

PROFILE OF ROADWAYS
 scale: horiz. 1" = 30', vert. 1" = 15'

TEST HOLE No. 1

TEST HOLE No. 2

TEST HOLE No. 3

N-3-4-5	10 AM, LOOSE SANDY CLAY 3'-6"	10 AM, LOOSE SANDY CLAY 3'-6"	10 AM, LOOSE SANDY CLAY 3'-6"
N-1-2-5	SAND 8'-6"	SAND 8'-6"	SAND 8'-6"
N-1-2-3	SOFT SILTY GREY CLAY 12'-0"	SOFT SILTY GREY CLAY 12'-0"	SOFT SILTY GREY CLAY 12'-0"
N-2-3-5	SOFT GREY CLAY SOME SAND & GRAVEL 29'-0"	SOFT GREY CLAY SOME SAND & GRAVEL 29'-0"	SOFT GREY CLAY SOME SAND & GRAVEL 29'-0"
N-2-3-4	SOFT GREY CLAY SOME SAND & GRAVEL 21'-0"	SOFT GREY CLAY SOME SAND & GRAVEL 21'-0"	SOFT GREY CLAY SOME SAND & GRAVEL 21'-0"
N-3-4-3	SOFT GREY CLAY SOME SAND & GRAVEL 22'-6"	SOFT GREY CLAY SOME SAND & GRAVEL 22'-6"	SOFT GREY CLAY SOME SAND & GRAVEL 22'-6"
N-2-3-4	MED. GREY CLAY SAND & GRAVEL 30'-6"	MED. GREY CLAY SAND & GRAVEL 30'-6"	MED. GREY CLAY SAND & GRAVEL 30'-6"
N-3-4-5	MED. GREY CLAY SAND & GRAVEL 31'-6"	MED. GREY CLAY SAND & GRAVEL 31'-6"	MED. GREY CLAY SAND & GRAVEL 31'-6"

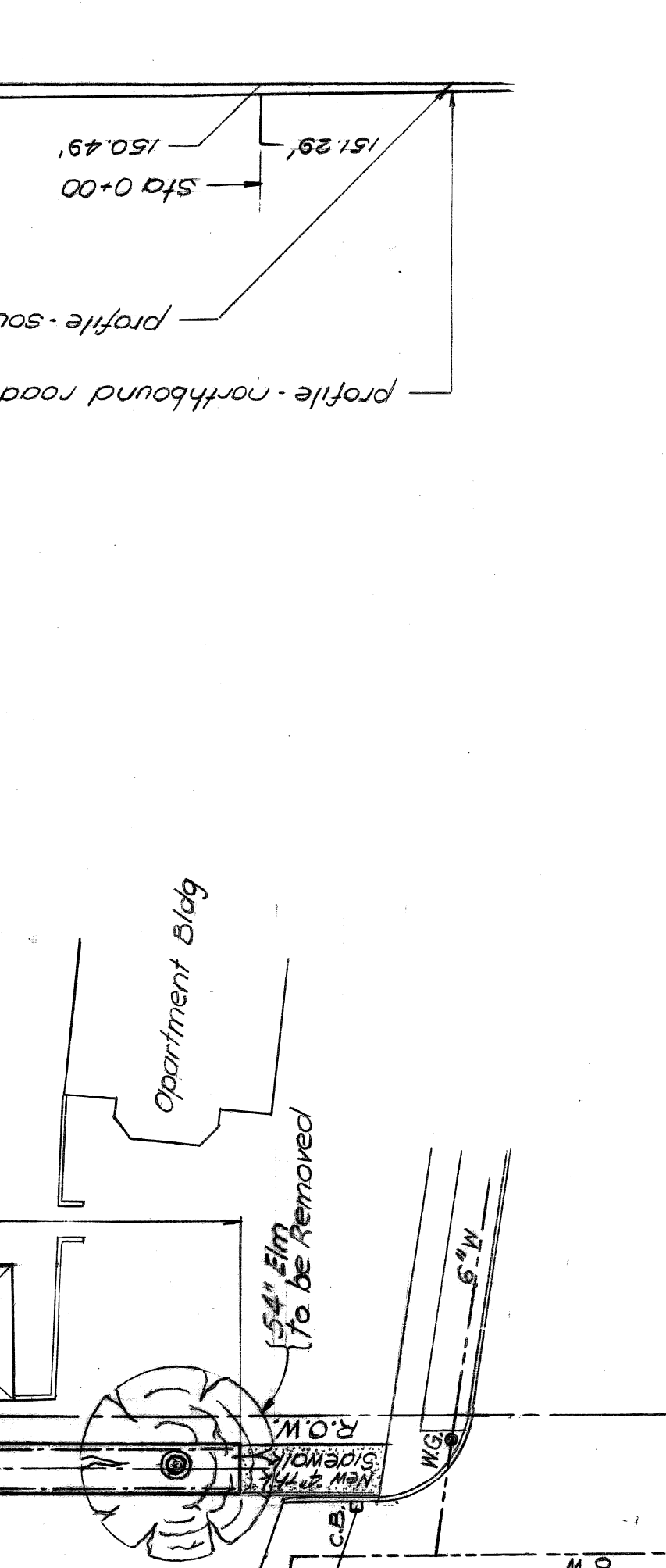
DRY ON COMPLETION W.L. & 26'-0" - 3 HRS AFTER COMPL.
 W.L. & 4'-6" - 24 HRS AFTER COMPLETION

TEST BORINGS

NOTE:
 N INDICATES NUMBER OF BLOWS TO DRIVE SAMPLER 6" USING 140" HAMMER FALLING 30".

NUMBER OF BLOWS PER 6" INCREMENT
 TRANSVERSE SHEAR STRENGTH P.S.F.

DEPTH
 COMPOSITION

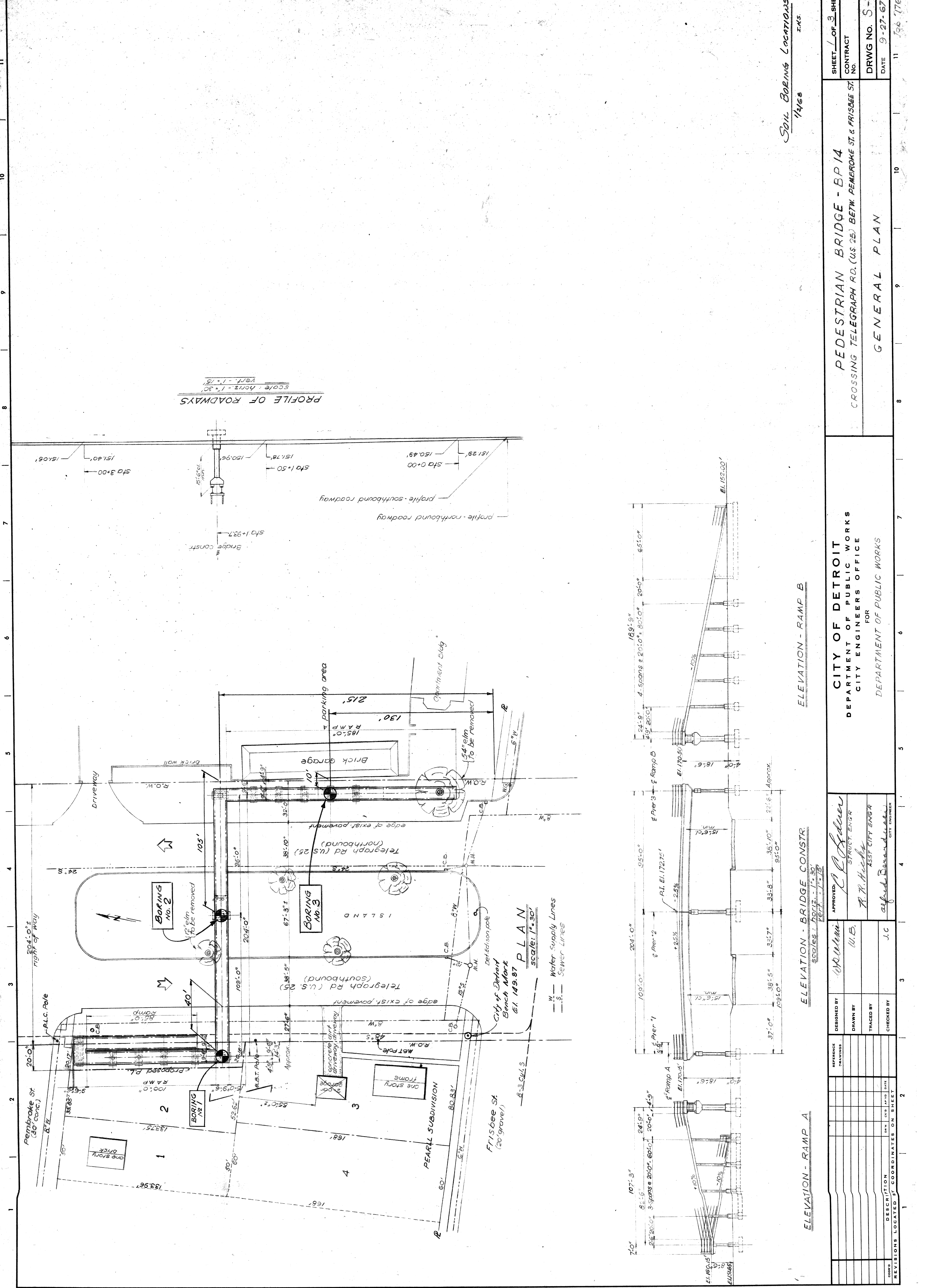


ELEVATION - BRIDGE CONSTR.
 scale: horiz. 1" = 30', vert. 1" = 15'

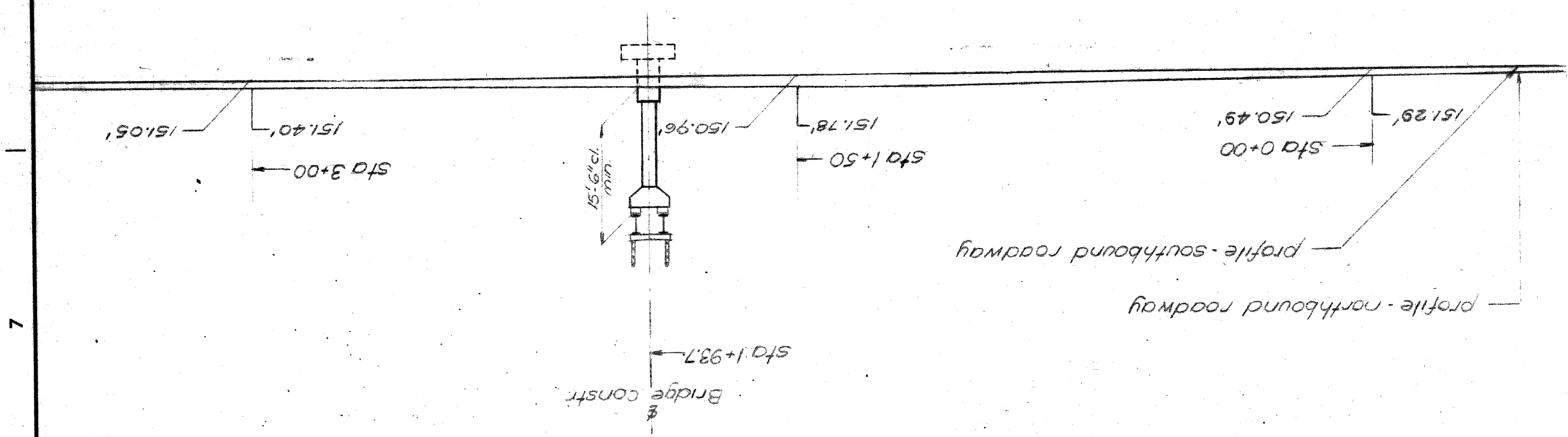
APPROVED: [Signature]

DESIGNED BY: [Signature]
 DRAWN BY: W.B.
 TRACED BY: [Signature]
 CHECKED BY: STAM J.C.

DESCRIPTION: [Blank]
 DATE: [Blank] DTD: [Blank] DATE: [Blank]
 REVISIONS LOCATED BY COORDINATES ON SHEET

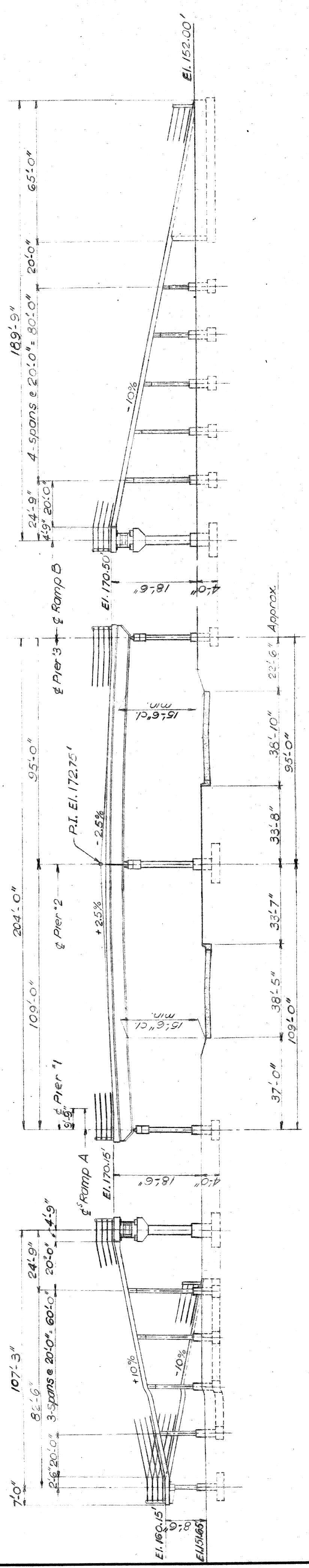


PROFILE OF ROADWAYS
Scale: Horiz. - 1" = 30'
Vert. - 1" = 15'



PLAN
City of Detroit
Bench Mark
El. 149.87
Scale: 1" = 30'

— W — Water Supply Lines
— S — Sewer Lines



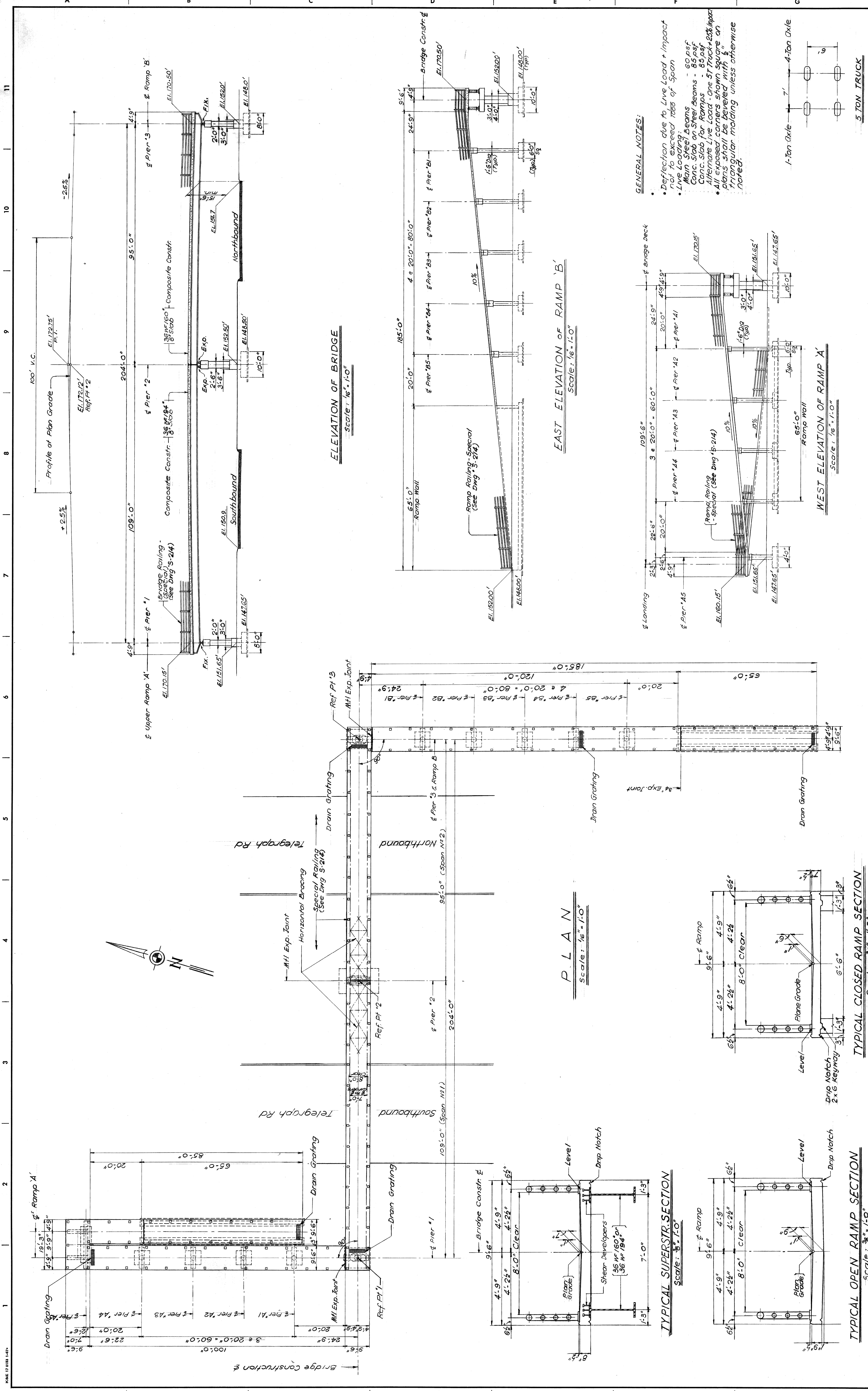
ELEVATION - RAMP A

ELEVATION - BRIDGE CONSTR

ELEVATION - RAMP B

SOIL BORING LOCATIONS
1/21/66
Z.M.S.

