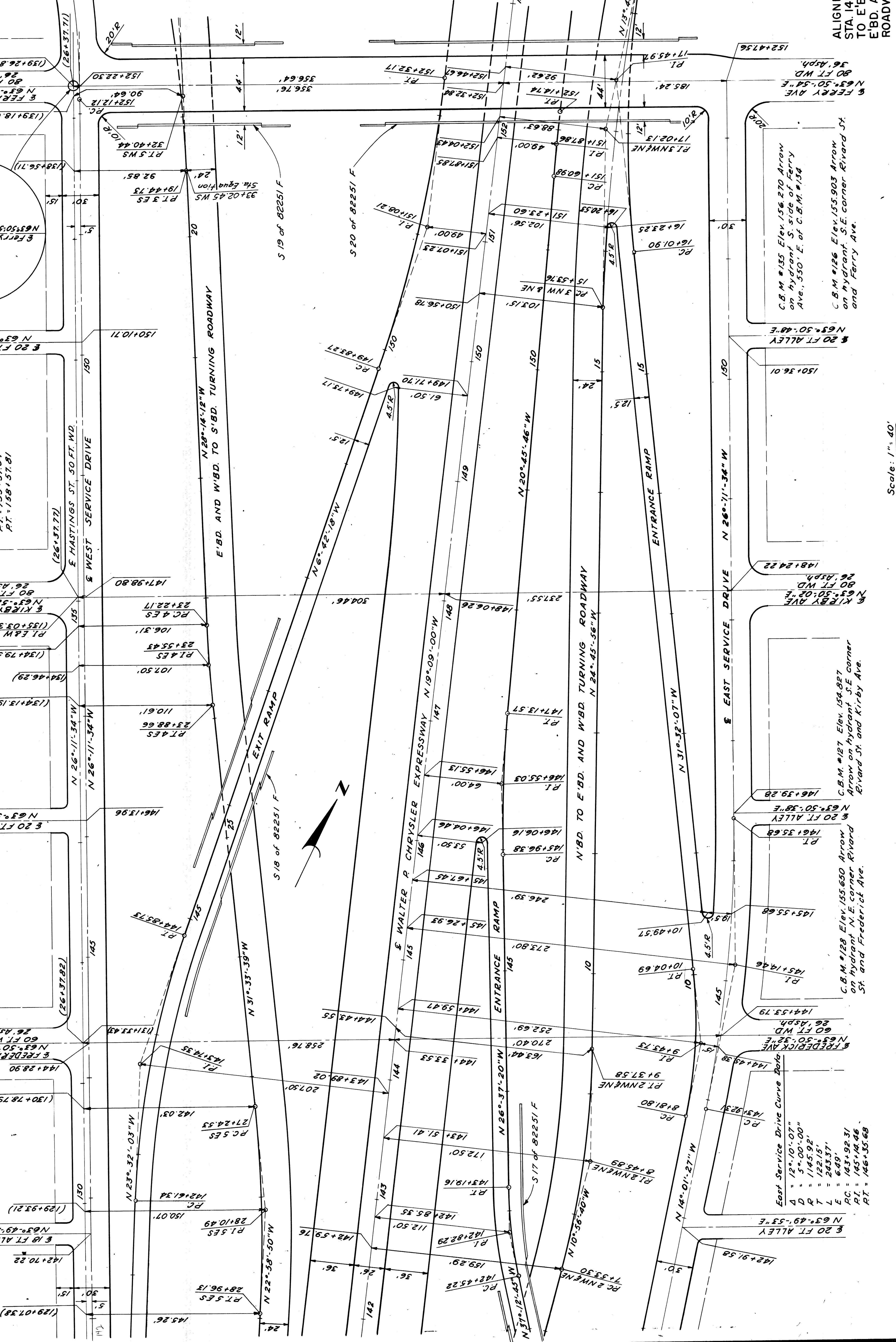
CURVE 2 N.E.N.W. Curve 3.96 & N.W.
 $\Delta = 19^{\circ}40'16''$ D = 71.05-34.8
 $D = 7^{\circ}50'00''$ D = 57.00-00.00
 $R = 763.94'$ R = 145.92
 $T = 92.59'$ T = 148.37'
 $L = 184.28'$ L = 235.81'
 $E = 5.59'$ E = 1.19-53.76
 $PC = 7+53.50$ PC = 15+53.76
 $PI = 17+02.13$ PI = 17+02.13
 $PT = 9+37.58$ PT = 18+49.57

CURVE DATA ENTRANCE RAMP TO TURNING ROADWAY

$\Delta = 17^{\circ}30'40''$ D = 17.57-45.7
 $\Delta = 14^{\circ}15'00''$ D = 6.15-00.00
 $R = 402.08'$ R = 91.67
 $T = 61.93'$ T = 144.07'
 $L = 122.89'$ L = 285.80'
 $E = 4.74'$ E = 11.25'
 $PC = 8+81.80$ PC = 16+01.90
 $PI = 9+43.73$ PI = 17+45.97
 $PT = 10+04.69$ PT = 18+87.70

NOTE:
 All street returns 15' R unless otherwise noted.
 All alley returns 5' R unless otherwise noted.
 Stations shown in brackets, thus (), refer to survey stationing.
 Bench Marks are on City of Detroit Datum.

ALIGNMENT OF WALTER P. CHRYSLER EXPRESSWAY
 STA. 142+00 TO STA. 153+00 AND N.B.D. CHRYSLER
 TO E.B.D. AND W.B.D. FORD TURNING ROADWAY AND
 E.B.D. AND W.B.D. FORD TO S.B.D. CHRYSLER TURNING
 ROADWAY



Curve Data West Service Drive
 $\Delta = 43^{\circ}01'04.32''$
 $D = 6^{\circ}39'44.88''$
 $R = 860'$
 $T = 338.98'$
 $L = 645.69'$
 $E = 64.37'$
 $PC = 152+12.12$
 $PI = 155+51.04$
 $PT = 158+57.81$

C.B.M. #154 Elev. 155.561 Arrow on Hydrant N.W. corner Hastings-St. and Ferry Ave.
 $\Delta = 43^{\circ}01'04.32''$
 $D = 6^{\circ}39'44.88''$
 $R = 860'$
 $T = 338.98'$
 $L = 645.69'$
 $E = 64.37'$
 $PC = 152+12.12$
 $PI = 155+51.04$
 $PT = 158+57.81$

C.B.M. #151 Elev. 155.385 Arrow on Hydrant N.W. corner of Hastings-St. and Frederick Ave.
 $\Delta = 12^{\circ}10'07''$
 $D = 5^{\circ}00'00''$
 $R = 1145.92'$
 $T = 122.15'$
 $L = 243.57'$
 $E = 6.49'$
 $PC = 143+92.31$
 $PI = 145+14.46$
 $PT = 146+35.68$

East Service Drive Curve Data
 $\Delta = 12^{\circ}10'07''$
 $D = 5^{\circ}00'00''$
 $R = 1145.92'$
 $T = 122.15'$
 $L = 243.57'$
 $E = 6.49'$
 $PC = 143+92.31$
 $PI = 145+14.46$
 $PT = 146+35.68$

Scale: 1" = 40'

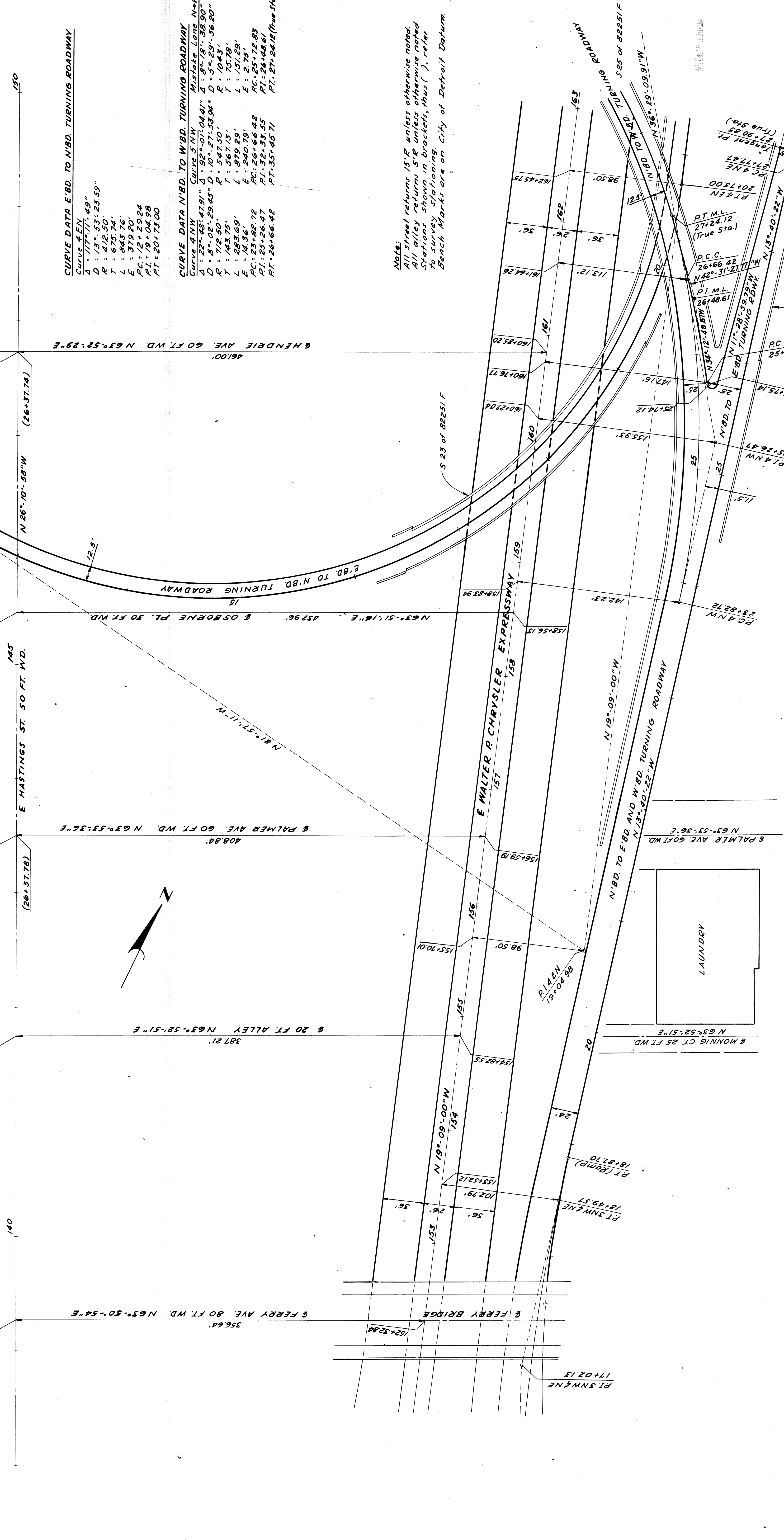
| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------|-----------|-----------|--------------|
| MI | 82251 | 16 | 26 |

| ROUTE | CITY | COUNTY |
|-------|-------|---------|
| 16 | Wayne | Detroit |

| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|-------|-----------|-----------|--------------|
| MI | 82251 | 16 | 26 |

| ROUTE | CITY | COUNTY |
|-------|-------|---------|
| 16 | Wayne | Detroit |

C.B.M. #134 Elev. 155.561 Arrow on hydrant NW corner Hastings St. and Ferry Ave.
 C.B.M. #135 Elev. 156.270 Arrow on hydrant S. side of Ferry Ave. 550' E of C.B.M. #134
 P.B.M. #41-210 Elev. 152.678 City of Detroit Monument N.E. corner of Hastings St. and Hendrie Ave.
 C.B.M. #122 Elev. 156.206 Arrow on hydrant 330' E of P.B.M. #41-210



CURVE DATA E.B.D. TO N'B.D. TURNING ROADWAY
 Curve 4 E.N.
 D: 177'-11".49"
 R: 15'-53".25.55"
 T: 412.50'
 L: 675.74'
 E: 579.60'
 AC: 12'-29".24"
 PI: 91'-04".98"
 PT: 201'-73.00"

CURVE DATA N'B.D. TO W.B.D. TURNING ROADWAY
 Curve 4 N.W.
 Curve 5 N.W.
 Mistake Lane N+W
 D: 22'-48".47.91"
 R: 10'-21".04.41"
 T: 8'-18".38.90"
 L: 8'-02".29.43"
 D: 10'-21".53.94"
 R: 5'-29".36.20"
 T: 547.50'
 L: 704.3'
 E: 879.29'
 AC: 25'-82".72"
 PI: 23'-26".47"
 PT: 26'-66.42"

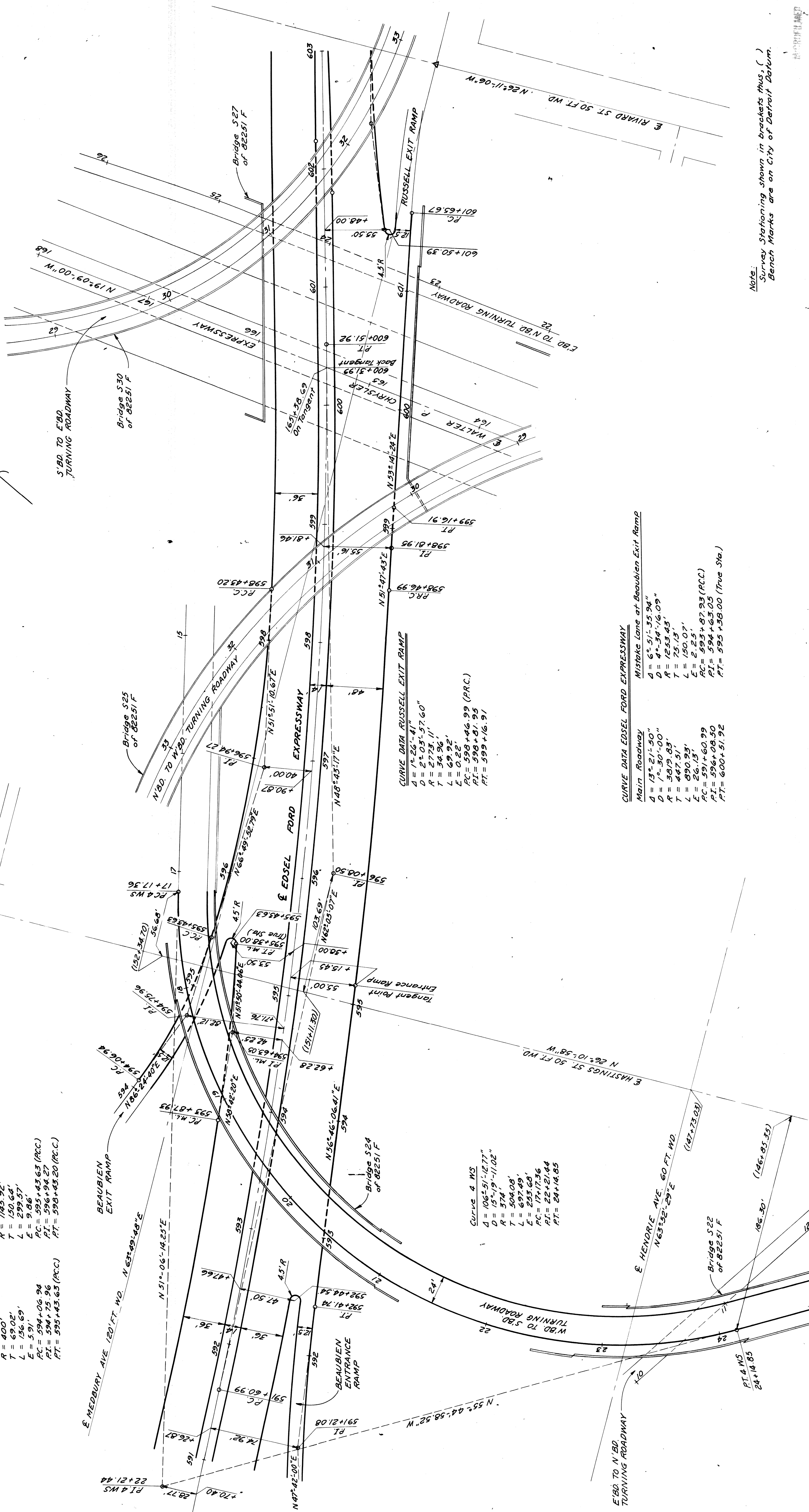
Note:
 All street returns 15' R unless otherwise noted.
 All alley returns 5' R unless otherwise noted.
 Stations shown in brackets, thus (), refer to survey stationing.
 Bench Marks are on City of Detroit Datum

Scale: 1"=40'

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| STATE | PROJ. NO. | PROJ. YEAR | SHEET NO. | TOTAL SHEETS |
|--------|-----------|------------|-----------|--------------|
| MI. | 275/28/25 | | | |
| COUNTY | CITY | | | |
| Wayne | Detroit | | | |
| ROUTE | | | | |
| 82251 | | | | 29 |

| STATE | PROJ. NO. | PROJ. YEAR | SHEET NO. | TOTAL SHEETS |
|--------|-----------|------------|-----------|--------------|
| MI. | 275/28/25 | | | |
| COUNTY | CITY | | | |
| Wayne | Detroit | | | |
| ROUTE | | | | |
| 82251 | | | | 29 |



CURVE DATA BEAUBIEN EXIT RAMP

$\Delta = 14^{\circ}58'42.12''$
 $D = 5^{\circ}00'00''$
 $R = 145.92'$
 $T = 150.64'$
 $L = 299.57'$
 $E = 9.86'$
 $PC = 595+43.63$ (P.C.C.)
 $PT = 598+43.20$ (P.C.C.)

CURVE DATA

$\Delta = 106^{\circ}51'42.77''$
 $D = 15^{\circ}19'11.02''$
 $R = 574'$
 $T = 594.08'$
 $L = 697.69'$
 $E = 233.69'$
 $PC = 17+17.36$
 $PT = 22+21.44$
 $PI = 24+14.85$

CURVE DATA EDSEL FORD EXPRESSWAY

Main Roadway

$\Delta = 13^{\circ}21'50''$
 $D = 7^{\circ}30'00''$
 $R = 339.83'$
 $T = 447.51'$
 $L = 890.93'$
 $E = 591+60.99$
 $PC = 594+63.05$
 $PT = 600+51.92$

Mistake Lane at Beaubien Exit Ramp

$\Delta = 6^{\circ}51'35.94''$
 $D = 4^{\circ}34'16.07''$
 $R = 123.45'$
 $T = 151.91'$
 $L = 200.07'$
 $E = 593+38.00$ (True Sta.)

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

Note:
 Survey Stationing shown in brackets thus, ()
 Bench Marks are on City of Detroit Datum.

ALIGNMENT OF EDSEL FORD EXPRESSWAY
 STA 591+00 TO STA 601+00 AND
 WESTBOUND FORD TO SOUTHBOUND
 CHRYSLER TURNING ROADWAY

Scale: 1" = 40'

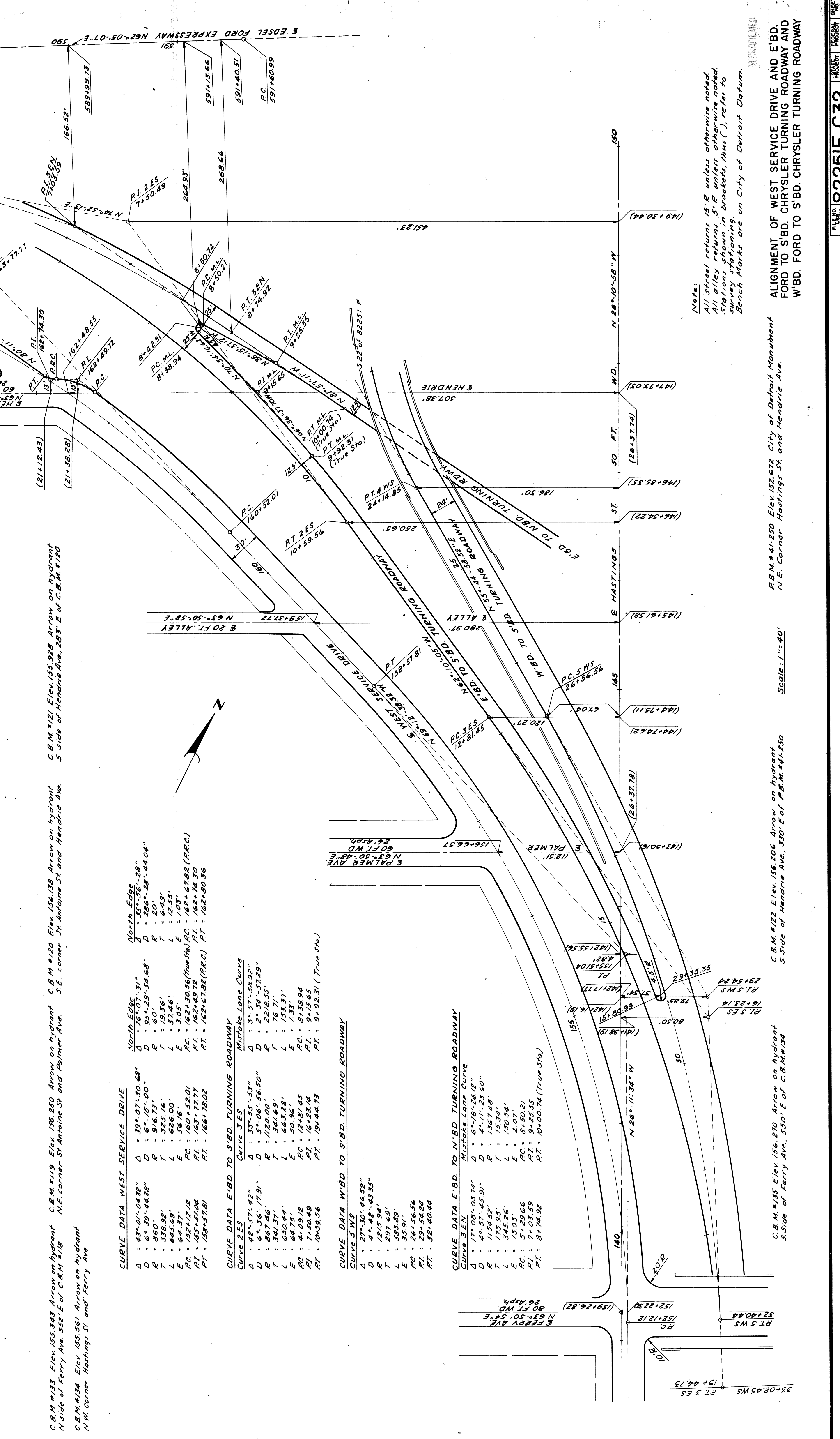
C.B.M. #122 Elev. 156.206 Arrow on Midpoint
 S. Side of Hendrie Ave. 350' E. of P.B.M. #41-250.

