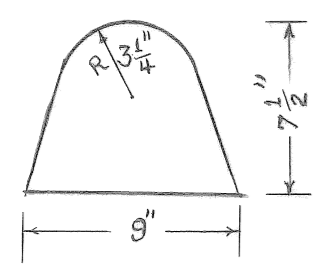


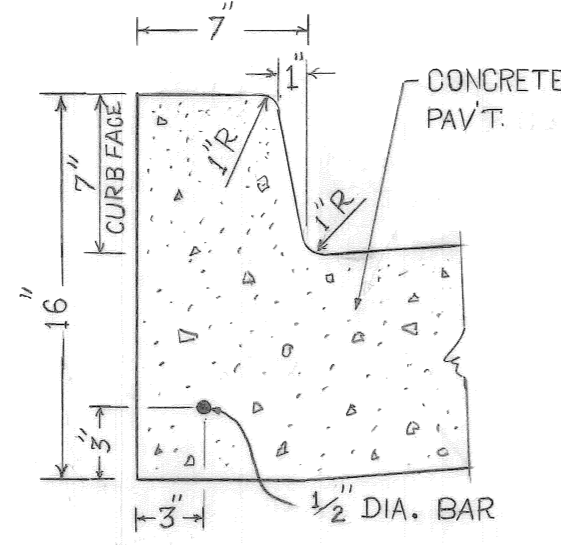
DETOUR ROADWAY CURVE DATA

CURVE # 1a		CURVE # 2a		CURVE # 3a	
Δ = 21°-59'-26"	D = 30°-48'-52"	Δ = 21°-59'-26"	D = 30°-48'-52"	Δ = 11°-20'-40"	D = 121°-54'-53"
R = 186.00'	T = 36.14'	R = 186.00'	T = 36.14'	R = 47.00'	T = 4.67'
L = 71.39'	E = 3.48'	L = 71.39'	E = 3.48'	L = 9.31'	E = 0.23'
PC = 16+60.86'	PT = 17+32.25'	PC = 18+52.25'	PT = 19+23.64'	PC = 19+97.54'	PT = 20+06.85'

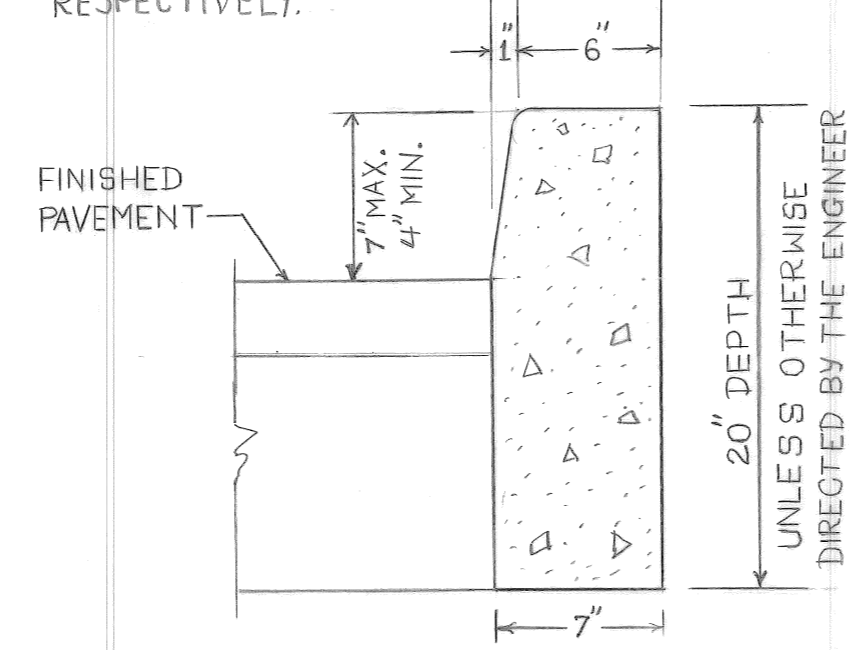
CURVE # 4a		CURVE # 5a	
Δ = 80°-38'-44"	D = 63°-46'-00"	Δ = 88°-00'-36"	D = 24°-06'-44"
R = 90.00'	T = 76.39'	R = 23.00'	T = 22.21'
L = 126.68'	E = 28.05'	L = 35.33'	E = 8.97'
PC = 20+73.93'	PT = 22+00.61'	PC = 24+67.79'	PT = 25+03.12'