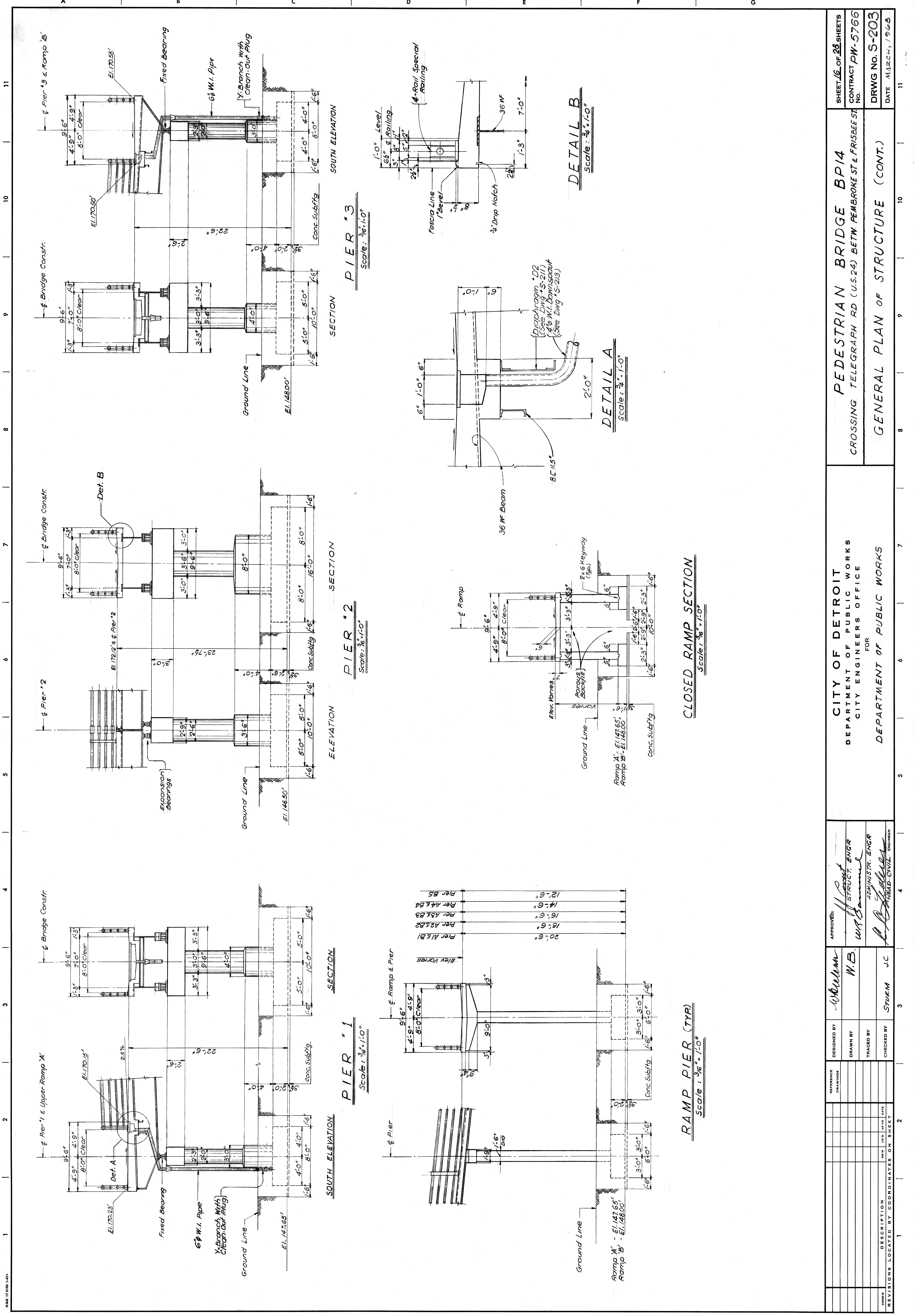
REVISIONS LOCATED BY COORDINATES ON SHEET	NO.	DATE	BY	CHECKED BY	TRACED BY	DRAWN BY	DESIGNED BY	REFERENCE DRAWINGS	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE FOR DEPARTMENT OF PUBLIC WORKS			PEDESTRIAN BRIDGE - BP 14 CROSSING TELEGRAPH RD. (U.S. 25) BETW. PEMROKE ST. & FRISBEE ST.			SHEET 1 OF 3 SHEETS
									CONTRACT NO.	DRWG NO. S-1	DATE 9-27-67	DATE 11 10 9 8 7 6 5 4 3 2 1			



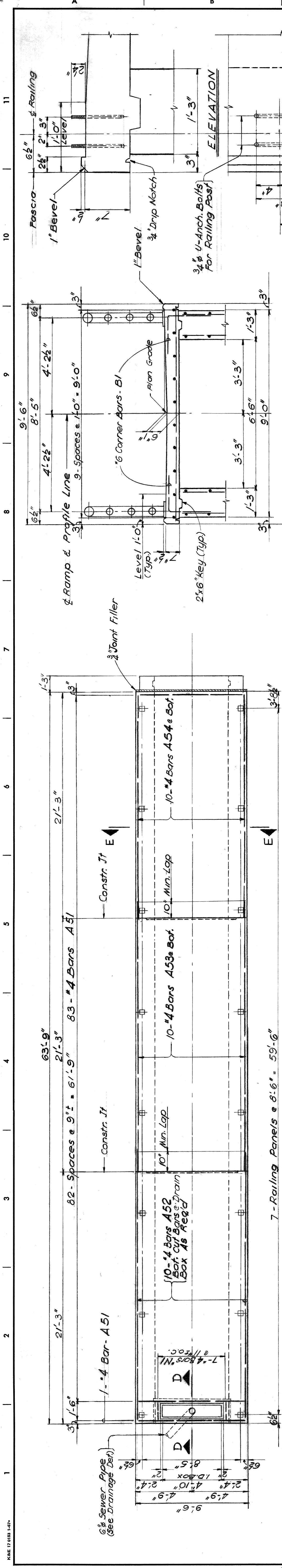
GENERAL NOTES:

- Deflection due to Live Load + Impact not to exceed 1/800 of Span
- Live Loading:
 - Main Steel Beams - 60 psf
 - Conc. Slab on Steel Beams - 85 psf
 - Alternate Live Load - One 5T Truck + 22% Impact
- All exposed corners shown square on plans shall be beveled with 1/4" triangular molding unless otherwise noted.

SHEET 15 OF 25 SHEETS		CONTRACT PM-5706	
CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE		PEDESTRIAN BRIDGE - BPIA CROSSING TELEGRAPH RD (US 24) BETW. PEMROKE ST. & FRISBEE ST.	
FOR DEPARTMENT OF PUBLIC WORKS		GENERAL PLAN OF STRUCTURE	
DESIGNED BY <i>W. B. Williams</i>	APPROVED <i>W. B. Williams</i> STRUCT. ENGR.	DESIGNED BY <i>W. B. Williams</i>	APPROVED <i>W. B. Williams</i> ADMINISTR. ENGR.
DRAWN BY <i>N. B.</i>	TRACED BY <i>Storm</i>	DRAWN BY <i>N. B.</i>	TRACED BY <i>Storm</i>
CHECKED BY <i>J. C.</i>	REVISIONS LOCATED BY COORDINATES ON SHEET	CHECKED BY <i>J. C.</i>	REVISIONS LOCATED BY COORDINATES ON SHEET
DATE: MARCH, 1966		DRWG NO. S-202	



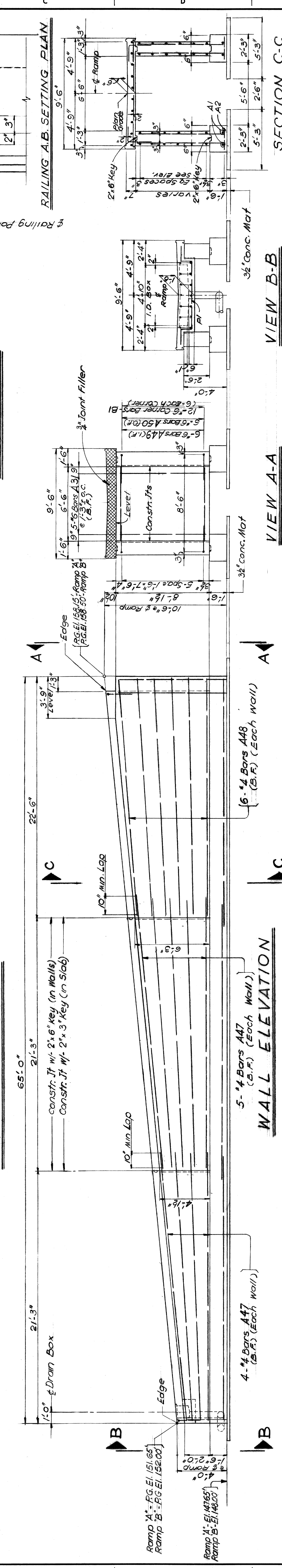
SHEET 16 OF 28 SHEETS		CONTRACT PW-5766	
DESIGNED BY: <i>William W.B.</i>		CITY OF DETROIT	
DRAWN BY: <i>W.B.</i>		DEPARTMENT OF PUBLIC WORKS	
TRACED BY: <i>STUEM J.C.</i>		CITY ENGINEERS OFFICE	
CHECKED BY: <i>STUEM J.C.</i>		FOR	
DESCRIPTION: RAMP PIER		DEPARTMENT OF PUBLIC WORKS	
REVISIONS LOCATED BY COORDINATES ON SHEET		GENERAL PLAN OF STRUCTURE (CONT.)	
NO. REV. DATE		PEDESTRIAN BRIDGE BPI4	
1		CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBROKE ST. & FRISBEE ST.	
2		NO.	
3		DRWG NO. S-203	
4		DATE MARCH, 1965	



PLAN - RAMP SLAB

7 - Railing Panels @ 8'-6" = 59'-6"

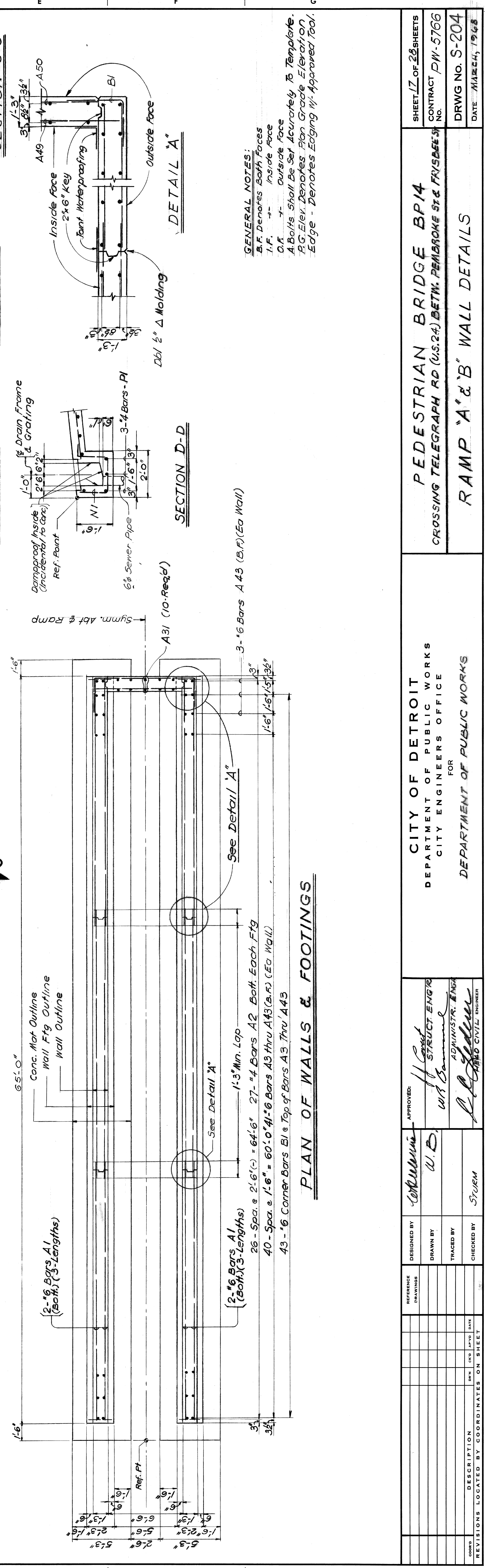
SECTION E-E



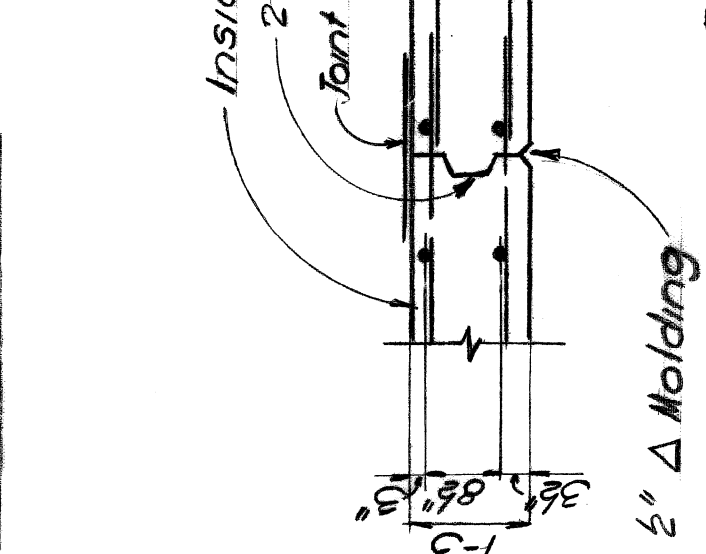
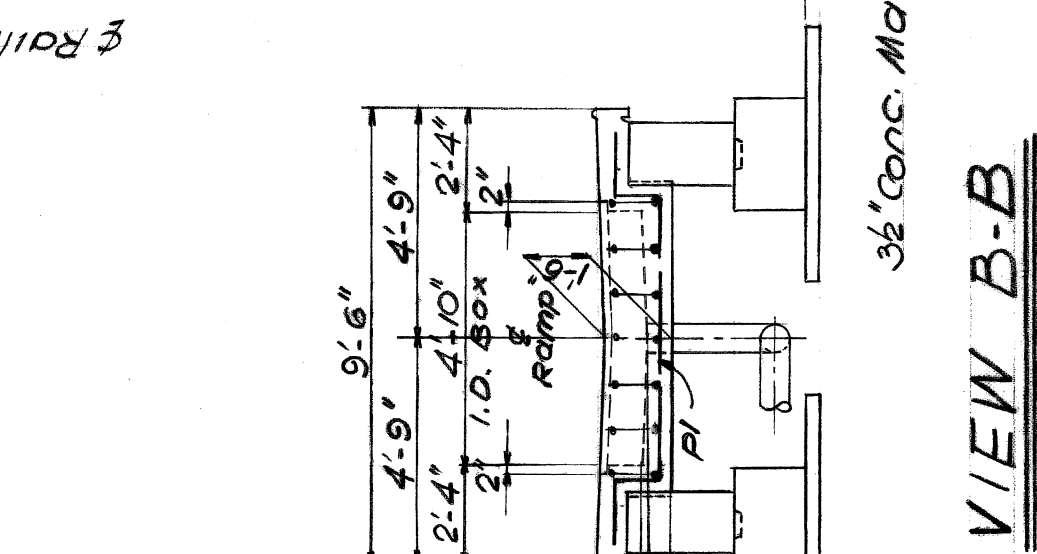
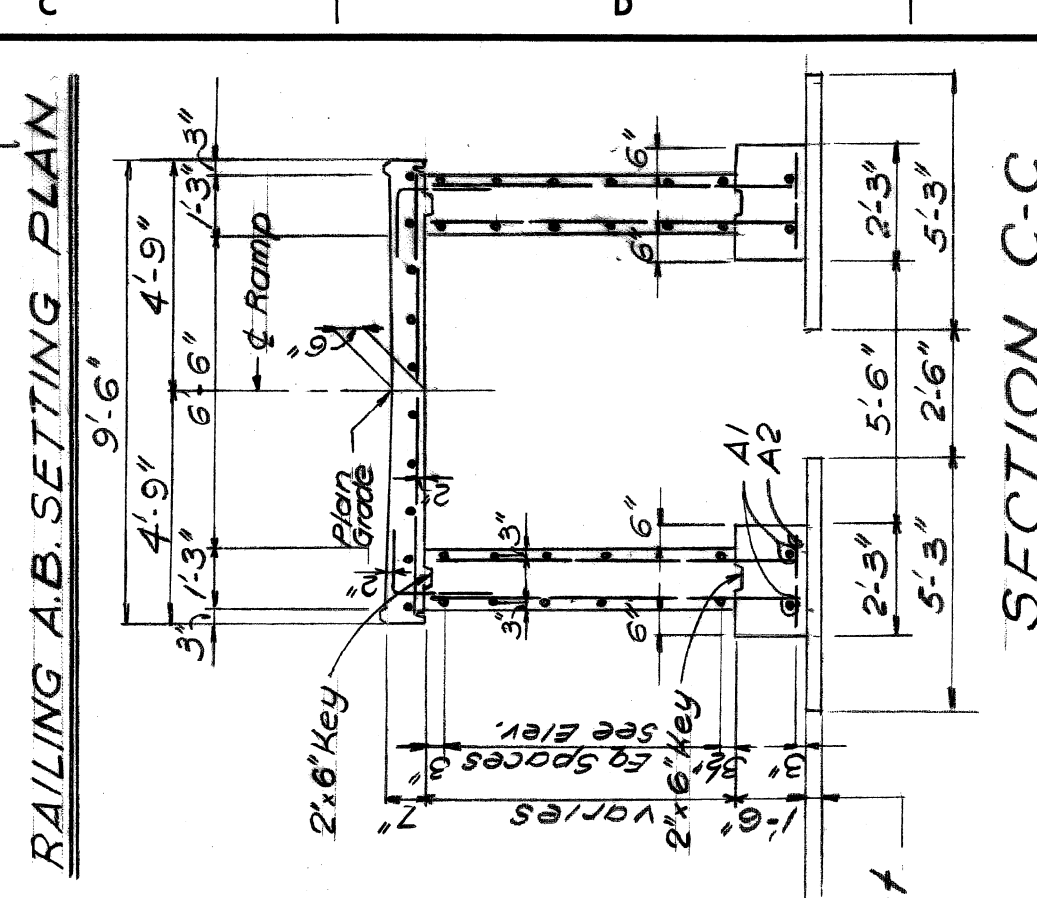
WALL ELEVATION