P.B.M. #41-250 Elev. 152.672 City of Detroit
 Monument N.E. Corner Hastings St. and Hendrie Ave.

C.B.M. #21 Elev. 155.928 Arrow on Midpoint
 S. Side of Hendrie Ave. 203' E. of C.D.M. #120.

| | | | |
|--------|---------------|-------|-------|
| STATE | PROJ. NO. | FEED | SHEET |
| MICH. | 2725/26/32 | 3 | 31 |
| COUNTY | CITY | SHEET | SHEET |
| Wayne | Detroit | 31 | 31 |
| ROUTE | PROJECT | DATE | BY |
| 92257 | Wayne Detroit | 3/1 | |

C.B.M. #133 Elev. 153.343 Arrow on hydrant N side of Ferry Ave. 342' E of C.B.M. #18
 N.E. corner St. Antoine St. and Palmer Ave.
 C.B.M. #119 Elev. 156.250 Arrow on hydrant S.E. corner St. Antoine St. and Hendrie Ave.
 C.B.M. #120 Elev. 156.138 Arrow on hydrant S side of Hendrie Ave. 283' E of C.B.M. #120
 C.B.M. #134 Elev. 155.561 Arrow on hydrant N.W. corner Hastings St. and Ferry Ave.
 C.B.M. #122 Elev. 156.206 Arrow on hydrant S side of Hendrie Ave. 330' E of P.B.M. #41-250
 C.B.M. #135 Elev. 156.270 Arrow on hydrant S side of Ferry Ave. 330' E of C.B.M. #134



CURVE DATA WEST SERVICE DRIVE

| | | | | | |
|-----------|-------------------|-------------------|------------|---------------------------|------------------------|
| Curve 2ES | Δ: 43° 01' 04.32" | D: 29° 07' 30.68" | North Edge | Δ: 26° 01' 31.31" | D: 55° 56' 28.28" |
| | R: 6° 39' 44.28" | R: 9' 16' 73" | | R: 60' | R: 286° 28' 44.04" |
| | T: 328.99' | T: 375.76' | | L: 19.56' | T: 6.49' |
| | L: 445.69' | L: 626.00' | | E: 3.05' | L: 12.55' |
| | E: 64.37' | E: 58.16' | | PC: 162+30.36 (True Sta.) | PC: 162+67.82 (P.R.C.) |
| | PC: 132+12.18 | PC: 160+52.01 | | PI: 162+49.72 | PI: 162+74.30 |
| | PI: 155° 51' 04" | PI: 163° 27' 77" | | PT: 162+67.82 (P.R.C.) | PT: 162+90.36 |
| | PT: 158° 57' 81" | PT: 166° 18' 02" | | | |

CURVE DATA E.B.D. TO S.B.D. TURNING ROADWAY

| | | | | | |
|-----------|------------------|------------------|--------------------|------------------|-------------------------|
| Curve 2ES | Δ: 48° 57' 42" | D: 33° 53' 53" | Mistake Lane Curve | Δ: 3° 57' 28.92" | D: 5° 57' 28.92" |
| | R: 6° 36' 17.91" | R: 5° 06' 56.50" | | R: 2° 34' 57.29" | R: 2218.55' |
| | T: 867.46' | T: 1120.00' | | T: 76.71' | T: 153.31' |
| | L: 341.37' | L: 341.69' | | L: 663.28' | L: 133' |
| | L: 650.44' | L: 663.28' | | E: 50.36' | E: 8+38.94 |
| | E: 64.75' | E: 50.36' | | PC: 12+81.45 | PC: 8+38.94 |
| | PC: 4+09.12 | PC: 12+81.45 | | PI: 16+23.14 | PI: 7+15.65 |
| | PI: 7+50.49 | PI: 16+23.14 | | PT: 9+15.65 | PT: 9+92.31 (True Sta.) |
| | PT: 10+39.56 | PT: 19+44.73 | | | |

CURVE DATA W.B.D. TO S.B.D. TURNING ROADWAY

| | | | | | |
|-----------|-------------------|------------------|--------------------|------------------|--------------------------|
| Curve 5WS | Δ: 27° 30' 46.52" | D: 4° 42' 43.35" | Mistake Lane Curve | Δ: 6° 18' 26.12" | D: 4° 11' 23.60" |
| | R: 1215.94' | R: 1367.49' | | R: 1367.49' | R: 75.34' |
| | T: 297.69' | T: 75.34' | | L: 150.54' | L: 2.07' |
| | L: 523.89' | L: 150.54' | | AC: 8+50.21 | AC: 8+50.21 |
| | E: 35.91' | E: 2.07' | | PI: 9+23.55 | PI: 10+00.74 (True Sta.) |
| | PC: 24+56.56 | PC: 8+50.21 | | PT: 8+74.52 | |
| | PI: 20+54.24 | PI: 9+23.55 | | | |
| | PT: 32+40.64 | PT: 8+74.52 | | | |

CURVE DATA E.B.D. TO N.B.D. TURNING ROADWAY

| | | | | | |
|-----------|-------------------|--------------------------|--------------------|------------------|--------------------------|
| Curve 3EN | Δ: 17° 08' 05.74" | D: 6° 18' 26.12" | Mistake Lane Curve | Δ: 6° 18' 26.12" | D: 4° 11' 23.60" |
| | R: 1156.32' | R: 1367.49' | | R: 1367.49' | R: 75.34' |
| | T: 175.93' | T: 75.34' | | L: 150.54' | L: 2.07' |
| | L: 345.26' | L: 150.54' | | AC: 8+50.21 | AC: 8+50.21 |
| | E: 15.03' | E: 2.07' | | PI: 9+23.55 | PI: 10+00.74 (True Sta.) |
| | PC: 5+29.66 | PC: 8+50.21 | | PT: 8+74.52 | |
| | PI: 7+03.59 | PI: 9+23.55 | | | |
| | PT: 8+74.52 | PT: 10+00.74 (True Sta.) | | | |

CURVE DATA W.B.D. TO N.B.D. TURNING ROADWAY

| | | | | | |
|-----------|-------------------|--------------------------|--------------------|------------------|--------------------------|
| Curve 3EN | Δ: 17° 08' 05.74" | D: 6° 18' 26.12" | Mistake Lane Curve | Δ: 6° 18' 26.12" | D: 4° 11' 23.60" |
| | R: 1156.32' | R: 1367.49' | | R: 1367.49' | R: 75.34' |
| | T: 175.93' | T: 75.34' | | L: 150.54' | L: 2.07' |
| | L: 345.26' | L: 150.54' | | AC: 8+50.21 | AC: 8+50.21 |
| | E: 15.03' | E: 2.07' | | PI: 9+23.55 | PI: 10+00.74 (True Sta.) |
| | PC: 5+29.66 | PC: 8+50.21 | | PT: 8+74.52 | |
| | PI: 7+03.59 | PI: 9+23.55 | | | |
| | PT: 8+74.52 | PT: 10+00.74 (True Sta.) | | | |

CURVE DATA E.B.D. TO S.B.D. TURNING ROADWAY

| | | | | | |
|-----------|-------------------|----------------|--------------------|------------------|--------------------------|
| Curve 3ES | Δ: 12° 18' 12.18" | D: 10° 00' 74" | Mistake Lane Curve | Δ: 6° 18' 26.12" | D: 4° 11' 23.60" |
| | R: 1215.94' | R: 1367.49' | | R: 1367.49' | R: 75.34' |
| | T: 297.69' | T: 75.34' | | L: 150.54' | L: 2.07' |
| | L: 523.89' | L: 150.54' | | AC: 8+50.21 | AC: 8+50.21 |
| | E: 35.91' | E: 2.07' | | PI: 9+23.55 | PI: 10+00.74 (True Sta.) |
| | PC: 24+56.56 | PC: 8+50.21 | | PT: 8+74.52 | |
| | PI: 20+54.24 | PI: 9+23.55 | | | |
| | PT: 32+40.64 | PT: 8+74.52 | | | |

CURVE DATA E.B.D. TO N.B.D. TURNING ROADWAY

| | | | | | |
|-----------|-------------------|--------------------------|--------------------|------------------|--------------------------|
| Curve 3EN | Δ: 17° 08' 05.74" | D: 6° 18' 26.12" | Mistake Lane Curve | Δ: 6° 18' 26.12" | D: 4° 11' 23.60" |
| | R: 1156.32' | R: 1367.49' | | R: 1367.49' | R: 75.34' |
| | T: 175.93' | T: 75.34' | | L: 150.54' | L: 2.07' |
| | L: 345.26' | L: 150.54' | | AC: 8+50.21 | AC: 8+50.21 |
| | E: 15.03' | E: 2.07' | | PI: 9+23.55 | PI: 10+00.74 (True Sta.) |
| | PC: 5+29.66 | PC: 8+50.21 | | PT: 8+74.52 | |
| | PI: 7+03.59 | PI: 9+23.55 | | | |
| | PT: 8+74.52 | PT: 10+00.74 (True Sta.) | | | |

| STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------|------------|-------------|-----------|--------------|
| MI. | 1757/59/33 | | | |
| CITY | COUNTY | ROUTE | SHEET NO. | TOTAL SHEETS |
| Detroit | Wayne | 82251 | 32 | |

| STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------|------------|-------------|-----------|--------------|
| MI. | 1757/59/33 | | | |
| CITY | COUNTY | ROUTE | SHEET NO. | TOTAL SHEETS |
| Detroit | Wayne | 82251 | 32 | |

C.B.M. #143 Elev. 155.728 Arrow on Hydrant
N.W. Corner Harper Ave. and Mansour Ave.

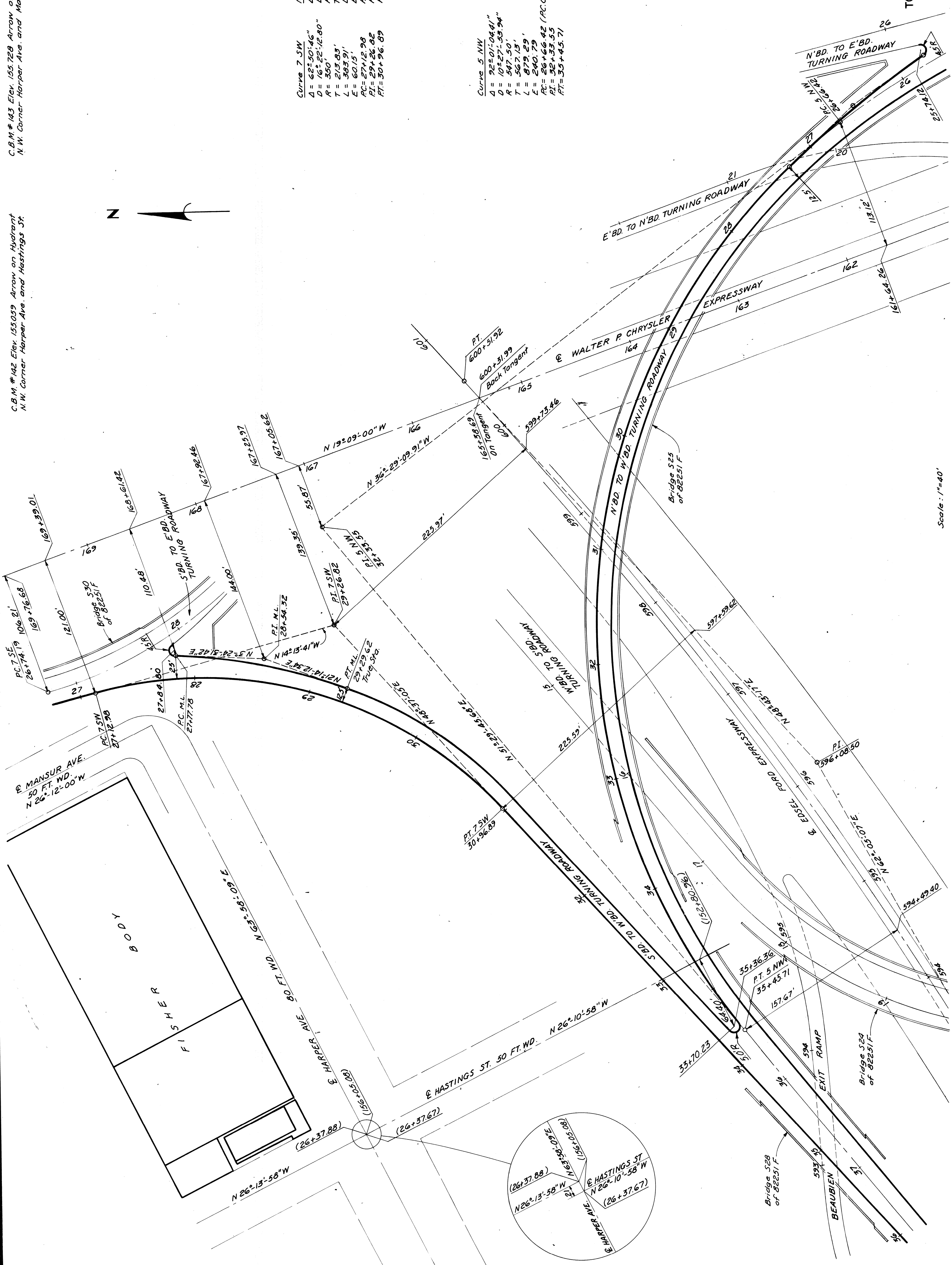
C.B.M. #142 Elev. 155.039 Arrow on Hydrant
N.W. Corner Harper Ave. and Hastings St.



Curve 7 SW
 $\Delta = 62^{\circ}50'46''$
 $D = 16^{\circ}22'12.80''$
 $R = 508.83'$
 $T = 276.54'$
 $L = 181.84'$
 $E = 599'$
 $PC = 29+11.51$
 $PT = 29+29.62$
 $PI = 29+20.56$
 $PI = 29+29.62$ (True Sta.)

Mistake Lane in SW
 $\Delta = 17^{\circ}49'40.92''$
 $D = 11^{\circ}44'28.42''$
 $R = 497.99'$
 $T = 76.54'$
 $L = 181.84'$
 $E = 599'$
 $PC = 27+77.28$
 $PT = 28+54.32$
 $PI = 28+29.62$ (True Sta.)

Curve 5 NW
 $\Delta = 92^{\circ}01'04.41''$
 $D = 10^{\circ}27'53.24''$
 $R = 547.50'$
 $T = 567.13'$
 $L = 879.29'$
 $E = 240.79'$
 $PC = 26+66.42$ (A.C.C.)
 $PT = 32+33.55$
 $PI = 35+45.71$



Note: Survey Stationing shown in brackets thus, ()
Bench Marks are on City of Detroit Datum.

ALIGNMENT OF SOUTHBOUND CHRYSLER
TO WESTBOUND FORD TURNING ROADWAY
AND NORTHBOUND CHRYSLER TO
WESTBOUND FORD TURNING ROADWAY

Scale: 1"=40'

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

| STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
|-------|-----------|-------|-----------|--------------|
| MICH. | 775(36)3 | | | |
| CITY | COUNTY | ROUTE | PROJECT | DATE |
| Wayne | Wayne | 22257 | Wayne | 1953 |
| STATE | PROJ. NO. | YEAR | SHEET NO. | TOTAL SHEETS |
| MICH. | 775(36)3 | | | |
| CITY | COUNTY | ROUTE | PROJECT | DATE |
| Wayne | Wayne | 22257 | Wayne | 1953 |

C.B.M. #145 Elev. 155,869 Arrow on Hydrant
N. side of Harper Ave, 253' E. of C.B.M. #144

C.B.M. #144 Elev. 155,964 Arrow on Hydrant
N.W. Corner Harper Ave. and Rivard St.

C.B.M. #143 Elev. 155,728 Arrow on Hydrant
N.W. Corner Harper Ave. and Mansur Ave.

C.B.M. #146 Elev. 155,610 Arrow on Hydrant
N.E. Corner Harper Ave. and Russell St.

Mistake Lane in S14E
 $\Delta = 17^\circ 49' 40.92''$
 $D = 11' 44' 28.42''$
 $P = 487.99'$
 $T = 76.54'$
 $L = 151.84'$
 $E = 597'$
 $PC = 271.8554$
 $PT = 281.6208$
 $PI = 291.3738$ (True Sta.)

Curve 7 SE
 $\Delta = 77^\circ 34' 24.79''$
 $D = 15^\circ 48' 20.64''$
 $P = 562.50'$
 $T = 293.06'$
 $L = 103.64'$
 $E = 452.90'$
 $PC = 261.7419$
 $PT = 294.6725$
 $PI = 311.6709$

Curve 8 SE
 $\Delta = 45^\circ 01' 32.95''$
 $D = 13^\circ 53' 23.59''$
 $P = 412.50'$
 $T = 170.97'$
 $L = 324.16'$
 $E = 34.03'$
 $PC = 311.6709$
 $PT = 341.3125$

Curve 5 NE
 $\Delta = 70^\circ 48' 54.54''$
 $D = 7^\circ 14' 00''$
 $P = 4645.60'$
 $T = 439.76'$
 $L = 876.90'$
 $E = 20.77'$
 $PC = 301.9255$
 $PT = 351.3237$
 $PI = 391.6346$

CURVE DATA EDSEL FORD
Main Roadway
 $\Delta = 73^\circ 27' 50''$
 $D = 7^\circ 30' 00''$
 $P = 3819.83'$
 $T = 447.51'$
 $L = 890.93'$
 $E = 26.73'$
 $PC = 591.6099$
 $PT = 596.0850$
 $PI = 600.5192$

Curve 4 NE
 $\Delta = 51^\circ 34' 44.46''$
 $D = 16^\circ 22' 12.80''$
 $P = 350'$
 $T = 169.12'$
 $L = 315.08'$
 $E = 38.72'$
 $PC = 271.7747$
 $PT = 294.4659$
 $PI = 304.9255$
 $PC = 5 NE$

Note:
Survey Stationing shown in brackets thus ()
Bench Marks are on City of Detroit Datum.

ALIGNMENT OF SOUTHBOUND CHRYSLER TO
EASTBOUND FORD TURNING ROADWAY, AND
NORTHBOUND CHRYSLER TO EASTBOUND
FORD TURNING ROADWAY

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

Scale 1"=40'

82251E C32

FILE NO. 82251E C32

PROJ. NO. 775(36)3

DATE 1/27/53

SHEET NO. 33

TOTAL SHEETS 33

| STATE | COUNTY | CITY | SHEET NO. | TOTAL SHEETS |
|-------|--------|---------|-----------|--------------|
| MI | Wayne | Detroit | 34 | 34 |

CURVE DATA EAST SERVICE DRIVE
 $\Delta = 62^{\circ}29'44''$
 $D = 104'10''26.92''$
 $R = 45.00'$
 $T = 33.37'$
 $L = 21.46'$
 $E = 1.31'$
 $PC = 155+00.62$
 $PT = 155+33.69$
 $PI = 155+60.51$

CURVE DATA EDSEL FORD
 $\Delta = 13^{\circ}21'50''$
 $D = 3819.83'$
 $R = 447.51'$
 $L = 430.33'$
 $E = 26.13'$
 $PC = 591+60.99$
 $PT = 596+08.50$
 $PI = 600+51.92$

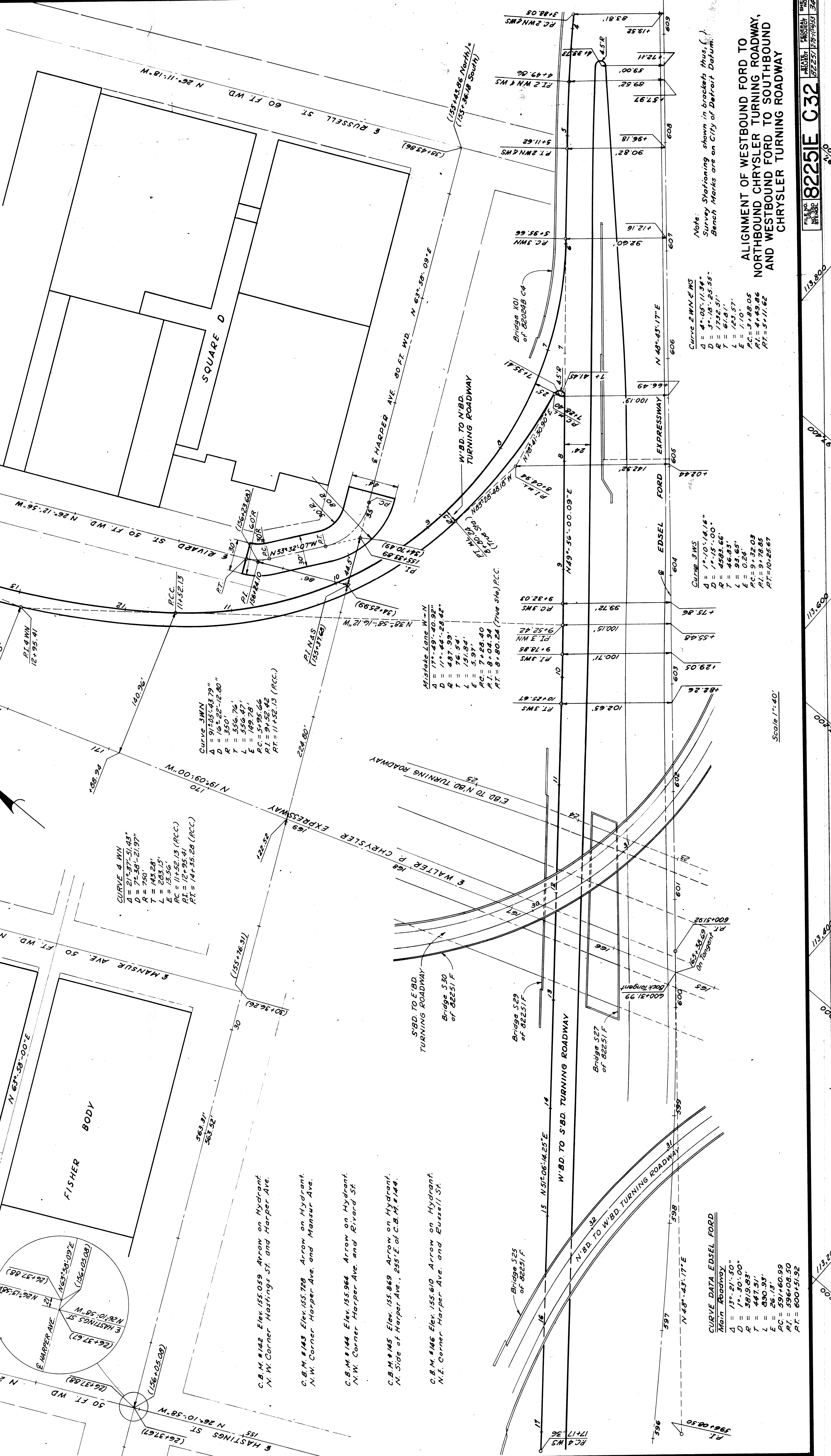
CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}10'14.14''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 4^{\circ}05'11.34''$
 $D = 3181.2355'$
 $R = 1732.31'$
 $L = 61.91'$
 $E = 125.57'$
 $PC = 1198.05$
 $PT = 1259.96$
 $PI = 1259.96$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$



CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

CURVE DATA EDSEL FORD
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

PLANS PREPARED BY
 DEPARTMENT OF PUBLIC WORKS
 CITY OF DETROIT
 BUREAU OF HIGHWAYS AND EXPRESSWAYS
 CITY ENGINEERS OFFICE

Scale 1"=40'

Traced by: F. Caballero
Checked by: J.M. Breda 11-15-59
Corrected by: J.M. Breda 1-20-60

82251 E C32

ALIGNMENT OF WESTBOUND FORD TO NORTHBOUND CHRYSLER TURNING ROADWAY, AND WESTBOUND FORD TO SOUTHBOUND CHRYSLER TURNING ROADWAY

Survey Stationing shown in brackets thus, () Bench Marks are on City of Detroit Datum.

