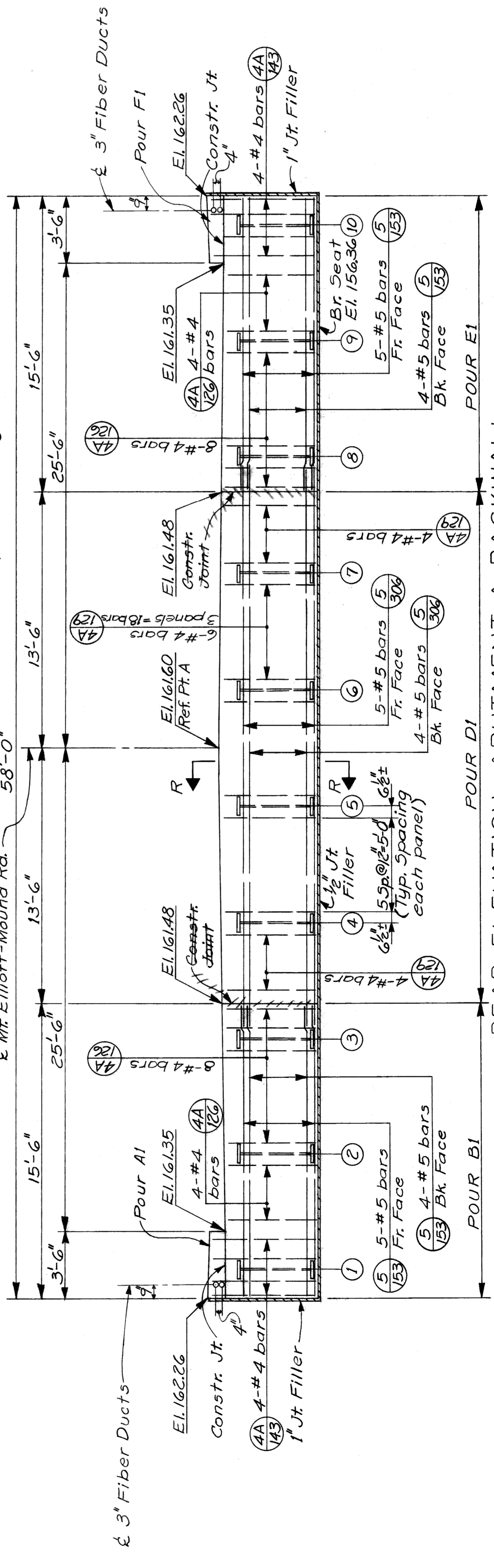


SECTION R-R
Scale: 3/4\"/>

Note: For Reinforcing Steel see Sheet 357.
For Section C-C see Sheet 353.
Curb denotes Safety Curb. For details of vertical constr. joint see Sheet 351.
For chord offsets to Bridge Constr. & see Sheet 365.

SLAB PLAN
(Not to Scale)
(Parapet & Railing not shown)



REAR ELEVATION - ABUTMENT A BACKWALL
Scale: 1/4\"/>

Work this Sheet with Sheets 351 thru 355 and 357 thru 365.

HAZELET & ERDAL CONSULTING ENGINEERS

MOUNT ELLIOTT-MOUND RD.
GRADE SEPARATION
CONCRETE DECK-SPAN 8

WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN

BOARD OF
WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN

APPROVED: [Signature]
ENGINEER OF STRUCTURES AND HIGHWAYS

APPROVED: [Signature]
ENGINEER OF DESIGN, STRUCTURES AND HIGHWAYS

As BUILT 9-17-71 E.A.B. & J.T.A.
REVISIONS

STATE PROJECT
COUNTY JOB
246
SHEET NO.
356
ISSUE NO. 1
DATE 5-13-68

H & E FILE NO. 603

| GROUP A | |
|----------|--------------|
| 2- 6-30 | T&B = 4 Bars |
| 2- 6-43 | T&B = 4 Bars |
| 2- 6-36 | T&B = 4 Bars |
| 2- 6-70 | T&B = 4 Bars |
| 2- 6-83 | T&B = 4 Bars |
| 2- 6-96 | T&B = 4 Bars |
| 2- 6-110 | T&B = 4 Bars |
| 2- 6-123 | T&B = 4 Bars |
| 2- 6-137 | T&B = 4 Bars |
| 2- 6-150 | T&B = 4 Bars |

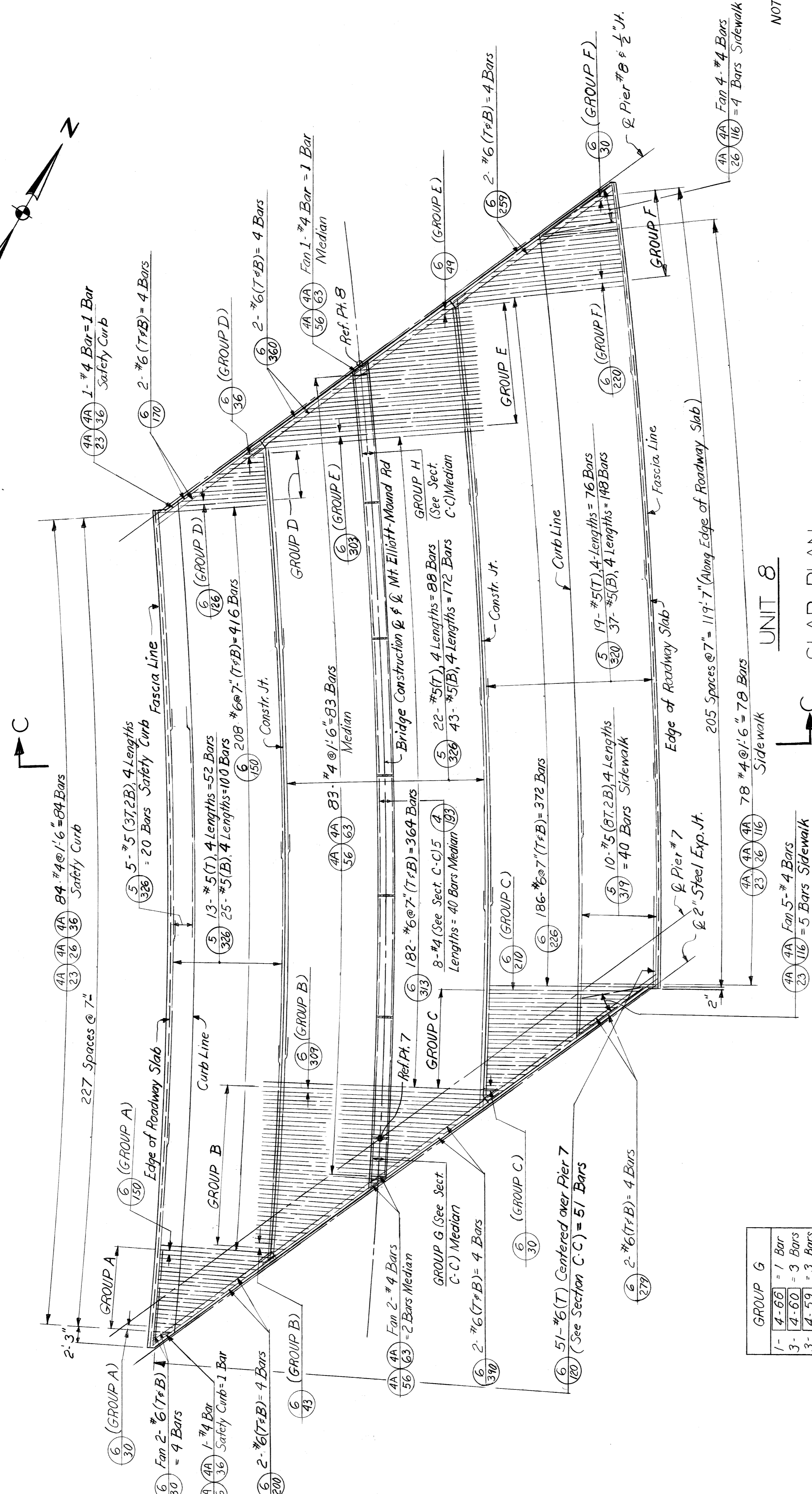
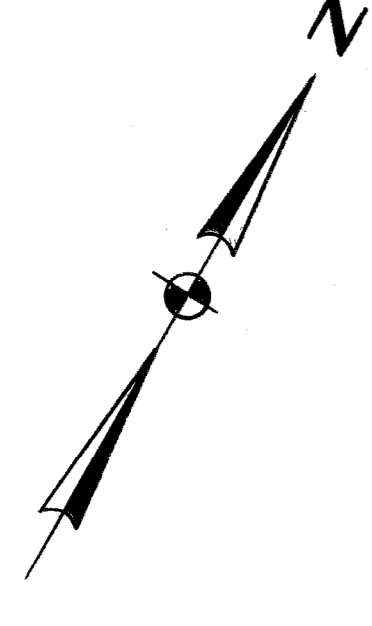
| GROUP B | |
|----------|--------------|
| 2- 6-43 | T&B = 4 Bars |
| 2- 6-57 | T&B = 4 Bars |
| 2- 6-71 | T&B = 4 Bars |
| 2- 6-85 | T&B = 4 Bars |
| 2- 6-99 | T&B = 4 Bars |
| 2- 6-113 | T&B = 4 Bars |
| 2- 6-127 | T&B = 4 Bars |
| 2- 6-141 | T&B = 4 Bars |
| 2- 6-155 | T&B = 4 Bars |
| 2- 6-169 | T&B = 4 Bars |
| 2- 6-183 | T&B = 4 Bars |
| 2- 6-197 | T&B = 4 Bars |
| 2- 6-211 | T&B = 4 Bars |
| 2- 6-225 | T&B = 4 Bars |
| 2- 6-239 | T&B = 4 Bars |
| 2- 6-253 | T&B = 4 Bars |
| 2- 6-267 | T&B = 4 Bars |
| 2- 6-281 | T&B = 4 Bars |
| 2- 6-295 | T&B = 4 Bars |