VIEW B-B

VIEW A-A



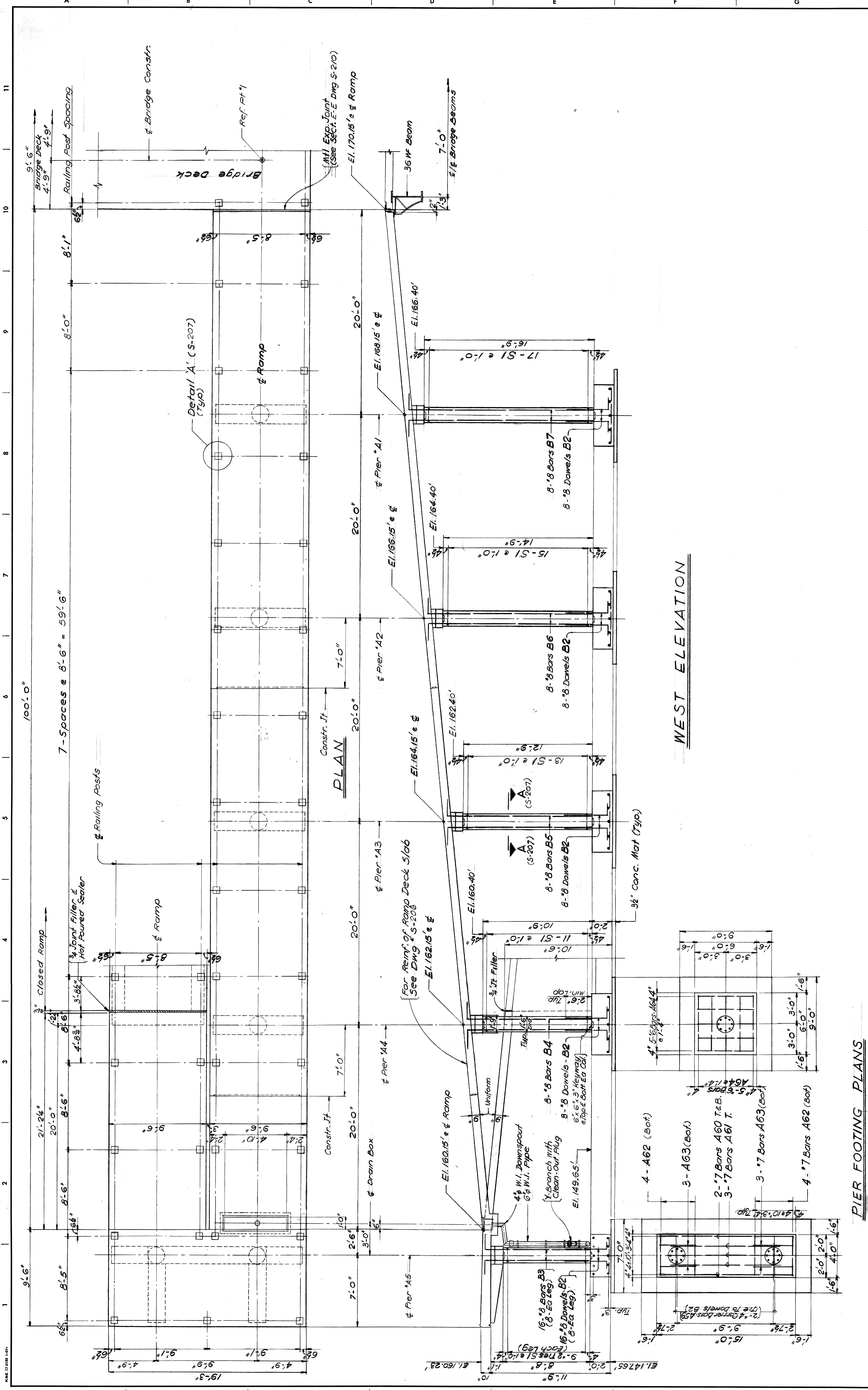
PLAN OF WALLS & FOOTINGS



DETAIL A'

GENERAL NOTES:
 B.F. Denotes Both Faces
 I.F. Inside Face
 O.F. Outside Face
 A Bolts Shall Be Set Accurately To Template.
 P.G. Elev. Denotes Plan Grade Elevation.
 Edge - Denotes Edging W/ Approved Tool.

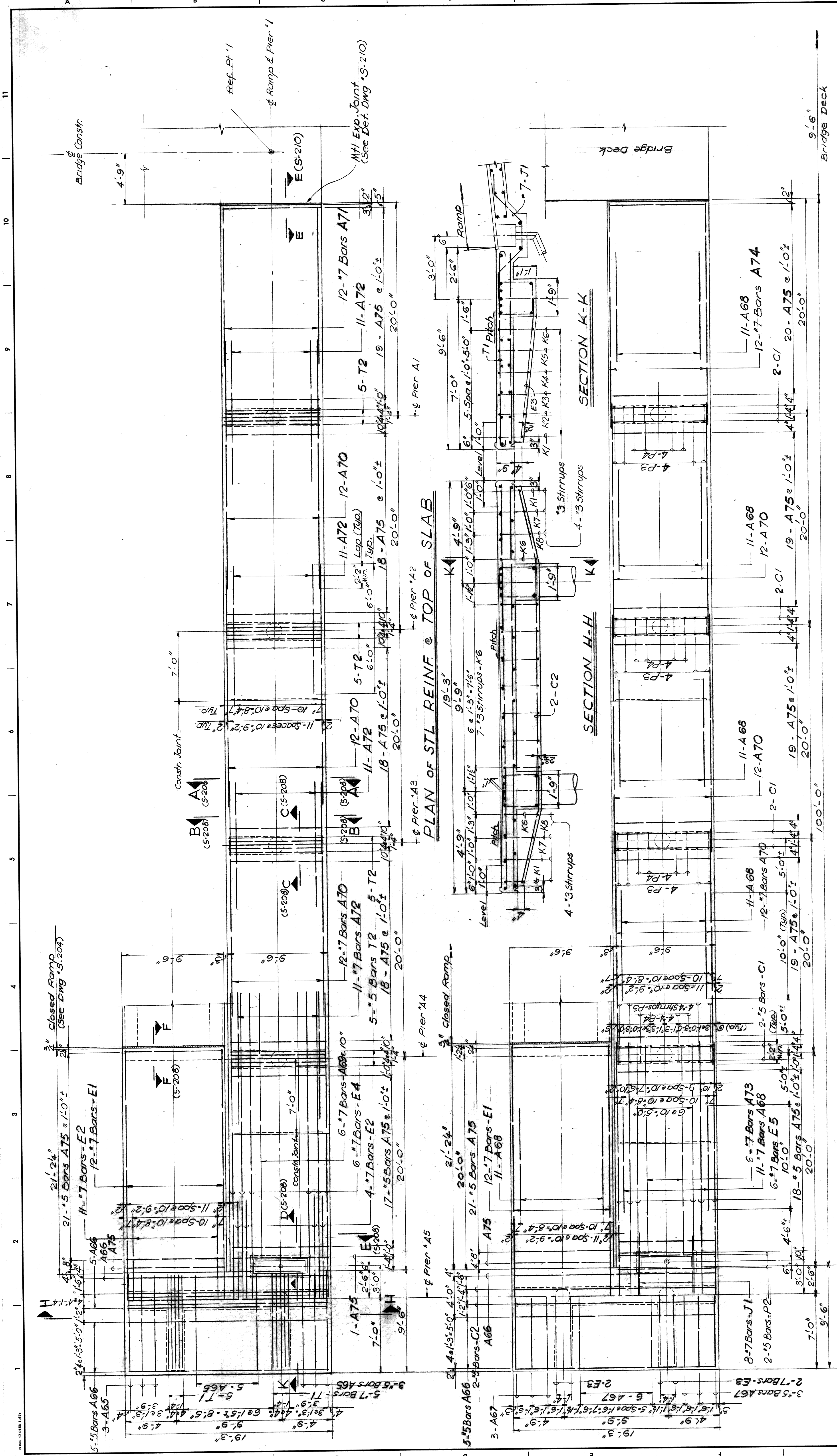
DESIGNED BY	W. B.	APPROVED	<i>[Signature]</i>
DRAWN BY	W. B.	STRUCT. ENGR.	<i>[Signature]</i>
TRACED BY		ADMINISTR. ENGR.	<i>[Signature]</i>
CHECKED BY	STURM	CIVIL ENGINEER	
DESCRIPTION	PEDESTRIAN BRIDGE BPIA		
REVISIONS LOCATED BY COORDINATES ON SHEET	CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBROKE ST & FRYSBEE ST		
	RAMP 'A' & 'B' WALL DETAILS		
	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE FOR DEPARTMENT OF PUBLIC WORKS		
	SHEET 17 OF 22 SHEETS CONTRACT PW-5766 DRAWG NO. S-204 DATE MARCH, 1968		



WEST ELEVATION

PIER FOOTING PLANS

DESIGNED BY	W. B. MULLIN	APPROVED	H. J. STRECH
DRAWN BY	N. B. MULLIN	ADMINISTRATOR	W. B. MULLIN
TRACED BY		HEAD CIVIL ENGINEER	L. J. STRECH
CHECKED BY	STURM		
DESCRIPTION	PIER FOOTING PLANS		
REVISIONS LOCATED BY COORDINATES ON SHEET			
CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE FOR DEPARTMENT OF PUBLIC WORKS			
PEDESTRIAN BRIDGE BP14 CROSSING TELEGRAPH RD (US. 24) BETW. PEMERROKE ST. & FRISBEE ST.			
PIERS & DECK - RAMP 'A'			
SHEET 18 OF 28 SHEETS	CONTRACT	NO.	PN-5766
DRWG NO.	S-205	DATE	MARCH, 1968



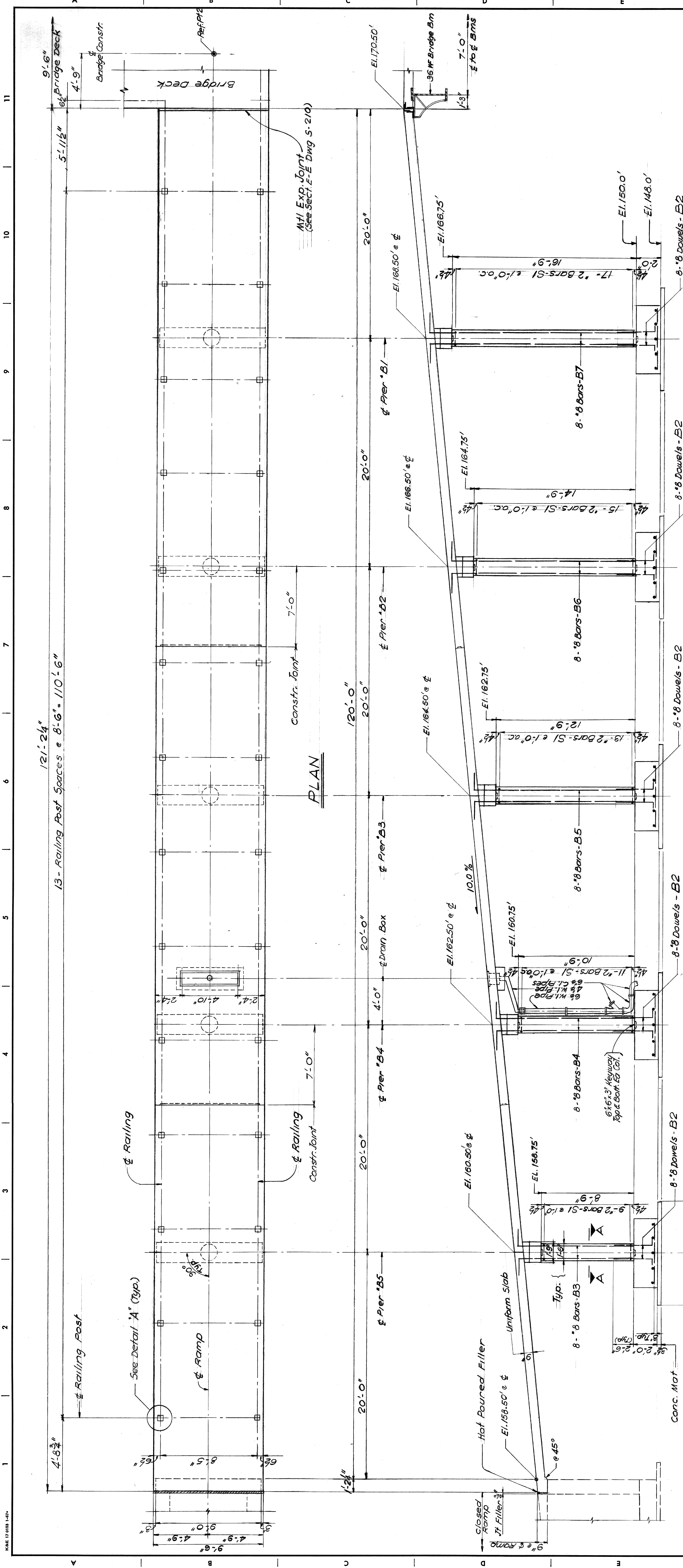
PLAN OF STL REINF. & TOP OF SLAB

SECTION H-H

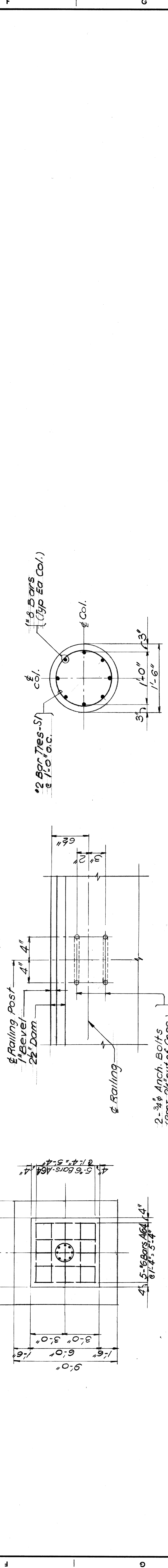
SECTION K-K

PLAN OF STL REINF. & BOT. OF SLAB

CORP.	REVISIONS LOCATED BY COORDINATES ON SHEET	DESIGNED BY	APPROVED	CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE	SHEET 29 OF 24 SHEETS CONTRACT PW-5766 NO.	
		DRAWN BY	W. S. [Signature]			PEDESTRIAN BRIDGE BP 14 CROSSING TELEGRAPH RD. (U.S. 24), BETW. PEMBRIDGE ST. & FRISBEE ST.
		CHECKED BY	W. S. [Signature]			
DATE	DATE	FOR		DATE	DATE	
		ADMINISTR. ENGR.		DEPT. OF PUBLIC WORKS	11	
		CIVIL ENGINEER		DECK SLAB - RAMP 'A'	10	