Curve 3 JMS
 $\Delta = 17^{\circ}10'14.14''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

Curve 2 W.N.F.W.S
 $\Delta = 4^{\circ}05'11.34''$
 $D = 3181.2355'$
 $R = 1732.31'$
 $L = 61.91'$
 $E = 125.57'$
 $PC = 1198.05$
 $PT = 1259.96$
 $PI = 1259.96$

Curve 4 W.N
 $\Delta = 21^{\circ}37'51.43''$
 $D = 7138-21.97''$
 $R = 750'$
 $T = 43.28'$
 $E = 13.56'$
 $PC = 11+52.13 (P.C.C.)$
 $PT = 14+35.28 (P.C.C.)$

Curve 3 JMS
 $\Delta = 91^{\circ}05'43.79''$
 $D = 161-22-12.80''$
 $R = 350'$
 $T = 356.76'$
 $L = 556.47'$
 $E = 149.78'$
 $PC = 5+95.66$
 $PT = 9+52.42$
 $PI = 11+52.13 (P.C.C.)$

Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

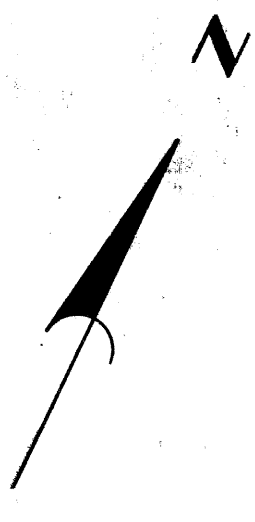
Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

Curve 3 JMS
 $\Delta = 17^{\circ}49'40.98''$
 $D = 1115.00'$
 $R = 4583.66'$
 $L = 46.83'$
 $E = 93.65'$
 $PC = 91+32.03$
 $PT = 10+25.67$

| | | | | | | | |
|--------|--------|-----------|------------|-----------|------------|-----------|----|
| STATE | MICH. | PROJ. NO. | 7751/60/23 | YEAR | 1966 | SHEET NO. | 35 |
| COUNTY | DETOIT | CITY | DETROIT | PROJ. NO. | 7751/60/23 | SHEET NO. | 35 |
| ROUTE | 4 | STATE | MICH. | PROJ. NO. | 7751/60/23 | SHEET NO. | 35 |
| ROUTE | 4 | STATE | MICH. | PROJ. NO. | 7751/60/23 | SHEET NO. | 35 |



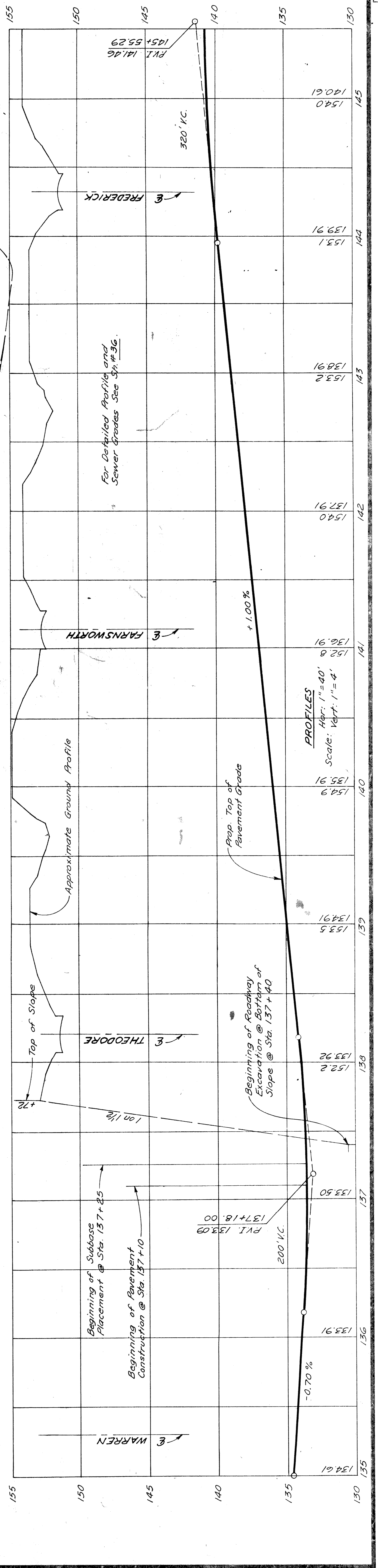
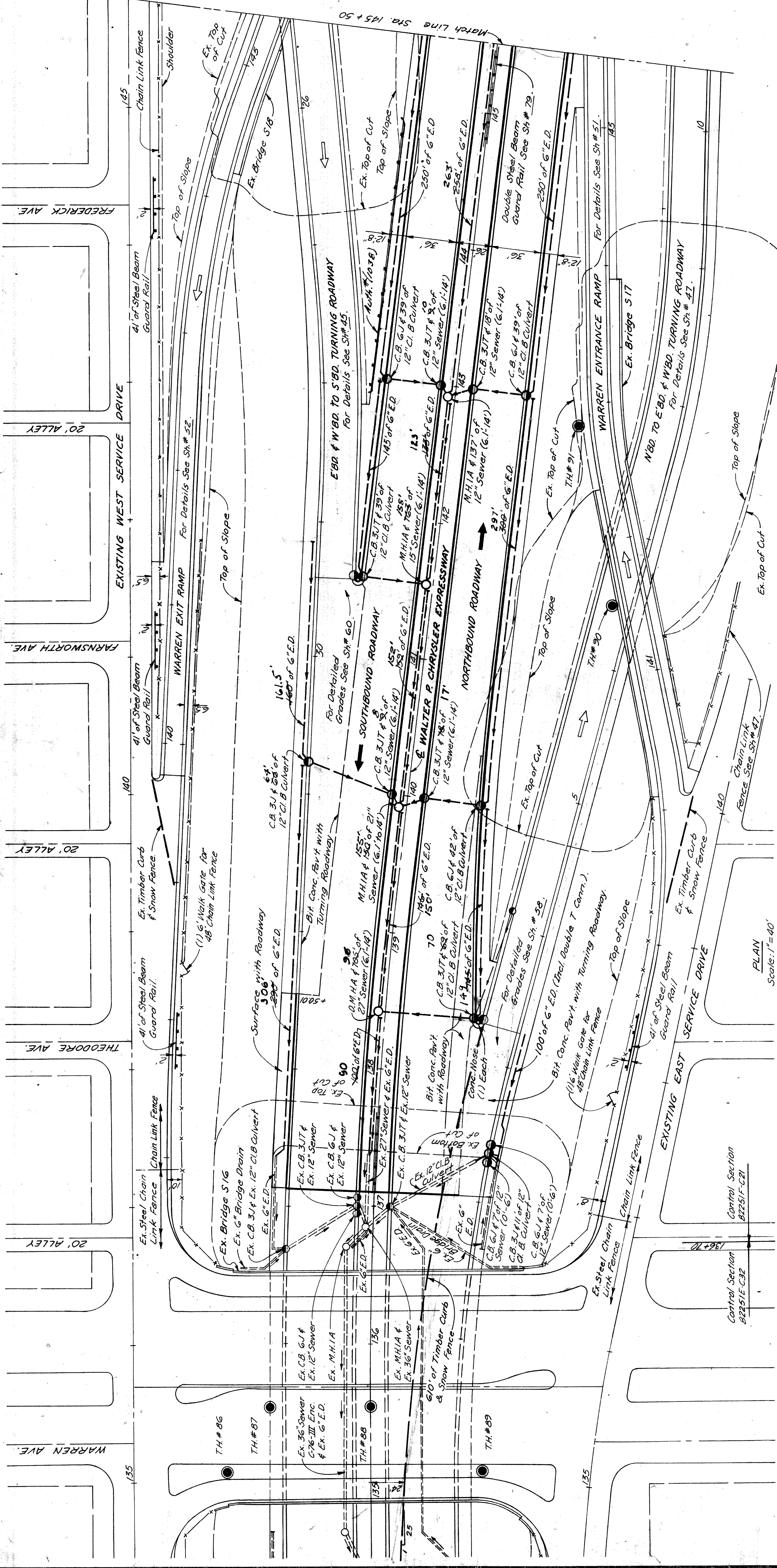
Guard Rail & Fence (This Sheet)
 Steel Beam Guard Rail (A.M. 1028) 164 Lin. Ft. 316.5
 48" Chain Link Fence 1006 91775 Lin. Ft. 1253
 6 Walk Gates for 48" Chain Link Fence - 2 Each

Drainage Auth. No's.
 1066
 1069
 1076
 1077
 1075

Notes:
 For Alignment See Sh. # 24 & Sh. # 25.
 For Grading See Sh. # 19.
 For Index of Soil Borings (Symbol) See Sh. # 80.
 For Location of Traffic Signs See Sh. # 79.

WALTER P. CHRYSLER
 EXPRESSWAY ITEMS
 STA. 137+10 TO STA. 145+50

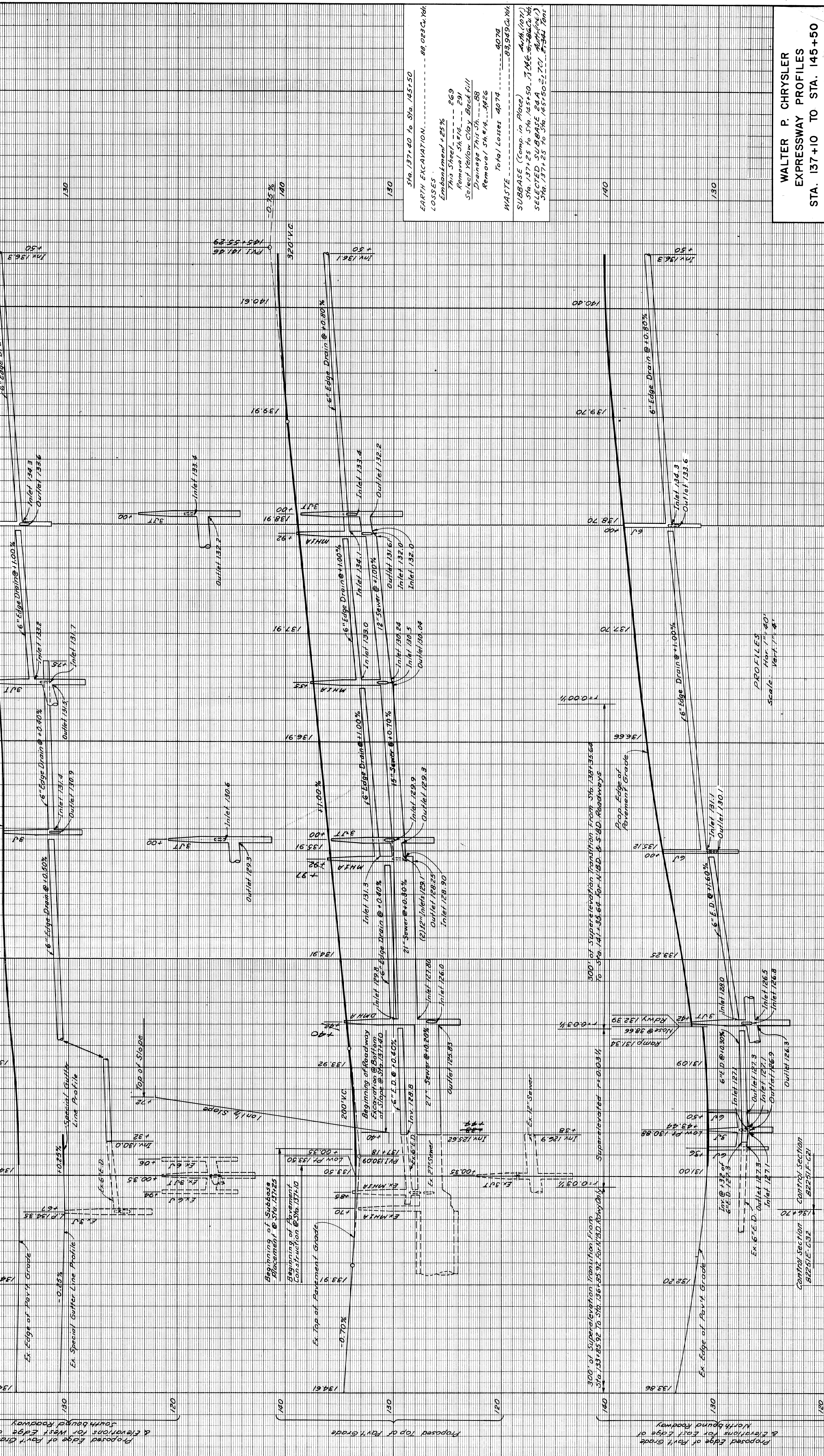
PROJECT NO. 82251E C32
 SHEET NO. 35



PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | |
|------|--------------|---------------|
| 7/62 | FOR APPROVAL | W. P. WICKREY |
| 7/62 | RECOMMENDED | R. P. WOLKMAN |
| 7/62 | CHECKED BY | M. J. KANNAS |
| 7/62 | DESIGNED BY | A. J. HARRIS |
| 7/62 | DATE | 7/62 |

| B.P. No. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|----------|---------|-----------|-------------|----------------------------------|--------------|
| 4 | MICH. | 275/26/53 | CITY | 36 | 36 |
| ROUTE | PROJECT | COUNTY | DATE | SHEET NO. <td>TOTAL SHEETS </td> | TOTAL SHEETS |
| 4 | 82251E | Wayne | 2/1/54 | 36 | 36 |



Sta. 137+40 to Sta. 145+50
 EARTH EXCAVATION..... 88,083 Cu. Yds.
 LOSSES
 Embankment +25%
 This Street 269
 Removal 154 7/4 --- 29
 Select Yellow Clay Backfill
 Drainage 7 1/4 --- 58
 Removal 5 1/4 --- 58
 Total Losses 4074
 WASTE ----- 83,989 Cu. Yds.
 SUBBASE (Comp. in Place) 4074
 Sta. 137+25 to Sta. 145+50. 7146.5 Cu. Yds.
 SELECTED SUBBASE 24 A
 Sta. 137+25 to Sta. 145+50 2,701
 5-34 780'

WALTER P. CHRYSLER
 EXPRESSWAY PROFILES
 STA. 137+10 TO STA. 145+50

82251E C32
 PROJECT NO. 82251
 SHEET NO. 36

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

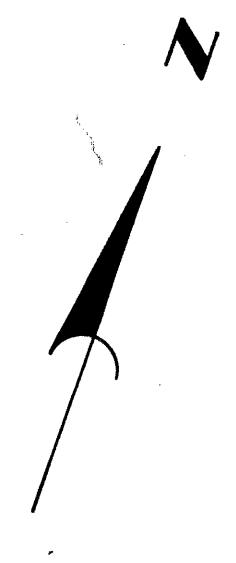
| | |
|----------------------------|------|
| OPERATION | DATE |
| PRELIMINARY R.O.W. CHECKED | 7/62 |
| FINAL DESIGN CHECKED | 7/62 |
| TRACED | 7/62 |
| FINAL O.C.W. CHECK | 7/62 |
| QUANTITIES CHECKED | 7/62 |
| QUANTITIES CHECKED | 7/62 |
| SQUAD | 7/62 |

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 TRACED
 FINAL O.C.W. CHECK
 QUANTITIES CHECKED
 QUANTITIES CHECKED
 SQUAD

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 TRACED
 FINAL O.C.W. CHECK
 QUANTITIES CHECKED
 QUANTITIES CHECKED
 SQUAD

82251E-C32
 CONTROL SECTION

| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|--------|-----------|-----------|--------------|
| MICH. | 172/2823 | 37 | 37 |
| COUNTY | CITY | SHEET NO. | TOTAL SHEETS |
| Wayne | Detroit | 37 | 37 |
| ROUTE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 82251E | | | |



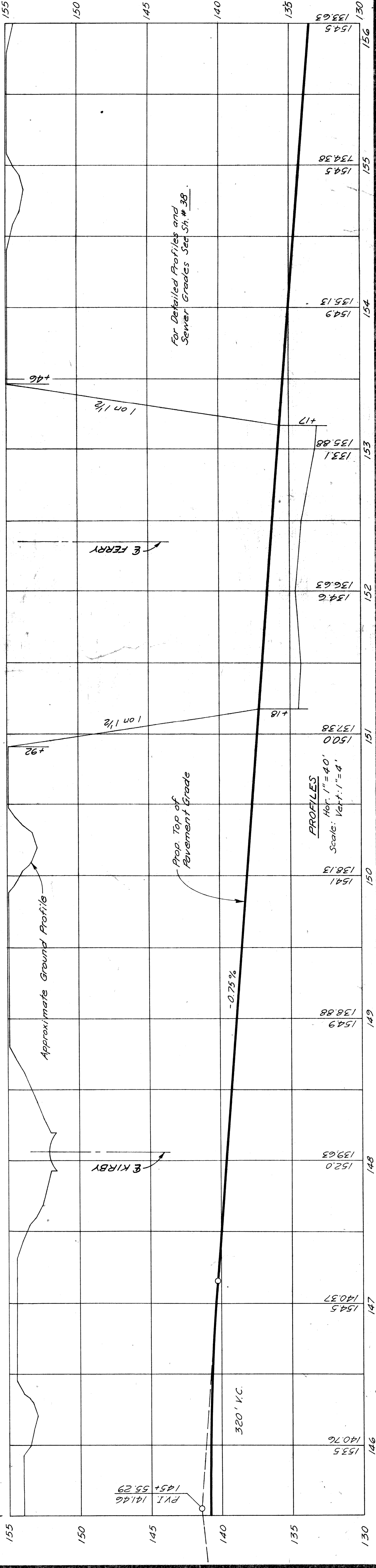
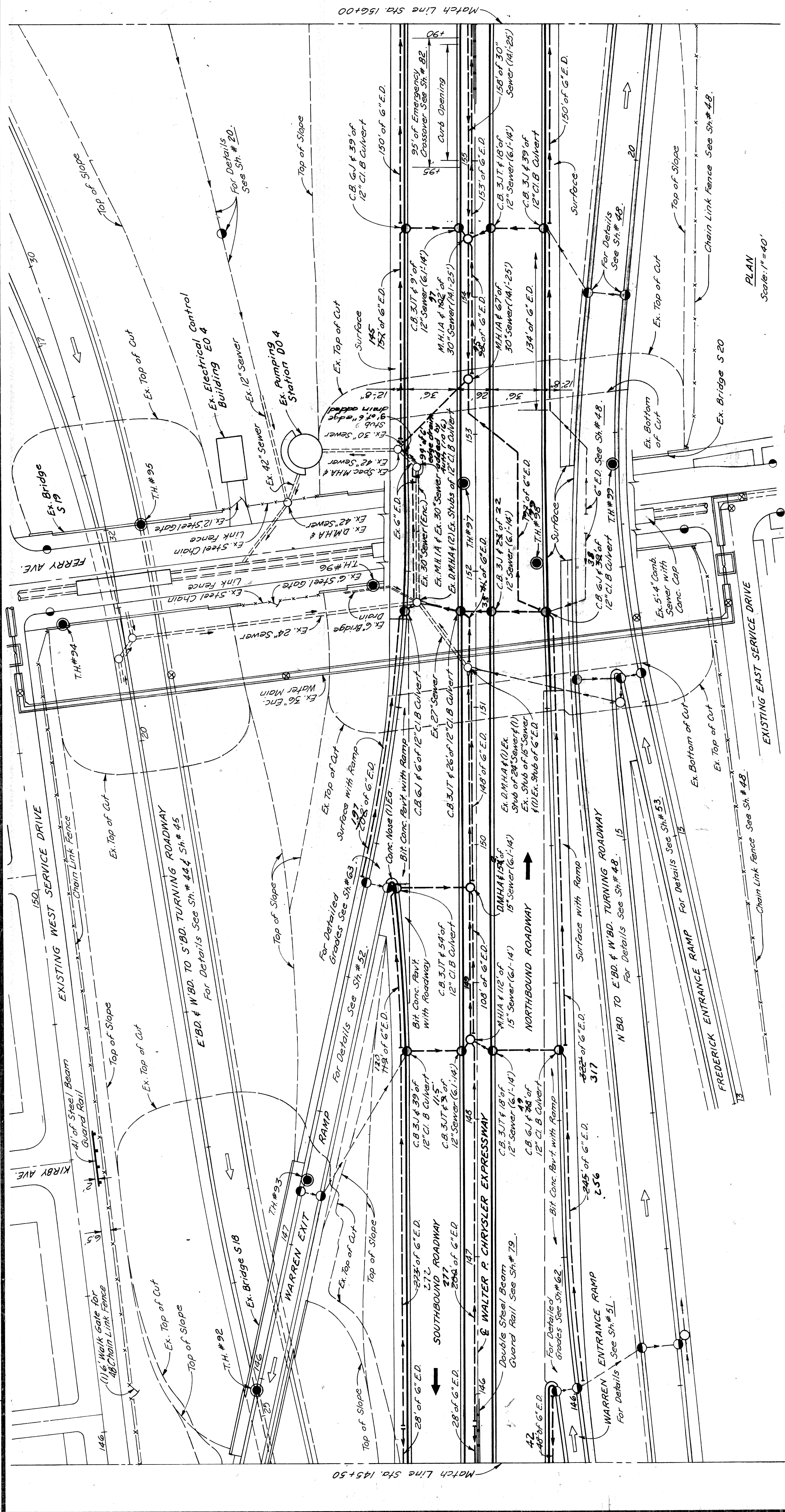
Drainage A.M.A. Nos.
 1066
 1046
 1035
 1069
 1037

Guard Rail & Fence (This Sheet)
 Steel Beam Guard Rail 41 Lin. Ft.
 6' Walk Gate for 48' Chain Link Fence 1 Each
 48' Chain Link Fence 610 Lin. Ft.

Notes:
 For Alignment See Sh. # 25 & Sh. # 26.
 For Grading See Sh. # 20.
 For Index of Soil Borings (Symbol) See Sh. # 80.
 For Location of Traffic Signs See Sh. # 29.

