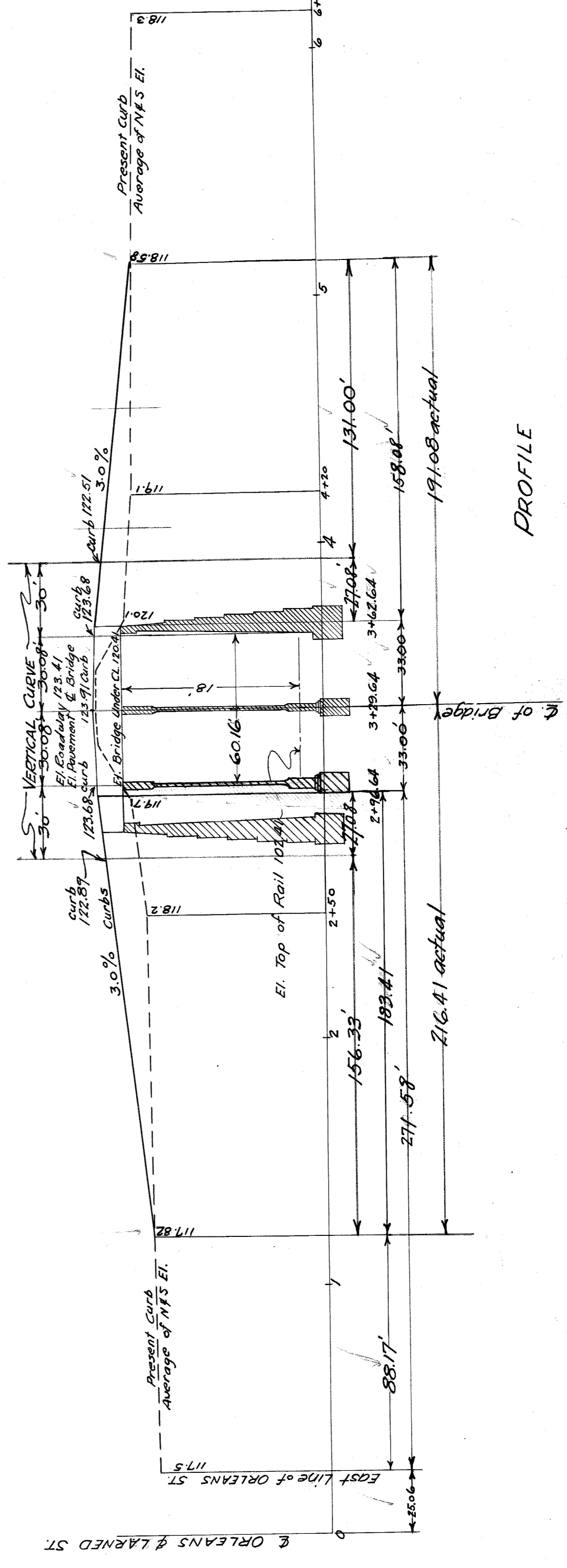
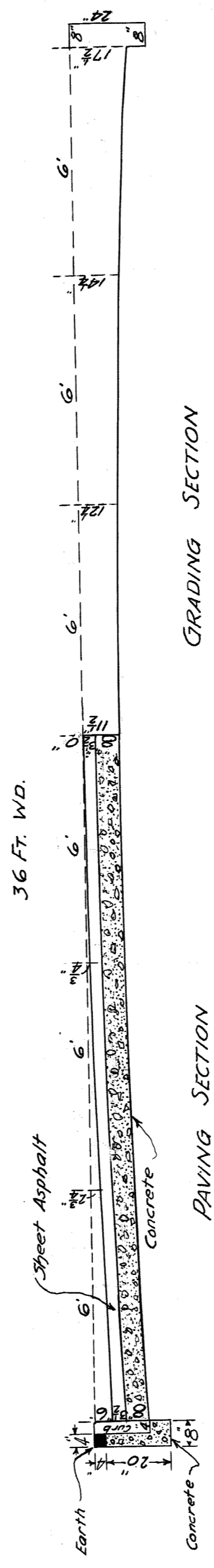
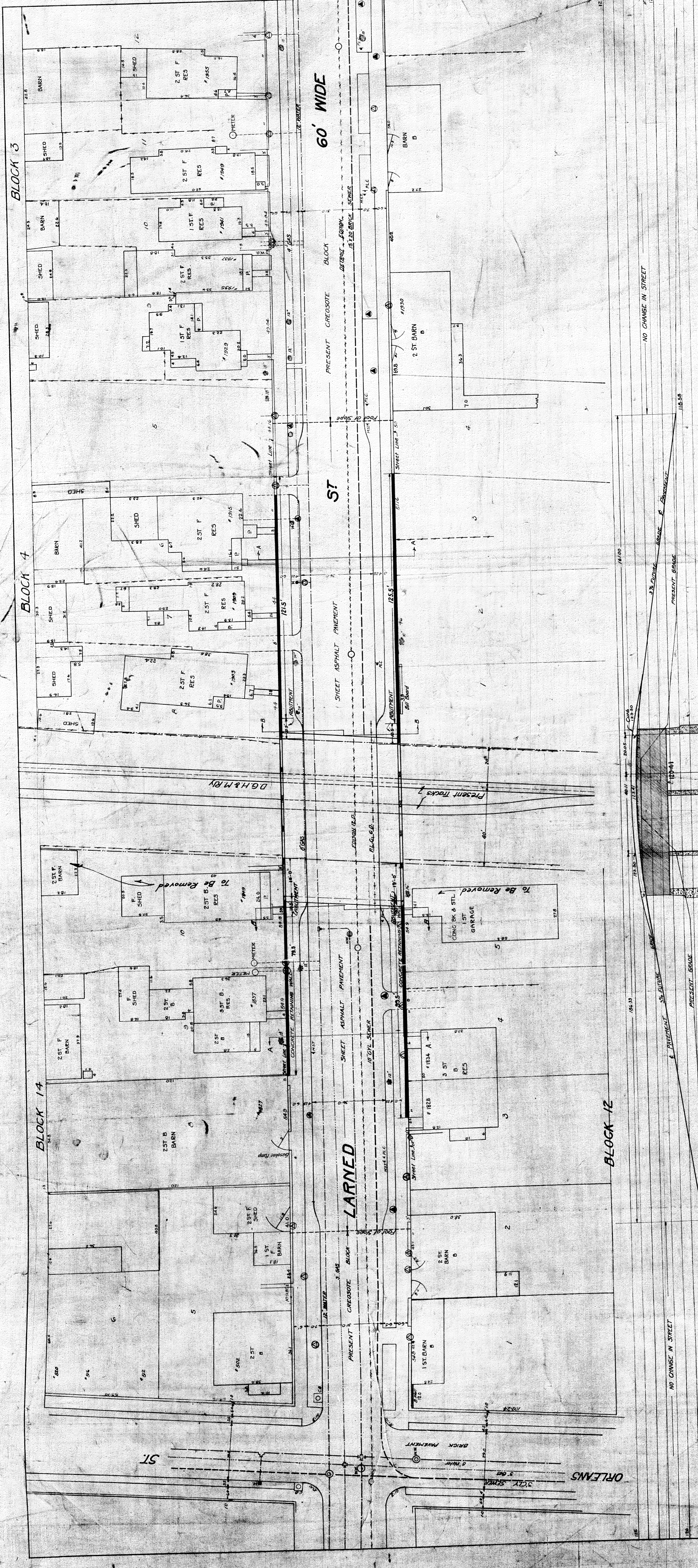