EAST ELEVATION

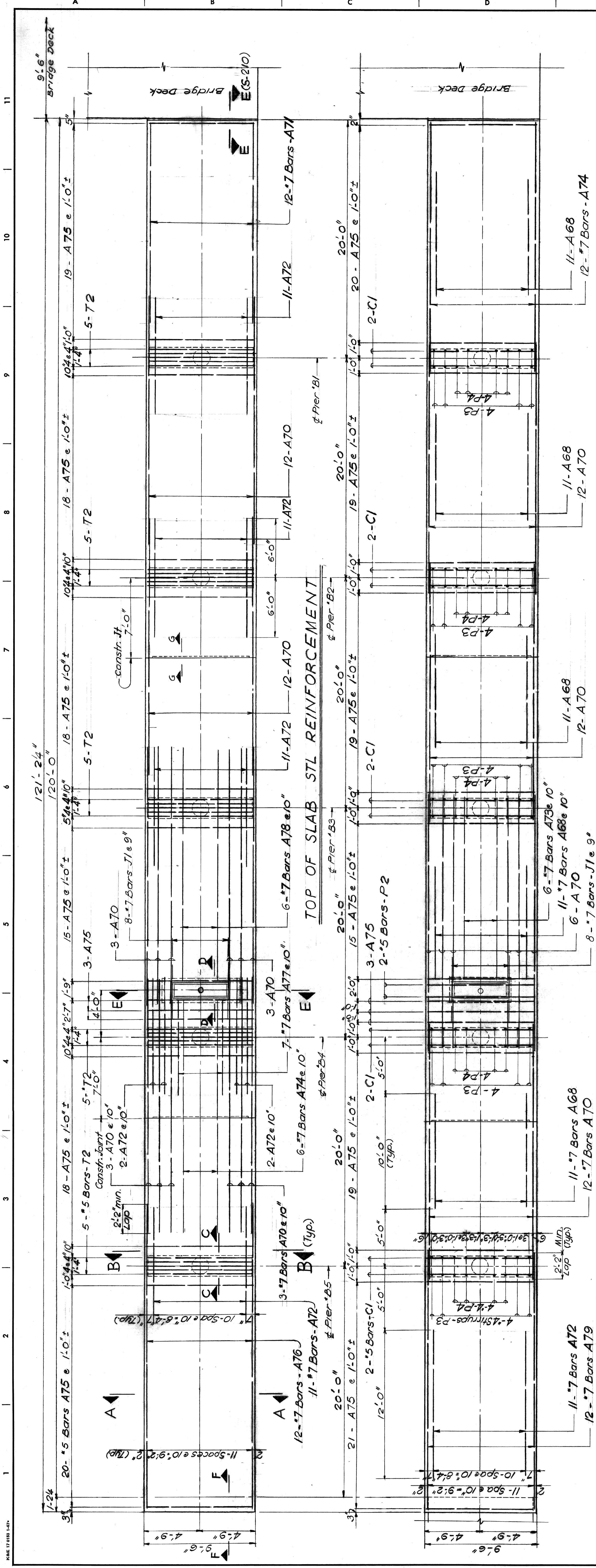


SECTION A-A

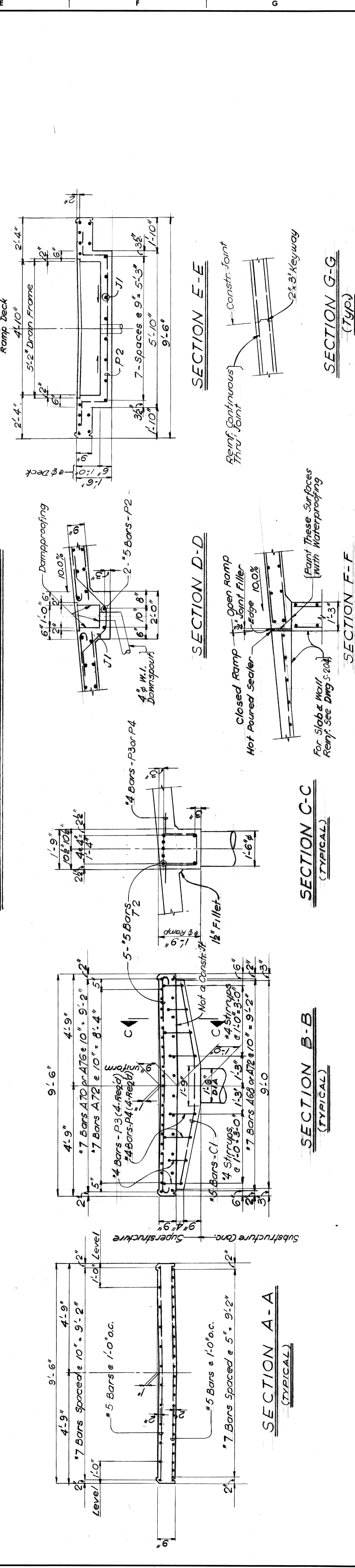
DETAIL 'A'

PIER FOOTING PLAN (Typ.)

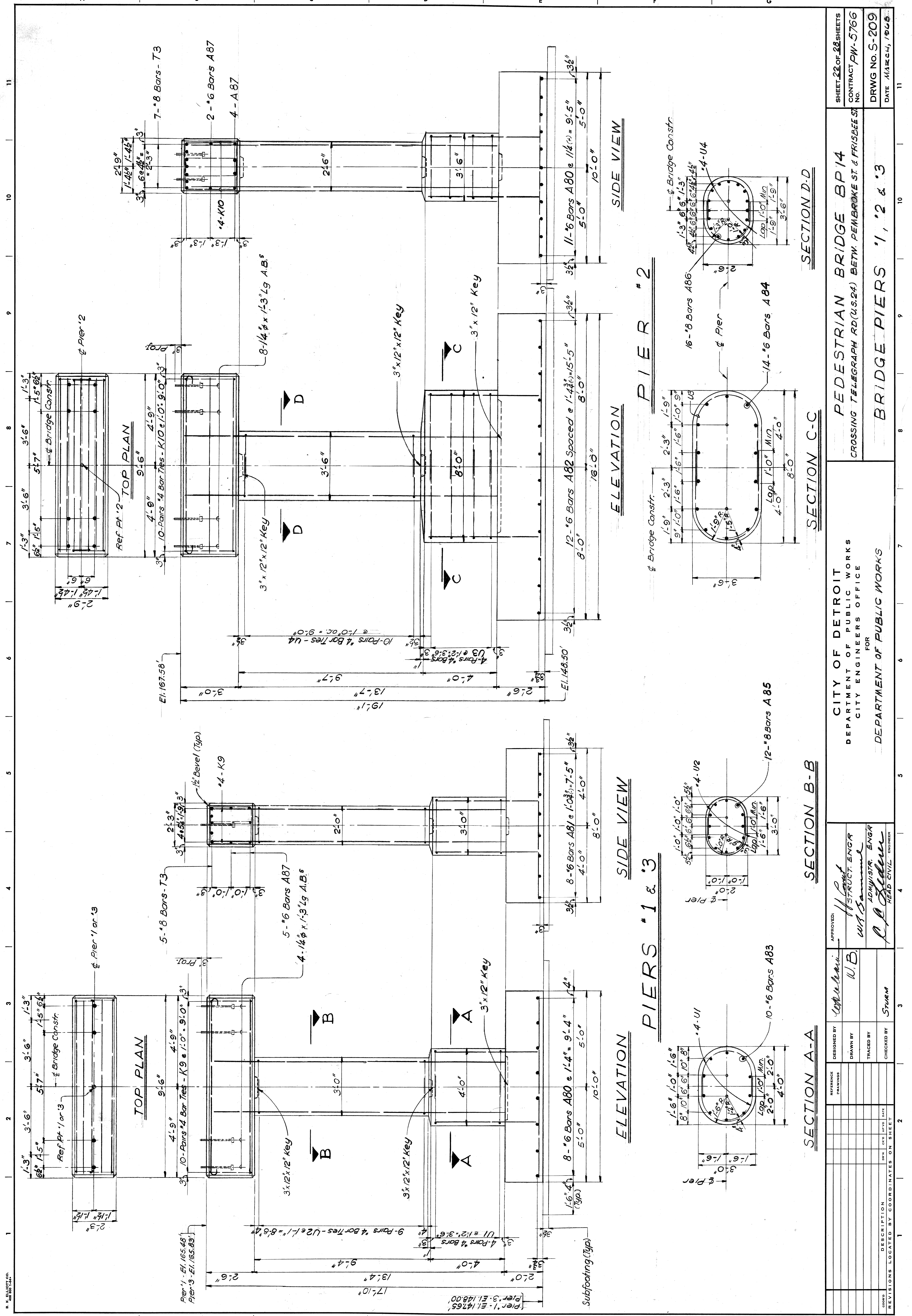
CONTRACT NO.	CONTRACT PW-5766
DATE	DATE MARCH, 1966
SHEET NO. OF SHEETS	SHEET 20 OF 26 SHEETS
PROJECT	PEDESTRIAN BRIDGE BPI4
DESCRIPTION	CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBRIDGE ST. & FRISBEE ST.
REVISIONS LOCATED BY COORDINATES ON SHEET	
DESIGNED BY	W. B. ...
DRAWN BY	W. B. ...
TRACED BY	Struem
CHECKED BY	Struem
APPROVED:	H. ...
FOR	ADMINISTR. ENGR
FOR	HEAD CIVIL ENGINEER
CITY OF DETROIT	DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE	FOR DEPARTMENT OF PUBLIC WORKS



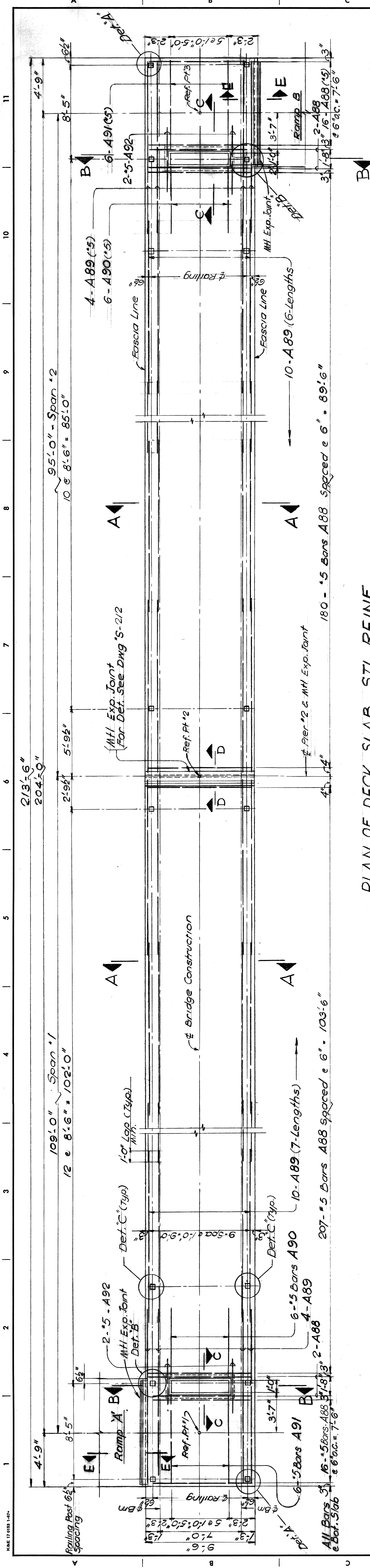
BOTTOM OF SLAB STL REINFORCEMENT



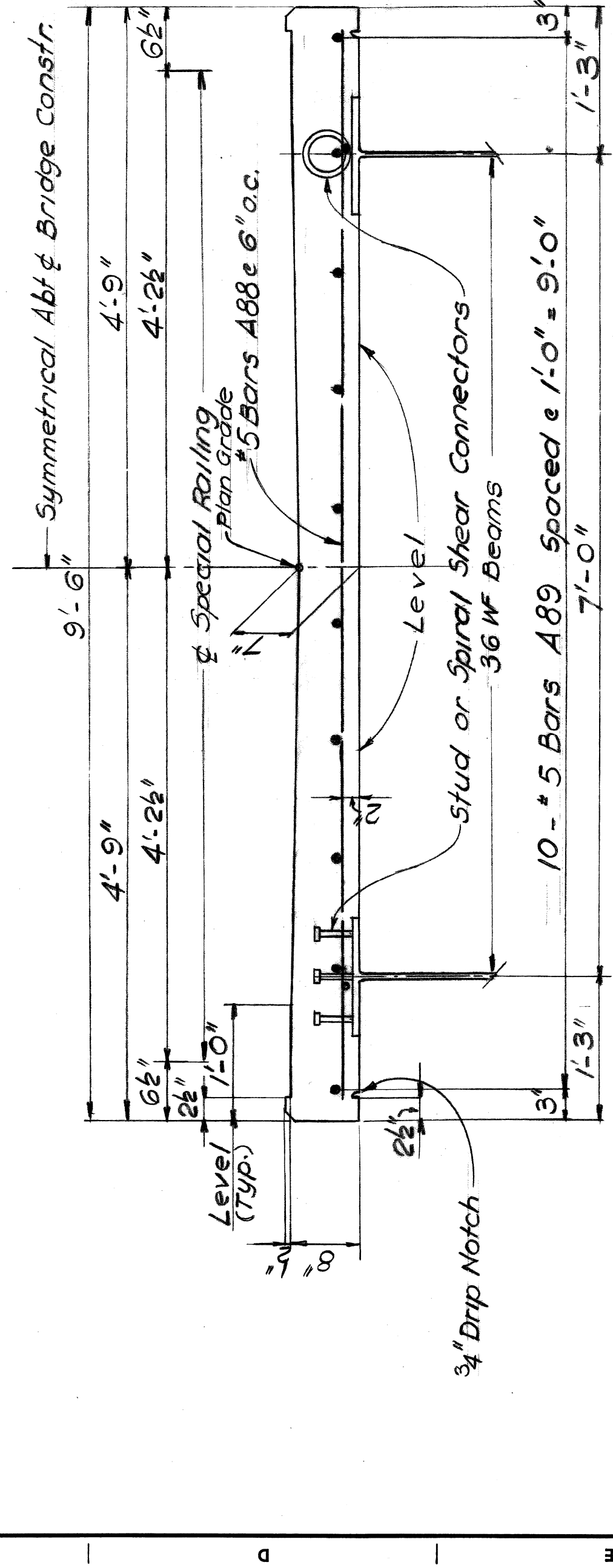
DESIGNED BY	W. A. McLaughlin	APPROVED	H. J. Conrad
DRAWN BY	W. B.	STRUCT. ENGR.	W. A. McLaughlin
TRACED BY		ADMINISTR. ENGR.	A. J. ...
CHECKED BY	Sturm	HEAD CIVIL ENGINEER	
REVISIONS LOCATED BY COORDINATES ON SHEET			
CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE FOR DEPARTMENT OF PUBLIC WORKS			
PEDESTRIAN BRIDGE BPI4 CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBORKE ST. & FRISBEE ST.			
PIERS & DECK - RAMP "B" (CONT.)			
SHEET	21 OF 28 SHEETS	CONTRACT	PW-5766
NO.		DRWG NO.	S-208
		DATE	MARCH, 1968