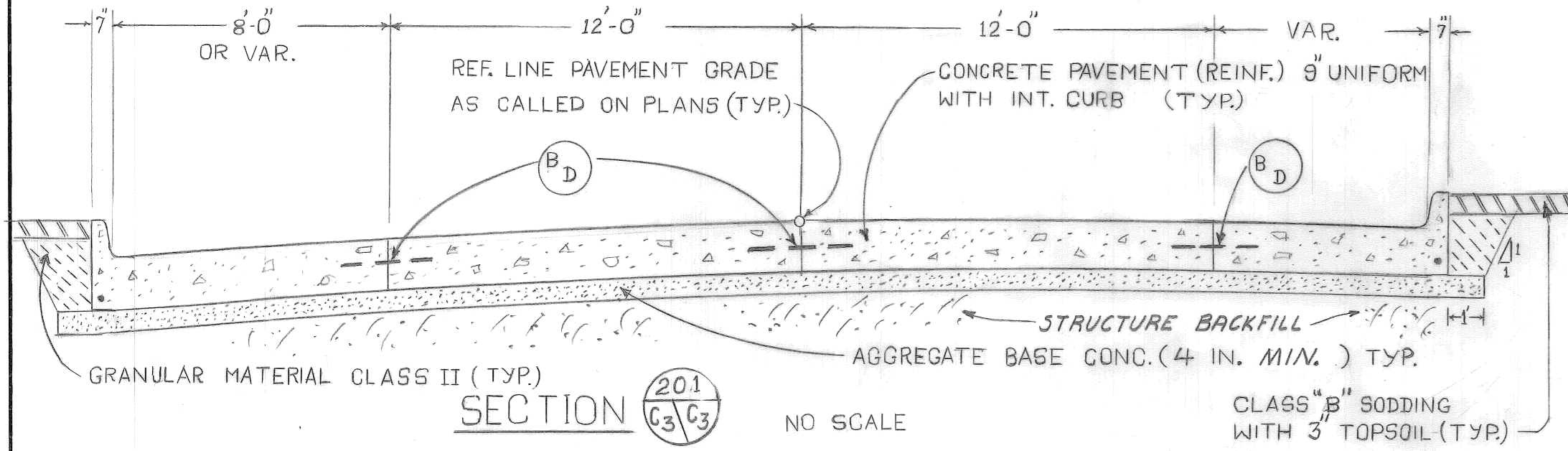
BITUMINOUS CONC. CURB DETAIL
NO SCALE



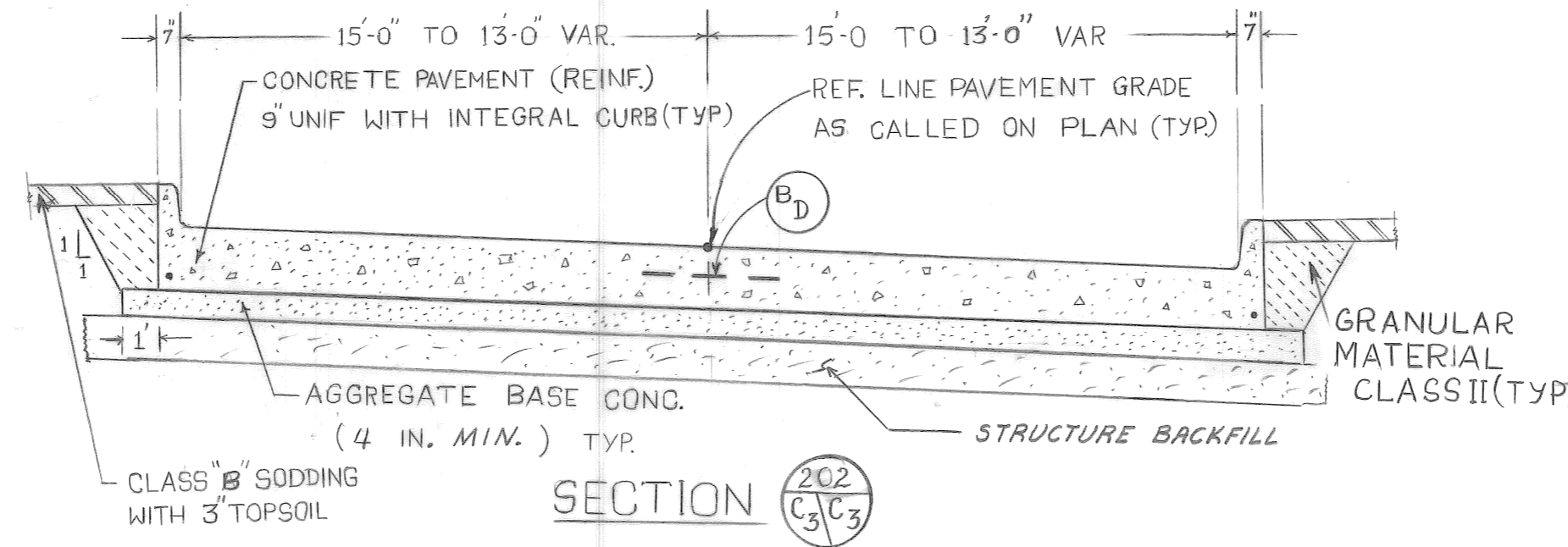
INTEGRAL CURB
NO SCALE