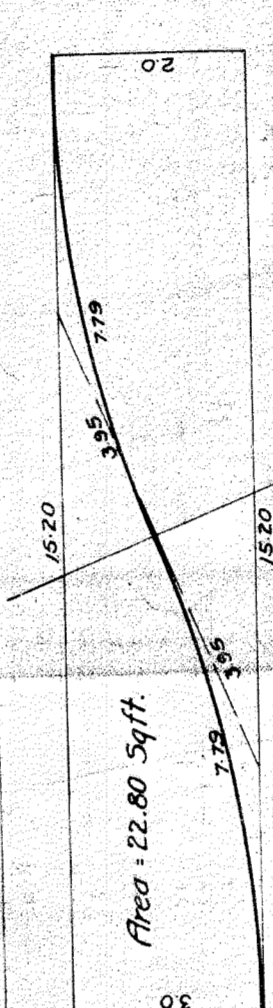
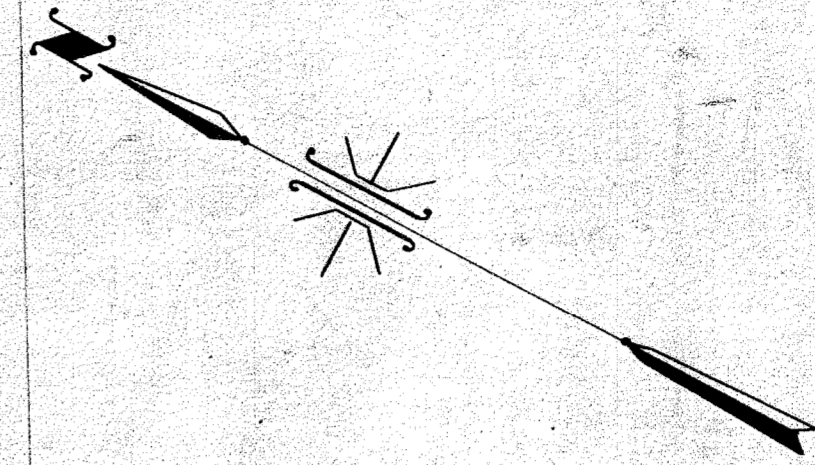
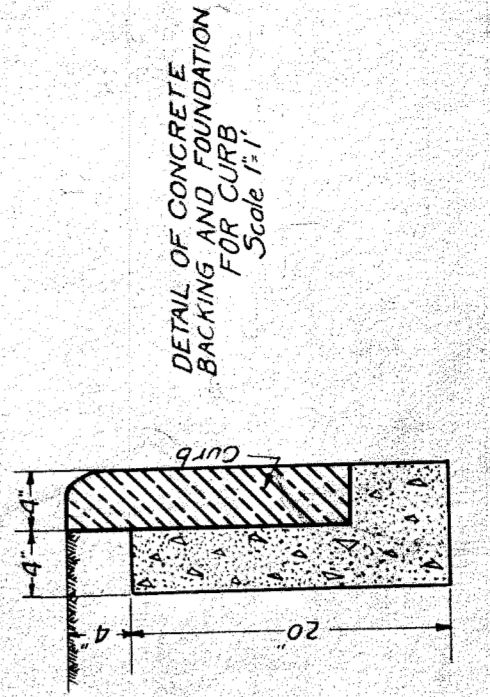
NOTE:
 New pavement to be surfaced with asphalt 3 1/2" thick instead of brick now used.
 Sidewalks to be rebuilt on present location except that the inner edge of walks shall not be closer than 6" from nearest coping edge of property retaining walls. Shade trees shall not be disturbed except where absolutely necessary to attain the above result.



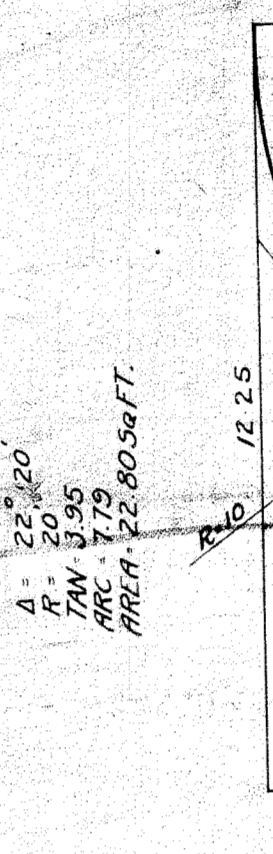
APPROVED *[Signature]* April 26th 1923.
 CITY ENGINEER
 CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEER'S OFFICE
 DEPARTMENT OF GRADE SEPARATION & BRIDGES
 DEQUINDRE ST. GRADE SEPARATION
 LARNED ST.
 Made by AKS Scales Mar 1923
 Checked by W.V. Plan 1" = 40'
 Vertical 1" = 10'