| GROUP C | |
|----------|--------------|
| 2- 6-30 | T&B = 4 Bars |
| 2- 6-43 | T&B = 4 Bars |
| 2- 6-57 | T&B = 4 Bars |
| 2- 6-71 | T&B = 4 Bars |
| 2- 6-85 | T&B = 4 Bars |
| 2- 6-99 | T&B = 4 Bars |
| 2- 6-113 | T&B = 4 Bars |
| 2- 6-127 | T&B = 4 Bars |
| 2- 6-141 | T&B = 4 Bars |
| 2- 6-155 | T&B = 4 Bars |
| 2- 6-169 | T&B = 4 Bars |
| 2- 6-183 | T&B = 4 Bars |
| 2- 6-197 | T&B = 4 Bars |
| 2- 6-211 | T&B = 4 Bars |
| 2- 6-225 | T&B = 4 Bars |
| 2- 6-239 | T&B = 4 Bars |
| 2- 6-253 | T&B = 4 Bars |
| 2- 6-267 | T&B = 4 Bars |
| 2- 6-281 | T&B = 4 Bars |
| 2- 6-295 | T&B = 4 Bars |

| GROUP D | |
|----------|--------------|
| 2- 6-36 | T&B = 4 Bars |
| 2- 6-53 | T&B = 4 Bars |
| 2- 6-70 | T&B = 4 Bars |
| 2- 6-87 | T&B = 4 Bars |
| 2- 6-104 | T&B = 4 Bars |
| 2- 6-121 | T&B = 4 Bars |

| GROUP E | |
|----------|--------------|
| 2- 6-41 | T&B = 4 Bars |
| 2- 6-69 | T&B = 4 Bars |
| 2- 6-84 | T&B = 4 Bars |
| 2- 6-109 | T&B = 4 Bars |
| 2- 6-124 | T&B = 4 Bars |
| 2- 6-149 | T&B = 4 Bars |
| 2- 6-164 | T&B = 4 Bars |
| 2- 6-189 | T&B = 4 Bars |
| 2- 6-204 | T&B = 4 Bars |
| 2- 6-229 | T&B = 4 Bars |
| 2- 6-244 | T&B = 4 Bars |
| 2- 6-269 | T&B = 4 Bars |
| 2- 6-284 | T&B = 4 Bars |
| 2- 6-309 | T&B = 4 Bars |

| GROUP F | |
|----------|--------------|
| 2- 6-30 | T&B = 4 Bars |
| 2- 6-53 | T&B = 4 Bars |
| 2- 6-73 | T&B = 4 Bars |
| 2- 6-96 | T&B = 4 Bars |
| 2- 6-116 | T&B = 4 Bars |
| 2- 6-139 | T&B = 4 Bars |
| 2- 6-159 | T&B = 4 Bars |
| 2- 6-180 | T&B = 4 Bars |
| 2- 6-200 | T&B = 4 Bars |
| 2- 6-220 | T&B = 4 Bars |



| GROUP G | |
|---------|----------|
| 1- 4-66 | = 1 Bar |
| 3- 4-60 | = 3 Bars |
| 3- 4-59 | = 3 Bars |
| 1- 4-50 | = 1 Bar |

| GROUP H | |
|----------|----------|
| 1- 4-189 | = 1 Bar |
| 3- 4-190 | = 3 Bars |
| 3- 4-196 | = 3 Bars |
| 1- 4-199 | = 1 Bar |

NOTE:
 Work this slt. with Sht. 356.
 For location of Longitudinal Steel
 See Section C-C, Sht. 353.
 Transverse steel shall be placed radially
 Longitudinal steel shall be placed
 concentric to Bridge Const. ϕ
 T denotes Top of Slab
 B denotes Bottom of Slab

Work this sheet with Shts. 351 thru 366 & 358 thru 365.

HAZELET & ERDAL CONSULTING ENGINEERS

| | | |
|--------|---------------|---------------|
| BRIDGE | STATE PROJECT | COUNTY JOB |
| ROAD | 246 | 246 |
| FILE | ISSUE NO. 1 | SHEET NO. 357 |
| | DATE 5-13-68 | |

MOUNT ELLIOTT-MOUND RD.
 GRADE SEPARATION
 CONCRETE DECK - SPAN 8

WAYNE COUNTY ROAD COMMISSIONERS
 DETROIT, MICHIGAN
 PHILIP J. NEUDECK
 WILLIAM E. KREGER
 MICHAEL BERRY

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

APPROVED
 [Signature]
 COUNTY HIGHWAY ENGINEER

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

APPROVED
 [Signature]
 ENGINEER OF STRUCTURES AND EXPRESSWAYS

H & E FILE NO. 603

| GROUP A | |
|----------|--------------|
| 2- 6-30 | T&B = 4 Bars |
| 2- 6-50 | T&B = 4 Bars |
| 2- 6-70 | T&B = 4 Bars |
| 2- 6-90 | T&B = 4 Bars |
| 2- 6-110 | T&B = 4 Bars |
| 2- 6-130 | T&B = 4 Bars |

| GROUP B | |
|----------|--------------|
| 2- 6-43 | T&B = 4 Bars |
| 2- 6-63 | T&B = 4 Bars |
| 2- 6-83 | T&B = 4 Bars |
| 2- 6-103 | T&B = 4 Bars |
| 2- 6-123 | T&B = 4 Bars |
| 2- 6-143 | T&B = 4 Bars |
| 2- 6-163 | T&B = 4 Bars |
| 2- 6-183 | T&B = 4 Bars |
| 2- 6-204 | T&B = 4 Bars |
| 2- 6-226 | T&B = 4 Bars |
| 2- 6-249 | T&B = 4 Bars |
| 2- 6-270 | T&B = 4 Bars |
| 2- 6-293 | T&B = 4 Bars |

| GROUP C | |
|----------|--------------|
| 2- 6-240 | T&B = 4 Bars |
| 2- 6-60 | T&B = 4 Bars |
| 2- 6-80 | T&B = 4 Bars |
| 2- 6-100 | T&B = 4 Bars |
| 2- 6-120 | T&B = 4 Bars |
| 2- 6-140 | T&B = 4 Bars |
| 2- 6-160 | T&B = 4 Bars |
| 2- 6-183 | T&B = 4 Bars |
| 1- 6-203 | T&B = 2 Bars |