WALTER P. CHRYSLER
 EXPRESSWAY ITEMS
 STA. 145+50 TO STA. 156+00

FILE NO. 82251E C32
 SHEET 37



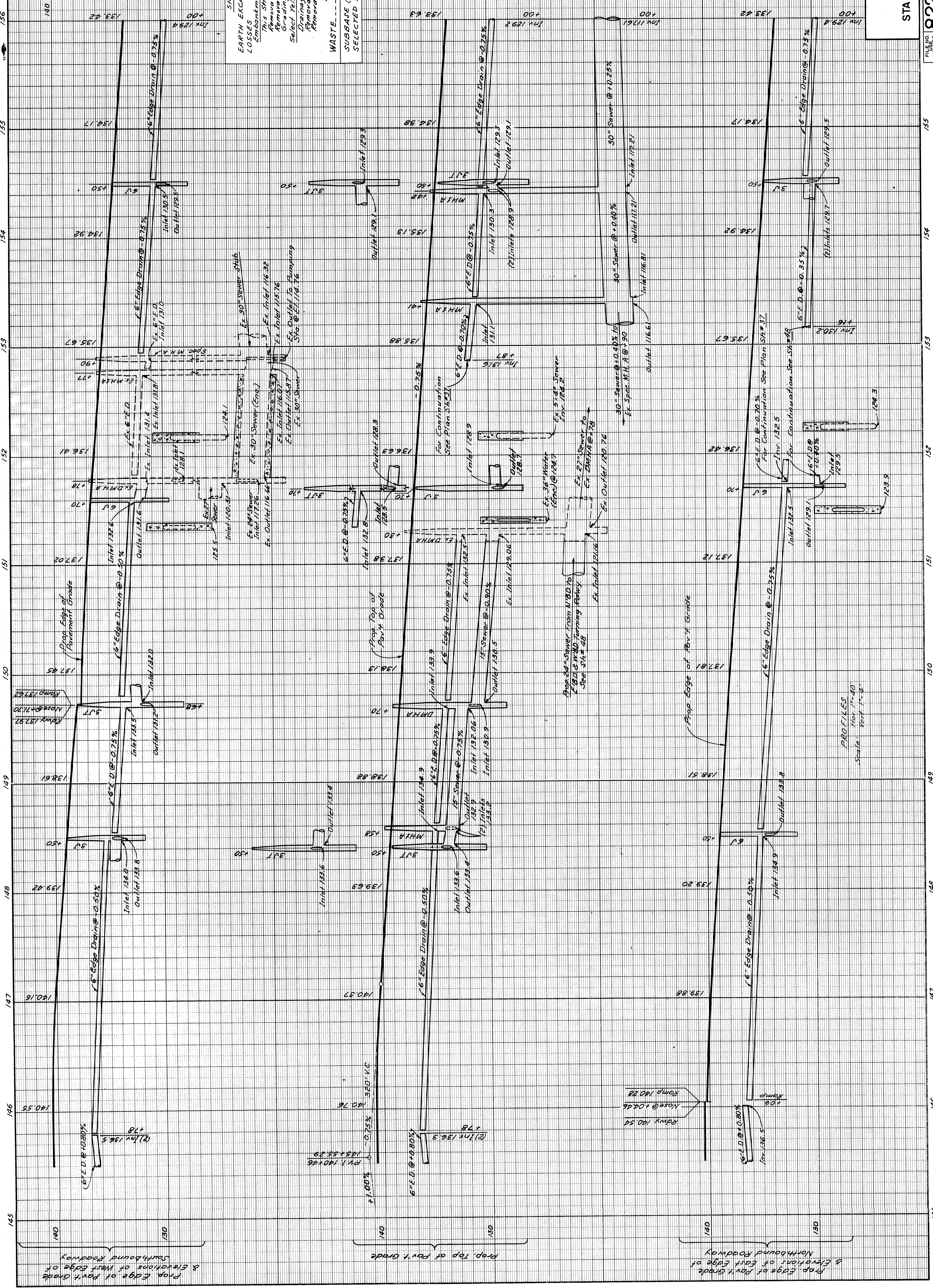
PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CIVIL ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | |
|------|--------------|---------------------------------|
| 7/62 | FOR APPROVAL | SM. ASSOC. ENGR. EXAM. DIVISION |
| 7/62 | RETURNED | M. R. HALL |
| 7/62 | CHECKED BY | W. W. WICKER |
| 7/62 | TRACED BY | W. W. WICKER |
| 7/62 | DRAWN BY | R. P. WOLKMAN |
| 7/62 | SOUND LEADER | W. W. WICKER |

| | | | |
|---------|-------------|------------|--------------|
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| MI | 175/150/23 | 38 | 38 |
| CITY | ROUTE | PROJECT | DATE |
| DETROIT | 82251 | EXPRESSWAY | 1953 |

| | | | |
|---------|-------------|------------|--------------|
| STATE | PROJECT NO. | SHEET NO. | TOTAL SHEETS |
| MI | 175/150/23 | 38 | 38 |
| CITY | ROUTE | PROJECT | DATE |
| DETROIT | 82251 | EXPRESSWAY | 1953 |

Sta. 145+50 To Sta. 156+00
 EARTH EXCAVATION
 Embankment 125%
 This Sheet
 Removal Sh. 1/6
 Grading Sh. 2/96
 Select Yellow Clay Backfill
 Drainage This Sh. 400
 Removal Sh. 1/6
 Removal Sh. 1/6
 70% Loose 1/3, 69%
 75% 1/7 Cu. Yds.
 WASTE
 SUBBASE (Compacted in Place) 125%
 SELECTED SUBBASE MATERIAL 125%
 125% 1/3 Cu. Yds.



WALTER P. CHRYSLER
 EXPRESSWAY PROFILES
 STA. 145+50 TO STA. 156+00
 82251E C32

FILE NO. PROJECT SHEET NO. 82251 175/150/23 38

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|----------------------------|------|
| OPERATION | DATE |
| PRELIMINARY R.O.W. CHECKED | |
| FINAL DESIGN CHECKED | |
| FINAL DESIGN | |
| FINAL DESIGN CHECKED | |
| TRACED | |
| QUANTITIES CHECKED | |
| FINAL R.O.W. CHECKED | |
| APPROVED | |

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

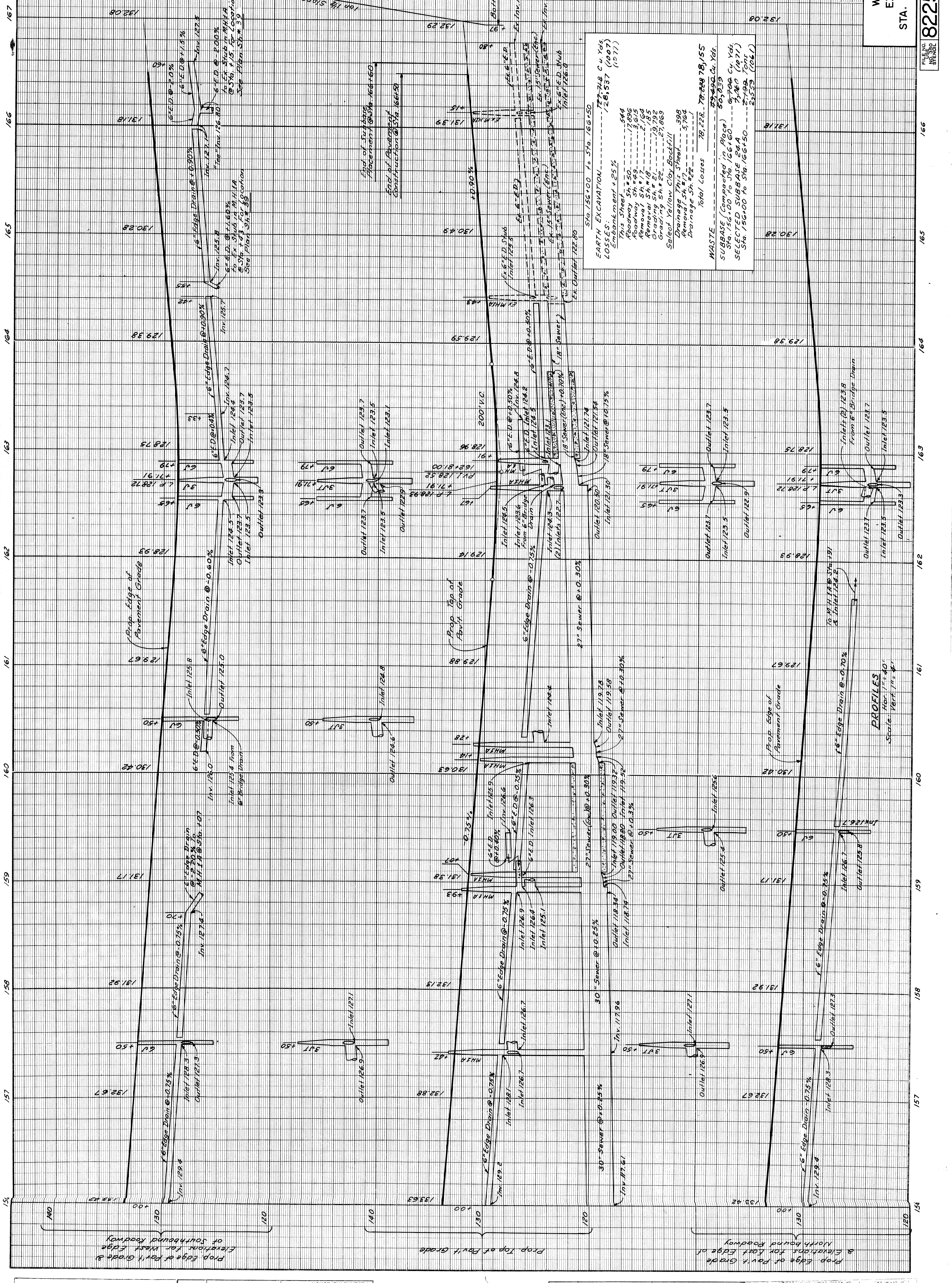
OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

OPERATION
 DATE
 PRELIMINARY R.O.W. CHECKED
 FINAL DESIGN CHECKED
 FINAL DESIGN
 FINAL DESIGN CHECKED
 TRACED
 QUANTITIES CHECKED
 FINAL R.O.W. CHECKED
 APPROVED

| | | | |
|--------|-----------|-----------|--------------|
| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| MICH | 7-51(55) | 40 | 40 |
| COUNTY | ROUTE | PROJECT | DATE |
| Wayne | 2221 | Detroit | |

WALTER P. CHRYSLER
EXPRESSWAY PROFILES
STA. 156+00 TO STA. 166+50

FILE NO. 82251E C32
SHEET 40 OF 40
DATE 12/15/54



EARTH EXCAVATION
 574.156+00 To 574.166+50
 128,537 (1007)
 10,711 (1071)
LOSSES:
 Embankment + 25%
 This Street 544
 Roadway Sh. # 50 17,896
 Roadway Sh. # 59 3,013
 Roadway Sh. # 18 2,185
 Roadway Sh. # 18 2,185
 Grading Sh. # 21 19,792
 Grading Sh. # 22 27,863
 Select Yellow Clay Backfill
 Drainage This Street 308
 Removal Sh. # 22 5,764
 Drainage Sh. # 22 1,111
 Total Losses 78,228.78-268.78/155

WASTE
 574.156+00 To 574.166+50
 50,729
 9,700 C4 Y65
 7,260 (471)
 2,799 T6M1
 2,553 (266)

PROFILES
 Hor. 1" = 40'
 Vert. 1" = 4'

| | | |
|----------------------------|------|----------------|
| OPERATION | DATE | BY |
| PRELIMINARY R.O.W. CHECKED | 7/62 | R.P. WILKINSON |
| FINAL DESIGN CHECKED | 7/62 | R.A. COMPTON |
| TRACKED | 7/62 | |
| FINAL R.O.W. CHECK | 7/62 | |
| QUANTITIES CHECKED | 7/62 | |
| SQUAD | 7/62 | |

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

OPERATION
 CHECKED
 PROFILE PLOTTED
 PLAN CHECKED
 FEDERAL INSPECTION
 PRELIMINARY GRADE
 GRADE INSPECTION

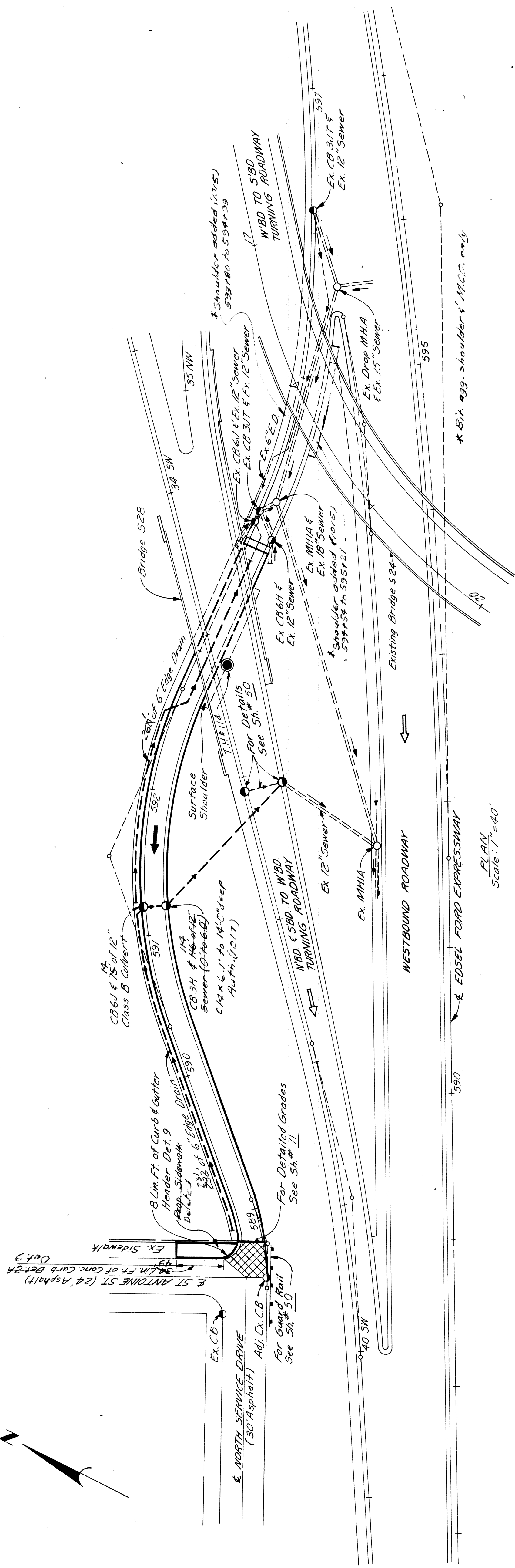
| DIV. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
|----------|-------|------------|-----------|--------------|
| 4 | MICH. | 7-72(60)57 | 54 | 54 |
| ROUTE | CITY | COUNTY | PROJECT | |
| 82251 | Wayne | Detroit | 54 | |

Drainage Auth. No's.
1266
1269

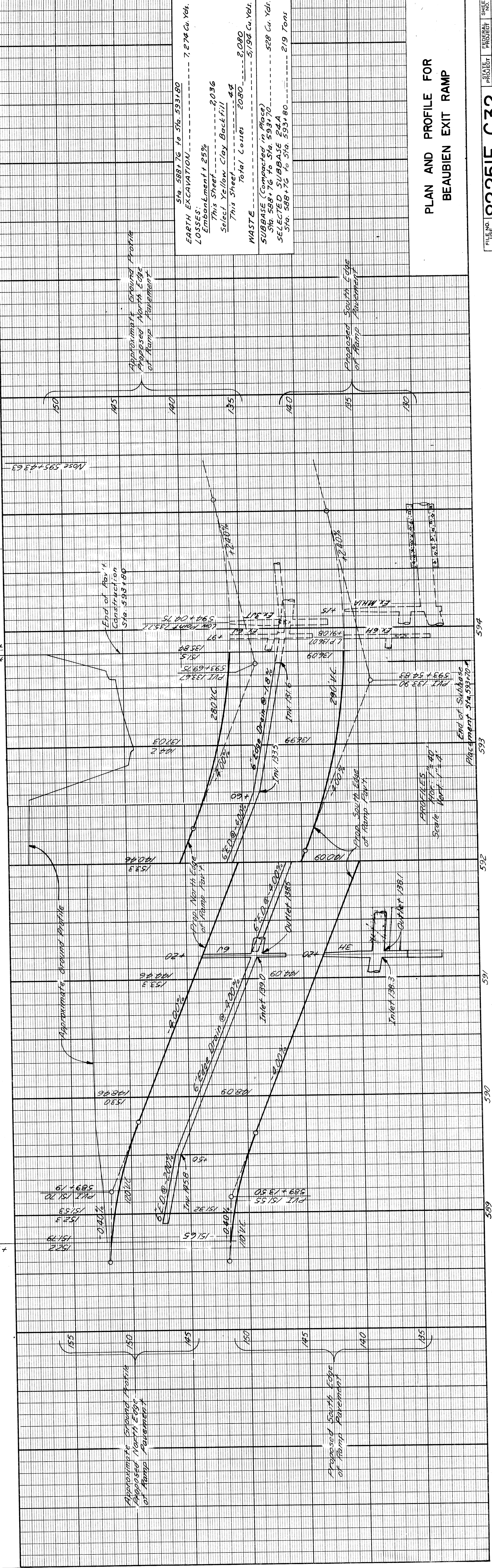
Notes:
For Alignment See Sht #28
For Grading See Sht #21
For Index of Soil Borings (Symbol) See Sht #80

Remove Asphalt Surface shown cross-hatched to provide for a smooth connection and to adjust crown. Replace with Bituminous Concrete Pavement.

61 54 Yds. Removing Asphalt Surface Total @ 54 Yds. Bituminous Conc. This Sheet 3 Tons Bit. Conc. Wearing Course 3 Tons Bit. Conc. Letteing Course



2" Bit. Conc. on Conc. Ramp Base Course - 9" Uniform Scale: 1"=40'



| | |
|---|----------------|
| Sta. 588+76 to Sta. 591+00 | 7,274 Cu. Yds. |
| EARTH EXCAVATION | |
| LOSSES: Embankment 22% | |
| This Street 2036 | |
| Subcl. Yellow Clay Backfill 48 | |
| This Sheet 2080 | |
| Total Cores 2080 | |
| 2,080 Cu. Yds. | |
| SUBBASE (Compacted in Place) | |
| Sta. 588+76 to Sta. 591+70 528 Cu. Yds. | |
| SELECTED SUBBASE 24.4 | |
| Sta. 588+76 to Sta. 591+80 219 Tons | |

PLAN AND PROFILE FOR
BEAUBIEN EXIT RAMP

| OPERATION | DATE | BY |
|----------------------------|------|----|
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| PLAN CHECKED | | |
| PROFILE CHECKED | | |
| PRELIMINARY GRADE | | |
| FEDERAL INSPECTION | | |

PLANS PREPARED BY
DEPARTMENT OF PUBLIC WORKS
CITY OF DETROIT
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

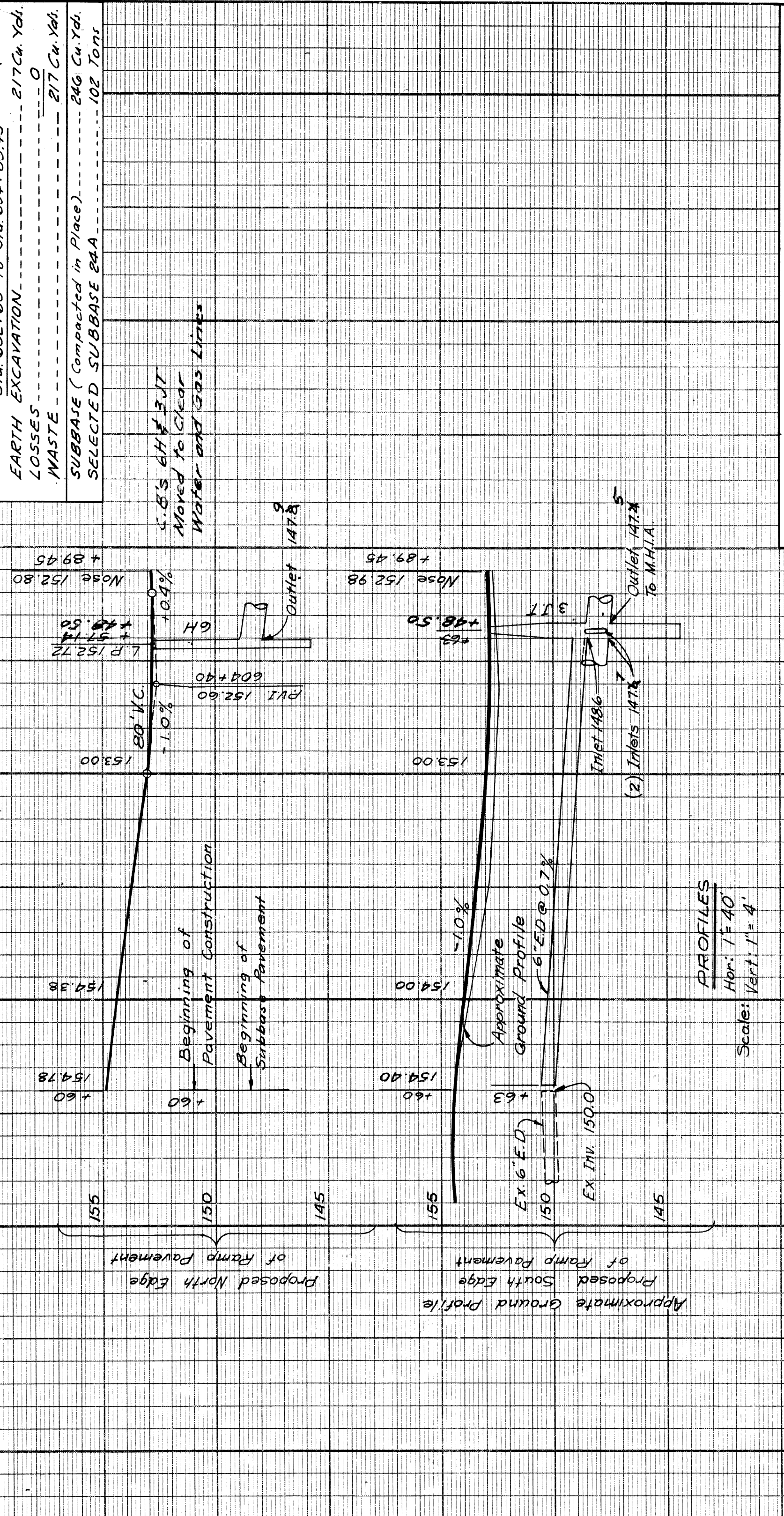
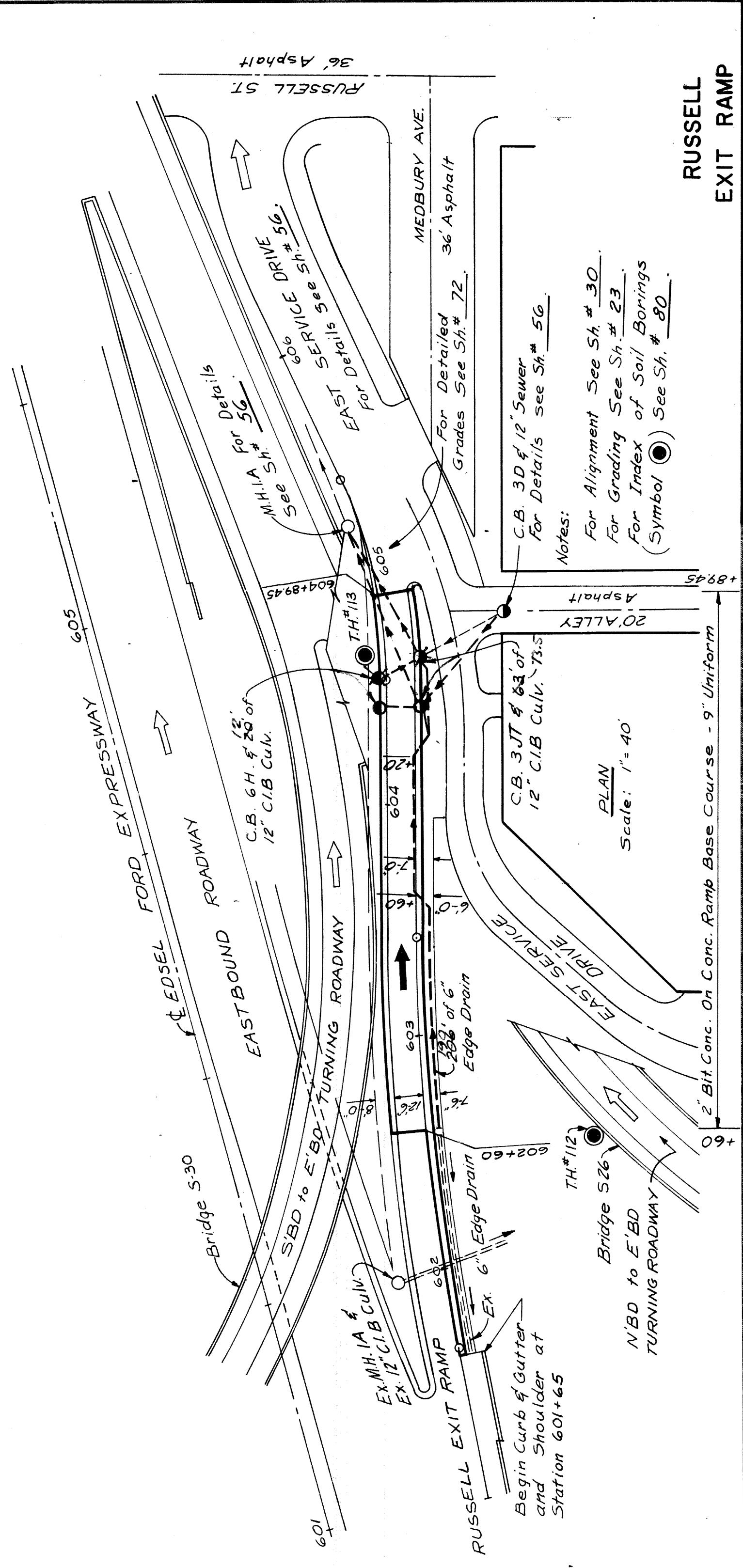
DATE: _____ BY: _____