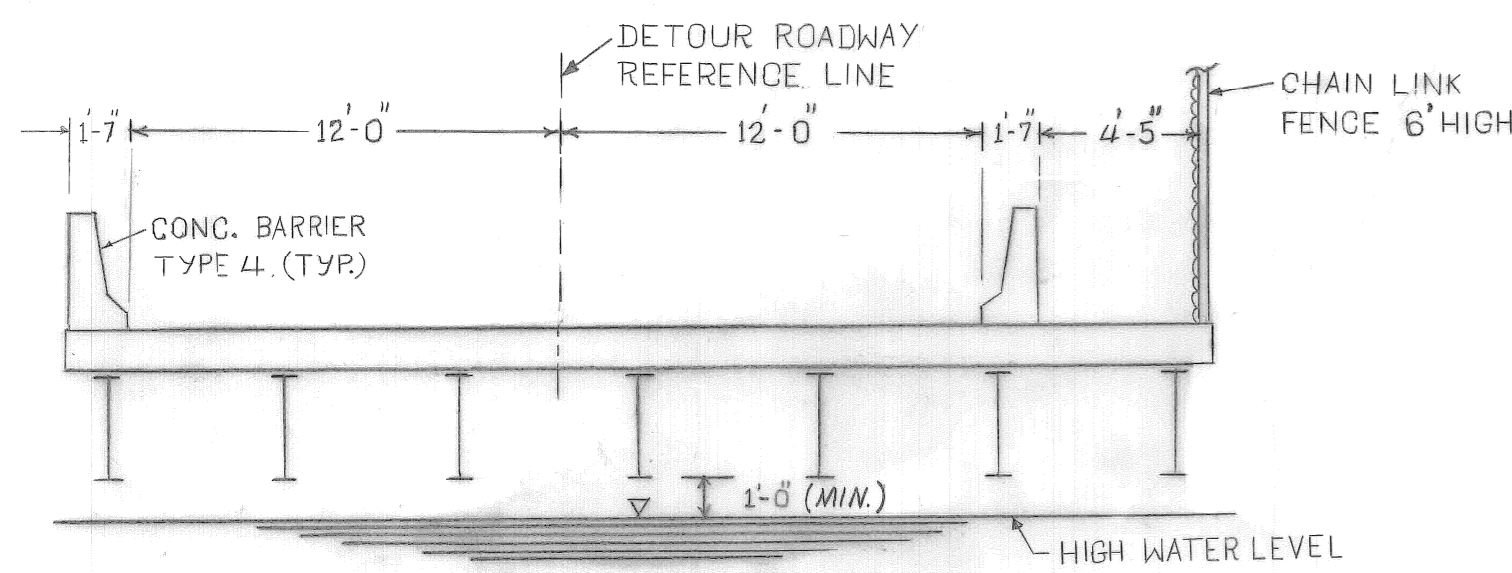
CONCRETE CURB, DETAIL CD
NO SCALE



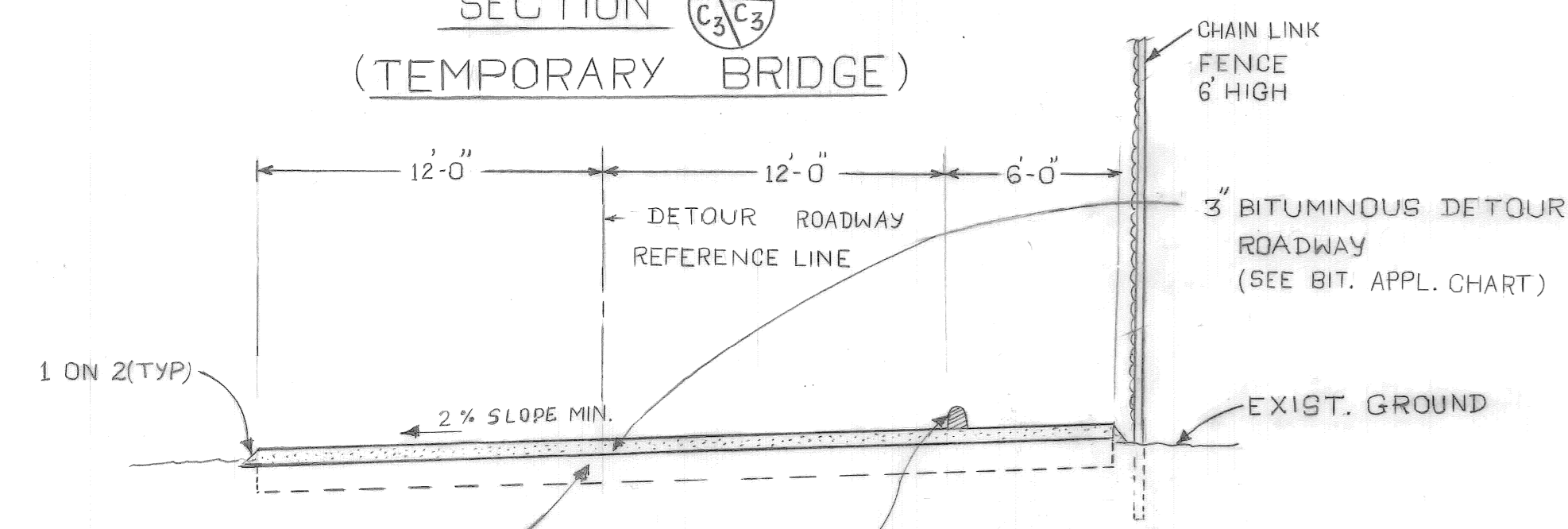
SECTION 201 (RIVERSIDE AVE. APPROACH)
NO SCALE