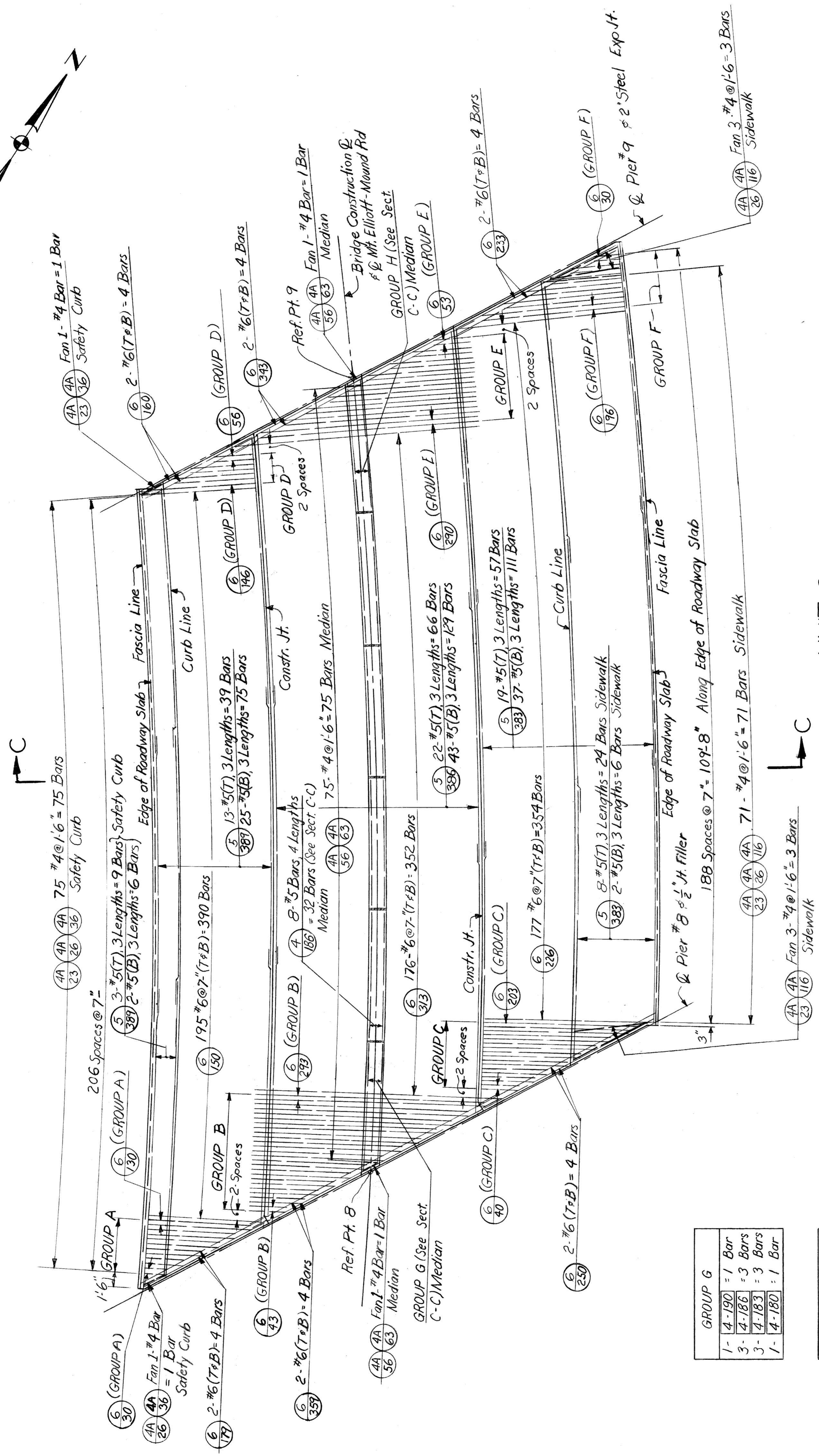
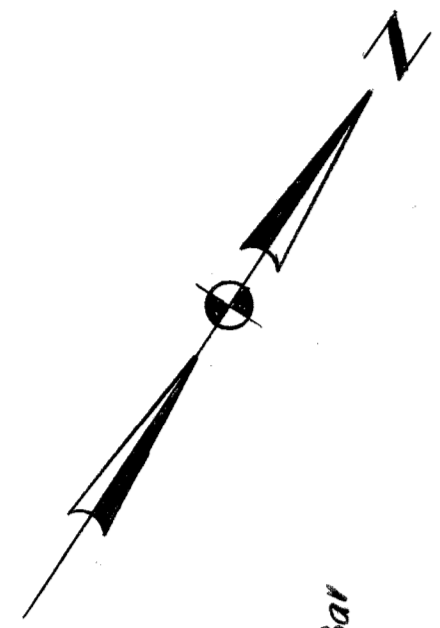
| GROUP D | |
|----------|--------------|
| 2- 6-56 | T&B = 4 Bars |
| 2- 6-86 | T&B = 4 Bars |
| 2- 6-116 | T&B = 4 Bars |
| 1- 6-146 | T&B = 2 Bars |

| GROUP E | |
|----------|--------------|
| 2- 6-53 | T&B = 4 Bars |
| 2- 6-83 | T&B = 4 Bars |
| 2- 6-113 | T&B = 4 Bars |
| 2- 6-143 | T&B = 4 Bars |
| 2- 6-173 | T&B = 4 Bars |
| 2- 6-200 | T&B = 4 Bars |
| 2- 6-230 | T&B = 4 Bars |
| 2- 6-260 | T&B = 4 Bars |
| 2- 6-290 | T&B = 4 Bars |

| GROUP F | |
|----------|--------------|
| 2- 6-30 | T&B = 4 Bars |
| 2- 6-66 | T&B = 4 Bars |
| 2- 6-99 | T&B = 4 Bars |
| 2- 6-130 | T&B = 4 Bars |
| 2- 6-163 | T&B = 4 Bars |
| 2- 6-196 | T&B = 4 Bars |

| GROUP G | |
|----------|----------|
| 1- 4-190 | = 1 Bar |
| 3- 4-186 | = 3 Bars |
| 3- 4-183 | = 3 Bars |
| 1- 4-180 | = 1 Bar |

| GROUP H | |
|----------|----------|
| 1- 4-180 | = 1 Bar |
| 3- 4-183 | = 3 Bars |
| 3- 4-186 | = 3 Bars |
| 1- 4-189 | = 1 Bar |



UNIT 9

SLAB PLAN
(REINFORCING STEEL)
Not to Scale

NOTE:
Work this Sht. with Sht. No. 358.
For location of longitudinal steel
see Section C-C Sht. No. 353.
Transverse steel shall be placed radially
Longitudinal steel shall be placed concentric
to Bridge Constr. &
T denotes Top of Slab
B denotes Bottom of Slab

Work this sheet with Shrs. 351 thru 358 & 360 thru 365.

HAZELET & ERDAL CONSULTING ENGINEERS

MOUNT ELLIOTT-MOUND RD.
GRADE SEPARATION
CONCRETE DECK - SPAN 9

WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN

PHILIP J. NEUDECK
WILLIAM E. KREGER

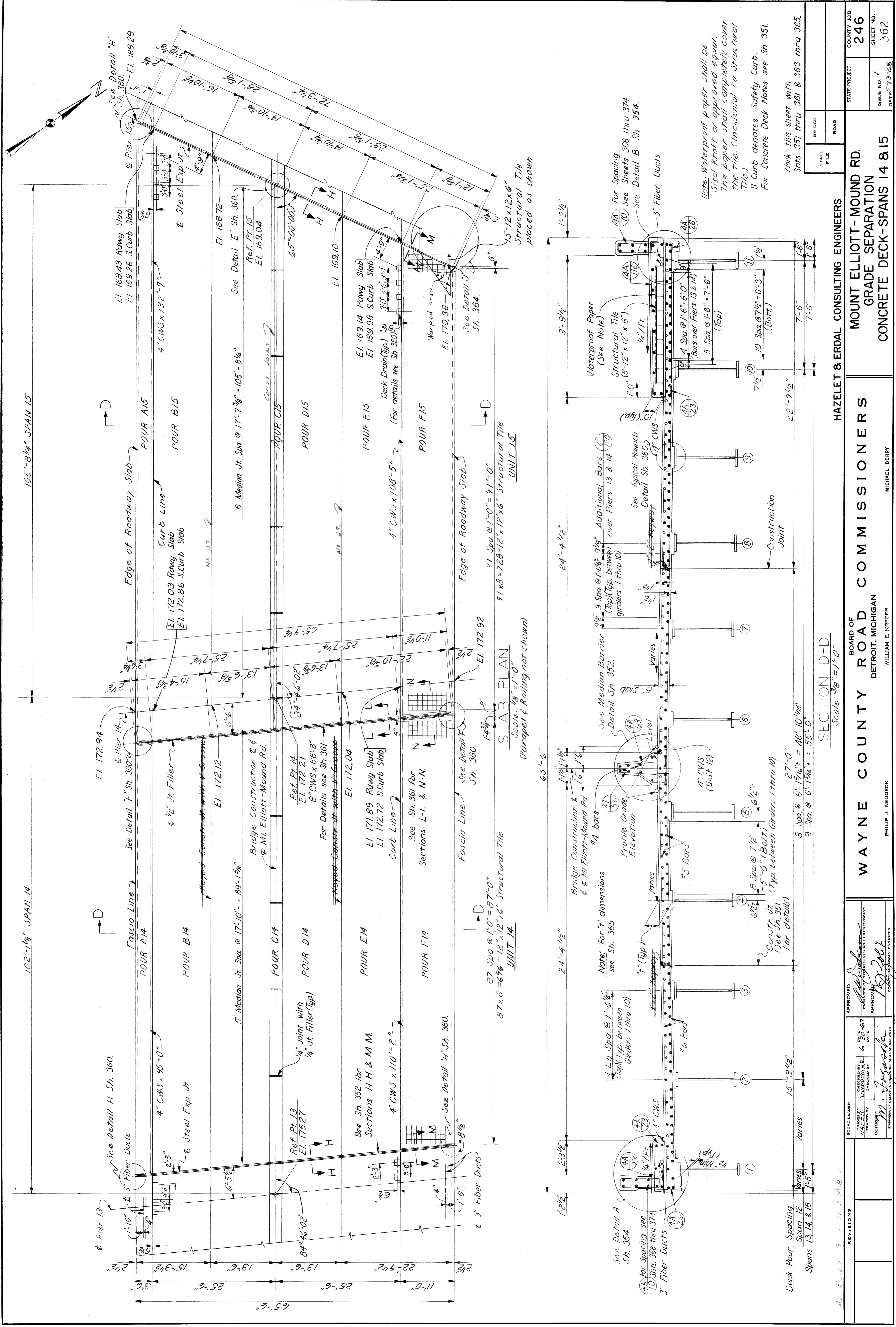
APPROVED
M. J. [Signature]
ENGINEER OF STRUCTURES AND EXPRESSWAYS