OPERATION: SURVEYED 7/62
PLAN PLOTTED 7/62
PLAN CHECKED 7/62
PROFILE CHECKED 7/62
PRELIMINARY GRADE 7/62
FEDERAL INSPECTION 7/62

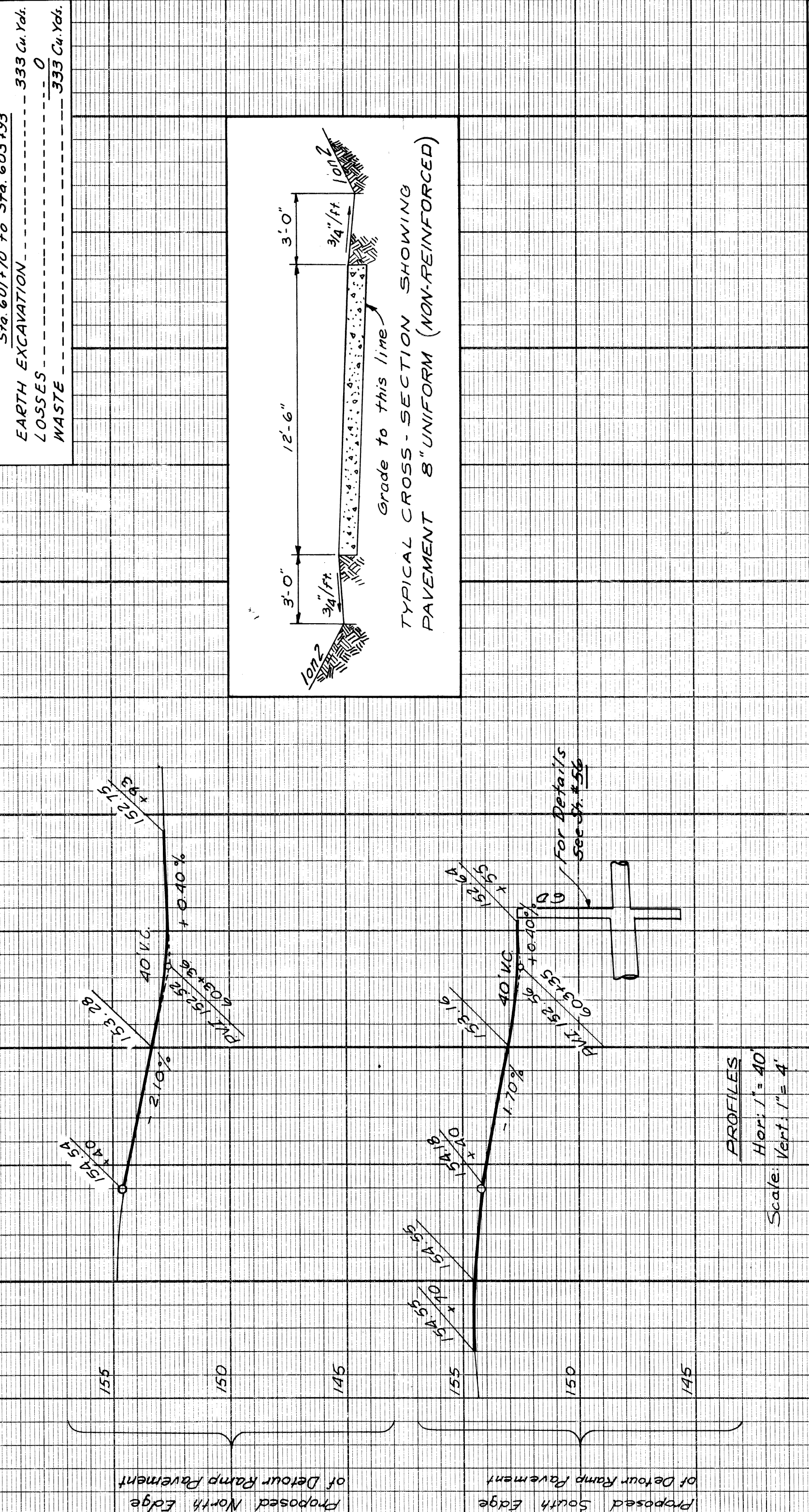
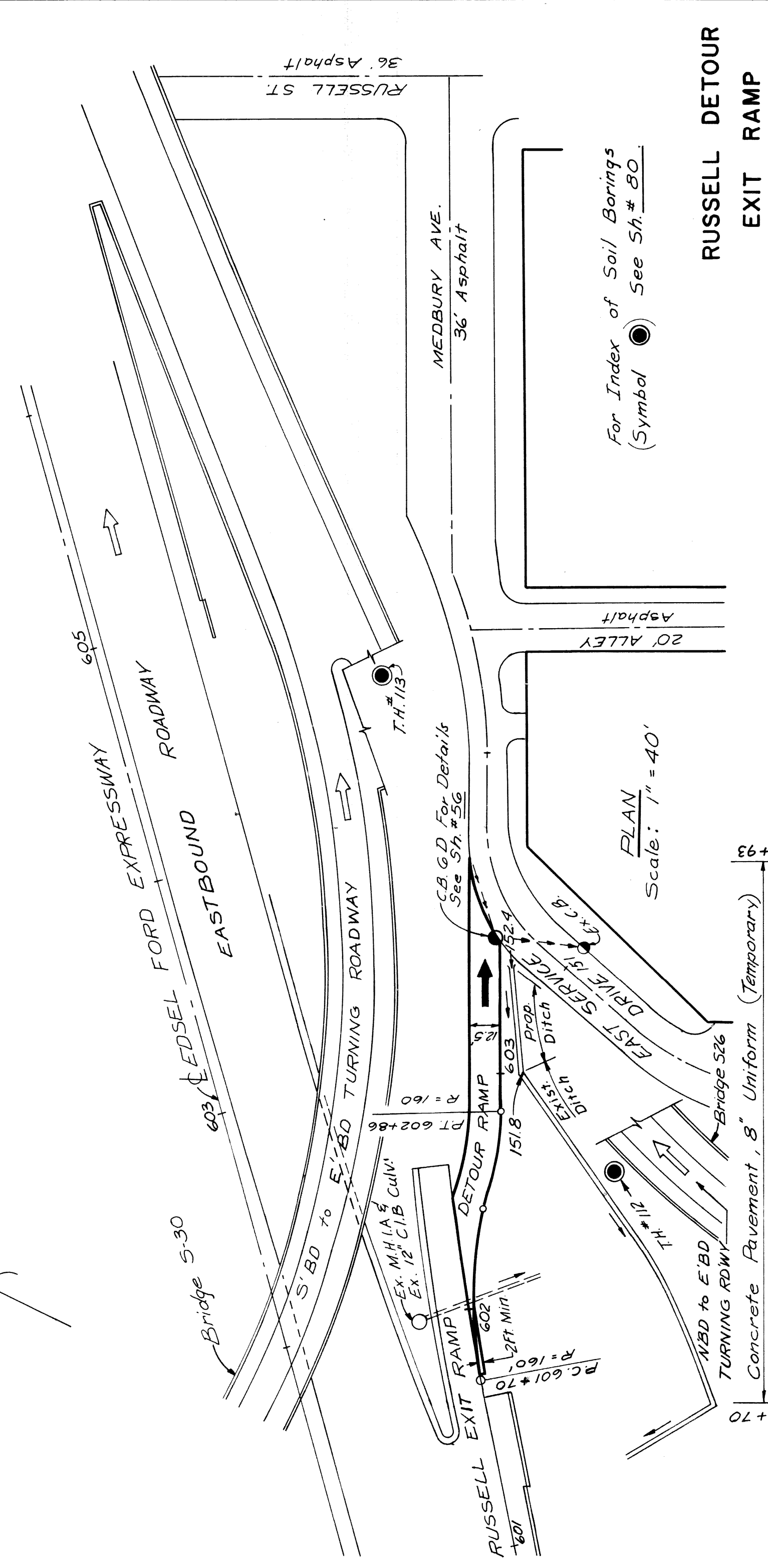
G.M. SMITHS
N.L. KANNAS
A.K. HALL

| | | | | | |
|-------|-------|-----------|-----------|--------------|----|
| STATE | MICH. | PROJ. NO. | 727/66/53 | SHEET NO. | 55 |
| CITY | Wayne | COUNTY | Detroit | TOTAL SHEETS | 55 |
| ROUTE | 82251 | PROJECT | Wayne | SHEET NO. | 55 |
| STATE | MICH. | PROJ. NO. | 727/66/53 | SHEET NO. | 55 |
| CITY | Wayne | COUNTY | Detroit | TOTAL SHEETS | 55 |
| ROUTE | 82251 | PROJECT | Wayne | SHEET NO. | 55 |

Drainage Auth. No. 1069
1969
1966



PLAN AND PROFILE FOR
RUSSELL EXIT RAMP
STA. 602+60 TO STA. 604+89.45



PLAN AND PROFILE FOR
RUSSELL DETOUR EXIT RAMP
TO EAST SERVICE DRIVE

| | | |
|------|----|------------------------------------|
| DATE | BY | OPERATION |
| | | PRELIMINARY O.W. CHECKED |
| | | FINAL DESIGN CHECKED |
| | | FINAL DESIGN CHECKED |
| | | QUANTITIES CHECKED |
| | | FINAL R.O.W. CHECK |
| | | TRACED |
| | | CITY ENGINEERS OFFICE |
| | | BUREAU OF HIGHWAYS AND EXPRESSWAYS |
| | | PLANS PREPARED BY |

| | | |
|------|----|------------------------------------|
| DATE | BY | OPERATION |
| | | PRELIMINARY O.W. CHECKED |
| | | FINAL DESIGN CHECKED |
| | | FINAL DESIGN CHECKED |
| | | QUANTITIES CHECKED |
| | | FINAL R.O.W. CHECK |
| | | TRACED |
| | | CITY ENGINEERS OFFICE |
| | | BUREAU OF HIGHWAYS AND EXPRESSWAYS |
| | | PLANS PREPARED BY |

| | | |
|------|----|------------------------------------|
| DATE | BY | OPERATION |
| | | PRELIMINARY O.W. CHECKED |
| | | FINAL DESIGN CHECKED |
| | | FINAL DESIGN CHECKED |
| | | QUANTITIES CHECKED |
| | | FINAL R.O.W. CHECK |
| | | TRACED |
| | | CITY ENGINEERS OFFICE |
| | | BUREAU OF HIGHWAYS AND EXPRESSWAYS |
| | | PLANS PREPARED BY |

W.A. LOCHER
J.A. CHATFIELD
W.A. LOCHER
J.A. CHATFIELD

| | | | | | | | |
|-------|------|---------|-------|-------|-------|---------|-------|
| STATE | MI | PROJECT | 82251 | CITY | Wayne | PROJECT | 82251 |
| ROUTE | 4 | PROJECT | 82251 | ROUTE | 4 | PROJECT | 82251 |
| YEAR | 1953 | PROJECT | 82251 | YEAR | 1953 | PROJECT | 82251 |

Remove Asphalt Surface shown cross-hatched to provide for a smooth connection and to adjust crown. Replace with Bituminous Concrete Pavt.

Drainage Auth. No. 1002

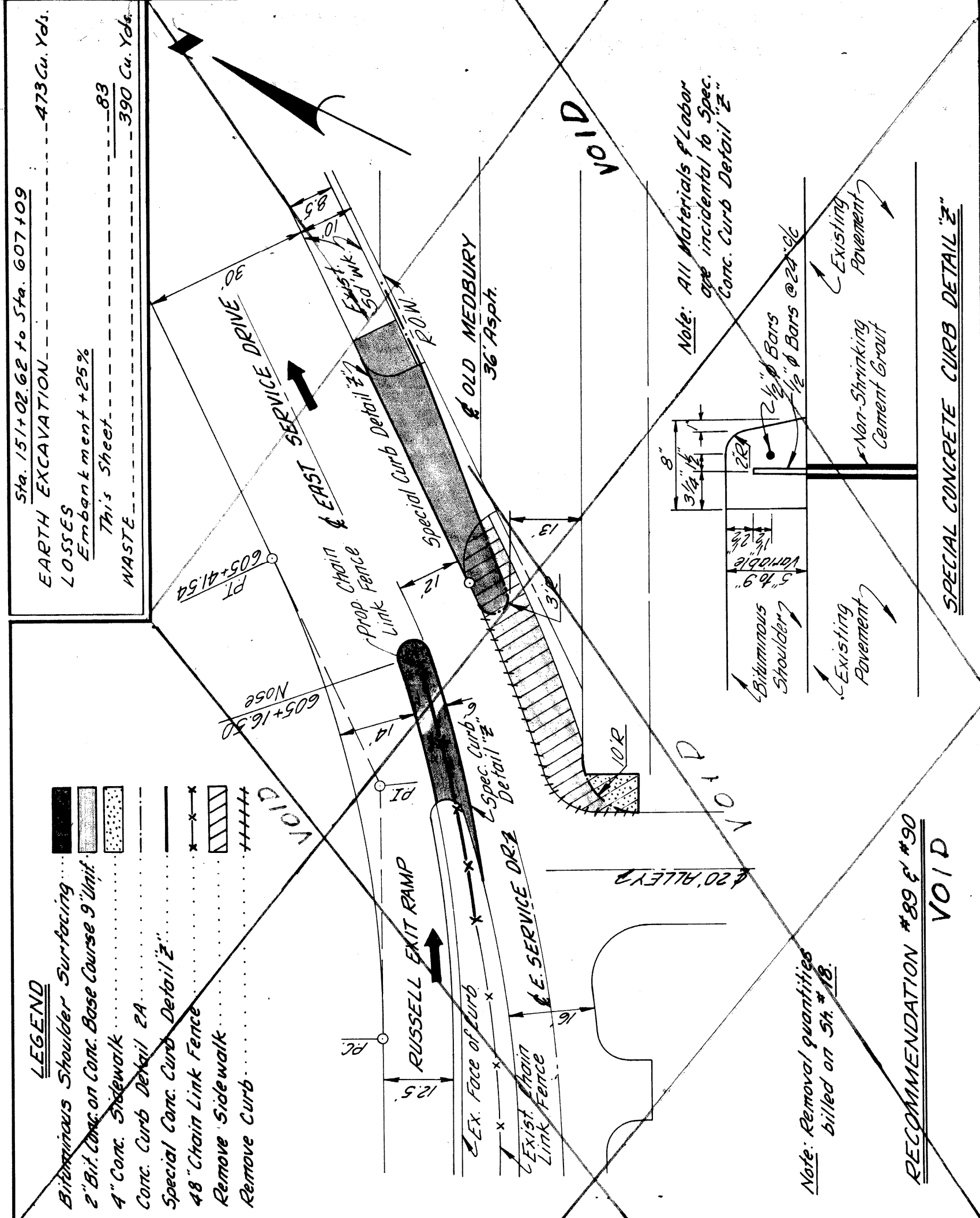
| Station | Remove Exst. Asphalt Sq. Yds. | Bit. Conc. Pavement Sq. Yds. |
|---------------|-------------------------------|------------------------------|
| 152+50 | 44 | 44 |
| 605+50 | 27 | 27 |
| 607+10 | 31 | 31 |
| Totals | 102 (1048) | 102-SR.5 (1074) |

4 & 6 Tons Bituminous Concrete Leveling Course.
 6 & 8 Tons Bituminous Concrete Wearing Course.

Fence (This Street)
 48" Chain Link Fence --- 950 Lin. Ft.
 6" Walk Gate for 48" Chain Link Fence --- 1 Each

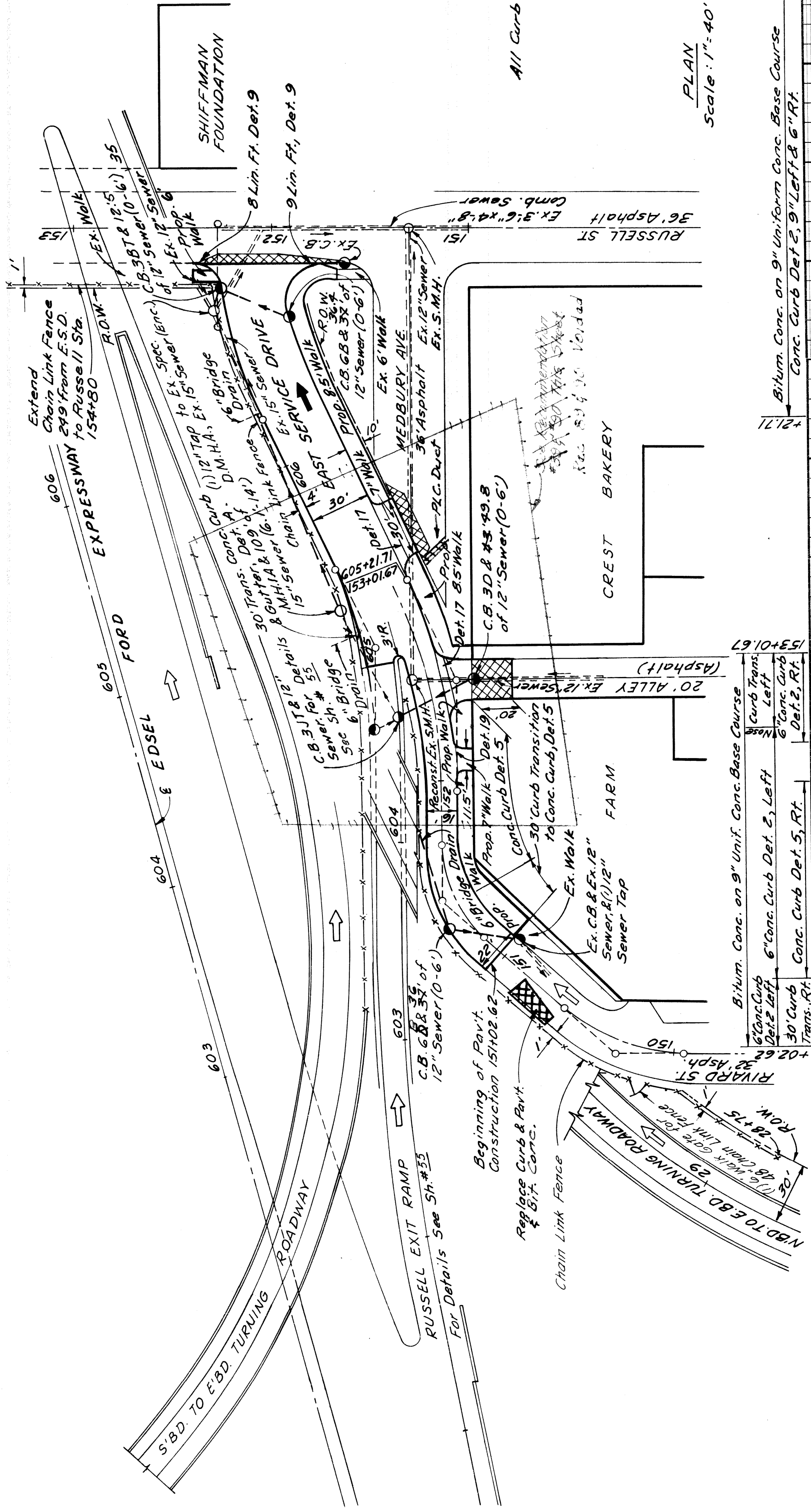
Notes:

For Alignment See Sh. # 30
 For Detailed Grades See Sh. # 72



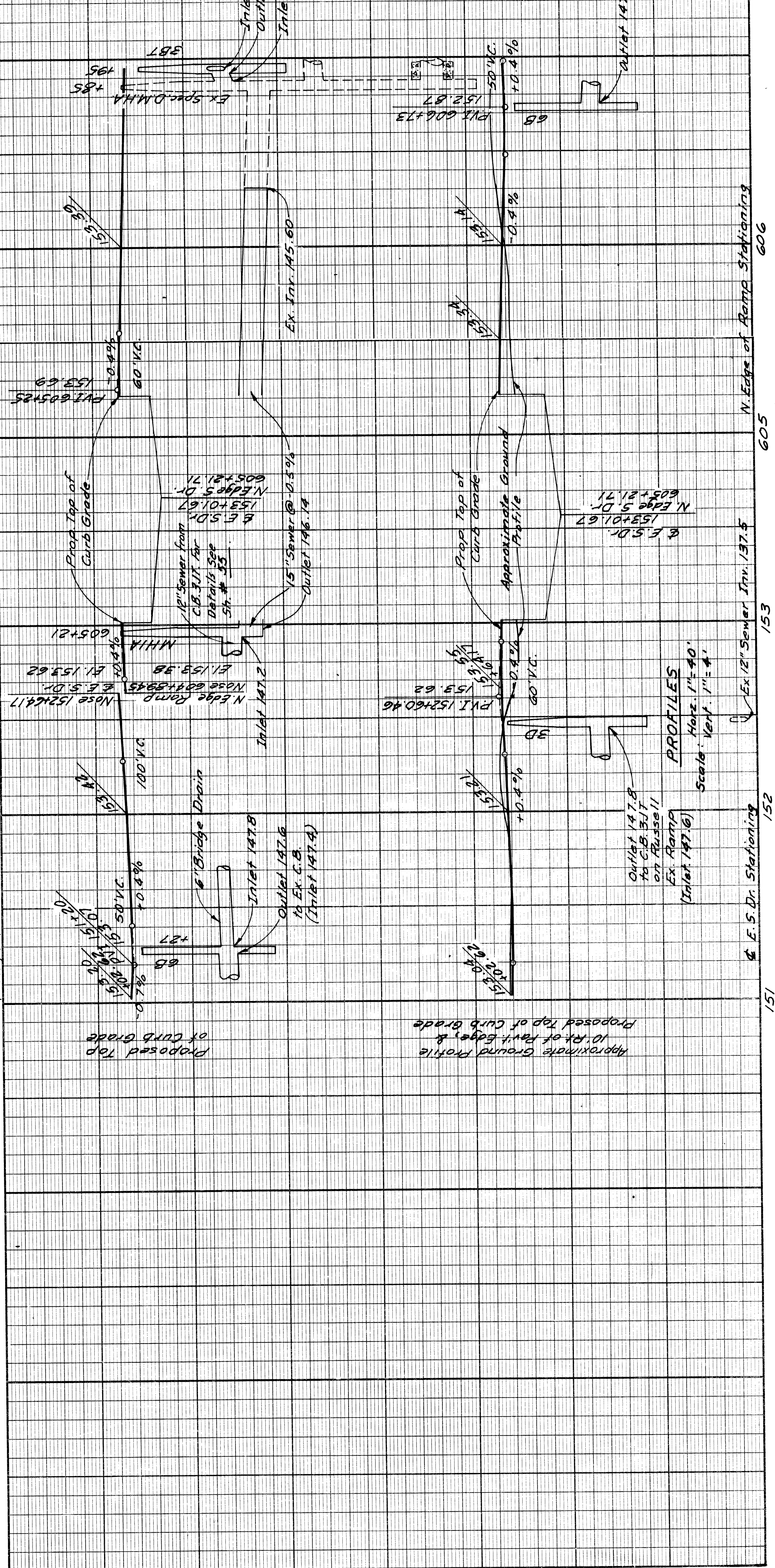
EAST SERVICE DRIVE
 RIVARD ST. TO RUSSELL ST.

FILE NO. PROJECT SHEET
82251E C32
 SHEET NO. 56



PLAN
 Scale: 1" = 40'

All Curb Placed as a Modified Det. 2

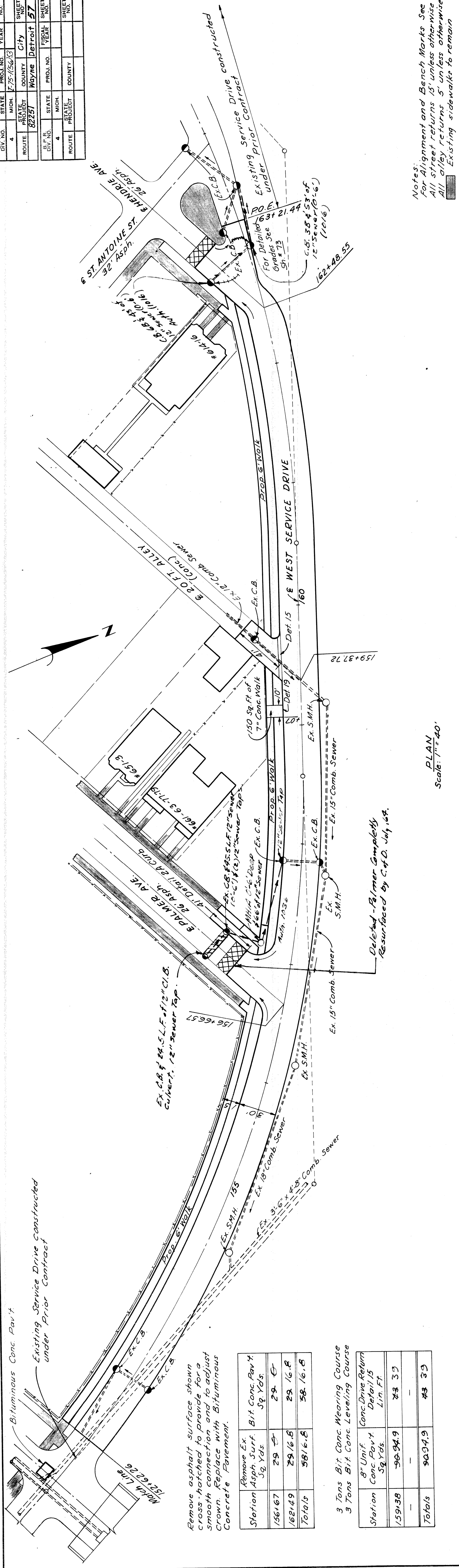


PROFILES
 Scale: Vert. 1" = 4'

| | |
|------------------------------------|----------------------------|
| PLANS PREPARED BY | OPERATION |
| CITY OF DETROIT | PRELIMINARY R.O.W. CHECKED |
| DEPARTMENT OF PUBLIC WORKS | FINAL DESIGN CHECKED |
| CITY ENGINEERS OFFICE | TRADED |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | FINAL R.O.W. CHECK |
| | QUANTITIES CHECKED |
| | SOUND |

| | | |
|-------|---------------------|--------------------|
| DATE | BY | OPERATION |
| 11/22 | J.P. WILKEY | SURVEYED |
| 11/22 | R.A. COMY | PLAN CHECKED |
| 11/22 | M.W. E. [Signature] | PROFILE PLOTTED |
| 11/22 | R.H. WILSON | PRELIMINARY GRADE |
| 11/22 | | GRADE INSPECTION |
| 11/22 | | FEDERAL INSPECTION |

| | | | |
|-------|-----------|-----------|--------------|
| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| MICH. | 7-7-15623 | 57 | 57 |
| CITY | COUNTY | DATE | |
| Wayne | Detroit | 8/22/51 | |



Notes:
 For Alignment and Bench Marks See Sh. 31.
 All street returns 15' unless otherwise noted.
 All alley returns 5' unless otherwise noted.
 Existing sidewalks to remain.

PLAN
 Scale: 1" = 40'

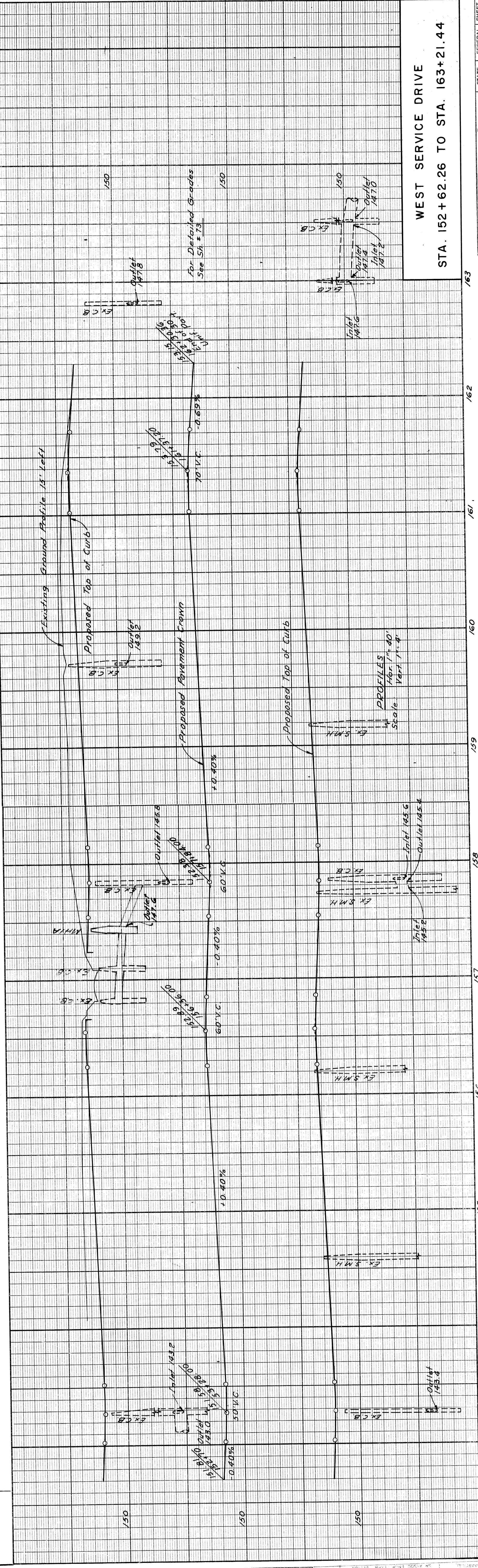
Remove asphalt surface shown cross hatched to provide for a smooth connection and to adjust crown. Replace with Bituminous Concrete Pavement.

| Station | Remove Ex. Asphalt Surf. Sq. Yds. | Bit. Conc. Pav't Sq. Yds. |
|---------|-----------------------------------|---------------------------|
| 156+67 | 29 | 29 |
| 162+49 | 29 | 29 |
| Totals | 58 | 58 |

| Station | 8" Unif. Conc. Drive Return Sq. Yds. | Detail 15 Lin. Ft. |
|---------|--------------------------------------|--------------------|
| 159+38 | 90.94 | 23 32 |
| Totals | 90.94 | 23 32 |

3 Tons Bit. Conc. Wearing Course
 3 Tons Bit. Conc. Leveling Course

9" Uniform Concrete Pavement
 Concrete Curb Detail 2, Lt. & Rt.



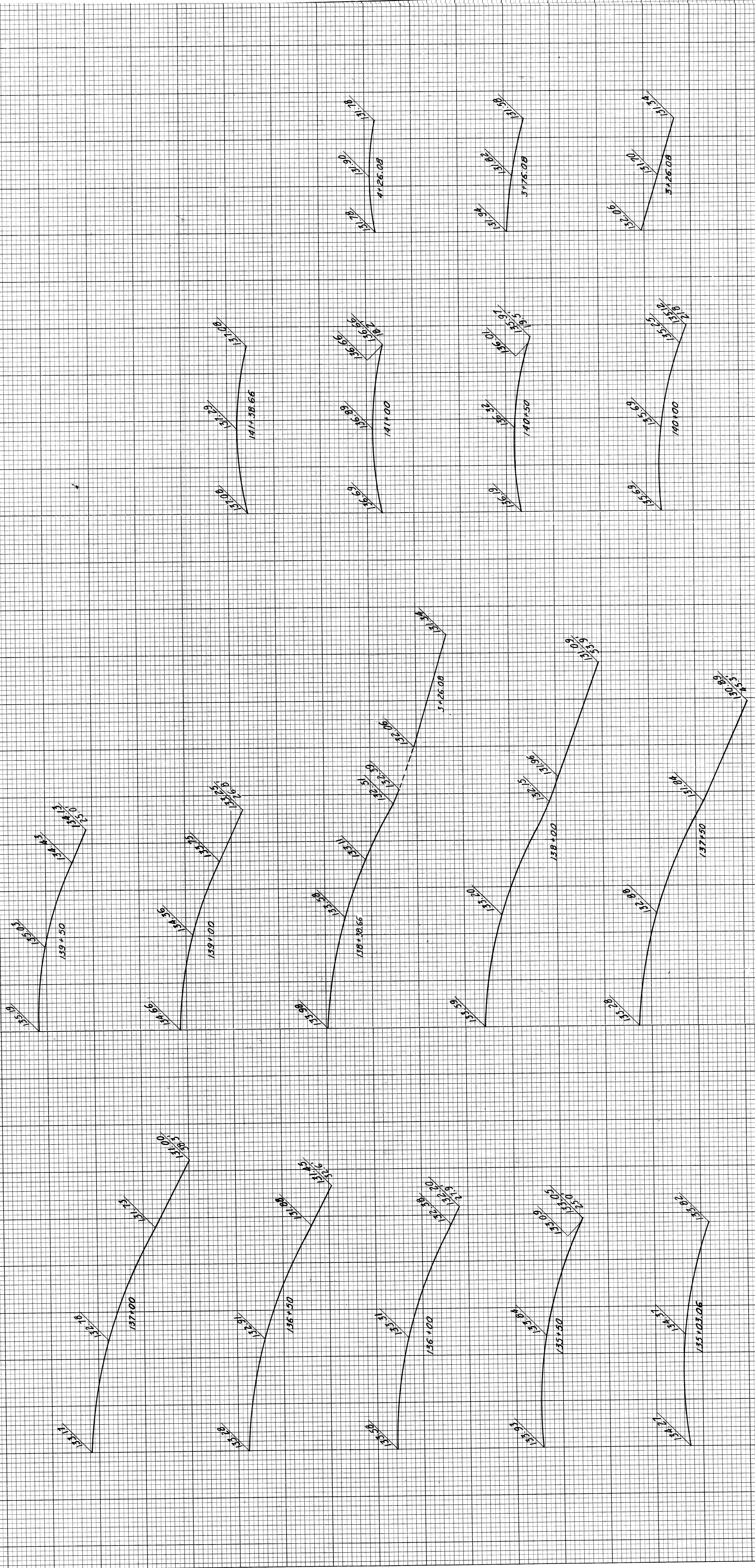
WEST SERVICE DRIVE
 STA. 152+62.26 TO STA. 163+21.44

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|----------------------|----------------------------|
| OPERATION | PRELIMINARY R.C.W. CHECKED |
| DESIGN | FINAL DESIGN CHECKED |
| TRAFFIC | TRAFFIC |
| FINAL DESIGN CHECKED | |
| FINAL DESIGN CHECKED | |
| QUANTITIES CHECKED | |
| QUANTITIES CHECKED | |
| SQUAD | |

| | | |
|------|--------------|--------------------|
| DATE | BY | OPERATION |
| 7/62 | J.V. WICKERY | SURVEYED |
| 7/62 | J.V. WICKERY | PLAN PLOTTED |
| 7/62 | J.V. WICKERY | PROFILE PLOTTED |
| 7/62 | J.V. WICKERY | PRELIMINARY GRADE |
| 7/62 | J.V. WICKERY | GENERAL INSPECTION |

| | | | | | |
|----------|-------|-----------|-------------|-----------|--------------|
| DIV. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| 1 | MICH. | 75-126 | 1954 | 59 | 101 |
| ROUTE | CITY | COUNTY | | | |
| 52251 | Wayne | Detroit | | | |



CROSS-SECTIONS
 Hor. 1"=10'
 Vert. 1"=1'

Note: For Plan and Profiles See Sta. # 58

DETAILED GRADES FOR N'BD. TO E'BD. & W'BD.
 TURNING ROADWAYS AT NORTHBOND ROADWAY

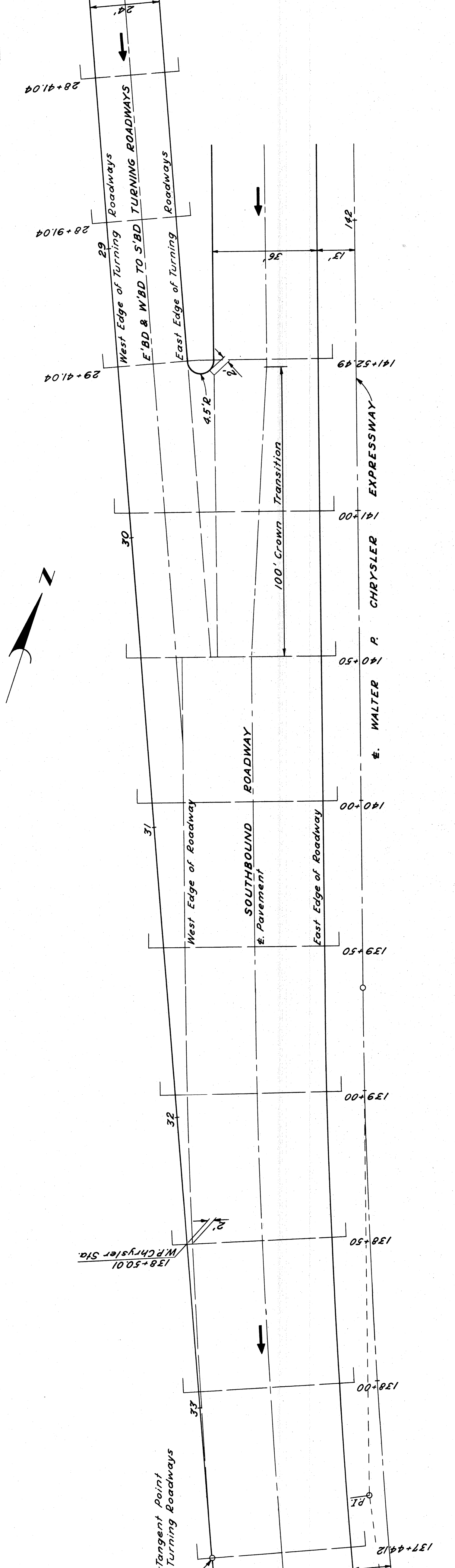
FILE NO. PROJECT SHEET NO. 82251 175/194/51 59

PLANS PREPARED BY
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | | | |
|------|----|---------|---------|
| DATE | BY | PLOTTED | CHECKED |
| | | | |

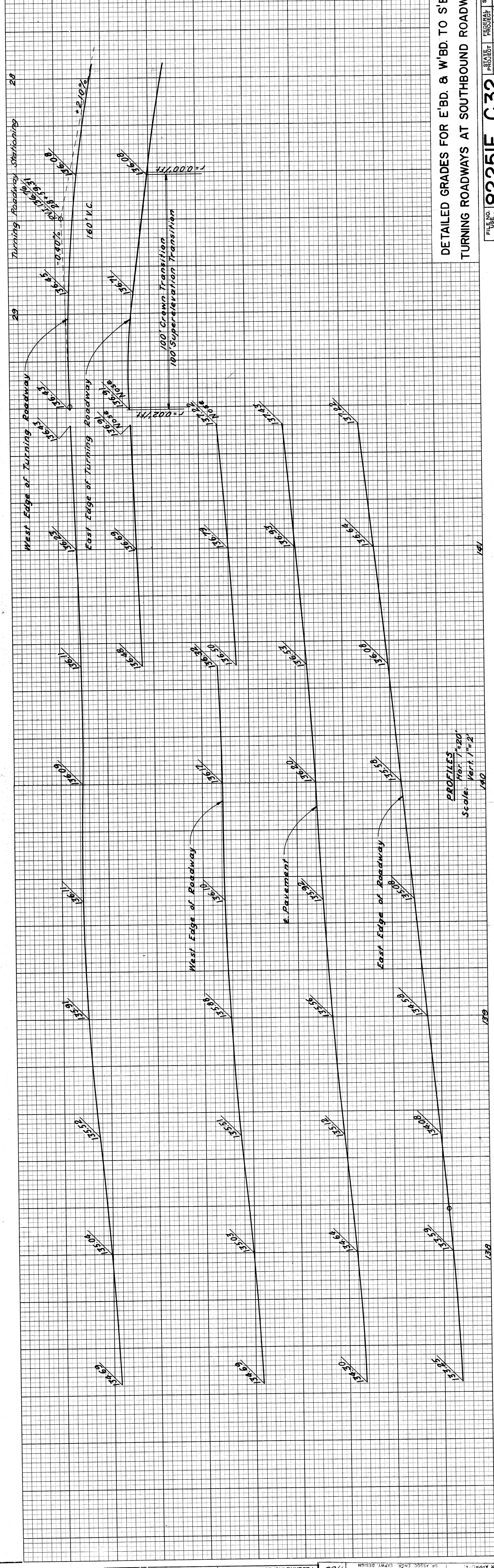
| | | |
|-------|---------------|----------------|
| 11/60 | J. V. Wickley | SCALE ENGINEER |
| 11/60 | J. H. Wickley | SCALE ENGINEER |
| 11/60 | R. S. Wickley | SCALE ENGINEER |
| 11/60 | J. M. Breda | SCALE ENGINEER |
| 11/60 | W. E. Wickley | SCALE ENGINEER |
| 11/60 | W. E. Wickley | SCALE ENGINEER |
| 11/60 | W. E. Wickley | SCALE ENGINEER |
| 11/60 | W. E. Wickley | SCALE ENGINEER |

| | | | |
|-------|-----------|-----------|--------------|
| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| MI. | 275/60/3 | 60 | 60 |
| CITY | CITY | SHEET NO. | TOTAL SHEETS |
| Wayne | Detroit | 60 | 60 |



PLAN Scale: 1"=20'

NOTE: For Cross-Sections See Sh. 61



PROFILES Scale: Hor. 1"=20' Vert. 1"=2'

DETAILED GRADES FOR E'BD. & W'BD. TO S'BD. TURNING ROADWAYS AT SOUTHBOUND ROADWAY

PROJECT NO. 82251E C32 STATE PROJECT NO. 175/60/31 SHEET NO. 60

| | | |
|------------------------------------|----|------|
| OPERATION | BY | DATE |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL R.O.W. CHECK | | |
| QUANTITIES CHECKED | | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | | |

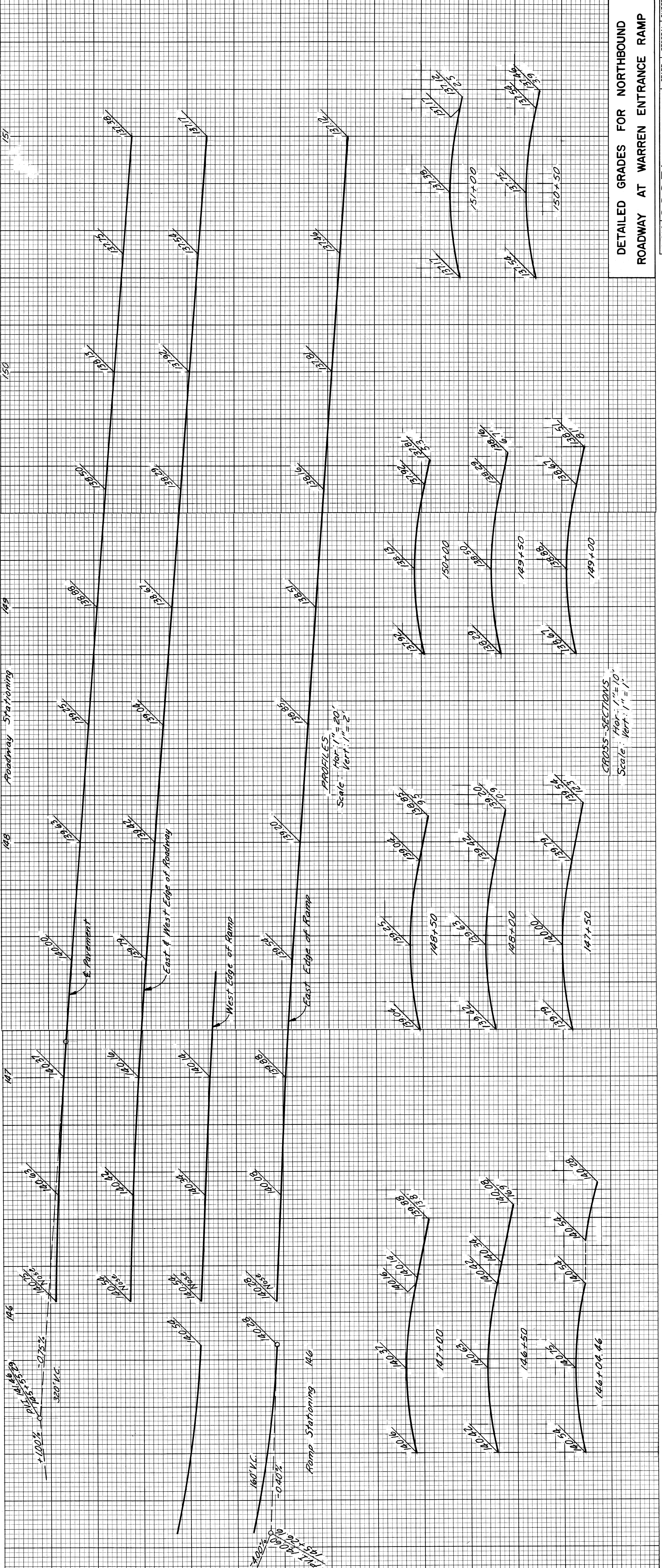
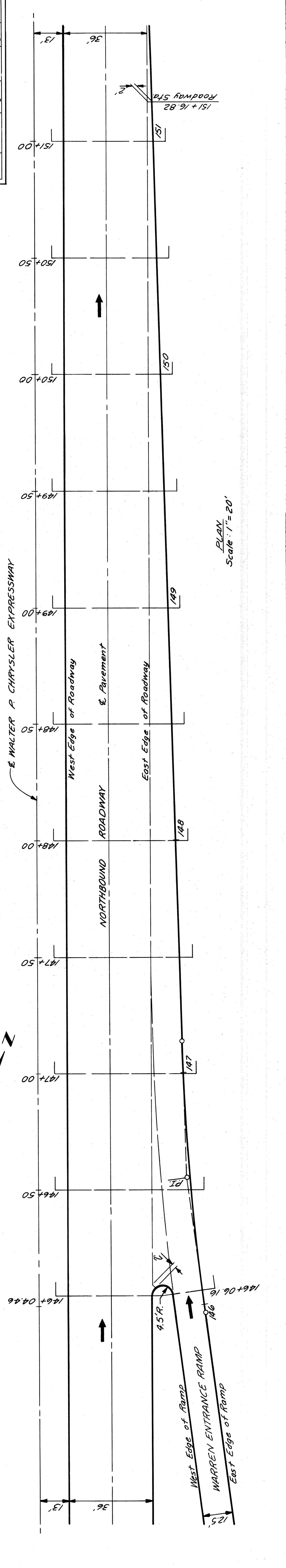
| | | |
|------------------------------------|----|------|
| OPERATION | BY | DATE |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL R.O.W. CHECK | | |
| QUANTITIES CHECKED | | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | | |

| | | |
|------------------------------------|----|------|
| OPERATION | BY | DATE |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL R.O.W. CHECK | | |
| QUANTITIES CHECKED | | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | | |

| | | |
|------------------------------------|----|------|
| OPERATION | BY | DATE |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL R.O.W. CHECK | | |
| QUANTITIES CHECKED | | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | | |

| | | |
|------------------------------------|----|------|
| OPERATION | BY | DATE |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FINAL R.O.W. CHECK | | |
| QUANTITIES CHECKED | | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | | |

| | | | | |
|-------------|------------|-------------|-----------|--------------|
| STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| MI. | 7-57(2033) | | 62 | |
| COUNTY | CITY | PROJECT | PROJECT | PROJECT |
| Wayne | Detroit | 82251 | 82251 | 82251 |
| ROUTE | | | | |
| 151 + 16.82 | | | | |



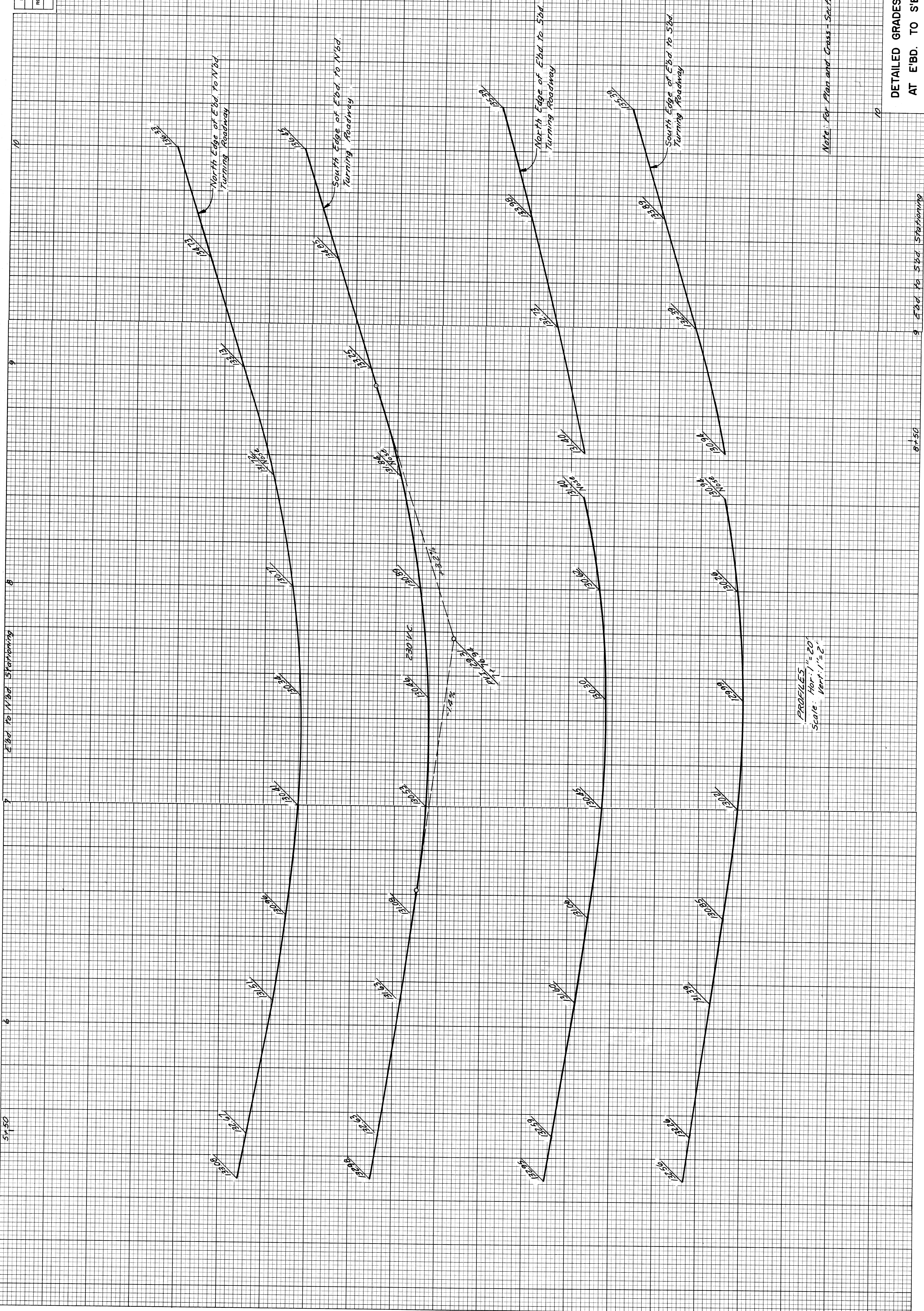
DETAILED GRADES FOR NORTHBOUND
ROADWAY AT WARREN ENTRANCE RAMP

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|---------|------|
| CHECKED | DATE |
| BY | |

| | |
|--------------|--------------|
| FOR APPROVAL | DATE |
| REVIEWED | 7/62 |
| DESIGNED | 7/62 |
| CHECKED | 7/62 |
| IN CHARGE | 7/62 |
| DESIGNED BY | G.M. SMITHS |
| IN CHARGE | J.V. WICKERY |

| | | | | |
|----------|--------|------------|-----------|--------------|
| B.P. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 4 | MICH. | 7-25(36)53 | 1 | 1 |
| ROUTE | COUNTY | CITY | PROJECT | SHEET NO. |
| 82251 | Wayne | Detroit | 65 | 65 |



PROFILES
Scale: Hor. 1"=20'
Vert. 1"=2'

Note: For Plan and Cross - Stationing See 5/4"64.

DETAILED GRADES FOR E'BD. TO N'BD.
AT E'BD. TO S'BD. TURNING ROADWAY

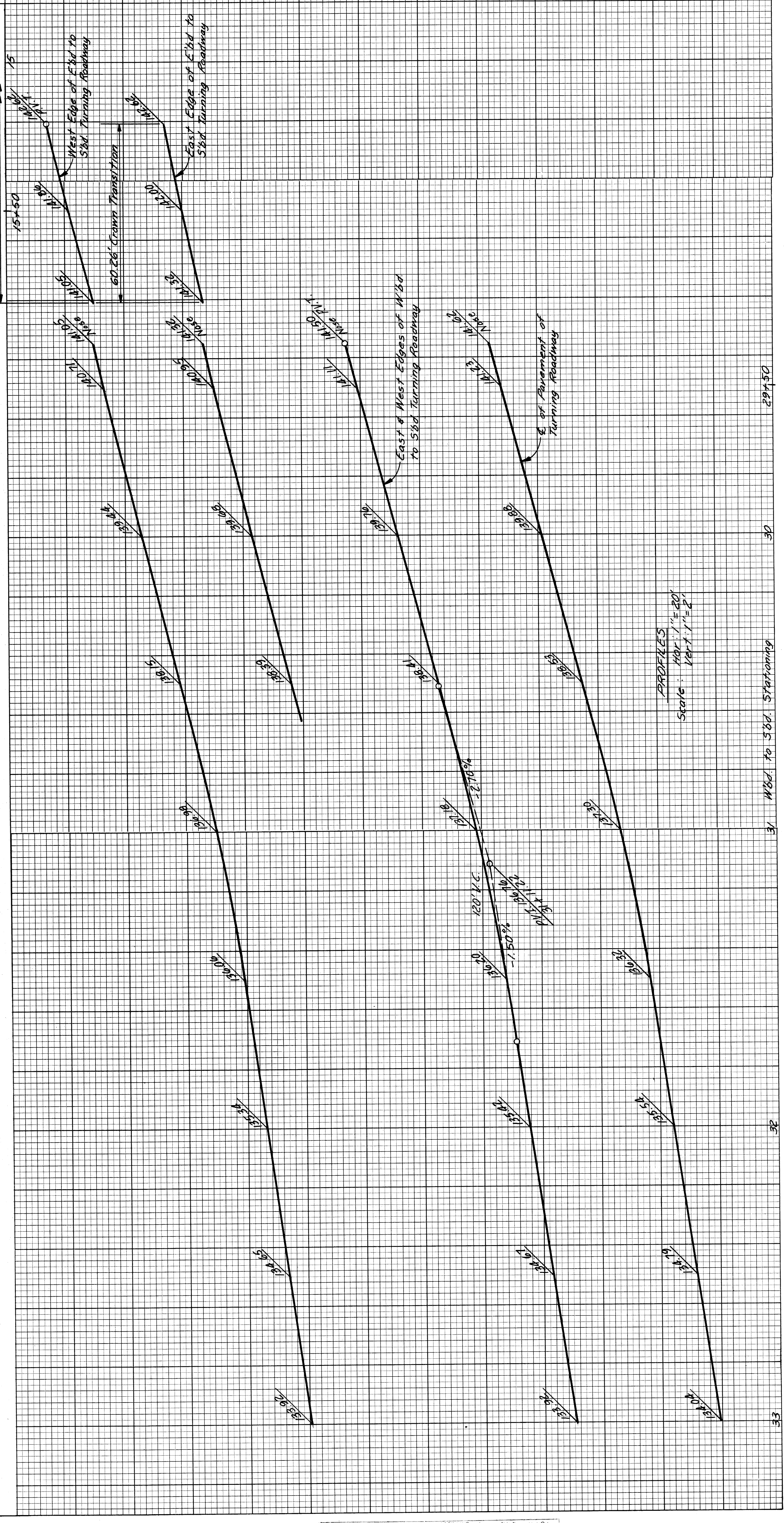
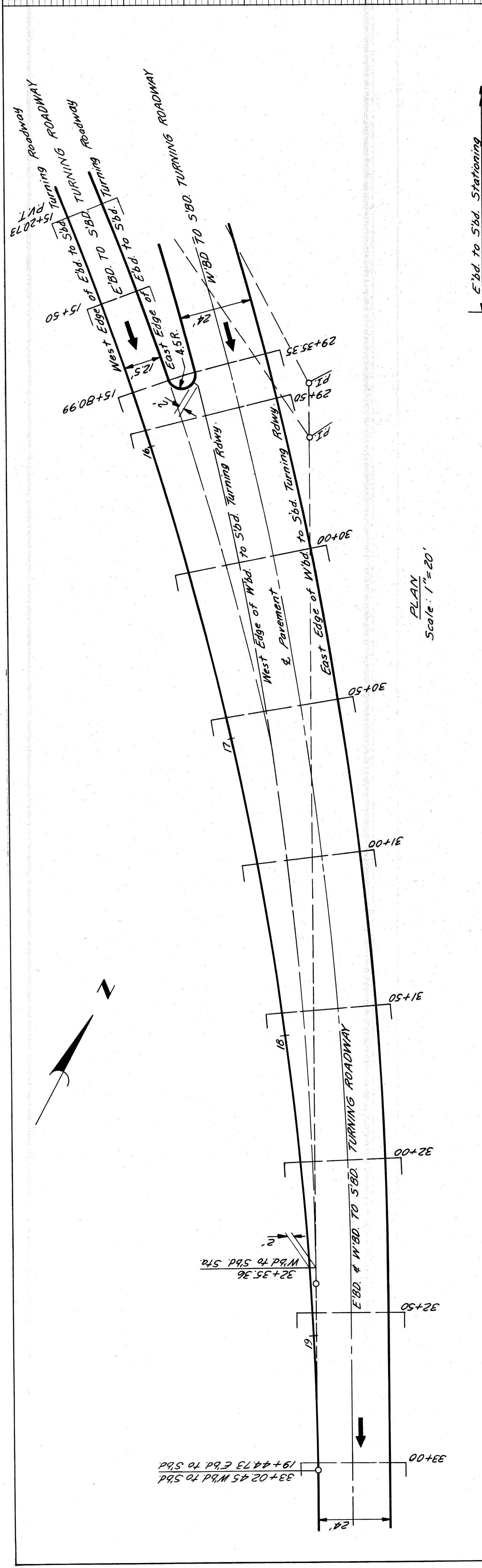
PROJECT NO. 82251
SHEET NO. 65

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

| | |
|------|----|
| DATE | BY |
| | |

| | | | |
|------|------|------|------|
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |

| | | | |
|--------|------------------------|-----------|--------------|
| STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| MICH. | 7-52(59)53 | 71 | 72 |
| COUNTY | CITY | SHEET NO. | TOTAL SHEETS |
| Wayne | Detroit | 66 | 66 |
| ROUTE | 82251 Wayne Detroit 66 | | |



CROSS-SECTIONS
PART 1 of 2
Scale: 1"=1'

DETAILED GRADES FOR
E'BD. TO S'BD. TURNING ROADWAY
AT WBD. TO SBD. TURNING ROADWAY

| | | |
|----------|---------|-------|
| FILE NO. | PROJECT | SHEET |
| 82251 | 82251 | 66 |

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

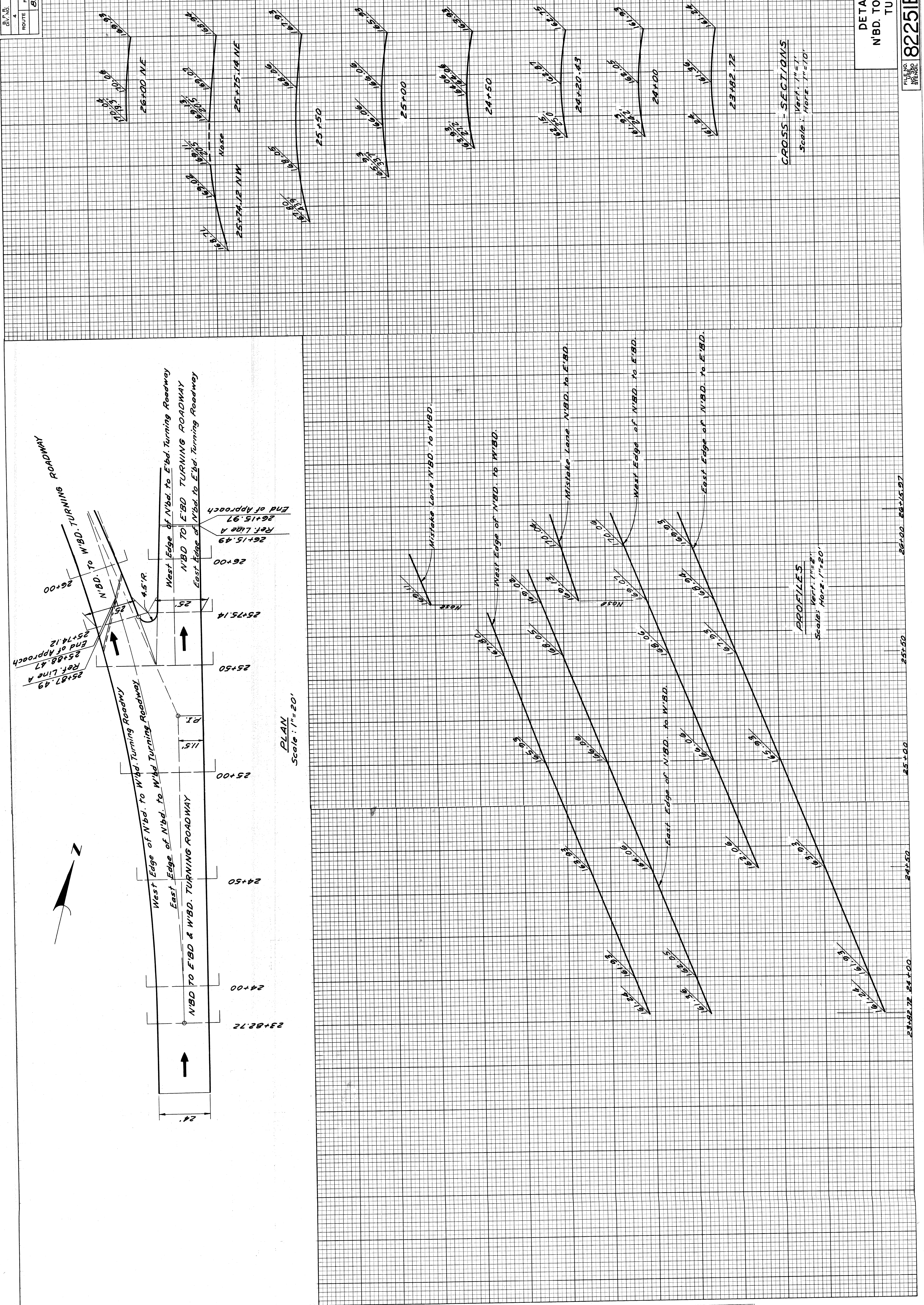
| | | |
|----------|----|------|
| CHECKED | BY | DATE |
| COMPUTED | | |
| PLOTTED | | |

| | | | |
|------|------|------|------|
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |
| 7/62 | 7/62 | 7/62 | 7/62 |

| | | |
|---------|---------------|-------|
| STATE | PROJ. NO. | SHEET |
| MICH. | 225/26633 | 67 |
| COUNTY | CITY | SHEET |
| Wayne | Detroit | 67 |
| PROJECT | ROUTE | |
| 82251 | Wayne Detroit | |

DETAILED GRADES FOR
N'BD. TO E'BD. AT N'BD. TO W'BD.
TURNING ROADWAY

FILE NO. 182251E C32
SHEET NO. 67

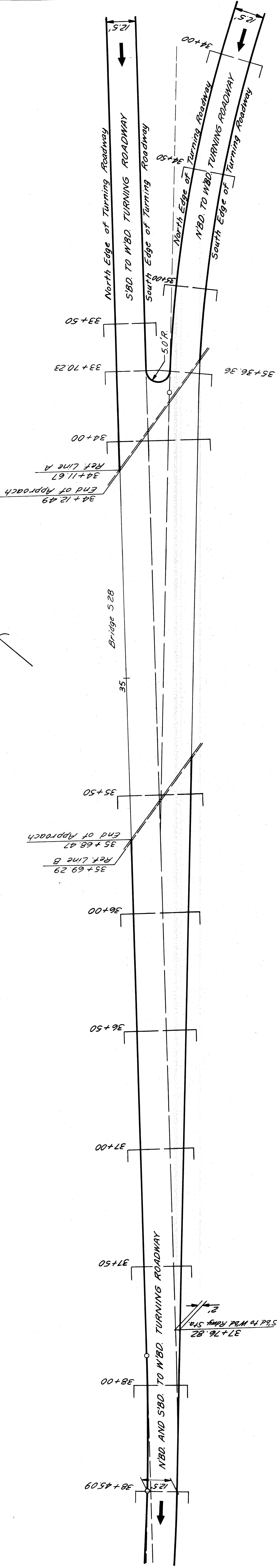
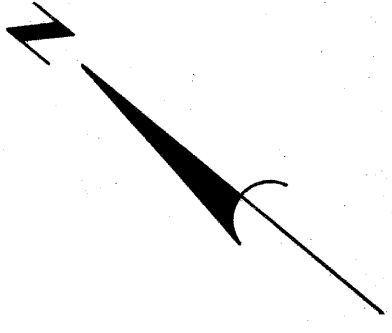


PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

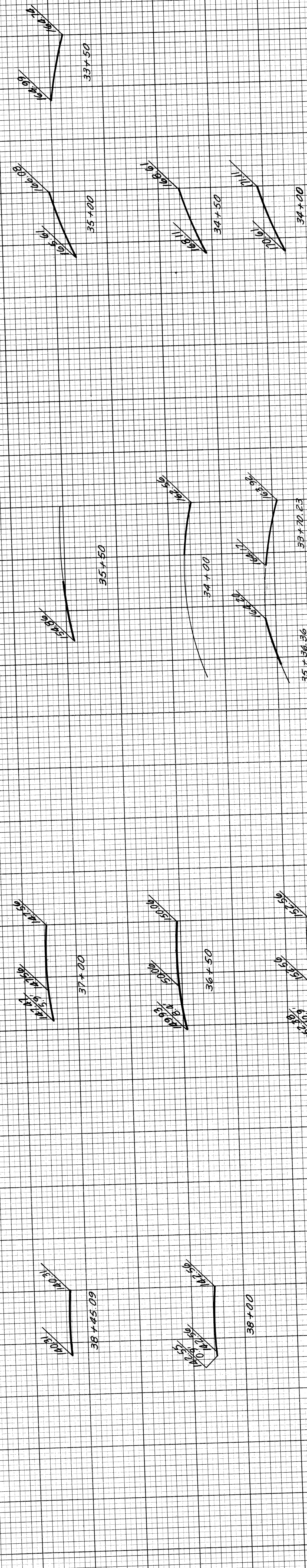
| | |
|------|----|
| DATE | BY |
| | |

| | |
|------|---------------|
| 7/62 | U. V. WICKERY |
| 7/62 | J. H. HOPKINS |
| 7/62 | W. H. HOPKINS |
| 7/62 | W. H. HOPKINS |

| | | | | |
|----------|--------|----------------|-----------|--------------|
| S.P. NO. | STATE | FED. PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 4 | MI | 7-257(50)33 | 68 | 68 |
| ROUTE | COUNTY | CITY | | |
| 82257 | Wayne | Detroit | | |



PLAN
Scale: 1" = 20'



CROSS SECTIONS
For I.F.D.
Scale: 1" = 1'

DETAILED GRADES FOR N'BD. TO W'BD.
AT S'BD. TO W'BD. TURNING ROADWAY

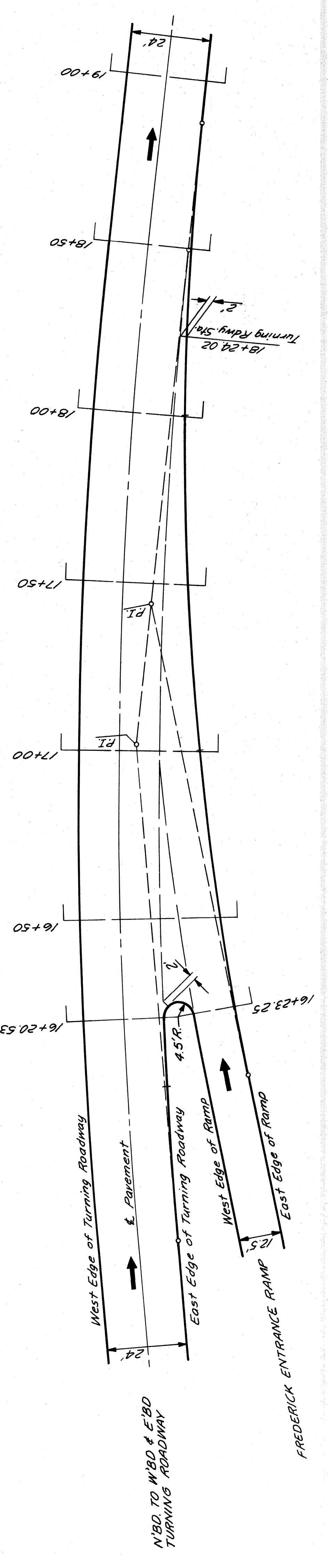
PROJECT NO. 82257
SHEET NO. 68
82257E C32

| | |
|------------------------------------|--|
| PLANS PREPARED BY | |
| CITY OF DETROIT | |
| DEPARTMENT OF PUBLIC WORKS | |
| CITY ENGINEERS OFFICE | |
| BUREAU OF HIGHWAYS AND EXPRESSWAYS | |
| QUANTITIES CHECKED | |
| SQUAD | |
| OPERATION | |
| PRELIMINARY TO W. CHECKED | |
| FINAL DESIGN CHECKED | |
| TRAFFIC ENGINEER | |
| FINAL R.G.W. CHECK | |

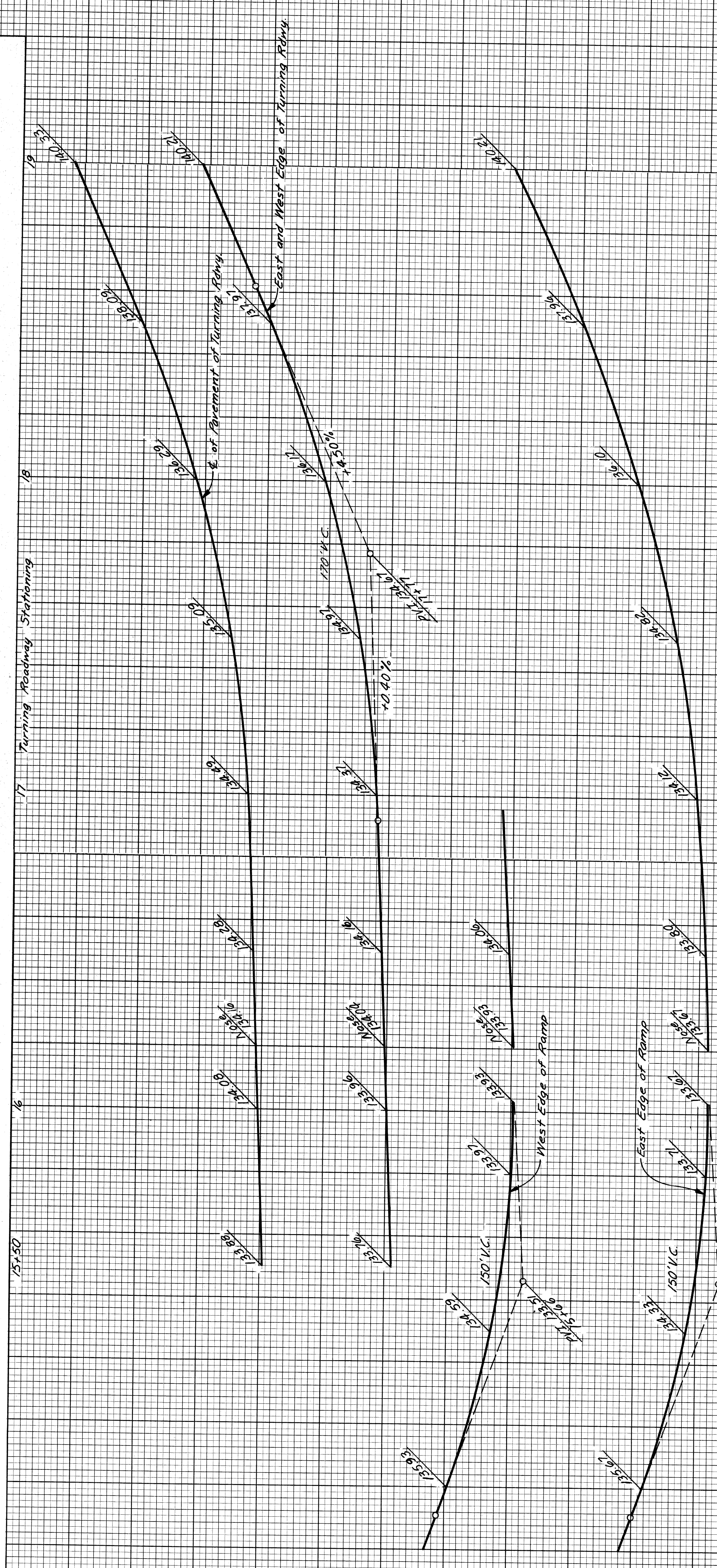
| | | |
|------|----|--------------------|
| DATE | BY | OPERATION |
| 7/62 | | SURVEYED |
| 7/62 | | PLAN PLOTTED |
| 7/62 | | PROFILE CHECKED |
| 7/62 | | PRELIMINARY GRADE |
| 7/62 | | FEDERAL INSPECTION |

FOR APPROVAL
RECOMMENDED
CORRECT
CHECKED BY
DATE
BY

| | | | | |
|-------|-------------|-------------|-----------|--------------|
| STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
| MICH. | 1335(66)123 | | 70 | 70 |
| CITY | COUNTY | CITY | SHEET NO. | TOTAL SHEETS |
| Wayne | Detroit | Wayne | 70 | 70 |



PLAN
Scale: 1" = 20'



PROFILES
Scale: 1" = 20'
Vert. 1" = 2'

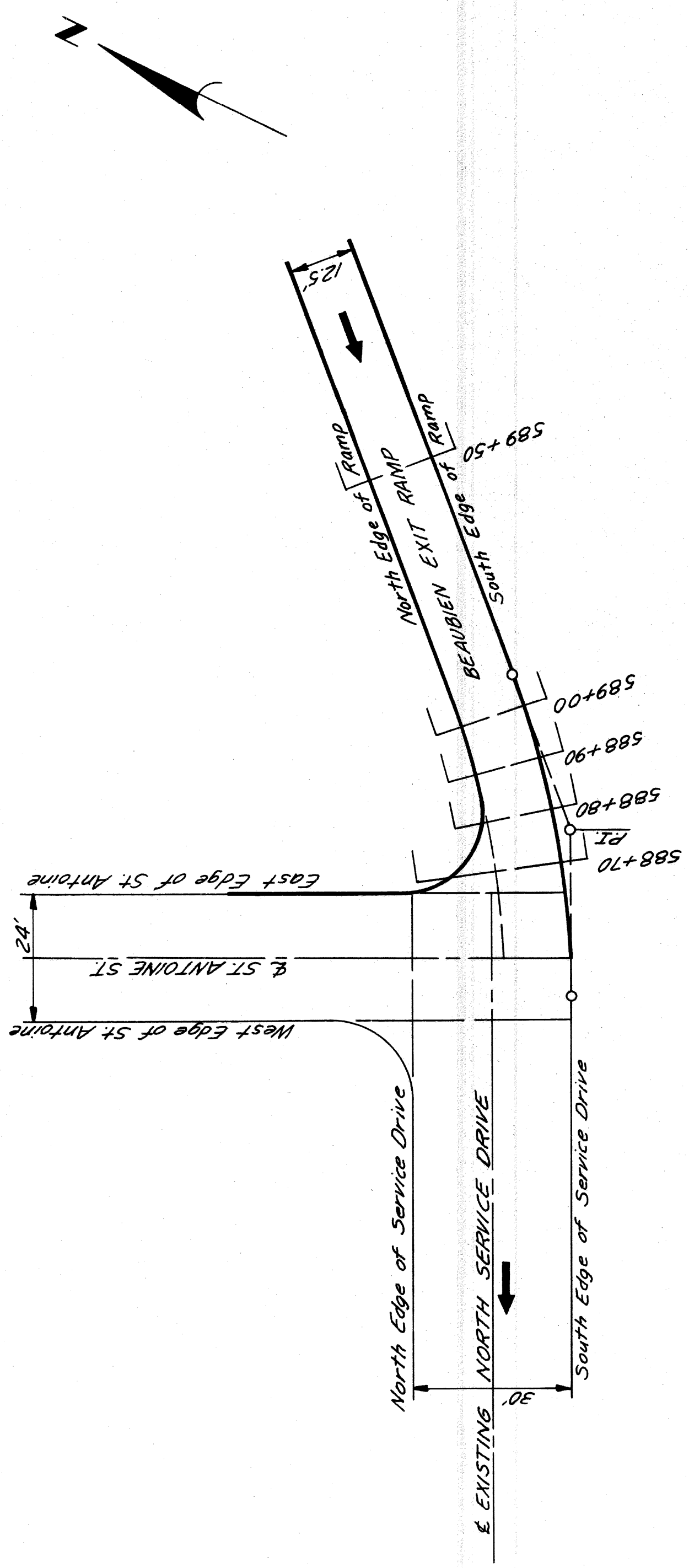
DETAILED GRADES FOR N'BD. TO E'BD. & W'BD.
TURNING RDWY. AT FREDERICK ENTRANCE RAMP

PLANS PREPARED BY
CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
BUREAU OF HIGHWAYS AND EXPRESSWAYS

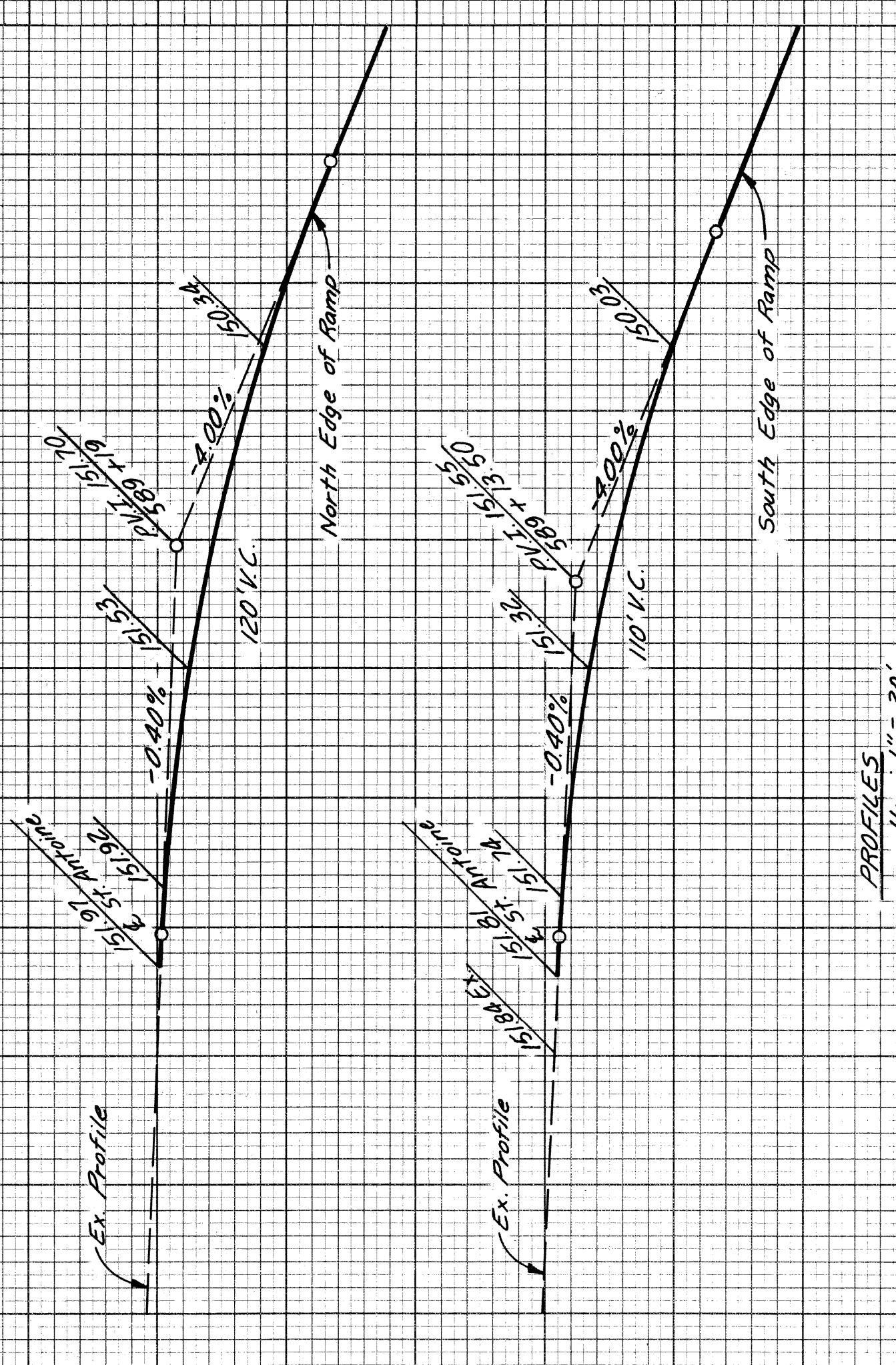
| | | |
|------|----|---------|
| DATE | BY | CHECKED |
| | | |

| | |
|------|-------------|
| 7/62 | U.P. WICKER |
| 7/62 | G.M. SMITH |
| 7/62 | R.A. COMLEY |
| 7/62 | W.H. HAYES |
| 7/62 | M.H. HAYES |

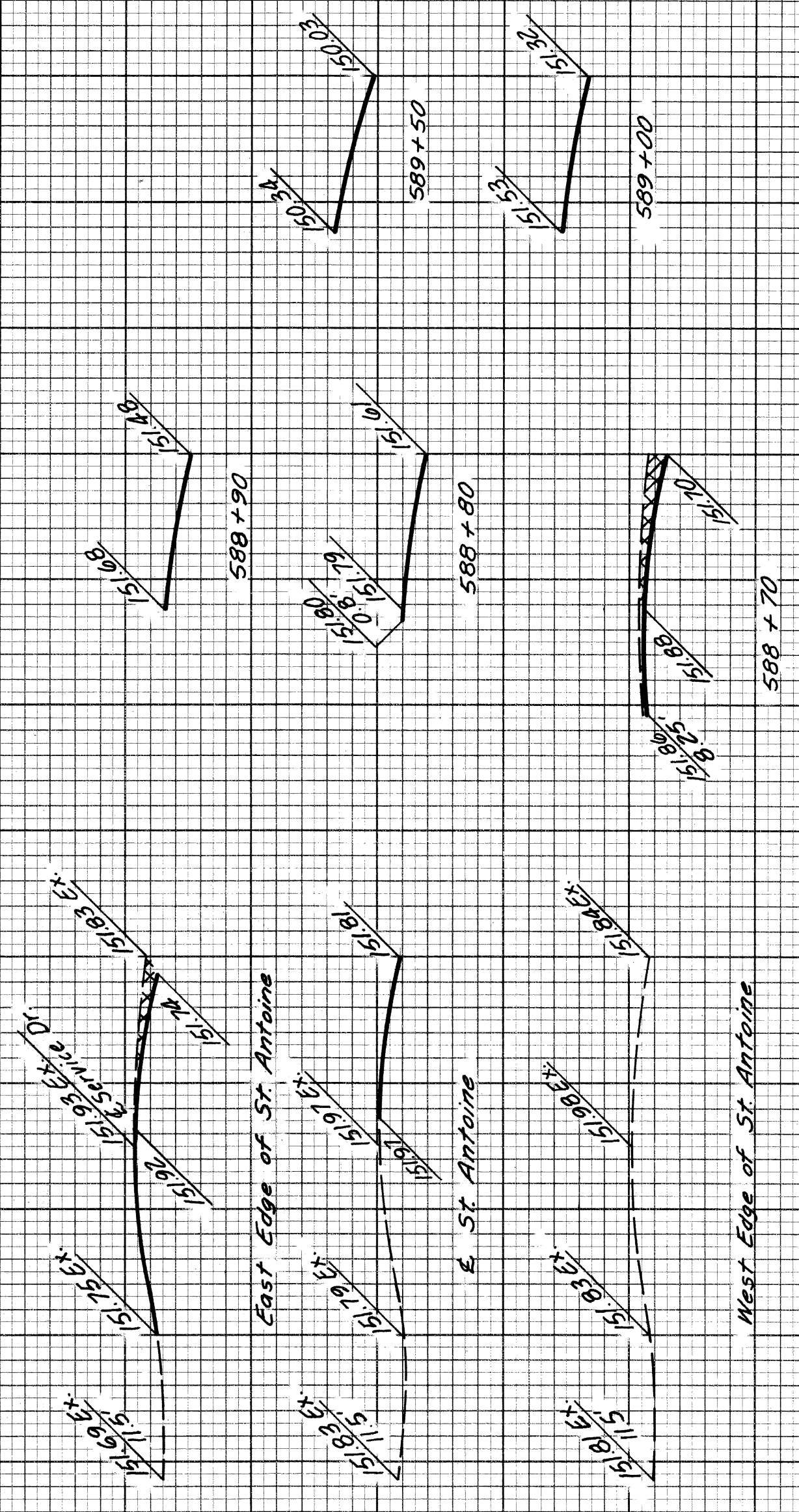
| | | | | |
|----------|---------------|------------|-----------|--------------|
| D.P. NO. | STATE | PROJ. NO. | SHEET NO. | TOTAL SHEETS |
| 4 | MICH. | 1-75/10015 | 17 | 17 |
| ROUTE | PROJECT | COUNTY | SHEET NO. | TOTAL SHEETS |
| 82251 | Wayne Detroit | | 17 | 17 |



PLAN
Scale: 1"=20'



PROFILES
Scale: Hor. 1"=20'
Vert. 1"=2'



CROSS-SECTIONS
Scale: Hor. 1"=10'
Vert. 1"=1'

DETAILED GRADES FOR BEAUBIEN
EXIT RAMP AT ST. ANTOINE

588 Ramp Stationing 589 590

| | | |
|----------------------------|------|----|
| OPERATION | DATE | BY |
| PRELIMINARY R.O.W. CHECKED | | |
| FINAL DESIGN CHECKED | | |
| FRAMED | | |
| QUANTITIES CHECKED | | |
| SQUAD | | |

| | | |
|--------------------|------|------------|
| OPERATION | DATE | BY |
| SURVEYED | 7/62 | G.M. SMITH |
| PLAN PLOTTED | 7/62 | |
| PLAN CHECKED | 7/62 | |
| PROFILES PLOTTED | 7/62 | |
| PROFILE CHECKED | 7/62 | |
| PRELIMINARY GRADE | 7/62 | |
| GRADE INSPECTION | 7/62 | |
| FEDERAL INSPECTION | 7/62 | |