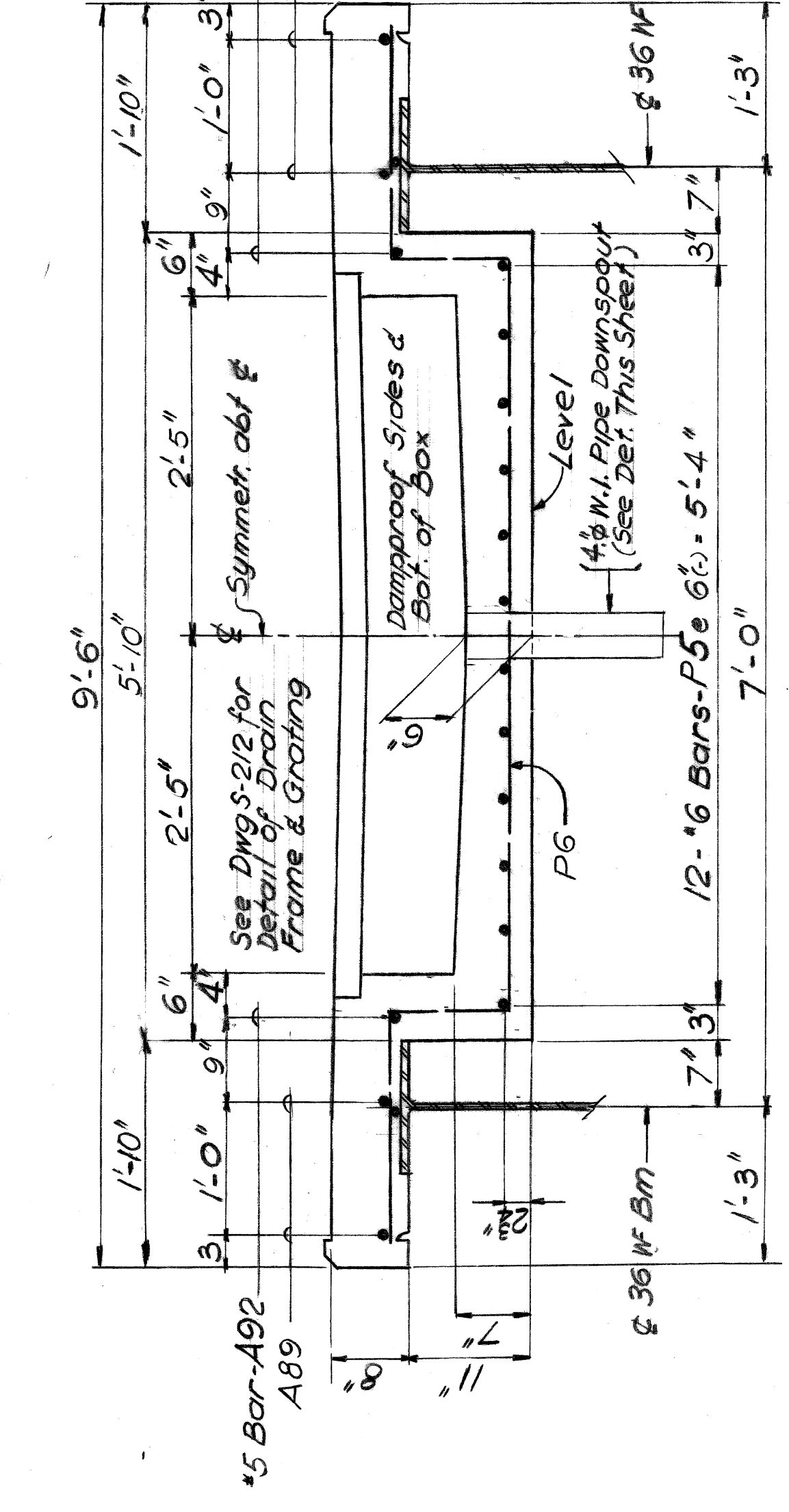
SHEET 22 OF 28 SHEETS		CONTRACT PW-5766		DRWG NO. S-209	
CITY OF DETROIT		PEDESTRIAN BRIDGE BP14		DATE MARCH, 1966	
DEPARTMENT OF PUBLIC WORKS		CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMROKE ST. & FRISBEE ST.			
CITY ENGINEERS OFFICE		FOR		BRIDGE PIERS '1, '2 & '3	
DEPARTMENT OF PUBLIC WORKS		APPROVED:			
		DESIGNED BY: <i>W. B. Stumm</i>			
		DRAWN BY: <i>W. B.</i>			
		TRACED BY: <i>Stumm</i>			
		CHECKED BY: <i>Stumm</i>			
ADMINISTR. ENGR		HEAD CIVIL ENGINEER			
REVISIONS LOCATED BY COORDINATES ON SHEET		DESCRIPTION		DATE	
1					
2					
3					
4					
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8					
9					
10					
11					



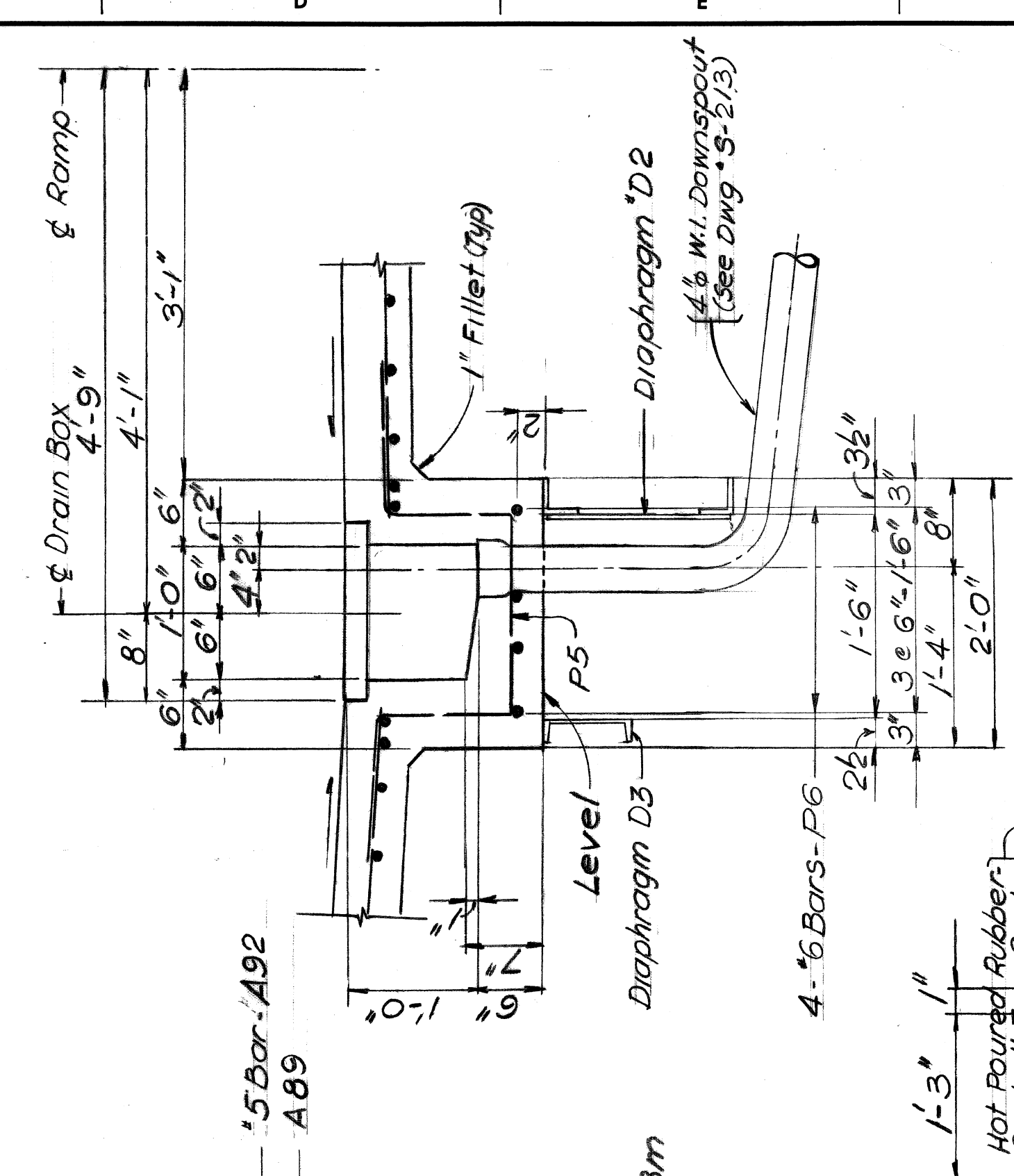
PLAN OF DECK SLAB STL. REINF.