CHECKED BY
M. J. [Signature]
DATE 6-25-67

REVISIONS

STATE PROJECT
COUNTY JOB
246
SHEET NO.
359
ISSUE NO. 1
DATE 5-23-68

H & E FILE NO. 603



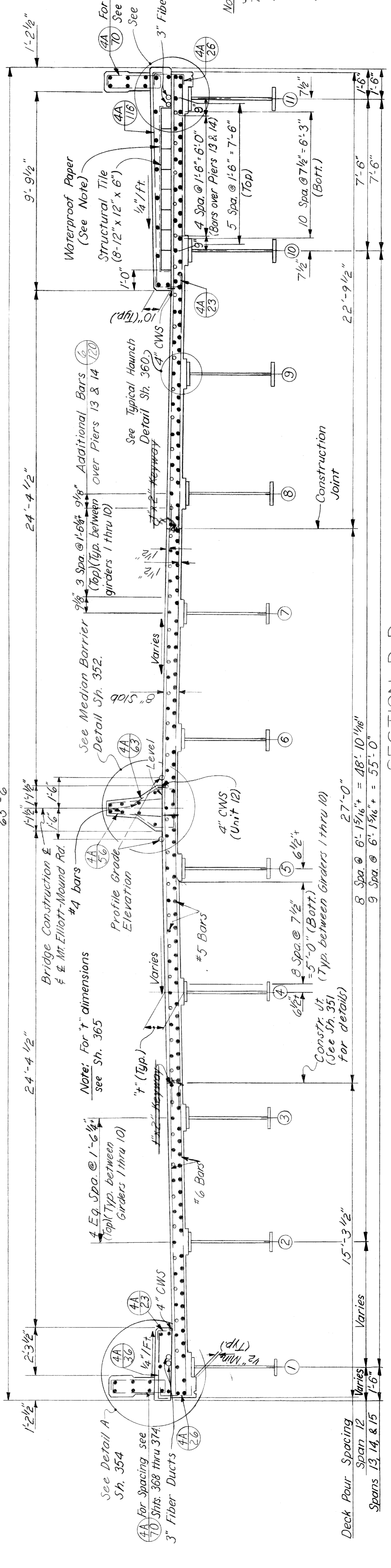
| | | | |
|---|--|--|--|
| WAYNE COUNTY ROAD COMMISSIONERS DETROIT, MICHIGAN PHILIP J. NEUDECK COUNTY HIGHWAY ENGINEER | | HAZELET & ERDAL CONSULTING ENGINEERS MOUNT ELLIOTT-MOUND RD. GRADE SEPARATION CONCRETE DECK-SPANS 14 & 15 MICHAEL BERRY | |
| APPROVED: <i>[Signature]</i> ENGINEER OF STRUCTURES AND EXPRESSWAYS DATE: 6-25-67 | | COUNTY PROJECT: 246 COUNTY JOB: 246 SHEET NO.: 362 ISSUE NO.: 1 DATE: 7-3-68 | |
| REVISIONS: | | STATE PROJECT: _____ STATE FILE: _____ ROAD: _____ | |

As Built 9-11-71 E.M.B.

SECTION D-D Scale: 3/8" = 1'-0"

Note: Waterproof paper shall be Sival Kraft or approved equal. The paper shall completely cover the tile. (Incidental to Structural Tile). S. Curb denotes Safety Curb. For Concrete Deck Notes see Sh. 351.

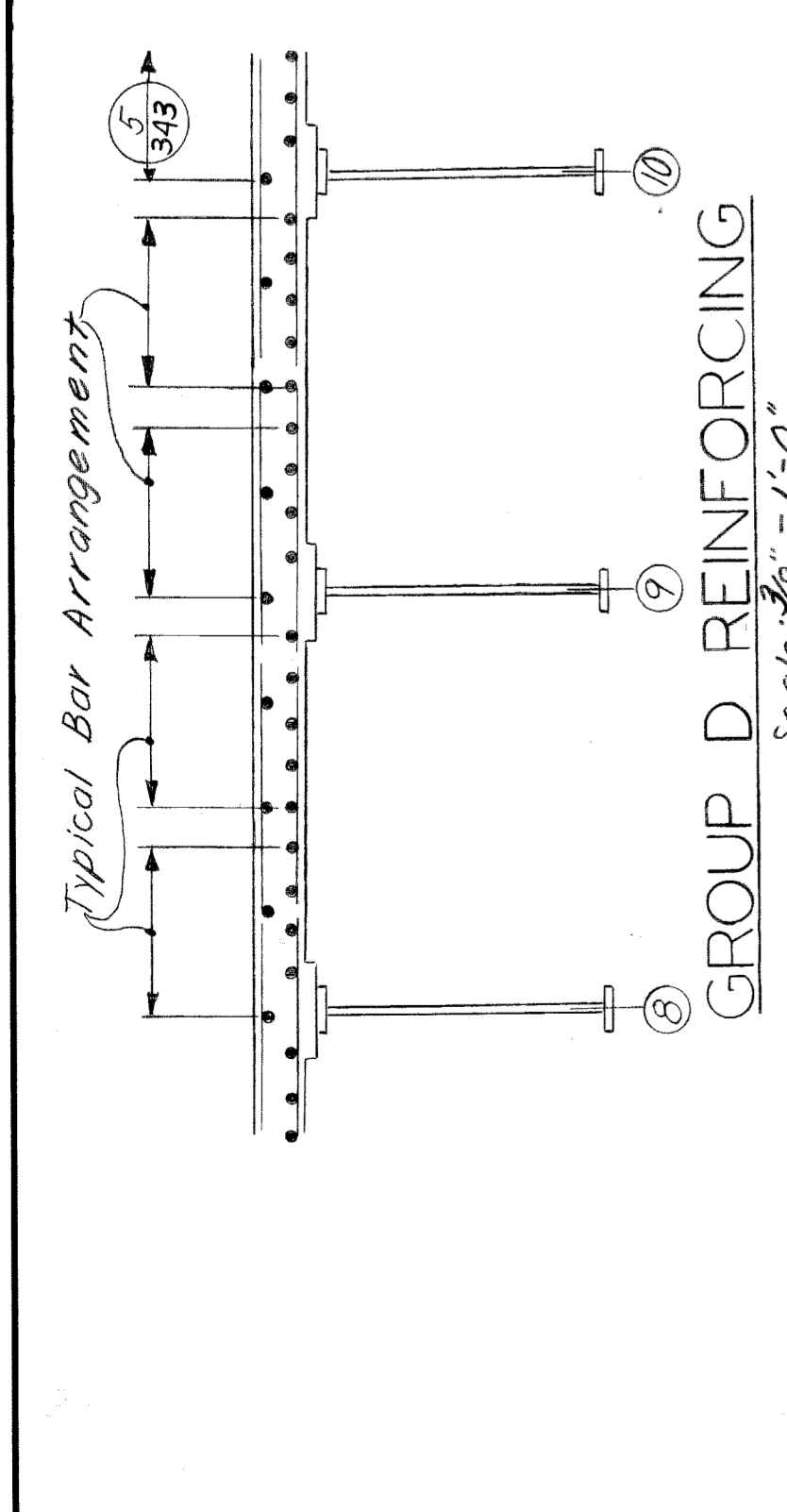
Work this sheet with Sh. 351 thru 361 & 363 thru 365.