SECTION 202 (ALTER ROAD APPROACH)
NO SCALE



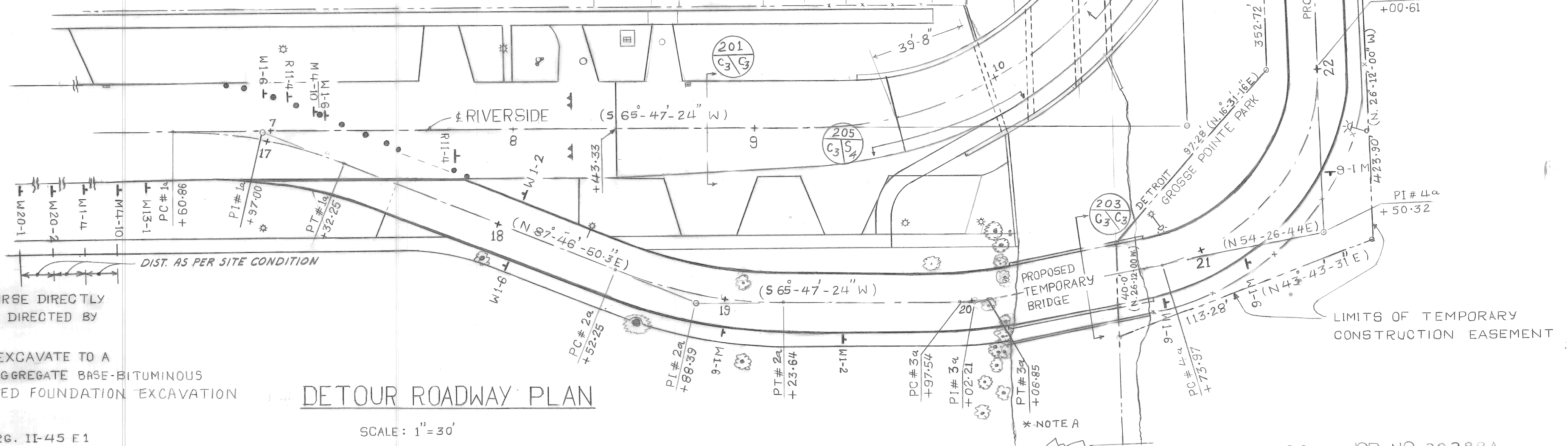
SECTION 203 (TEMPORARY BRIDGE)
NO SCALE



SECTION 204 (TEMPORARY DETOUR ROADWAY)
NO SCALE

JOINT LEGEND

- (B) LONGITUDINAL BULKHEAD JOINT, ACCORDING TO M.D.O.T. STD. DRG. II-41 E
- (D) LONGITUDINAL LANE TIE JOINT WITH TIE BAR, ACCORDING TO M.D.O.T. STD. DRG. II-41 E
- (B) OPTIONAL, B OR D.



DETOUR ROADWAY PLAN
SCALE: 1" = 30'

BITUMINOUS APPLICATION CHART

ITEM	RATE OF APPLICATION	ESTIMATED THICKNESS	ASPHALT PENETRATION	REMARKS / LOCATION
BITUMINOUS MIX. COARSE AGG. CALC	170 LBS / S.YD.	1 1/2 INCHES	120-150	FOR DETOUR ROADWAY
BITUMINOUS MIX. COARSE AGG. CATC	170 LBS / S.YD.	1 1/2 INCHES	120-150	FOR DETOUR ROADWAY
* BITUMINOUS MIX No. 1500L (20AAA)	160 LBS / S.YD.	1 1/2 INCHES	85-100	FROM STA. 13+10.00 TO STA. 13+33.45
* BITUMINOUS MIX No. 1500T (20AAA)	160 LBS / S.YD.	1 1/2 INCHES	85-100	FROM STA. 13+10.00 TO STA. 13+33.45