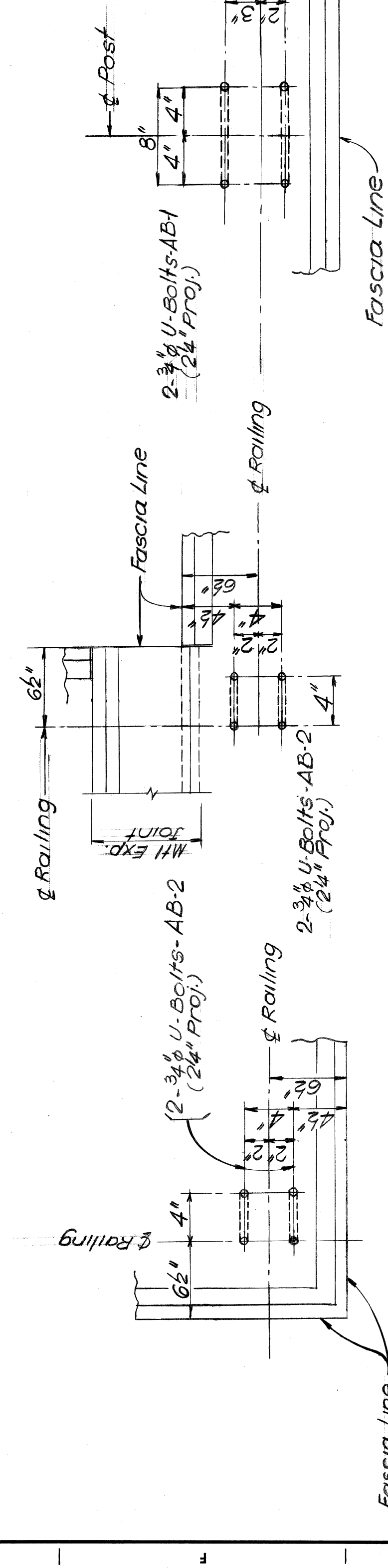
SECTION A-A



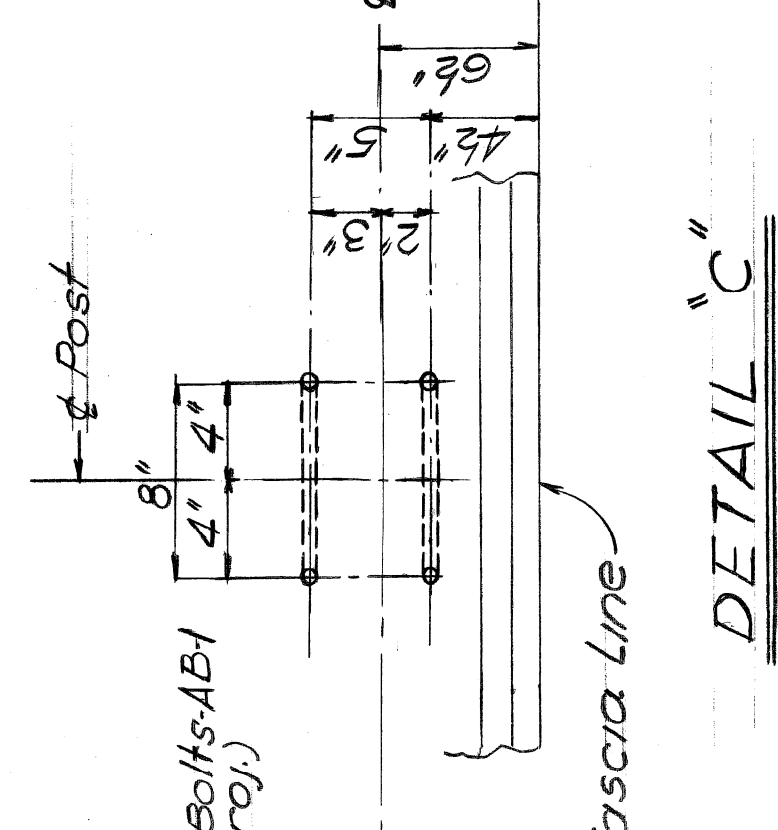
SECTION B-B



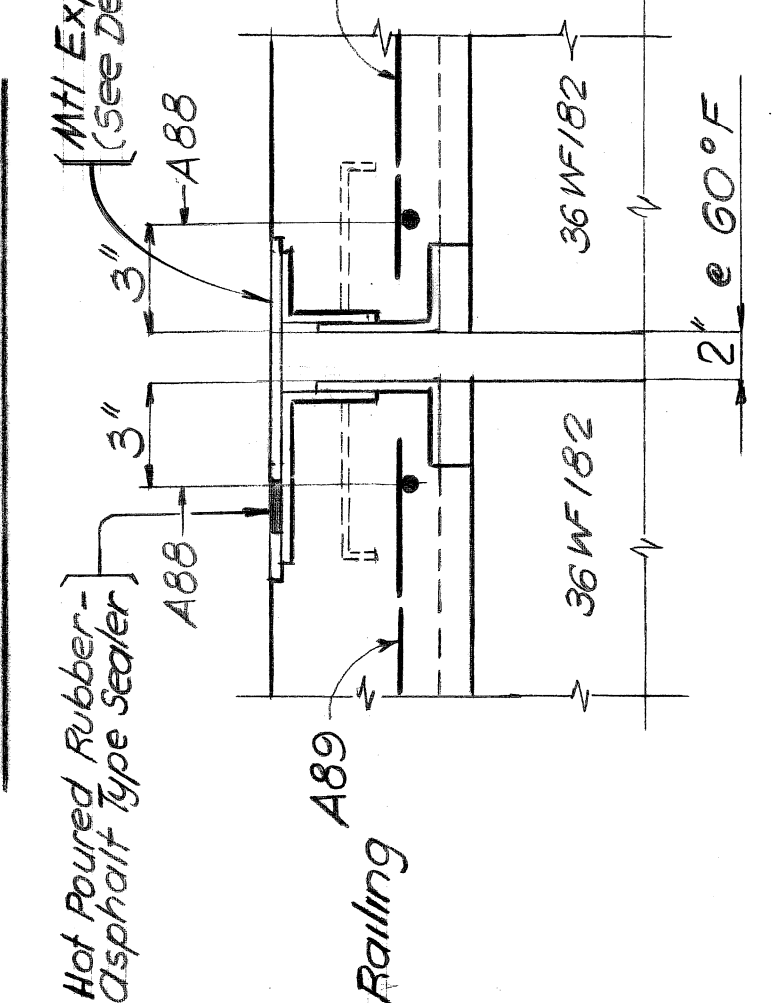
SECTION C-C



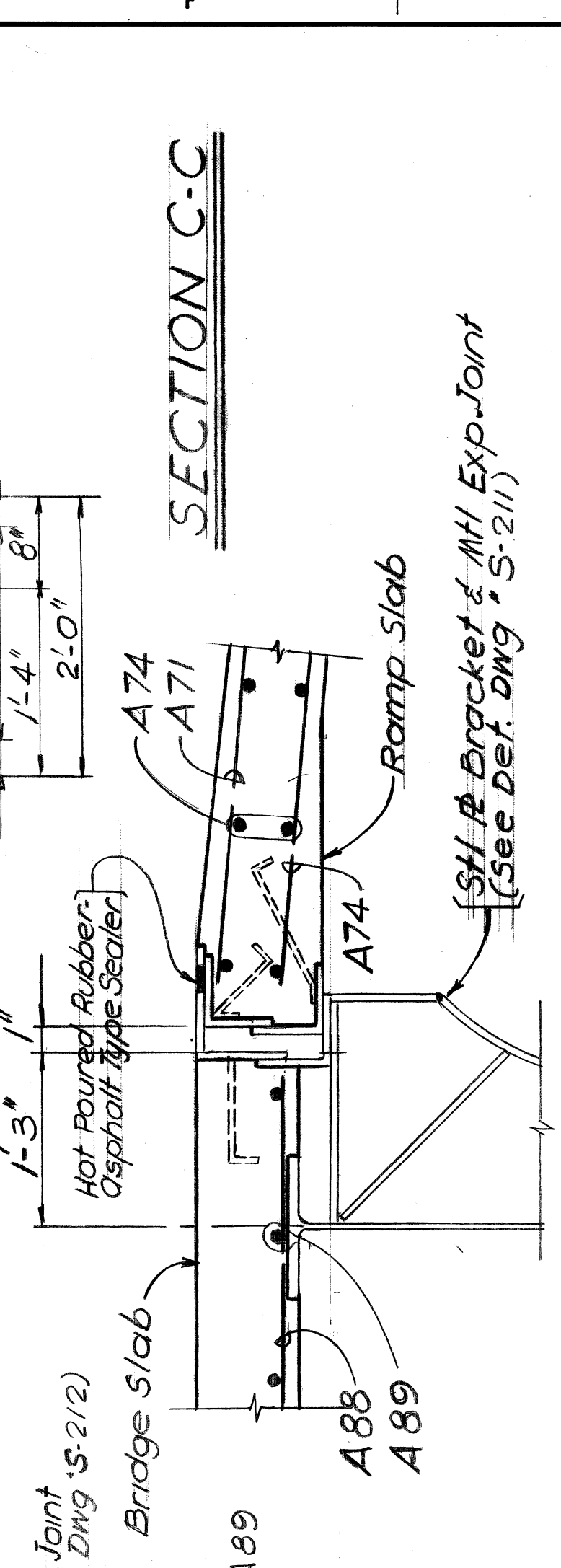
DETAIL A



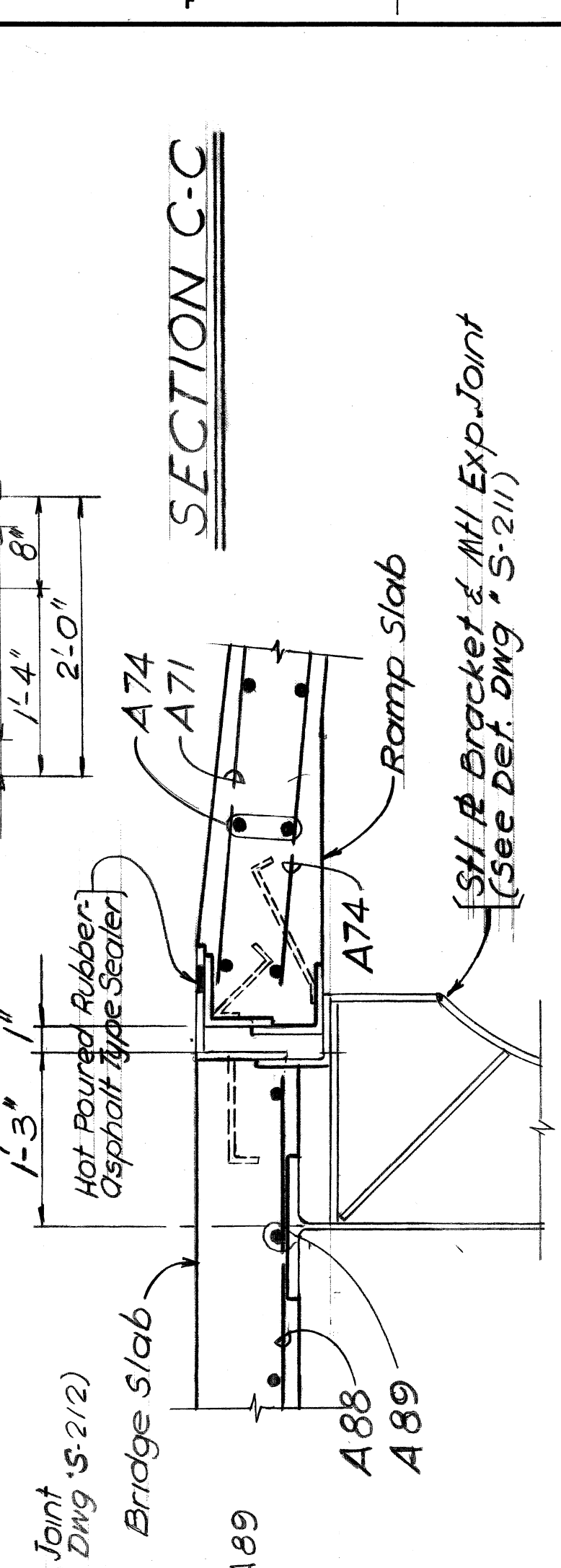
DETAIL B



DETAIL C

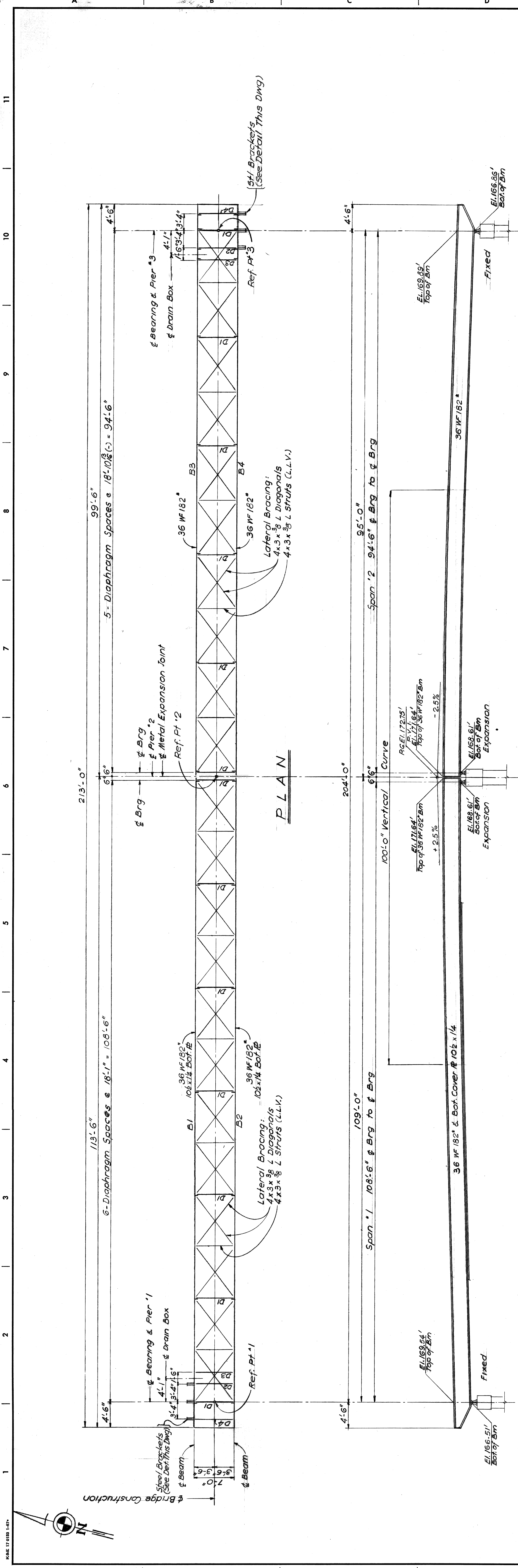


SECTION D-D



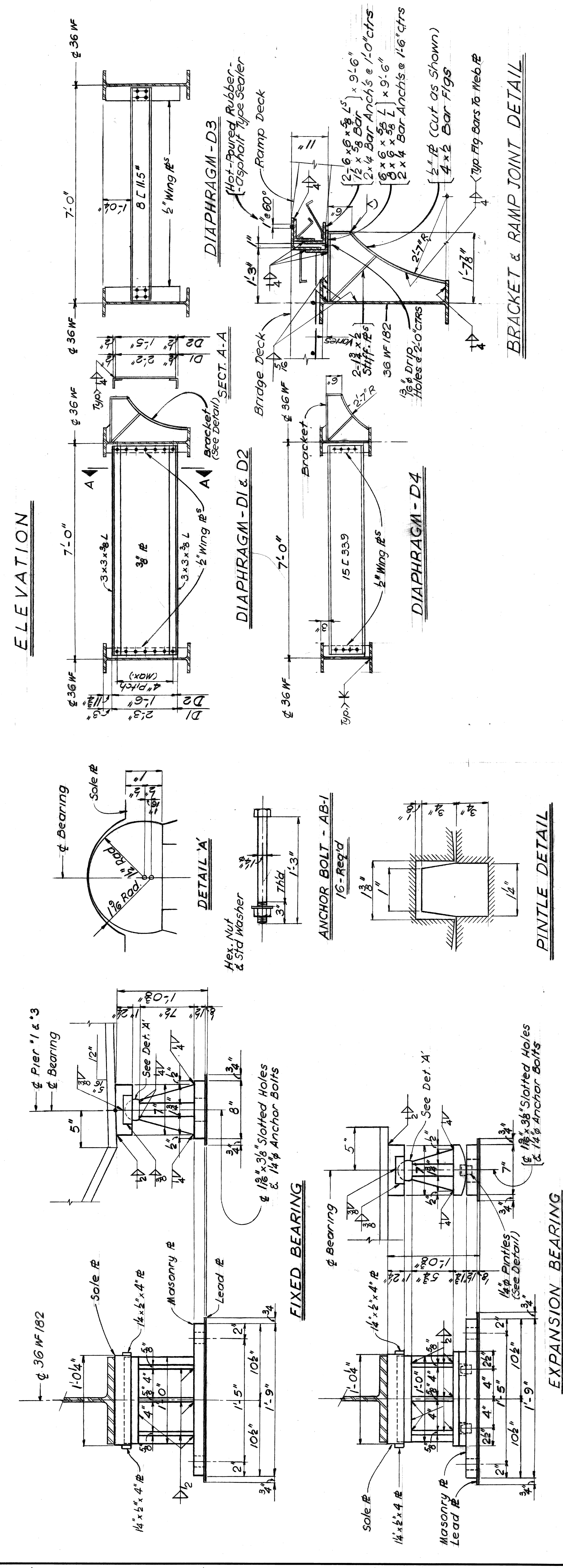
SECTION E-E

SHEET 23 OF 28 SHEETS		CONTRACT PW-5766		DRWG NO. S-210		DATE MARCH, 1928	
CITY OF DETROIT DEPARTMENT OF PUBLIC WORKS CITY ENGINEERS OFFICE				PEDESTRIAN BRIDGE BPIA CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBAKOE ST. & FRISBEE ST.			
FOR DEPARTMENT OF PUBLIC WORKS				SUPERSTRUCTURE DETAILS			
DESIGNED BY	W. A. M. M.	APPROVED	H. G. S.	DESIGNED BY	W. A. M. M.	APPROVED	H. G. S.
DRAWN BY	W. B.	DRAWN BY	W. B.	DRAWN BY	W. B.	DRAWN BY	W. B.
TRACED BY		TRACED BY		TRACED BY		TRACED BY	
CHECKED BY	STUEM	CHECKED BY	STUEM	CHECKED BY	STUEM	CHECKED BY	STUEM
DESCRIPTION		DESCRIPTION		DESCRIPTION		DESCRIPTION	
REVISIONS LOCATED BY COORDINATES ON SHEET		REVISIONS LOCATED BY COORDINATES ON SHEET		REVISIONS LOCATED BY COORDINATES ON SHEET		REVISIONS LOCATED BY COORDINATES ON SHEET	



STRUCTURAL STEEL NOTES:

- Design: Michigan Department of State Highways Specifications for Current Highway Bridges - 1955 Edition & Current Highway Bridges - Specifications for Highway Bridges
- Live Load - 85 psf for Beams
- Alternate Live Load - One 5-ton (Truck + Impact)
- Fabrication: Michigan Department of State Highways Standard Specifications for Road and Bridge Construction - 1957 Edition.
- Shop Connections shall Be Welded As Shown On Plans
- Field Connections shall Be Bolted With 3/4" High Strength Bolts Except As Noted.
- Beams To Have Camber Shown On Camber Diagram This Camber Is To Be Measured With Beam Lying On Its Side Allowable Camber Tolerance ± 4 Inching Is To Be Used If Necessary To Assure Camber Permanency Within The Above Tolerance Dead Load Deflection of Steel Above Is Calculated To Be:
 - At Midpoint of Span $\cdot 1 \cdot 1/2$
 - At Midpoint of Span $\cdot 2 \cdot 1$
- Shear Developers May Be Spirals or Studs At the Contractors Option



SHEET 24 OF 28 SHEETS		CONTRACT PW-5706	
DESIGNED BY: <i>W. J. Stumm</i>		DRAWN BY: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		TRACED BY: <i>STURM</i>	
CHECKED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
DESIGNER'S SIGNATURE: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		CHECKED BY: <i>STURM</i>	
TRACED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
CHECKED BY: <i>STURM</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
APPROVED: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
DESIGNED BY: <i>W. J. Stumm</i>		DRAWN BY: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		TRACED BY: <i>STURM</i>	
CHECKED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
DESIGNER'S SIGNATURE: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		CHECKED BY: <i>STURM</i>	
TRACED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
CHECKED BY: <i>STURM</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
APPROVED: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
DESIGNED BY: <i>W. J. Stumm</i>		DRAWN BY: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		TRACED BY: <i>STURM</i>	
CHECKED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
DESIGNER'S SIGNATURE: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
DRAWN BY: <i>W. J. Stumm</i>		CHECKED BY: <i>STURM</i>	
TRACED BY: <i>STURM</i>		APPROVED: <i>W. J. Stumm</i>	
CHECKED BY: <i>STURM</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	
APPROVED: <i>W. J. Stumm</i>		ADMINISTRATOR'S SIGNATURE: <i>W. J. Stumm</i>	

PEDESTRIAN BRIDGE BPI4
 CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMROKE ST & FRISBEE ST

STRUCTURAL STEEL DETAILS

CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE
 FOR
 DEPARTMENT OF PUBLIC WORKS