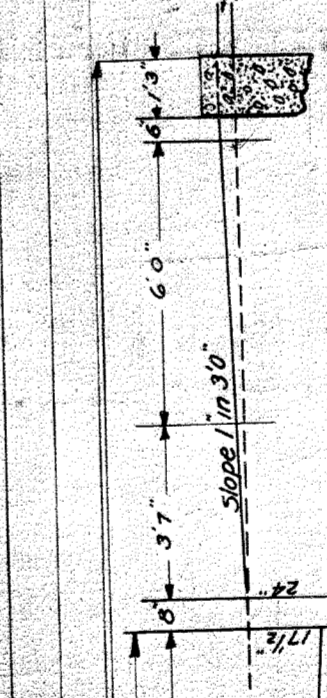
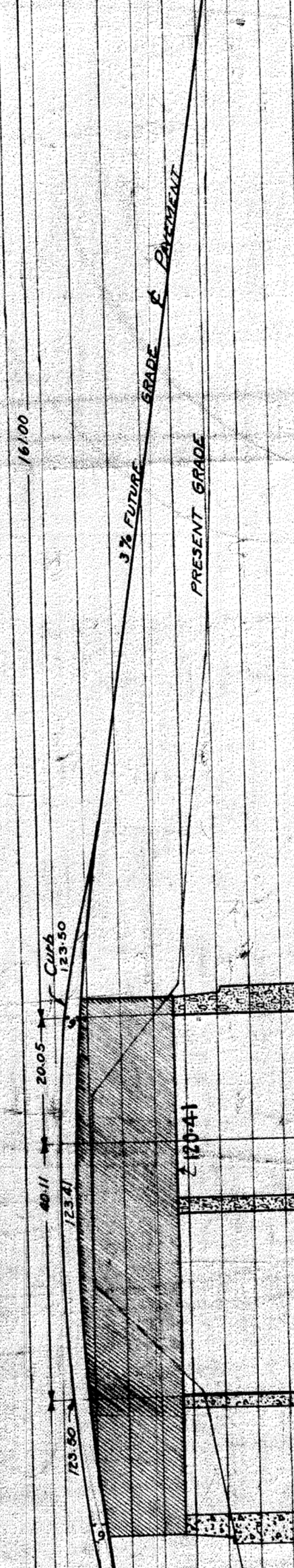
- LEGEND**
- ⊙ Water Shut off
 - ⊙ Gas Shut off
 - ⊙ Tree Diameter of trunk shown in inches
 - ⊙ 2" Trolley Pole
 - ⊙ 1" P.C. Pole Lighting Commission Pole
 - ⊙ 1" Fire Hydrant
 - ⊙ Meter Gate



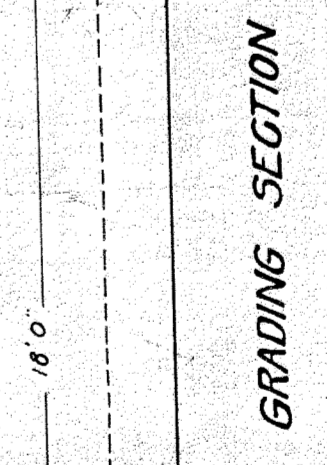
DETAILS OF REVERSE CURVE AT WIDENING OF PAVEMENT FROM 30 FEET TO 36 FEET



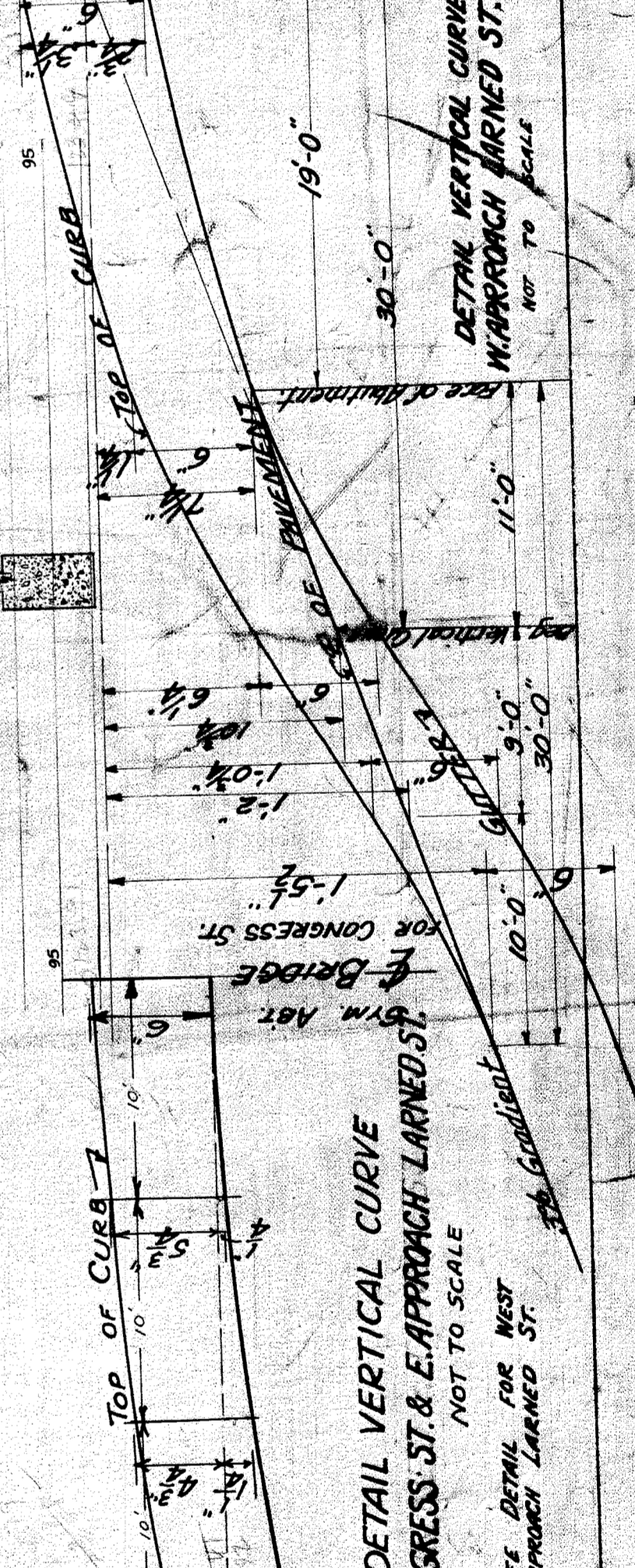
DETAILS OF REVERSE CURVE AT WIDENING OF SIDEWALK FROM 6 FEET TO 12 FEET



SECTION A.A.
TO BE USED FROM FOOT OF SLOPE TO A POINT 30 FEET FROM FACE OF ABUTMENT
Scale 3/4" = 1'