BITUMINOUS BOND COAT 0-0.10 GAL / S.YD. TO RIGID BASES AND 0-0.05 GAL / S.YD. TACK COAT BETWEEN LAYERS

USE BITUMINOUS MIX NO. 1500L (20AAA) FOR WEDGING REQUIRED BETWEEN STA. 13+10.00 AND STA. 13+33.45

CONSTRUCTION SIGNING LEGEND

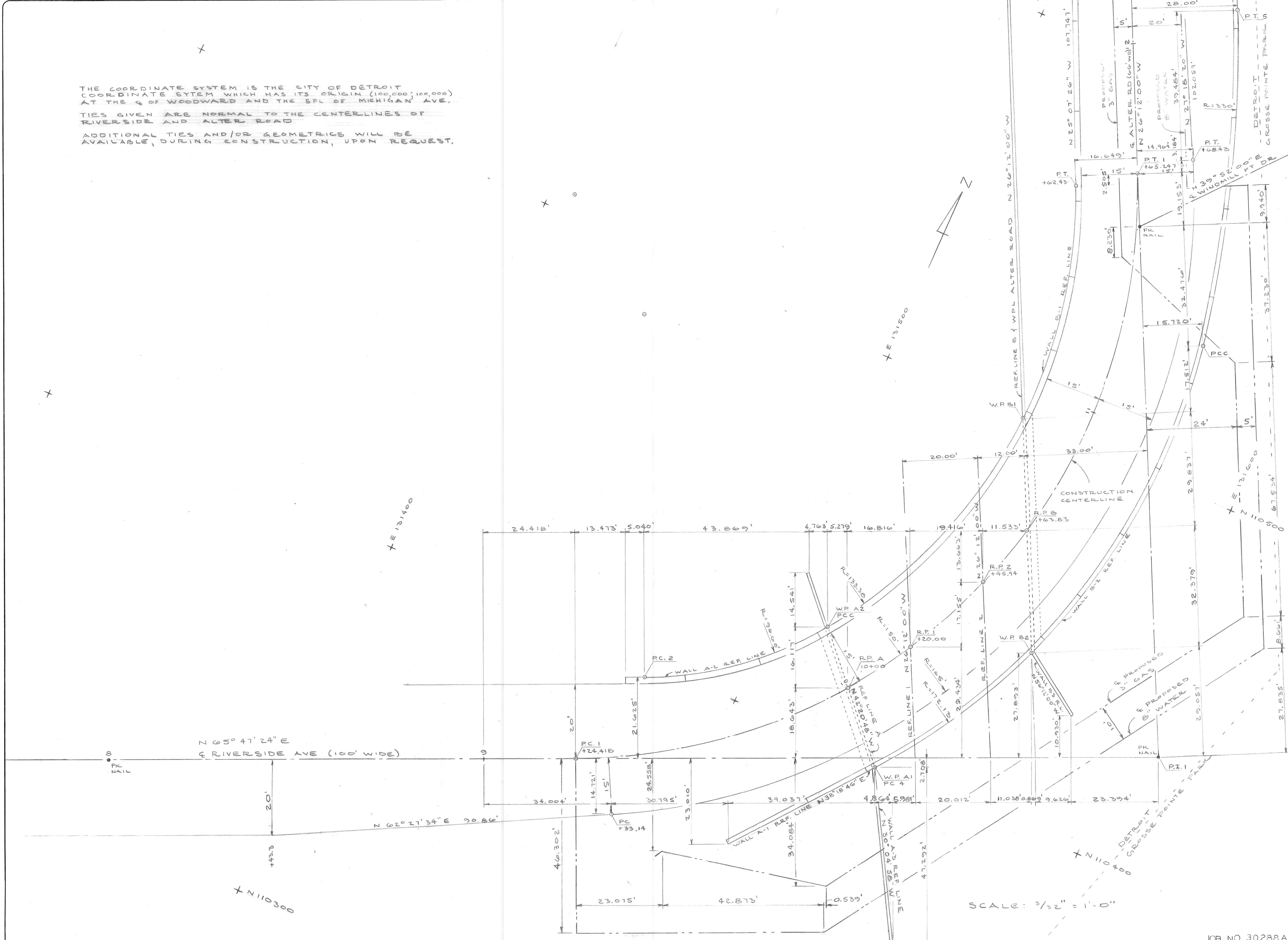
- W20-1 = CONSTRUCTION AHEAD
- W20-2 = DETOUR AHEAD
- ▲▲▲ = BARRICADE TYPE III (LIGHTED)
- M4-10 = DETOUR-LEFT OR RIGHT ARROW
- R11-2 = ROAD CLOSED
- W1-2 = WARNING ARROW
- W1-4 = WARNING ARROW
- W1-6 = WARNING ARROW (TARGET)
- = BARRICADE TYPE II (LIGHTED)
- R11-4 = ROAD CLOSED TO THRU TRAFFIC
- W13-1 = SPEED LIMIT 10 M.P.H.

* PAYMENT FOR AS CONSTRUCTED QUANTITIES OF BITUMINOUS MIX 1500 L & T WILL BE AT UNIT PRICES FOR BITUMINOUS MIX COARSE AGG. CALC & CATC RESPECTIVELY.

NOTE A: THE CONTRACTOR SHALL PROVIDE SIGNAGE AT THE DETROIT RIVER AND NORTH OF THE BRIDGE INDICATING THAT THE CANAL IS CLOSED TO BOAT TRAFFIC DURING CONSTRUCTION. THE SIGNAGE SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER AND BE INCIDENTAL TO SIGN-TYPE A TEMPORARY.