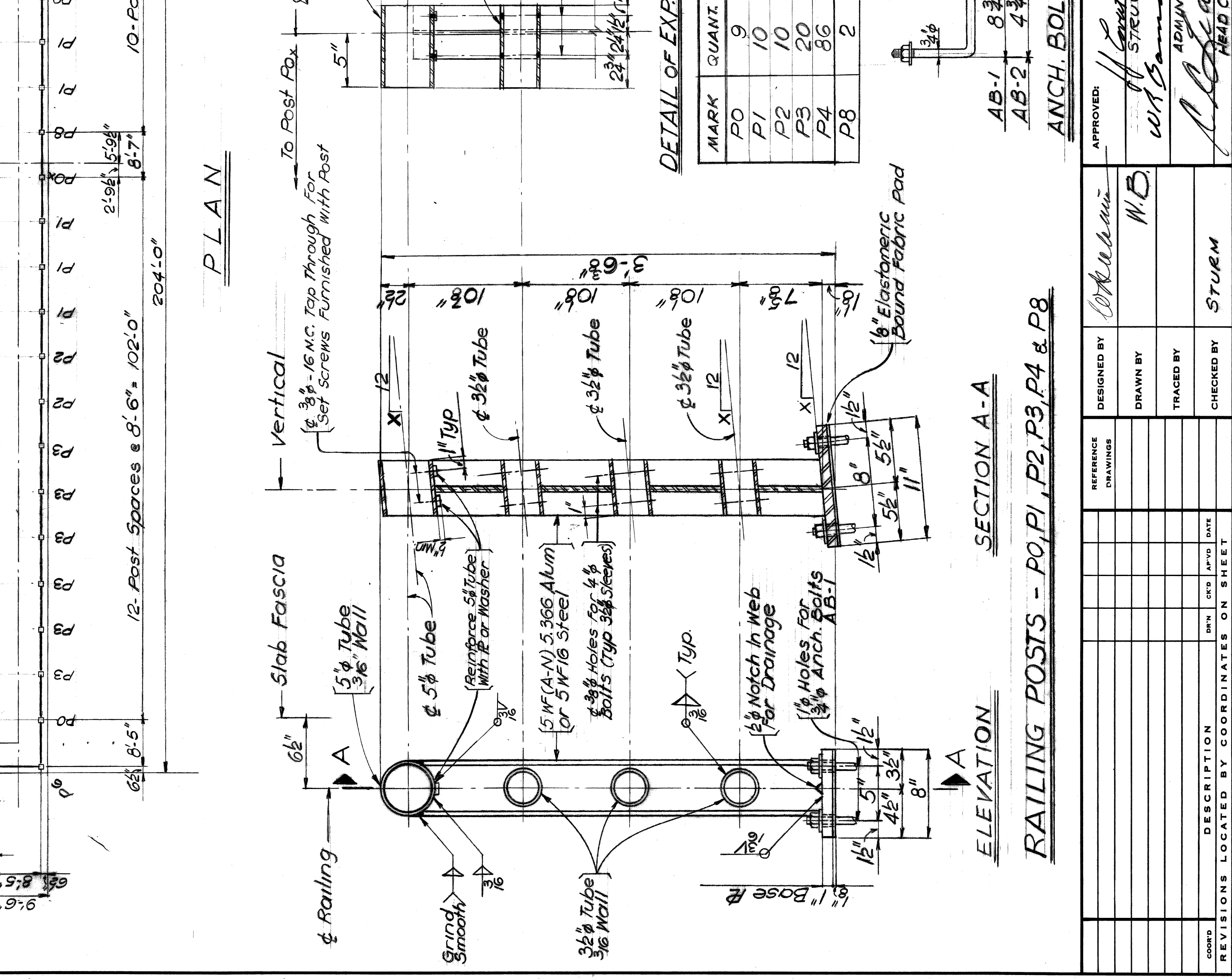
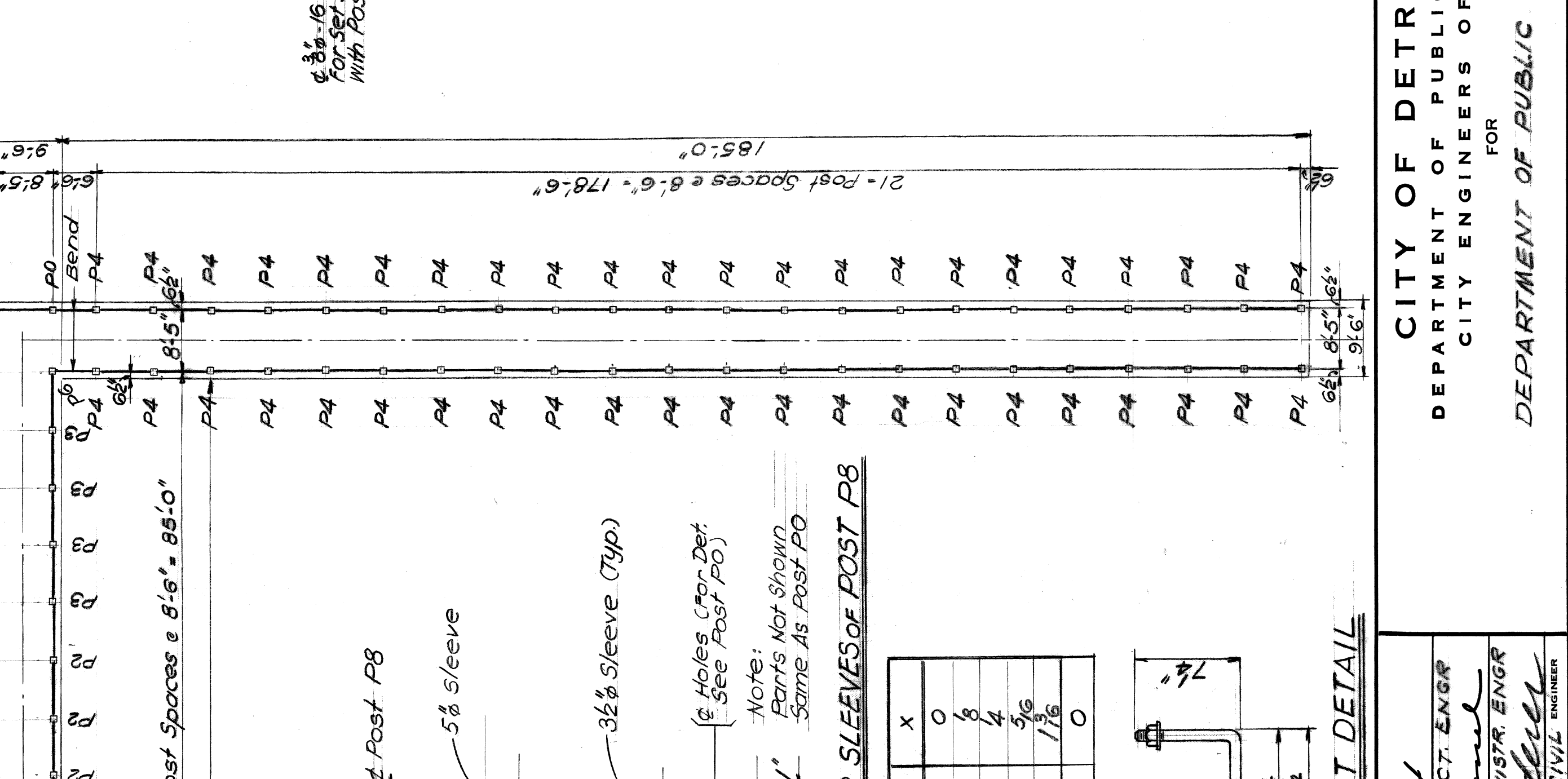
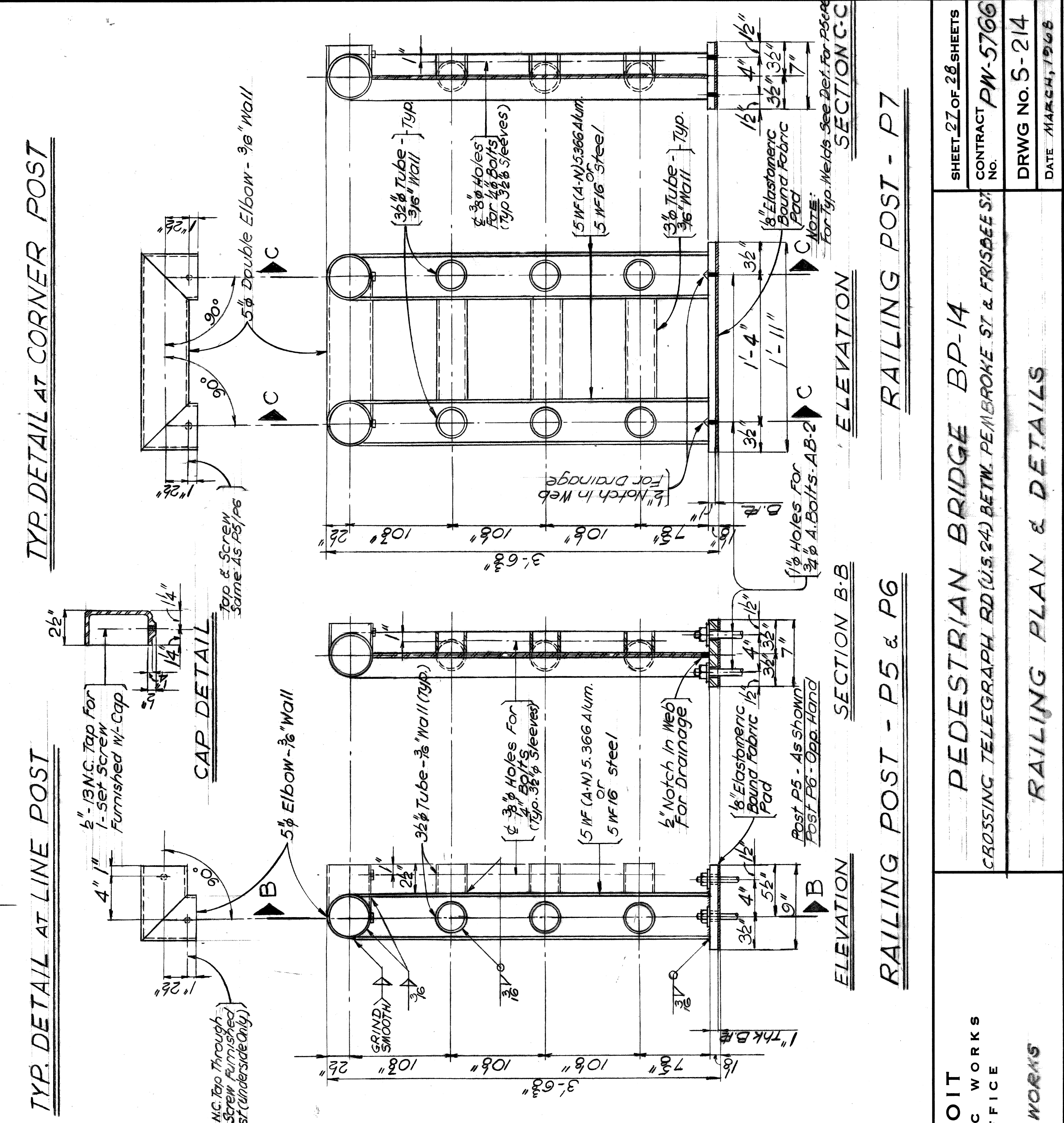
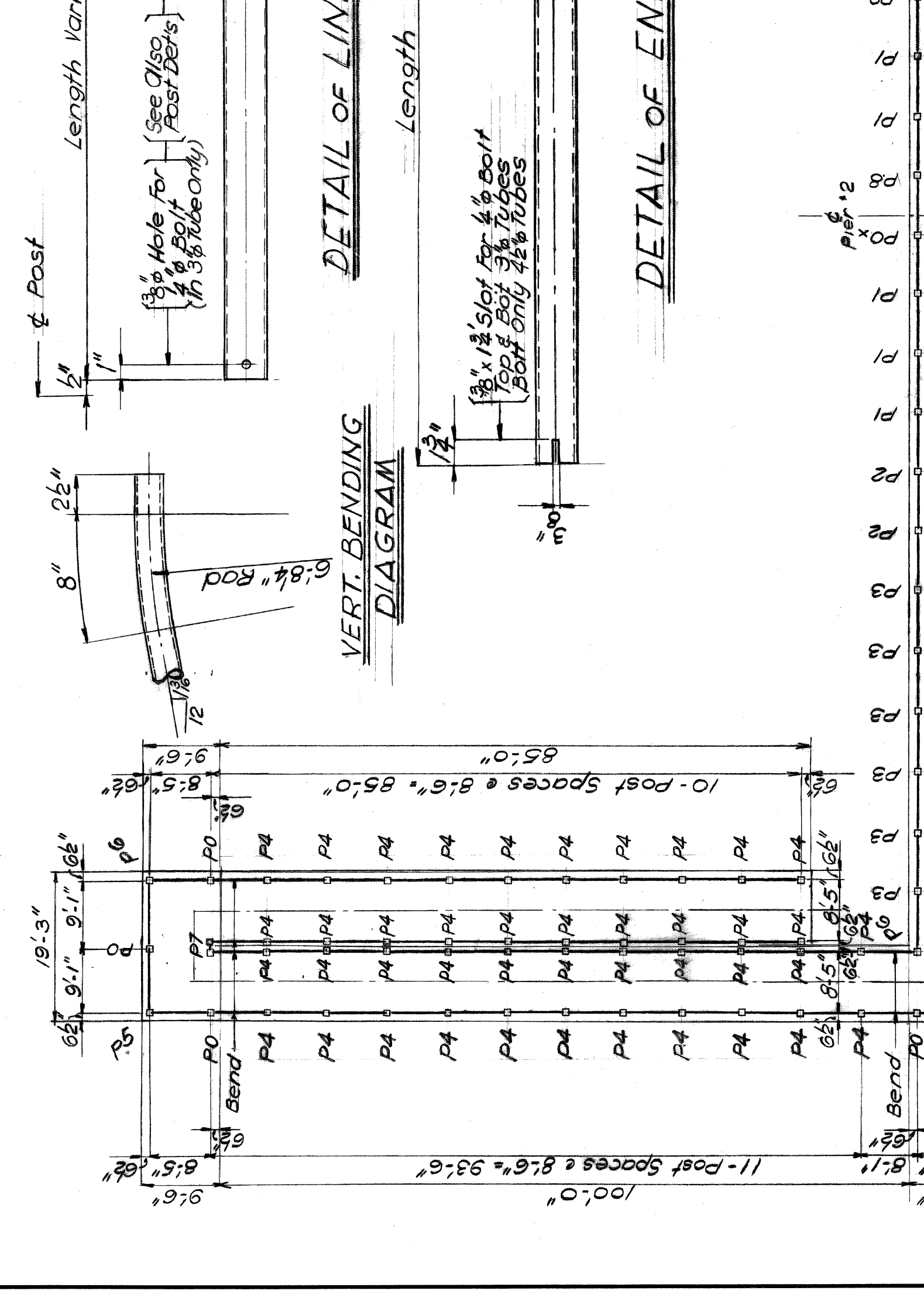
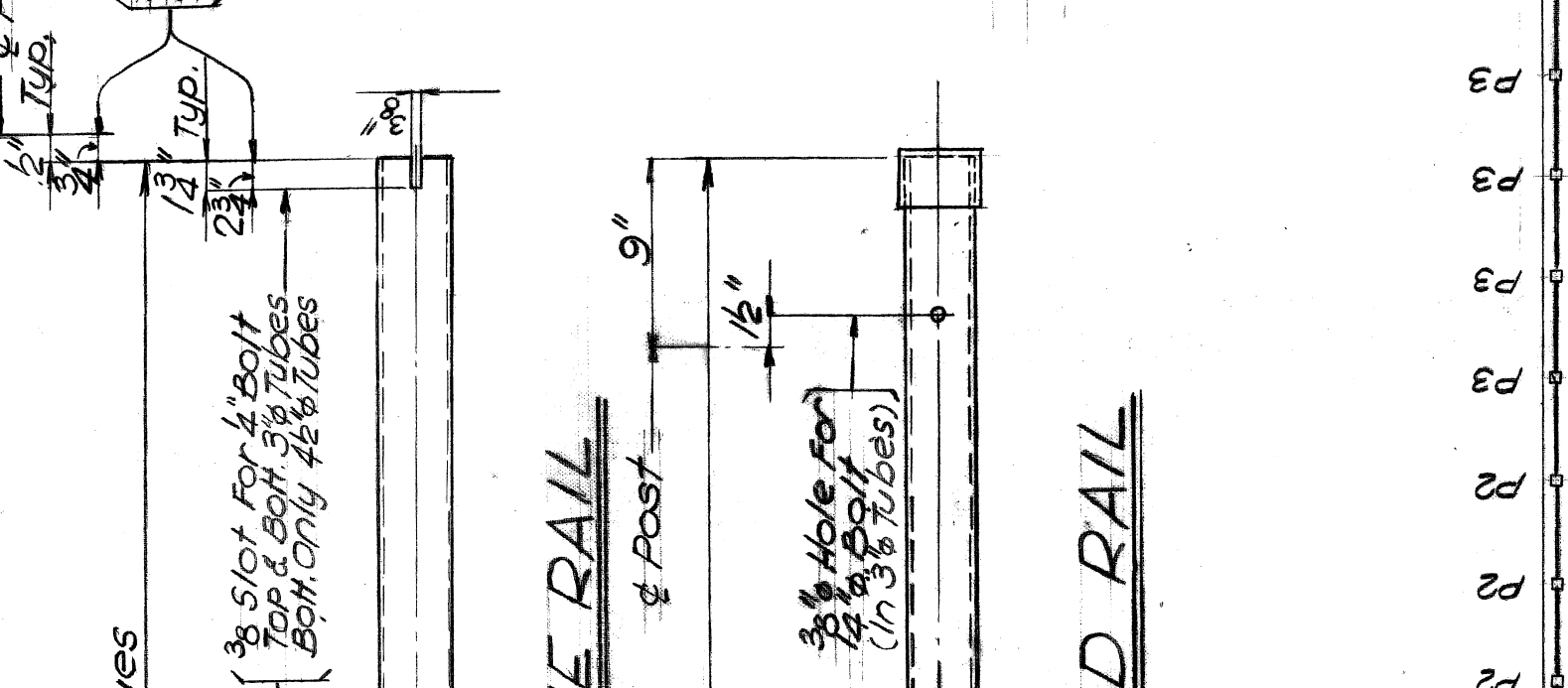
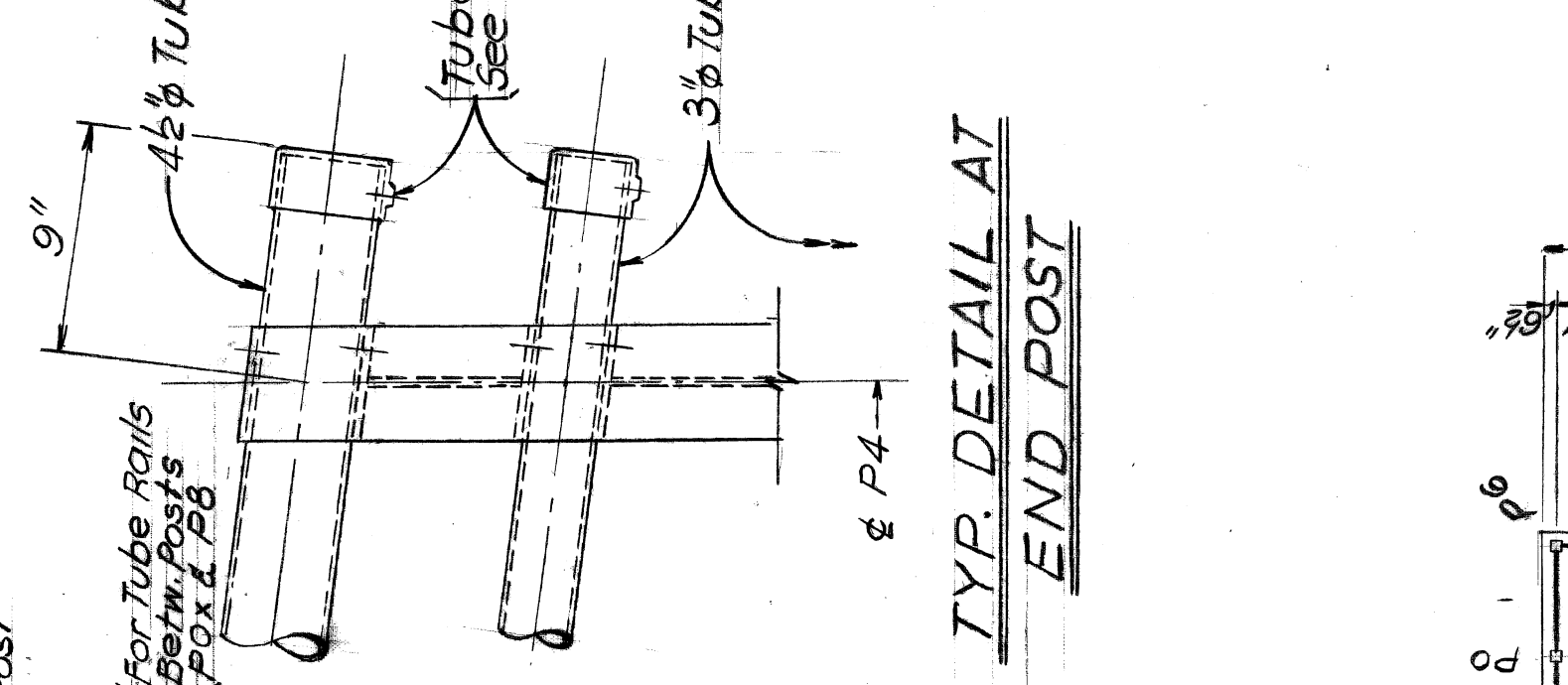
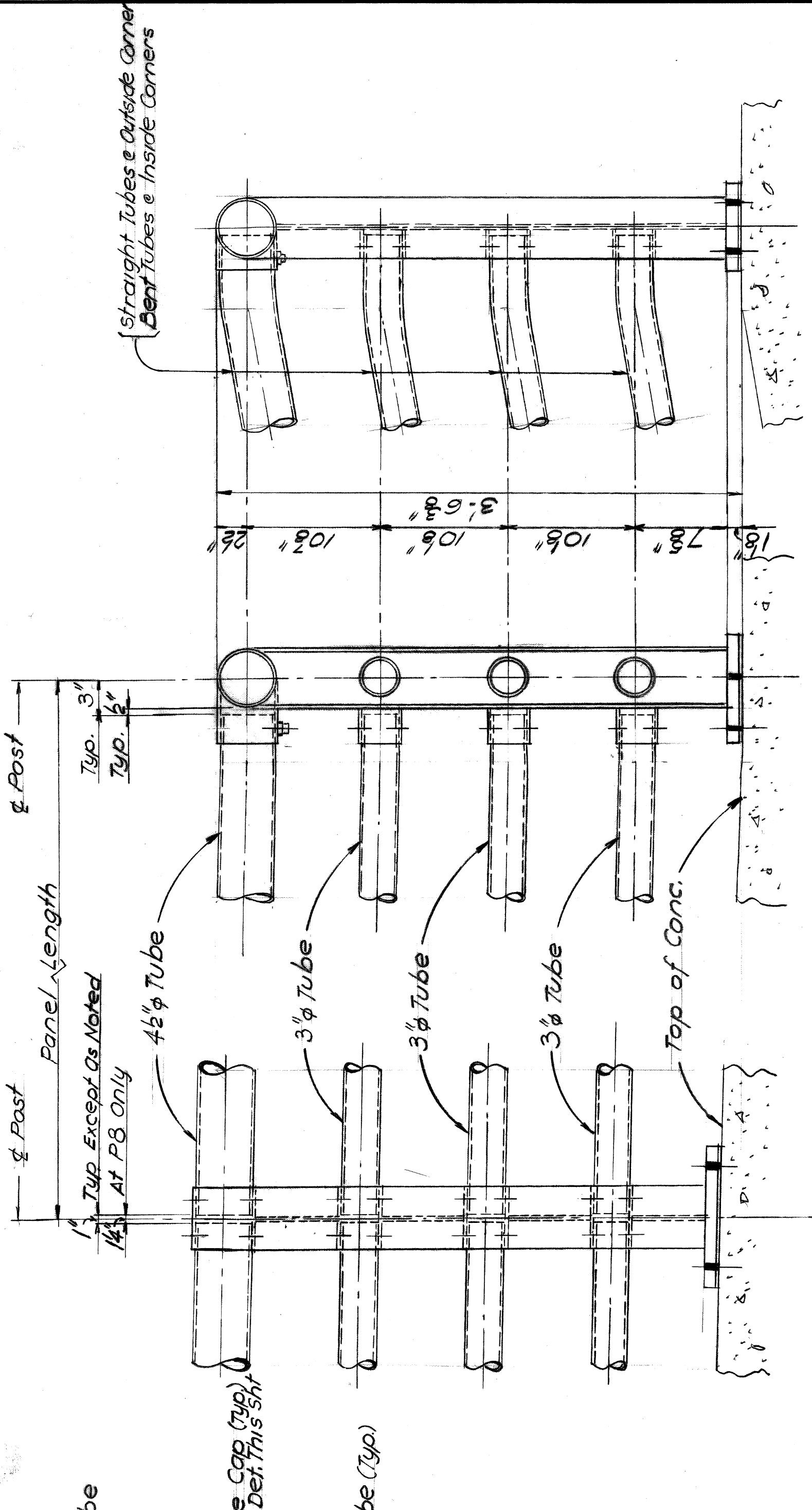
REVISIONS LOCATED BY COORDINATES ON SHEET