SECTION B.B.
TO BE USED AT JUNCTION OF BRIDGE AND STREET APPROACH
Scale 3/4" = 1'



DETAIL VERTICAL CURVE
CONGRESS ST. & APPROACH LARNED ST.
NOT TO SCALE

DETAILS OF CURB AT DRIVEWAYS
Scale 3/4" = 1'

Approved: *[Signature]*
Chief Engineer, Detroit Railway

Approved: *[Signature]*
City Engineer

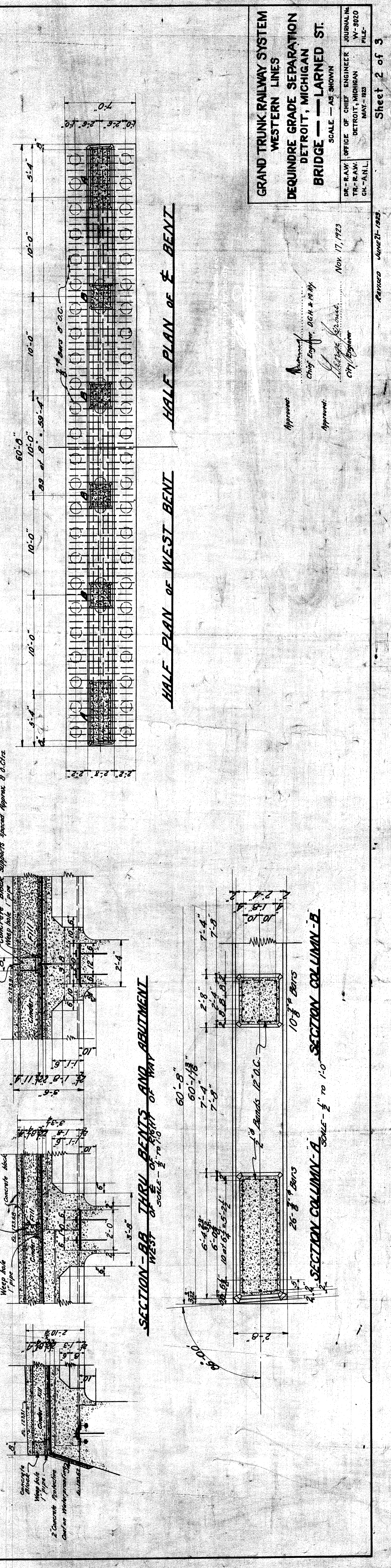
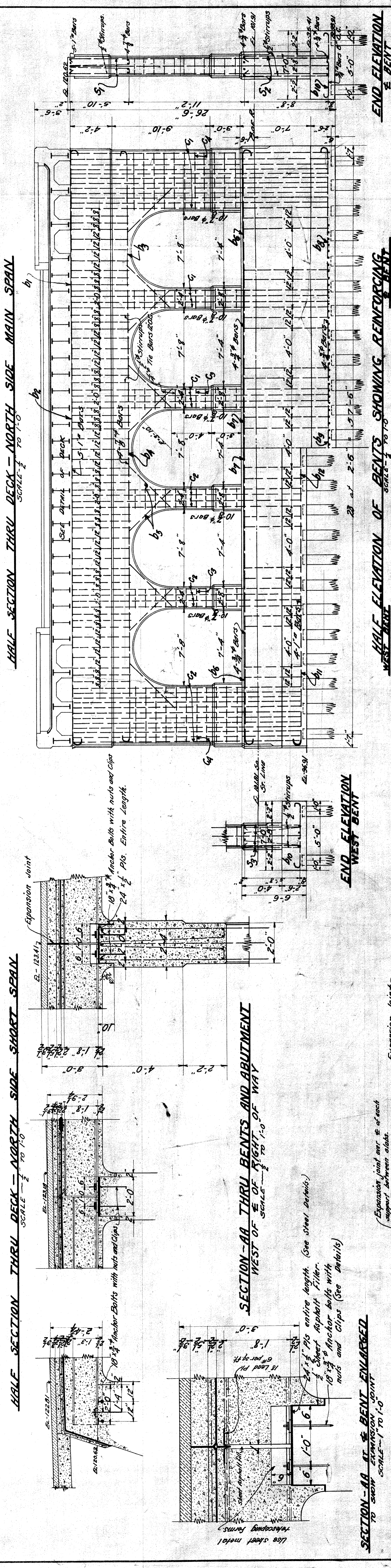
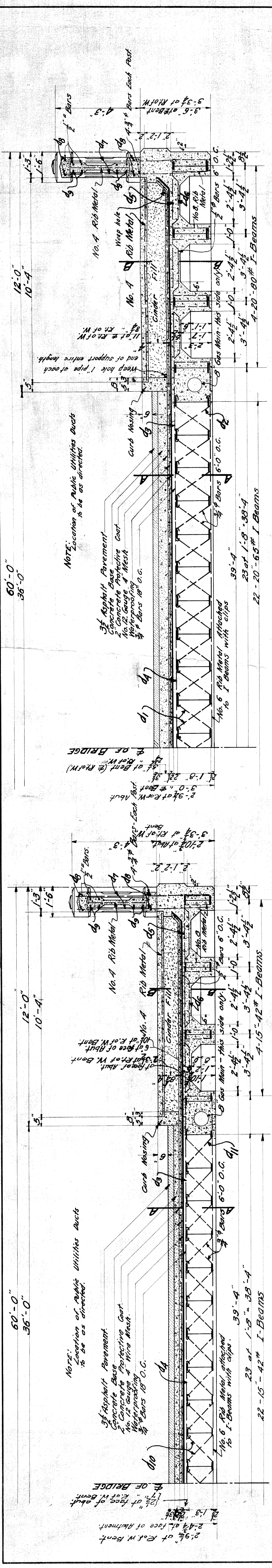
NOV 17, 1923

**GRAND TRUNK RAILWAY SYSTEM
WESTERN LINES
DEQUINDRE GRADE SEPARATION
DETROIT, MICHIGAN.
GENERAL PLAN LARNED ST**

Scale 1" = 20' Except as noted

DR. F. C. ...
OFFICE OF CHIEF ENGINEER
DETROIT, MICH.
MAY 1923.