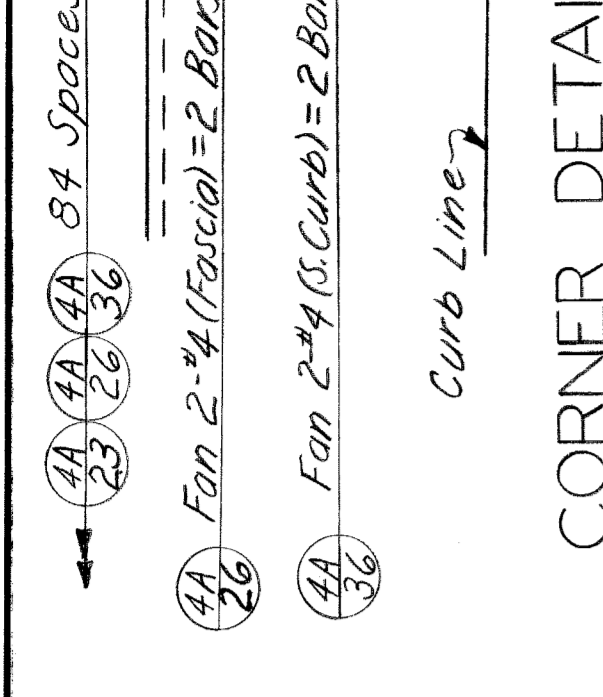
SLAB PLAN Scale: 1/8" = 1'-0" (Parapet & Railing not shown)

UNIT 14 Scale: 1/8" = 1'-0"

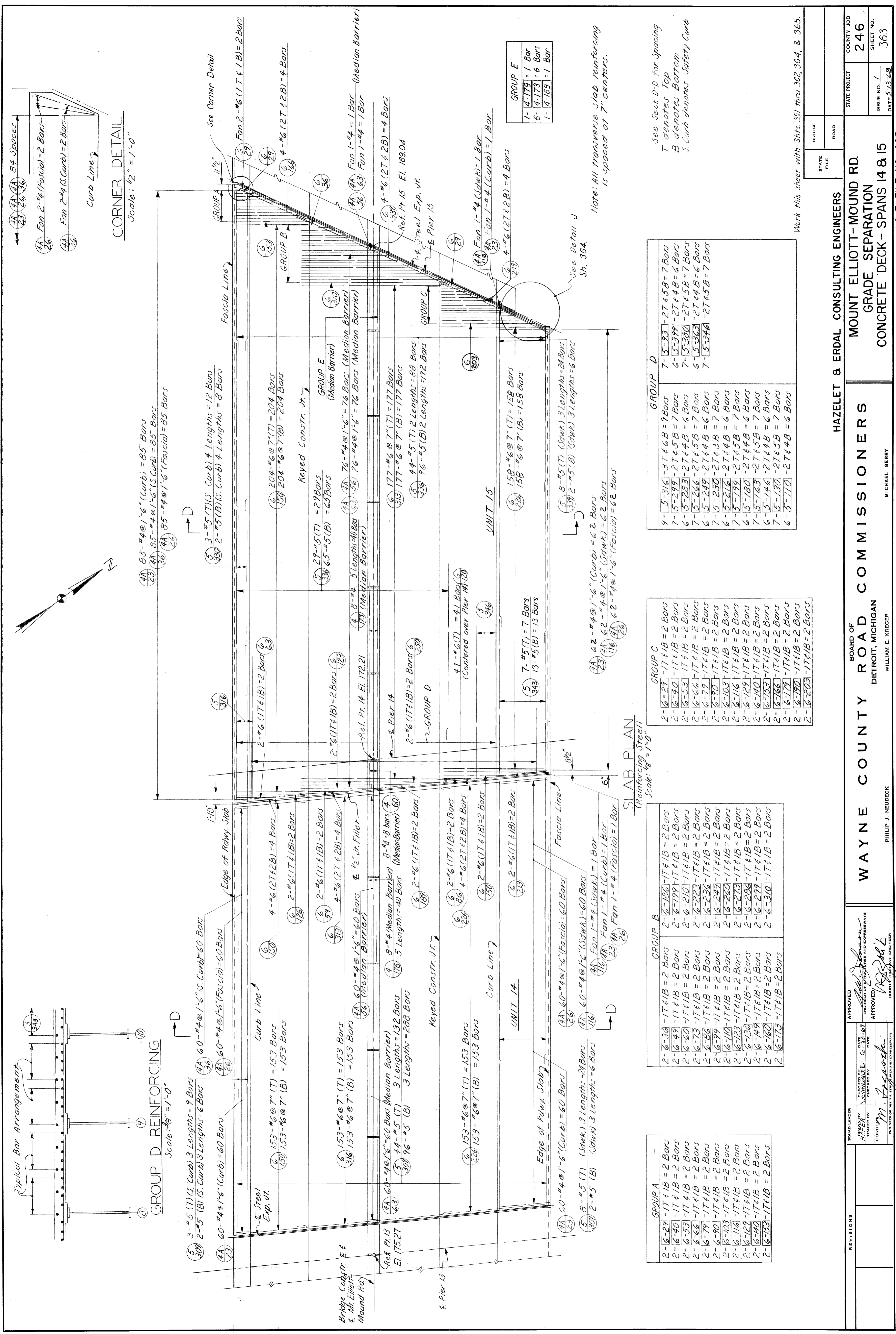
UNIT 15 Scale: 1/8" = 1'-0"



GROUP D REINFORCING
Scale: 3/8" = 1'-0"



CORNER DETAIL
Scale: 1/2" = 1'-0"



GROUP A

| | |
|---------|-------------------|
| 2-6-29 | -1T & 1B = 2 Bars |
| 2-6-40 | -1T & 1B = 2 Bars |
| 2-6-53 | -1T & 1B = 2 Bars |
| 2-6-66 | -1T & 1B = 2 Bars |
| 2-6-79 | -1T & 1B = 2 Bars |
| 2-6-90 | -1T & 1B = 2 Bars |
| 2-6-103 | -1T & 1B = 2 Bars |
| 2-6-116 | -1T & 1B = 2 Bars |
| 2-6-129 | -1T & 1B = 2 Bars |
| 2-6-140 | -1T & 1B = 2 Bars |
| 2-6-153 | -1T & 1B = 2 Bars |
| 2-6-166 | -1T & 1B = 2 Bars |

GROUP B

| | |
|---------|-------------------|
| 2-6-36 | -1T & 1B = 2 Bars |
| 2-6-49 | -1T & 1B = 2 Bars |
| 2-6-60 | -1T & 1B = 2 Bars |
| 2-6-73 | -1T & 1B = 2 Bars |
| 2-6-86 | -1T & 1B = 2 Bars |
| 2-6-99 | -1T & 1B = 2 Bars |
| 2-6-110 | -1T & 1B = 2 Bars |
| 2-6-123 | -1T & 1B = 2 Bars |
| 2-6-136 | -1T & 1B = 2 Bars |
| 2-6-149 | -1T & 1B = 2 Bars |
| 2-6-162 | -1T & 1B = 2 Bars |
| 2-6-175 | -1T & 1B = 2 Bars |

GROUP C

| | |
|---------|-------------------|
| 2-6-29 | -1T & 1B = 2 Bars |
| 2-6-40 | -1T & 1B = 2 Bars |
| 2-6-53 | -1T & 1B = 2 Bars |
| 2-6-66 | -1T & 1B = 2 Bars |
| 2-6-79 | -1T & 1B = 2 Bars |
| 2-6-90 | -1T & 1B = 2 Bars |
| 2-6-103 | -1T & 1B = 2 Bars |
| 2-6-116 | -1T & 1B = 2 Bars |
| 2-6-129 | -1T & 1B = 2 Bars |
| 2-6-140 | -1T & 1B = 2 Bars |
| 2-6-153 | -1T & 1B = 2 Bars |
| 2-6-166 | -1T & 1B = 2 Bars |
| 2-6-179 | -1T & 1B = 2 Bars |
| 2-6-190 | -1T & 1B = 2 Bars |
| 2-6-203 | -1T & 1B = 2 Bars |

GROUP D

| | |
|---------|-------------------|
| 9-5-316 | -3T & 6B = 9 Bars |
| 7-5-299 | -2T & 5B = 7 Bars |
| 6-5-283 | -2T & 4B = 6 Bars |
| 7-5-330 | -2T & 5B = 7 Bars |
| 6-5-266 | -2T & 4B = 6 Bars |
| 6-5-249 | -2T & 4B = 6 Bars |
| 7-5-230 | -2T & 5B = 7 Bars |
| 6-5-216 | -2T & 4B = 6 Bars |
| 6-5-199 | -2T & 4B = 6 Bars |
| 6-5-180 | -2T & 4B = 6 Bars |
| 7-5-163 | -2T & 5B = 7 Bars |
| 6-5-146 | -2T & 4B = 6 Bars |
| 7-5-130 | -2T & 5B = 7 Bars |
| 6-5-110 | -2T & 4B = 6 Bars |

GROUP E

| | |
|---------|----------|
| 1-4-179 | = 1 Bar |
| 6-4-173 | = 6 Bars |
| 1-4-169 | = 1 Bar |

See Sect. D-D for Spacing
T denotes Top
B denotes Bottom
S. Curb denotes Safety Curb

Work this sheet with Sfrs. 351 thru 362, 364, & 365.