DATE: MARCH, 1966

DRWG NO. S-211

CONTRACT PW-5706

SHEET 24 OF 28 SHEETS



RAILING POSTS - P0, P1, P2, P3, P4 & P8

RAILING POST - P5 & P6

RAILING POST - P7

RAILING PLAN & DETAILS

CITY OF DETROIT
DEPARTMENT OF PUBLIC WORKS
CITY ENGINEERS OFFICE
FOR
DEPARTMENT OF PUBLIC WORKS

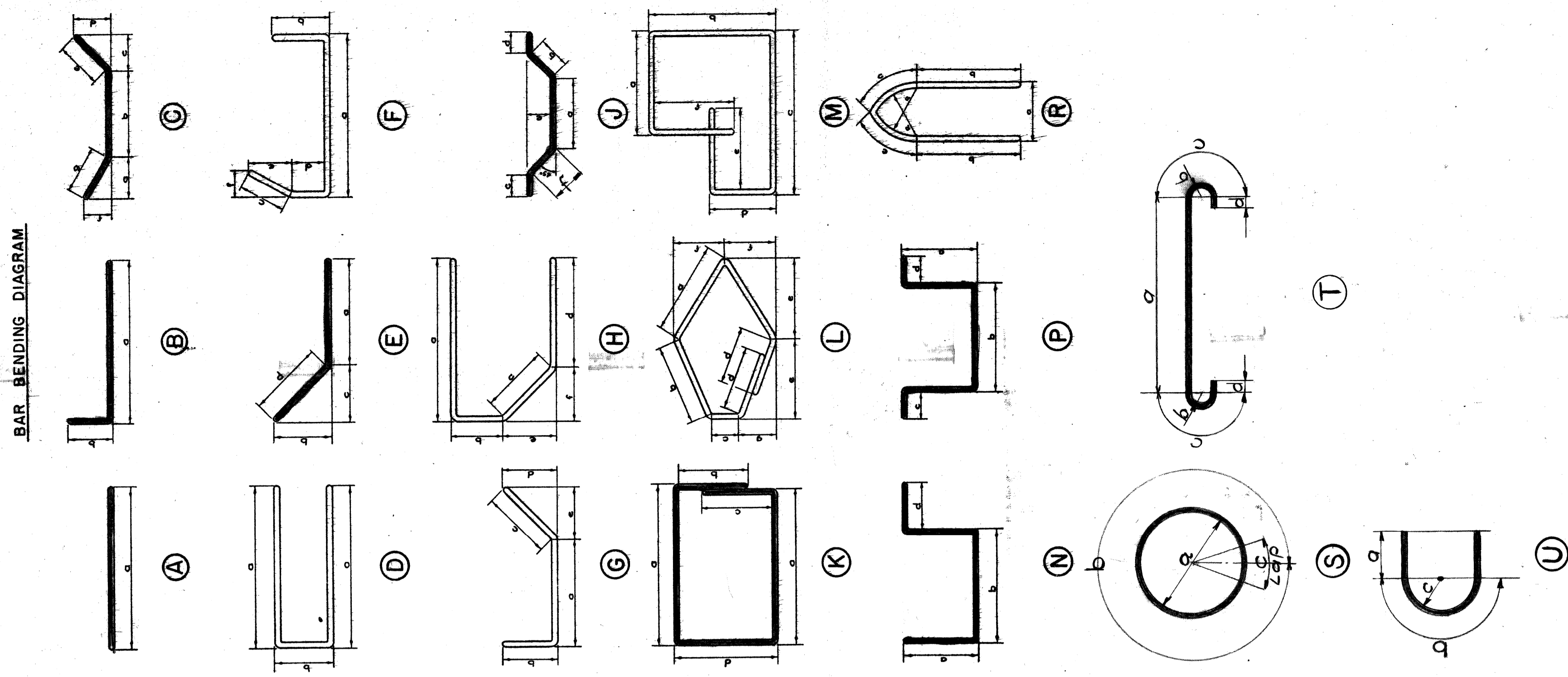
PEDESTRIAN BRIDGE BP-14
CROSSING TELEGRAPH RD (U.S. 24) BETW. PEMBROKE ST & FRISBEE ST

CONTRACT PW-5766
DRWG NO. S-214
DATE: MARCH, 1968

REVISIONS LOCATED BY COORDINATES ON SHEET

DESIGNED BY: *W. B. ...*
DRAWN BY: *M. B. ...*
TRACED BY: *STURM*
CHECKED BY: *STURM*

APPROVED: *W. B. ...*
ADMINISTR. ENGR
HEAD CIVIL ENGINEER



BAR	DIMENSIONS									SIZE	LENGTH	NO. REQ'D	TOTAL WT.
	a	b	c	d	e	f	g						
P1	1'-0"	6'-0"	1'-6"	1'-6"						#4	11'-0"	6	44
P2	1'-11"	5'-6"	1'-2"	1'-2"						#5	10'-0"	4	42
P3	9'-1"	1'-5"	8"	8"						#4	4'-3"	36	103
P4	1'-0"	1'-5"	8"	8"						#4	4'-9"	36	114
P5	1'-0"	1'-6"	1'-3"	1'-3"						#6	6'-0"	24	216
P6	1'-0"	5'-6"	1'-9"	1'-9"						#6	11'-0"	8	332
S1	1'-14"	3'-6"	8"							#2	4'-2"	139	97
T1	8'-7"	28"	78"	38"						#7	10'-6"	10	215
T2	8'-7"	28"	74"	35"						#5	10'-6"	45	404
T3	8'-4"	4"	1'-0 1/2"	6 1/2"						#8	11'-6"	17	520
U1	1'-11"	3'-10"	1'-2 3/4"							#4	6'-0"	16	64
U2	1'-0"	2'-7"	9 3/4"								4'-7"	36	110
U3	2'-9 1/2"	4'-7"	1'-5"								10'-2"	8	54
U4	1'-0"	3'-4"	1'-0 1/2"							#4	5'-4"	20	71

BAR	DIMENSIONS									SIZE	LENGTH	NO. REQ'D	TOTAL WT.
	a	b	c	d	e	f	g						
A77	9'-0"									#7	9'-0"	7	129
A78	2'-6"									#1	21'-6"	6	264
A79	23'-0"									#7	23'-0"	12	565
A80	9'-6"									#6	9'-6"	27	366
A81	7'-6"										7'-6"	16	180
A82	15'-6"										15'-6"	12	280
A83	5'-6"										5'-6"	20	165
A84	6'-0"									#6	6'-0"	14	126
A85	15'-6"									#8	15'-6"	24	693
A86	16'-3"									#8	16'-3"	16	693
A87	9'-0"									#6	9'-0"	16	216
A88	9'-0"									#5	9'-0"	42	390
A89	15'-0"										15'-0"	38	2160
A90	6'-0"										6'-0"	12	75
A91	7'-9"										7'-9"	12	97
A92	5'-0"									#5	5'-0"	4	21
B1	2'-6"	2'-6"								#6	5'-0"	196	1472
B2	4'-3"	6"								#8	4'-9"	88	1120
B3	10'-4"	1'-2"								#8	11'-6"	24	735
B4	12'-1"	1'-2"								#8	13'-3"	16	566
B5	14'-1"	1'-2"								#8	15'-3"	16	650
B6	16'-1"	1'-2"								#8	17'-3"	16	736
B7	18'-1"	1'-2"								#8	19'-3"	16	823
C1	3'-2 1/2"	2'-0"	3'-2 1/2"	7 3/4"	3'-3"	7 3/4"	3'-3"			#5	8'-6"	18	160
C2	3'-6"	1'-6"	3'-6"	9 3/4"	3'-7"	9 3/4"	3'-7"			#5	18'-8"	2	39
E1	2'-11"	2 1/4"	2'-4 1/2"	2'-5"	2'-6"					#7	23'-6"	24	1153
E2	5'-0"	2 1/4"	2'-5 1/2"	2'-6"						#7	7'-6"	15	230
E3	5'-9 1/2"	1 3/4"	10 1/2"	10 1/2"						#7	6'-9"	4	54
E4	26'-0"	2 1/4"	2'-5 1/2"	2'-6"						#7	28'-6"	6	350
E5	21'-3"	2 1/4"	2'-5 1/2"	2'-6"						#7	23'-9"	6	289
J1	1'-0"	1'-9"	1'-3"	1'-3"	1'-2 3/4"	1'-3"				#7	6'-6"	16	216
K1	9 1/2"	1'-0"	1'-0"	1'-5 1/2"						#3	5'-1"	4	8
K2	11"	1'-0"	1'-0"	1'-5 1/2"							5'-4"	2	4
K3	1'-0 1/2"	1'-0"	1'-0"	1'-5 1/2"							5'-7"	2	4
K4	1'-2"	1'-0"	1'-0"	1'-5 1/2"							5'-0"	2	4
K5	1'-3 1/2"	1'-0"	1'-0"	1'-5 1/2"							6'-1 1/2"	2	5
K6	1'-5"	1'-0"	1'-0"	1'-5 1/2"							6'-4"	11	26
K7	1'-0 1/4"	1'-0"	1'-0"	1'-5 1/2"							5'-6"	2	4
K8	1'-3"	1'-0"	1'-0"	1'-5 1/2"						#3	6'-0"	2	5
K9	2'-1 1/2"	7 3/4"	7 3/4"	1'-0 1/2"						#4	6'-6"	40	174
K10	2'-7 1/2"	11 1/2"	11 1/2"	1'-8"	1'-8"					#4	8'-9"	20	117
N1	1'-0"	1'-6"								#4	4'-0"	14	37

BAR	DIMENSIONS									SIZE	LENGTH	NO. REQ'D	TOTAL WT.
	a	b	c	d	e	f	g						
A1	22'-6"									#6	22'-6"	24	810
A2	1'-0"									#4	11'-0"	108	132
A3	3'-3"									#6	3'-3"	8	39
A4	3'-5"										3'-7"	8	43
A5	3'-7"										3'-9"	8	45
A6	3'-9"										3'-10"	8	46
A7	3'-10"										4'-0"	8	48
A8	4'-0"										4'-2"	8	50
A9	4'-2"										4'-4"	8	52
A10	4'-4"										4'-6"	8	54
A11	4'-6"										4'-7"	8	55
A12	4'-7"										4'-9"	8	57
A13	4'-9"										4'-11"	8	59
A14	4'-11"										5'-1"	8	61
A15	5'-1"										5'-3"	8	63
A16	5'-3"										5'-5"	8	65
A17	5'-5"										5'-6"	8	66
A18	5'-6"										5'-8"	8	68
A19	5'-8"										5'-10"	8	70
A20	5'-10"										6'-0"	8	72
A21	6'-0"										6'-2"	8	74
A22	6'-2"										6'-3"	8	75
A23	6'-3"										6'-5"	8	77
A24	6'-5"										6'-7"	8	79
A25	6'-7"										6'-9"	8	81
A26	6'-9"										6'-11"	8	83
A27	6'-11"										7'-0"	8	84
A28	7'-0"										7'-2"	8	86
A29	7'-2"										7'-4"	8	88
A30	7'-4"										7'-6"	18	203
A31	7'-6"										7'-8"	8	92
A32	7'-8"										7'-9"	8	93
A33	7'-9"										7'-11"	8	95
A34	7'-11"										8'-1"	8	97
A35	8'-1"										8'-3"	8	99
A36	8'-3"										8'-5"	8	101
A37	8'-5"										8'-6"	8	102
A38	8'-6"										8'-8"	8	104
A39	8'-8"										8'-10"	8	106
A40	8'-10"										9'-0"	8	108
A41	9'-0"										9'-2"	8	110
A42	9'-2"									#6	9'-3"	32	445
A43	9'-3"												
A44													
A45													
A46													
A47	23'-0"									#4	23'-0"	72	1110
A48	20'-6"									#4	20'-6"	48	658
A49	8'-6"									#6	8'-6"	6	77
A50	6'-4"									#6	6'-4"	6	58
A51	9'-2"									#4	9'-2"	168	1030
A52	22'-6"										22'-6"	20	300
A53	22'-8"										22'-8"	20	304
A54	20'-3"									#4	20'-3"	20	270
A55													
A56													
A57													
A58	3'-6"									#4	3'-6"	2	5
A59	14'-6"									#7	14'-6"	4	119
A60	11'-9"										11'-9"	3	72
A61	3'-6"										3'-6"	8	57
A62	4'-9"									#7	4'-9"	6	58
A63	5'-6"									#6	5'-6"	90	744
A64	9'-0"									#5	9'-0"	11	104
A65	8'-9"									#5	8'-9"	12	110
A66	10'-0"									#7	10'-0"	121	2475
A67	25'-0"										25'-0"	6	307
A68	25'-0"										25'-0"	150	6745
A69	16'-6"										16'-6"	24	801
A70	22'-0"										22'-0"	103	2526
A71	20'-0"										20'-0"	12	491
A72	20'-0"										20'-0"	30	1239
A73	20'-6"									#5	20'-6"	458	4419
A74	20'-6"									#7	20'-6"	12	651

Note:-
 All right angle bends in Reinforcing Steel to be made about a pin of the minimum diameter allowed by the Standard Specifications.
 All bar numbers this sheet to be prefixed BP14

Grand Total Steel Reinforcement

CITY OF DETROIT
 DEPARTMENT OF PUBLIC WORKS
 CITY ENGINEERS OFFICE

FOR
 DEPARTMENT OF PUBLIC WORKS

PEDESTRIAN BRIDGE BP.14
 CROSSING TELEGRAPH RD (U.S.24) BETW. PEMROKE ST. & FRISBEE ST.

STEEL REINFORCEMENT DETAILS

DESIGNED BY: *W.P. ...*
 DRAWN BY: *U.B.*
 TRACED BY: *...*
 CHECKED BY: *STUM*

APPROVED: *H. ...*
W.P. ...
ADMINISTR. ENGR.
HEAD CIVIL ENGINEER

REVISIONS LOCATED BY COORDINATES ON SHEET

SHEET 28 OF 28 SHEETS
 CONTRACT PW-5766
 DRWG NO. S-215
 DATE MARCH, 1968

11
 10
 9
 8
 7
 6
 5
 4
 3
 2