REVISED JUNE 19, 1923



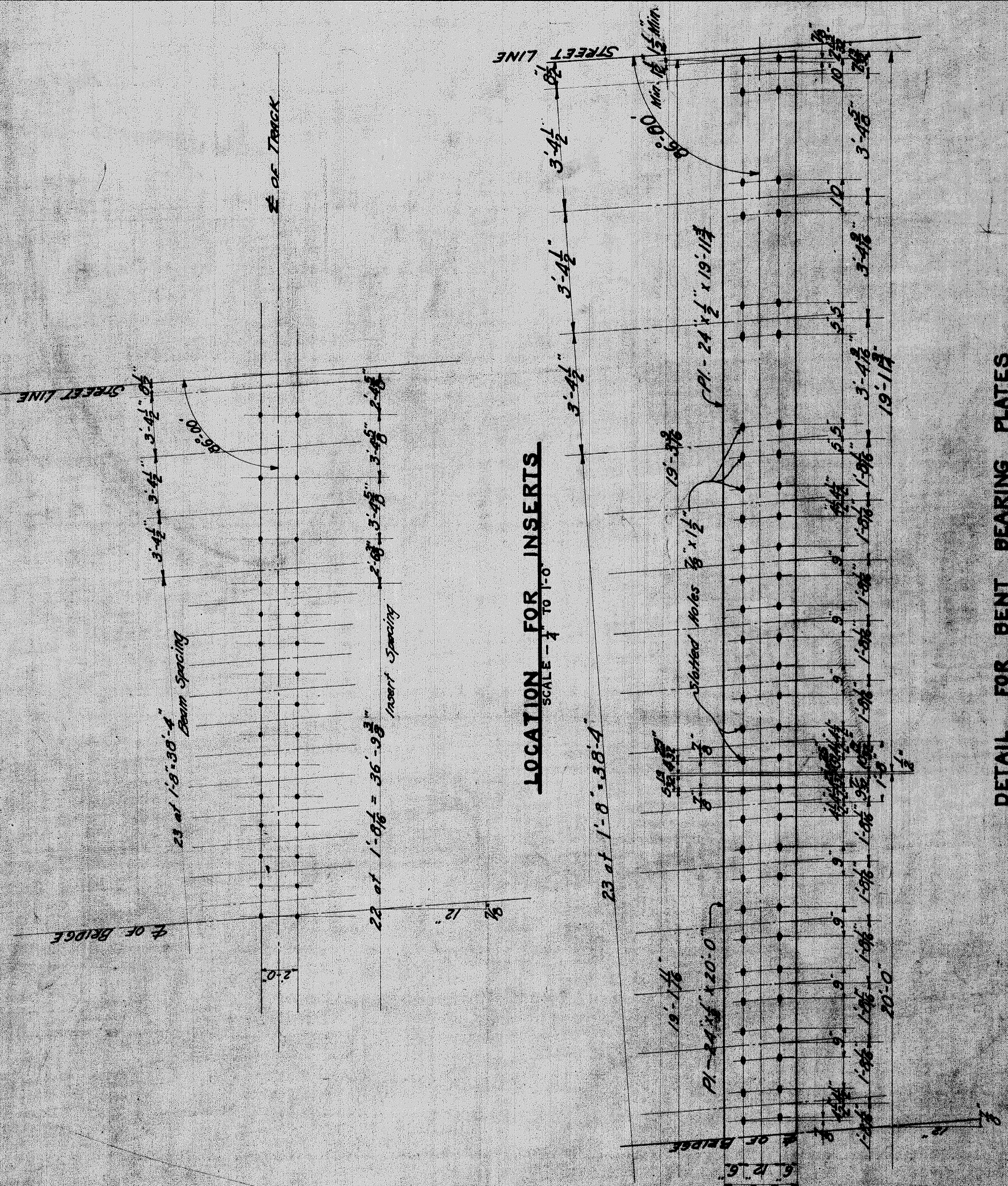
Approved: *[Signature]*
 Chief Engineer, D.C.H. & M.W.
 Approved: *[Signature]*
 City Engineer
 Nov 17, 1923

BILL OF REINFORCING BARS				BENDING DIAGRAM	
LOCATION	No.	MARK	SIZE	LENGTH	
DECK	232	d ₁	3/4"	6'-6"	
DECK	24	d ₂	3/4"	3'-10"	
DECK	112	d ₃	3/4"	16'-0"	
DECK	56	d ₄	3/4"	14'-0"	STRAIGHT.
DECK (Sidewalk)	112	d ₅	3/4"	11'-6"	
DECK (Sidewalk)	340	d ₆	1/2"	12'-0"	
DECK (for Railing)	40	d ₇	3/4"	6'-0"	STRAIGHT.
RAILING (Bottom)	32	d ₈	1/2"	15'-0"	"
RAILING (Web)	64	d ₉	1/2"	15'-6"	
DECK	84	d ₁₀	3/4"	5'-0"	
DECK	8	d ₁₁	3/4"	3'-1"	
BENT (Top)	20	b ₁	1"	22'-0"	
BENT (Top)	10	b ₂	1"	24'-0"	STRAIGHT.
BENT (Top)	8	b ₃	3/4"	28'-0"	
BENT (Top)	4	b ₄	3/4"	12'-0"	STRAIGHT
BENT (Overhead)	20	b ₅	3/4"	14'-0"	
BENT (Under Rib)	16	b ₆	3/4"	28'-0"	
BENT (Under Rib)	8	b ₇	3/4"	12'-0"	STRAIGHT.
BENT (Bottom)	8	b ₈	3/4"	22'-0"	
BENT (Bottom)	4	b ₉	3/4"	28'-0"	STRAIGHT.
BENT (Bottom)	180	b ₁₀	3/4"	8'-6"	
W. BENT (Bottom)	8	b ₁₁	1"	22'-0"	
W. BENT (Bottom)	4	b ₁₂	1"	28'-0"	STRAIGHT
BENT (Column)	32	c ₁	3/8"	25'-0"	
W. BENT (Column)	32	c ₂	3/8"	22'-0"	"
BENT (Column Tie)	88	c ₃	1/4"	8'-6"	
BENT (Column Tie)	44	c ₄	1/4"	25'-0"	
BENT (Strut)	90	s ₁	1/4"	26'-0"	
BENT (Lower Strut)	20	s ₂	1/4"	20'-0"	
W. BENT (Strut)	20	s ₃	1/4"	25'-0"	