HAZELT & ERDAL CONSULTING ENGINEERS

BOARD OF **WAYNE COUNTY ROAD COMMISSIONERS**
DETROIT, MICHIGAN

PHILIP J. NEUDECK
ENGINEER OF DESIGN, SURVEYING AND EXPRESSWAYS

APPROVED: *[Signature]*
DATE: 6-6-67
ENGINEER OF SURVEYING AND EXPRESSWAYS

REVISIONS

STATE PROJECT: **MOUNT ELLIOTT-MOUND RD. GRADE SEPARATION CONCRETE DECK - SPANS 14 & 15**

STATE FILE: _____ ROAD: _____

COUNTY JOB: **246**

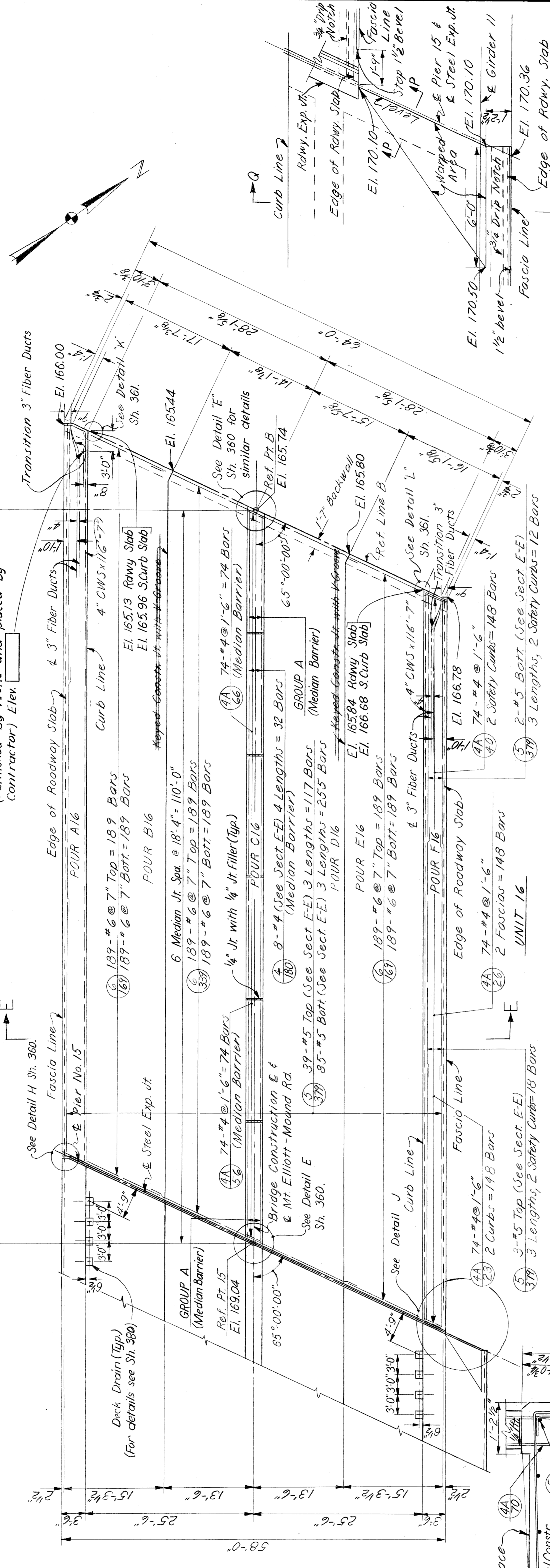
ISSUE NO.: _____ SHEET NO.: **363**

DATE: 5-23-68

H & E FILE NO. 603

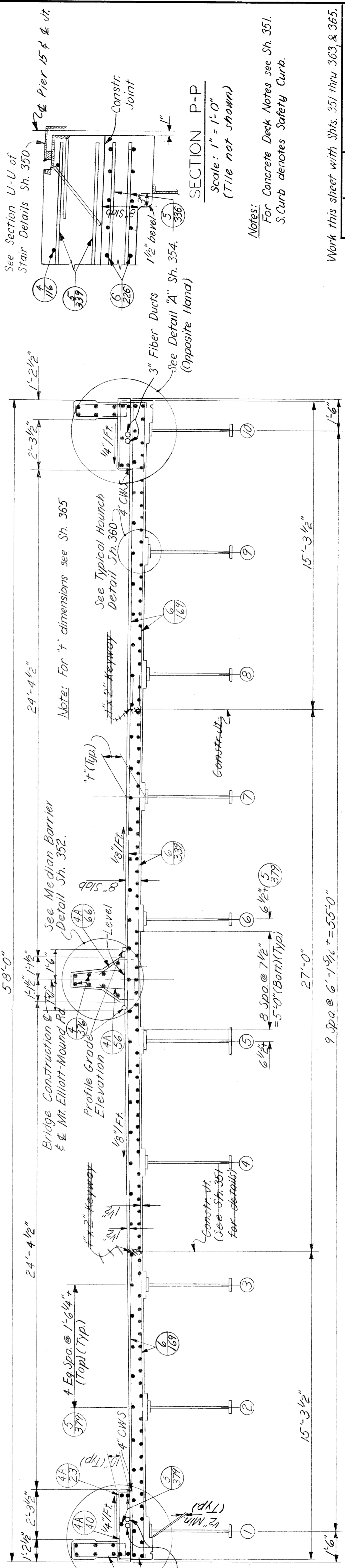
110'-0" SPAN 16

Standard WCR Monument for Bench Mark
(Furnished by WCRAC and placed by
Contractor) Elev.



| 2 GROUPS of A | |
|---------------|----------------------|
| 1-4-176 | (2 Groups) = 2 Bars |
| 6-4-180 | (2 Groups) = 12 Bars |
| 1-4-186 | (2 Groups) = 2 Bars |

SLAB PLAN
Scale: 1/8" = 1'-0"
(Railings & Parapet not shown)



SECTION Q-Q
Scale: 3/4" = 1'-0"
(Structural Tile & 3" Fiber
Ducts not shown)

See Detail X
Sh. 354
For Spacing
See Sheets 374
368 thru 374
3" Fiber Ducts

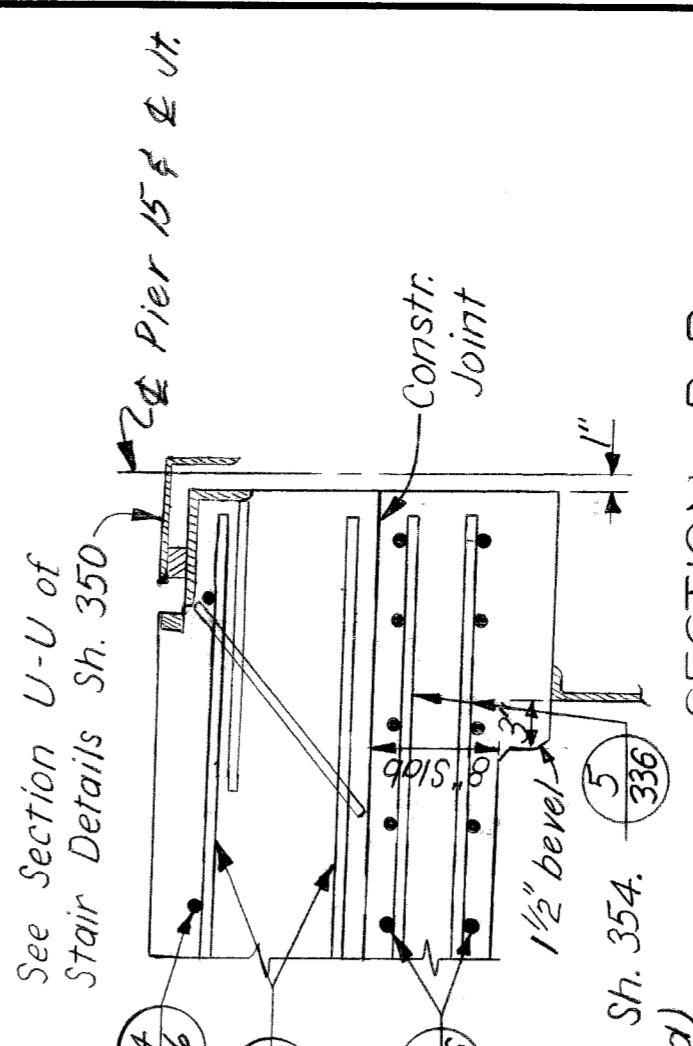
Deck Pour Spacing

SECTION E-E
Scale: 3/8" = 1'-0"