designed by <u>S. PATEL</u> drawn by _____ checked by _____ approved: _____	revisions 1 2 3 4 5 6 7 8 9 10
CITY OF DETROIT city engineering department for	
BRIDGE RECONSTRUCTION RIVERSIDE AVENUE OVER FOX CREEK BW-246 DETOUR ROADWAY	
a.o. 87-22-12 contract no. sheet of drawing no. C-3 date NOV. 89	

THE COORDINATE SYSTEM IS THE CITY OF DETROIT
 COORDINATE SYSTEM WHICH HAS ITS ORIGIN (100,000; 100,000)
 AT THE 4 OF WOODWARD AND THE SPL OF MICHIGAN AVE.
 TIES GIVEN ARE NORMAL TO THE CENTERLINES OF
 RIVERSIDE AND ALTER ROAD
 ADDITIONAL TIES AND/OR GEOMETRICS WILL BE
 AVAILABLE, DURING CONSTRUCTION, UPON REQUEST.



no.	description	date

designed by **RAC**
 drawn by **RAC**
 checked by
 approved:

CITY OF DETROIT
 city engineering department
 for DEPARTMENT OF PUBLIC WORKS

BRIDGE RECONSTRUCTION

RIVERSIDE AVENUE OVER FOX CREEK BW-246

ALIGNMENT AND STAKE OUT

a.o. contract no.
 sheet of drawing no. C-4
 date NOV.89