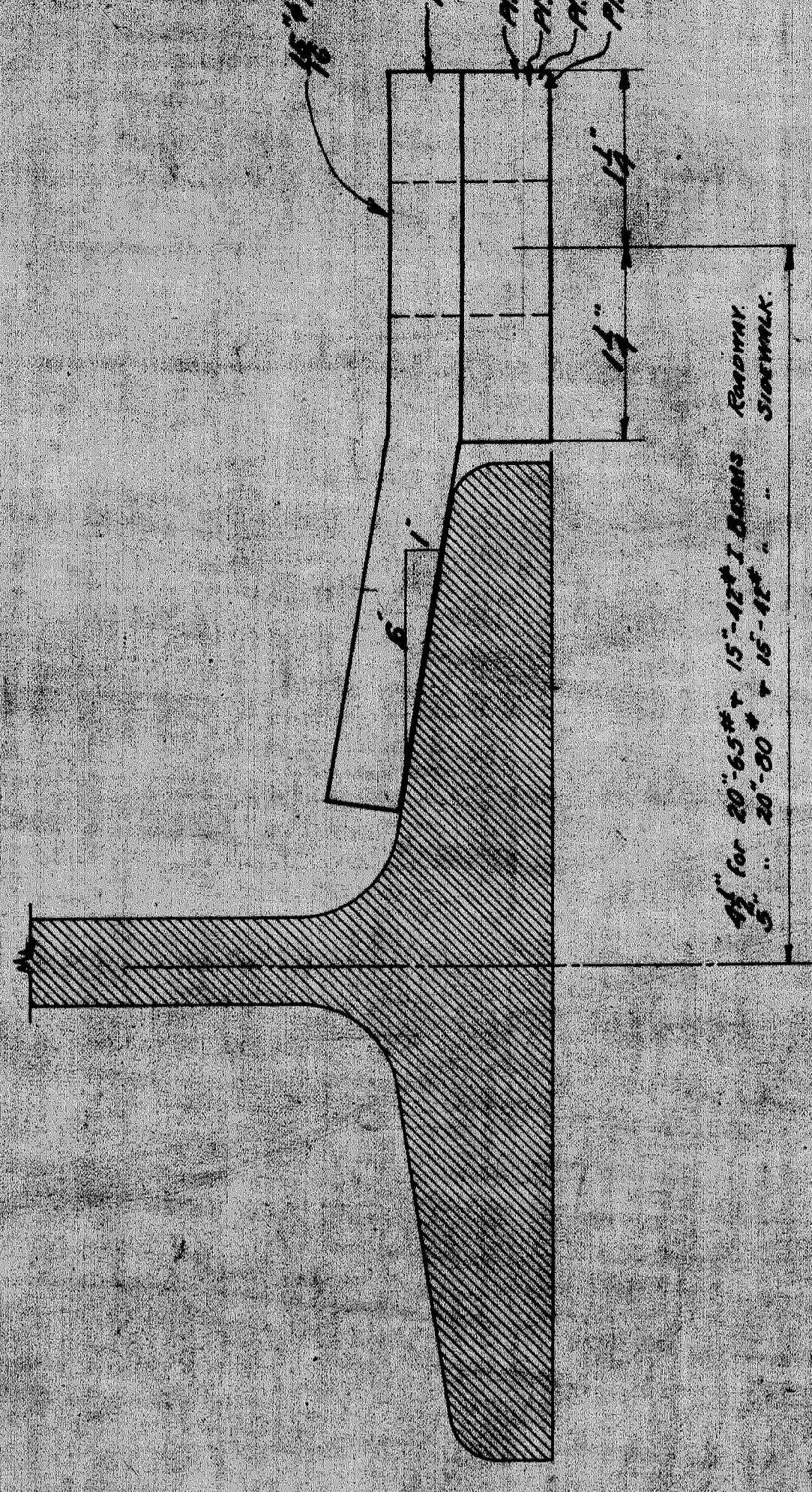
BILL OF FLOOR BEAMS				
LOCATION	No.	MARK	SIZE	LENGTH
DECK (Street)	22	B ₁	20" x 65"	32'-2"
DECK (Street)	6	B ₂	20" x 65"	32'-2"
DECK (Street)	22	B ₃	20" x 65"	31'-0"
DECK (Street)	6	B ₄	20" x 60"	31'-0"
DECK (Street)	30	B ₅	15" x 42"	20'-2"

BILL OF WIRE MESH		
LOCATION	No. SHEETS	DESCRIPTION
RAILING	8	32" x 13'-6" TRUSSON RIB METAL No. 4 OR EQUIVALENT.
SIDEWALK	56	32" x 10'-2" " " " " No. 5 " " " "
DECK (Bottom)	18	48" x 16'-0" " " " " No. 6 " " " "
DECK	28	48" x 15'-0" TRUSSON RIB METAL No. 8 OR EQUIVALENT.
DECK	8	48" x 14'-6" TRUSSON RIB METAL No. 6 OR EQUIVALENT.
DECK (Sidewalk)	60	64" x 5'-0" TRUSSON RIB METAL No. 8 OR EQUIVALENT.
DECK (Sidewalk)	18	64" x 4'-0" TRUSSON RIB METAL No. 8 OR EQUIVALENT.
DECK (Bottom)	1335 sq. ft.	No. 12 GAUGE 4 in WIRE MESH

MISCELLANEOUS MATERIAL	
No.	DESCRIPTION
4-PLS. 24" x 18" x 1/4"	SEE DETAIL
2-PLS. 24" x 18" x 1/4"	SEE DETAIL
360	SWEDGE BOLTS WITH NUTS
360	STRUCTURAL STEEL PLS. SEE DETAIL.
250	SLOTTED INSERTS AND BLAST PLATES