9 Sp. @ 6'-1 3/4" = 55'-0"

Notes:
For Concrete Deck Notes see Sh. 351.
S.Curb denotes Safety Curb.

DETAIL J
Note: For detail
of stairs see
Scale: 3/8" = 1'-0" Spts. 348 thru
350.



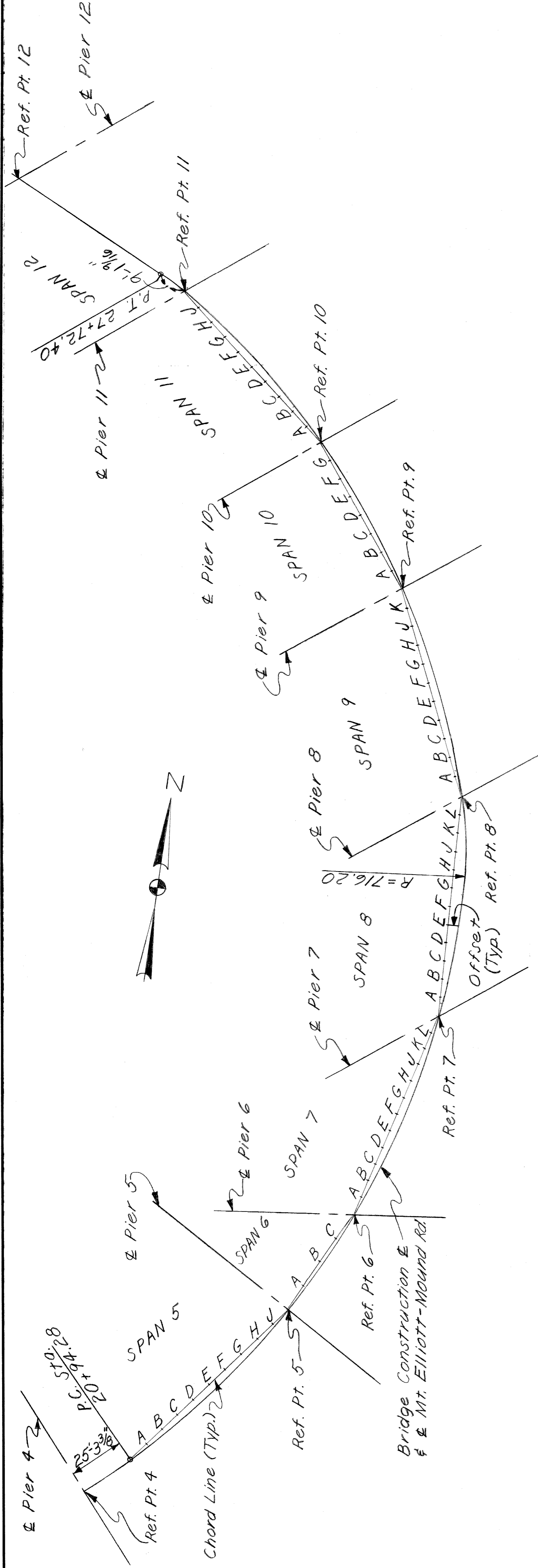
SECTION P-P

Scale: 1" = 1'-0"
(Tile not shown)

Notes:
For Concrete Deck Notes see Sh. 351.
S.Curb denotes Safety Curb.

Work this sheet with Shts. 351 thru 363 & 365.

| | | | |
|---|--------------------------------------|---------------|------------|
| | HAZELET & ERDAL CONSULTING ENGINEERS | STATE PROJECT | COUNTY JOB |
| MOUNT ELLIOTT-MOUND RD. GRADE SEPARATION CONCRETE DECK - SPAN 16 | | STATE FILE | 246 |
| WAYNE COUNTY ROAD COMMISSIONERS | | BRIDGE | SHEET NO. |
| DETROIT, MICHIGAN | | ROAD | 364 |
| PHILIP J. NEUDECK ENGINEER OF BRIDGE, STRUCTURES AND HIGHWAYS | | DATE: 5-12-68 | |
| APPROVED <i>Michael Berry</i> ENGINEER OF BRIDGE, STRUCTURES AND HIGHWAYS | | DATE: 7-15-67 | |
| DRAWN BY HAZELET & ERDAL | | CHECKED BY | |
| CORRECTED BY <i>Michael Berry</i> | | DATE | |
| AS BUILT 9-17-71 E.M.B. & L.H.M. | | REVISIONS | |
| H&E FILE NO. 603 | | | |



LAYOUT OF BRIDGE CONSTRUCTION
Not to Scale

| LOCATION | CHORD LENGTH (Along Chord) | PT.A | PT.B | PT.C | PT.D | PT.E | PT.F | PT.G | PT.H | PT.I | PT.J | PT.K | PT.L |
|----------|----------------------------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|-----------|------|-----------|
| SPAN 5 | 96'-8 1/2" | 0'-7 1/8" | 1'-0 1/2" | 1'-4 1/2" | 1'-6 3/8" | 1'-7 3/8" | 1'-6 3/8" | 1'-4 1/2" | 1'-0 1/2" | 0'-7 1/8" | | | |
| SPAN 6 | 46'-8 1/8" | 0'-3 3/8" | 0'-4 1/2" | 1'-3 1/2" | 1'-8 3/8" | 2'-3 3/8" | 2'-3 3/8" | 2'-3 3/8" | 2'-0 3/4" | 1'-8 3/8" | 1'-3 1/2" | | 0'-8 1/2" |
| SPAN 7 | 115'-3 3/8" | 0'-8 1/2" | 1'-3 1/2" | 1'-9 3/8" | 2'-1 3/8" | 2'-4" | 2'-4" | 2'-4" | 2'-1 3/8" | 1'-9 3/8" | 1'-4" | | 0'-8 1/2" |
| SPAN 8 | 117'-3 1/6" | 0'-9 3/8" | 1'-4" | 1'-9 3/8" | 2'-1 3/8" | 2'-4" | 2'-4" | 2'-0 3/4" | 1'-8 3/8" | 1'-3 1/2" | 1'-4" | | 0'-8 1/2" |
| SPAN 9 | 112'-0 3/16" | 0'-5 3/8" | 0'-9 1/2" | 1'-0" | 1'-0 3/4" | 1'-0" | 0'-9 1/2" | 0'-5 3/8" | 1'-8 3/8" | 1'-3 1/2" | 0'-8 1/2" | | |
| SPAN 10 | 78'-0 1/4" | 0'-7 3/8" | 1'-2 1/8" | 1'-0" | 1'-9 1/8" | 1'-10" | 1'-9 1/8" | 1'-6 1/2" | 1'-2 1/8" | 0'-7 3/8" | | | |
| SPAN 11 | 102'-4 1/8" | 0'-7 3/8" | 1'-2 1/8" | 1'-6 1/2" | 1'-9 1/8" | 1'-10" | 1'-9 1/8" | 1'-6 1/2" | 1'-2 1/8" | 0'-7 3/8" | | | |