DETAIL FOR BENT BEARING PLATES



DETAIL FOR I BEAM CLIPS

Approved: Chief Engineer DCM & M. E.
 Approved: City Engineer
 Nov. 17, 1923

GRAND TRUNK RAILWAY SYSTEM
 WESTERN LINES
 DEQUINDRE GRADE SEPARATION
 DETROIT, MICHIGAN
 BRIDGE — LARNED ST.
 SCALE — AS SHOWN
 DE. PLAN OFFICE OF CHIEF ENGINEER W-5021
 TR. PLAN DETROIT, MICHIGAN
 CH. PLAN 1887-1923
 JOURNAL No. 1147
 Sheet 3 of 3

Revised January 1923

File X0693

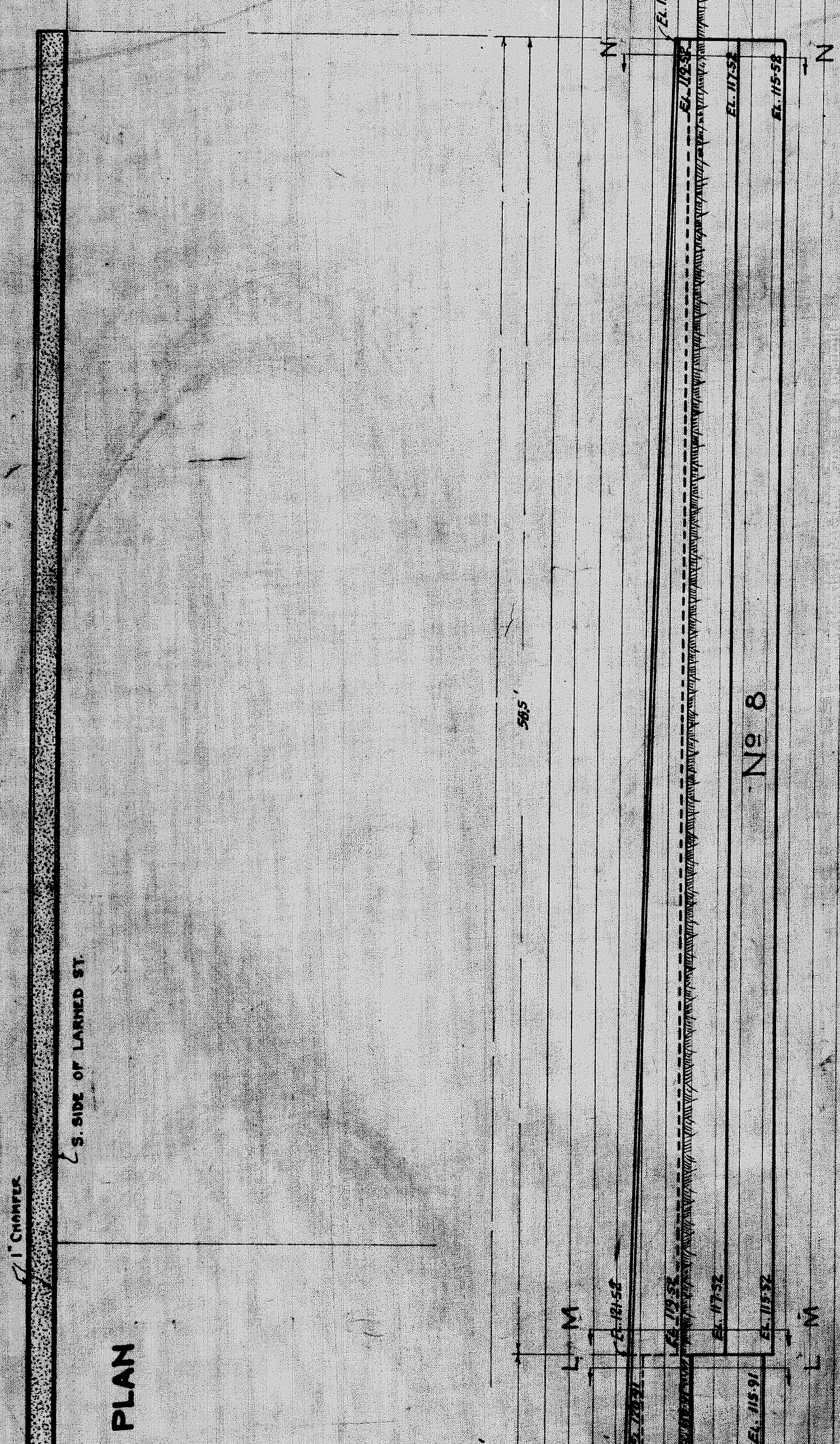
PLAN



ELEVATION

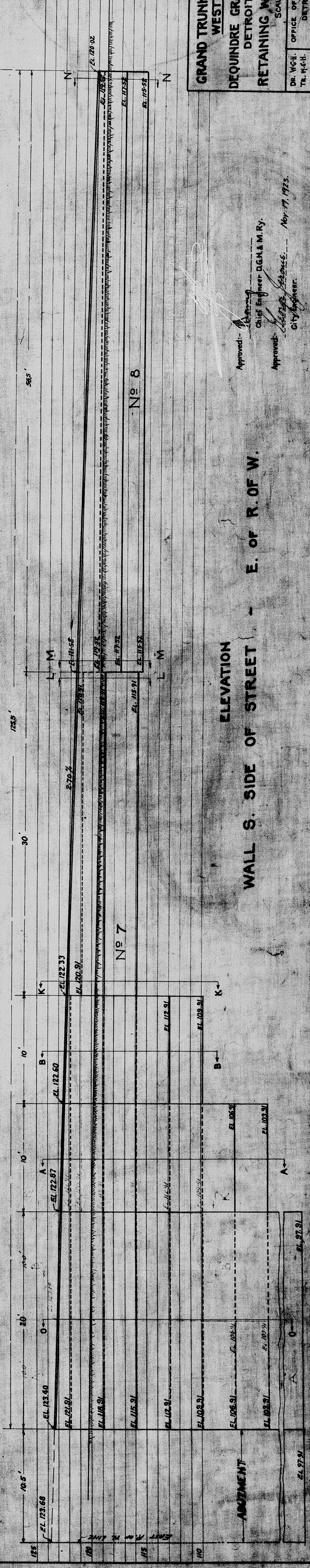
WALL N. SIDE OF STREET - E. OF R. OF W.

PLAN



ELEVATION

WALL S. SIDE OF STREET - E. OF R. OF W.

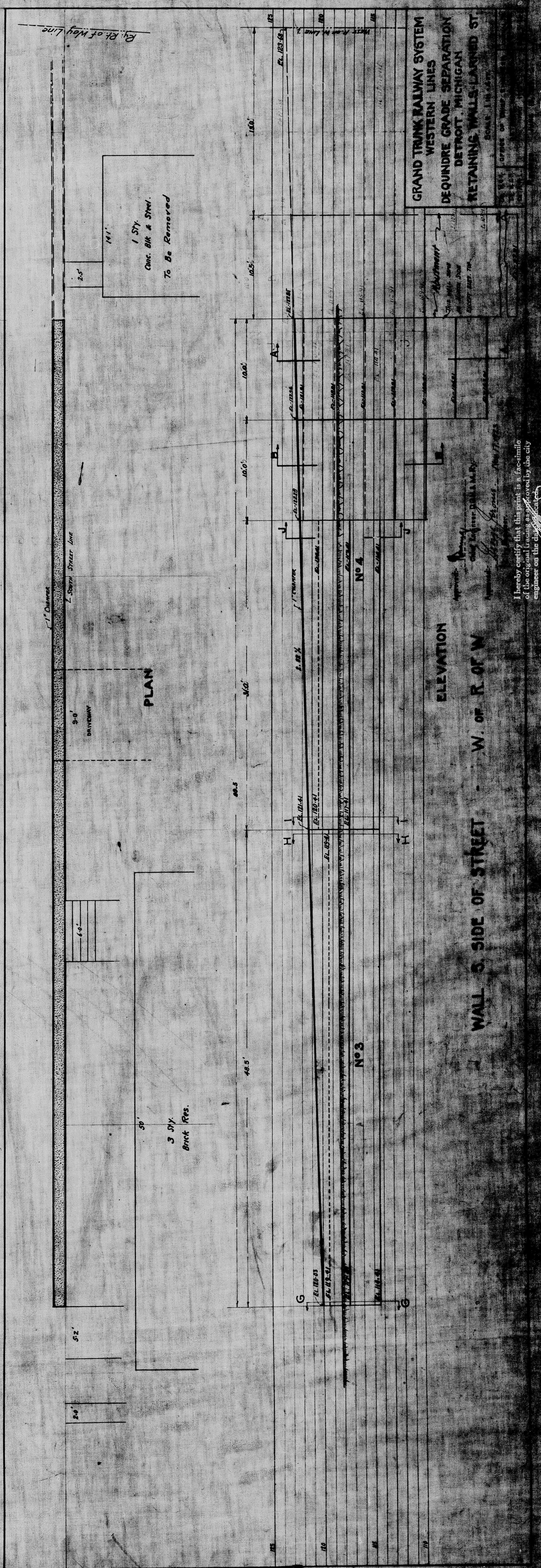
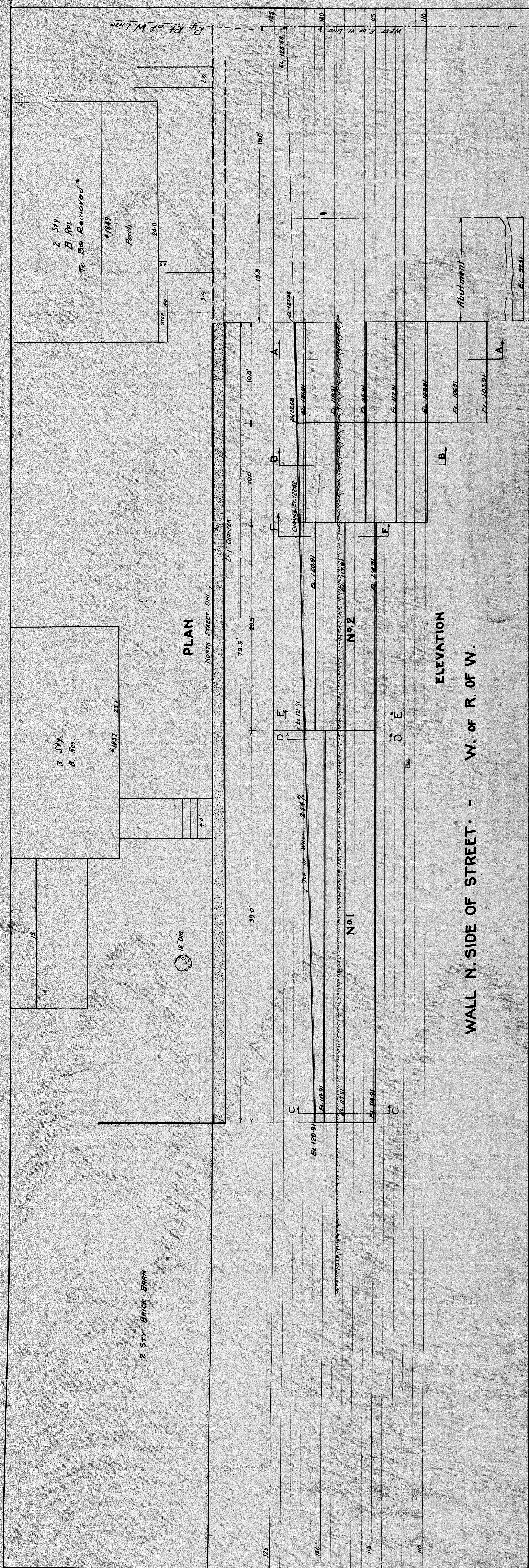


GRAND TRUNK RAILWAY SYSTEM
WESTERN LINES
DEQUINDRE GRADE SEPARATION
DETROIT, MICHIGAN
RETAINING WALLS, LARNED ST.
SCALE 1" = 4 FT.

Approved: *[Signature]*
Chief Engineer, D.G.M. & M. Ry.
Approved: *[Signature]*
City Engineer.
Nov. 17, 1913.

DR. W.C.H. OFFICE OF CHIEF ENGINEER, JOURNAL IN
DETROIT, MICHIGAN, MAY, 1913, NO. 301.
W.C.H. MAY, 1913, FILE # 44-2
Revised June 30, 1923.

File No 69-6



GRAND TRUNK RAILWAY SYSTEM
WESTERN LINES
DEQUINDRE GRADE SEPARATION
DETROIT, MICHIGAN
RETAINING WALLS-LANARD ST.

I hereby certify that this print is a fac-simile of the original drawing approved by the city engineer on the date indicated.

[Signature]
City Engineer