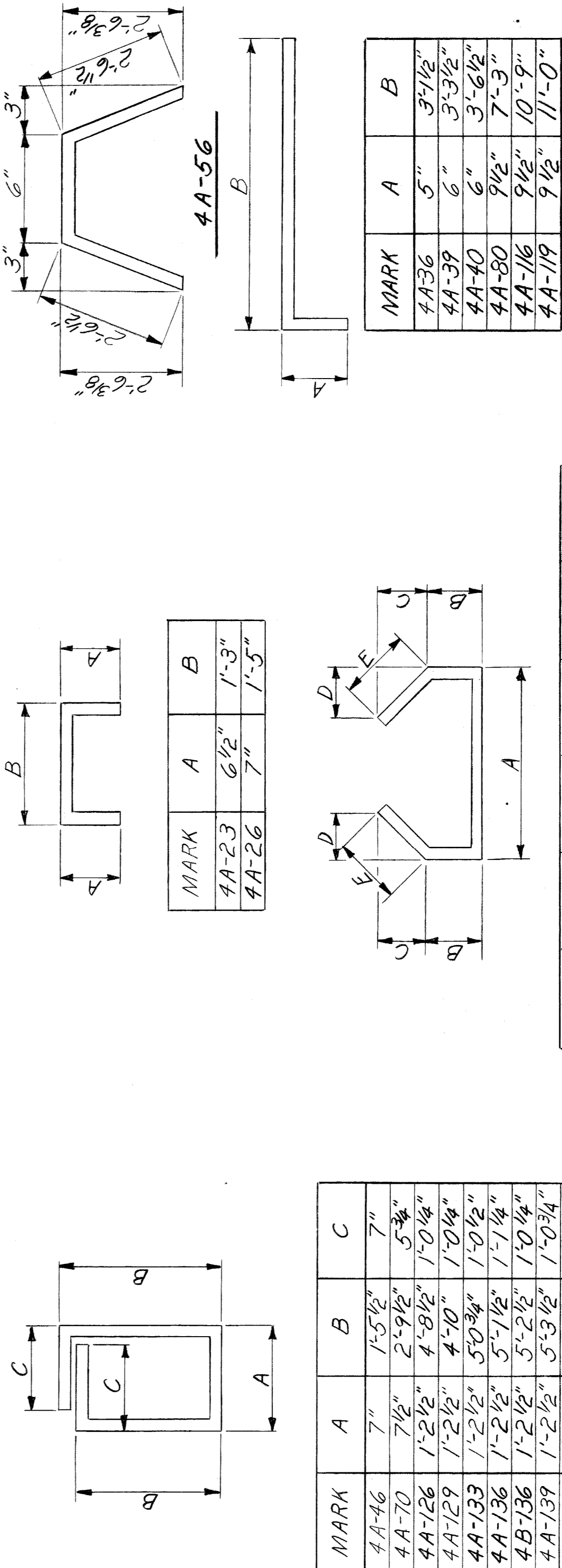
Note: Offsets are measured perpendicular to the chord lines.

QUANTITIES

| | | |
|--|-----------------|-----------------|
| Grade A (10B Slag) Concrete - Super. 907,958 | 3,271 Cu. Yds. | 907,958 Lbs. |
| Steel Reinforcement | 650 Sq. Ft. | 650 Sq. Ft. |
| Dampproofing | 236 Sq. Ft. | 236 Sq. Ft. |
| Joint Waterproofing | 467 Sq. Ft. | 467 Sq. Ft. |
| 1/2" Joint Filler | 4,310 Lin. Ft. | 4,310 Lin. Ft. |
| 1" Joint Filler | 343 Lbs. | 343 Lbs. |
| Hot-Poured Rubber-Asphalt Type Filler | 1,140 Lbs. | 1,140 Lbs. |
| 4" Copper Waterstop | 30,130 Sq. Ft. | 30,130 Sq. Ft. |
| Rubber Surface Finish | 13,095 Sq. Yds. | 13,095 Sq. Yds. |
| Deck Waterproofing | 467 Gals. | 467 Gals. |
| Water-Reducing Retarding Admixture | 8,720 Each | 8,720 Each |
| Structural Tile | 204 Cu. Yds. | 204 Cu. Yds. |
| Grade A (10B Slag) White Concrete - Median Barrier | 252 Sq. Ft. | 252 Sq. Ft. |
| 1/4" Joint Filler | | |

"t" DIMENSIONS (Inches)

| Location | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 |
|-----------------------|-------|--------|--------|--------|--------|--------|--------|--------|--------|-------|-------|
| Girder Units 1 thru 4 | 8 1/2 | 9 | 9 3/4 | 10 1/2 | 10 1/2 | 10 1/2 | 10 1/2 | 9 3/4 | 9 | 8 1/2 | — |
| Floorbeam UNIT 5 | 9 1/4 | 9 3/8 | 10 1/2 | 11 1/8 | 11 1/8 | 11 1/8 | 11 1/8 | 12 1/2 | 13 1/4 | 12 | 9 1/8 |
| UNIT 6 | 8 3/8 | 9 1/8 | 9 1/8 | 9 1/8 | 9 | 10 | 9 1/8 | 9 1/8 | 9 1/8 | 8 3/8 | 9 1/4 |
| UNIT 7 & 8 | 8 1/2 | 10 1/2 | 9 | 9 | 10 | 11 1/2 | 9 | 10 | 9 | 9 | 9 |
| UNIT 9 | 8 1/2 | 9 1/2 | 10 1/2 | 8 3/4 | 9 1/2 | 11 | 8 3/4 | 9 1/2 | 8 3/4 | 9 1/2 | 9 |
| UNIT 10 | 8 1/2 | 9 1/2 | 11 1/2 | 8 3/4 | 10 1/2 | 11 | 8 3/4 | 10 1/2 | 8 3/4 | 9 3/4 | 8 3/4 |
| UNIT 11 | 8 1/2 | 9 | 10 1/2 | 8 3/4 | 9 1/2 | 10 1/2 | 8 3/4 | 9 1/2 | 8 3/4 | 9 | 8 1/2 |
| UNIT 12 | 8 1/2 | 8 3/4 | 9 | 8 3/4 | 9 | 9 | 8 3/4 | 9 | 8 3/4 | 8 1/2 | 8 1/2 |
| UNIT 13 thru 15 | 8 3/8 | 8 3/8 | 9 | 8 3/8 | 9 | 9 | 8 3/8 | 9 | 8 3/8 | 8 1/2 | 8 1/2 |
| UNIT 16 | 8 1/2 | 9 | 9 1/2 | 8 3/8 | 9 | 9 1/2 | 10 | 8 3/8 | 8 3/8 | 9 | — |



BAR BENDING DIAGRAMS

| MARK | A | B | C | D | E |
|-------|-------|----|-------|-------|-------|
| 4A-63 | 2'-0" | 8" | 1'-3" | 1'-3" | 1'-6" |
| 4A-66 | 2'-3" | 8" | 1'-3" | 1'-3" | 1'-6" |

BAR BENDING DIAGRAMS

| MARK | A | B | C |
|--------|-----------|-----------|-----------|
| 4A-46 | 7" | 1'-5 1/2" | 7" |
| 4A-70 | 7 1/2" | 2'-9 1/2" | 5 3/4" |
| 4A-126 | 1'-2 1/2" | 4'-8 1/2" | 1'-0 1/4" |
| 4A-129 | 1'-2 1/2" | 4'-10" | 1'-0 1/4" |
| 4A-133 | 1'-2 1/2" | 5'-0 3/4" | 1'-0 1/4" |
| 4A-136 | 1'-2 1/2" | 5'-1 1/2" | 1'-1 1/4" |
| 4B-136 | 1'-2 1/2" | 5'-3 1/2" | 1'-0 3/4" |
| 4A-139 | 1'-2 1/2" | 5'-2 1/2" | 1'-0 1/4" |
| 4B-139 | 1'-2 1/2" | 5'-4 1/4" | 1'-0" |
| 4A-140 | 1'-2 1/2" | 5'-5 1/2" | 1'-0 1/4" |
| 4A-143 | 1'-2 1/2" | 5'-6 1/2" | 1'-0 3/4" |
| 4A-153 | 1'-4" | 5'-1 1/2" | 1'-1" |

Work this sheet with Shts. 351 thru 364.

HAZLET & ERDAL CONSULTING ENGINEERS

| | |
|---------------|---------|
| STATE PROJECT | 246 |
| ISSUE NO. | 365 |
| DATE | 5/23/65 |

MOUNT ELLIOTT-MOUND RD.
GRADE SEPARATION
CONCRETE DECK DETAILS

WAYNE COUNTY ROAD COMMISSIONERS
DETROIT, MICHIGAN

BOARD OF ROAD ENGINEERS AND SURVEYORS
APPROVED: [Signature]
CORRECTED BY: [Signature]
DATE: 7-15-67

APPROVED: [Signature]
CORRECTED BY: [Signature]
DATE: 7-15-67

WILLIAM E. KREGER
PHILIP J. NEUDECK

