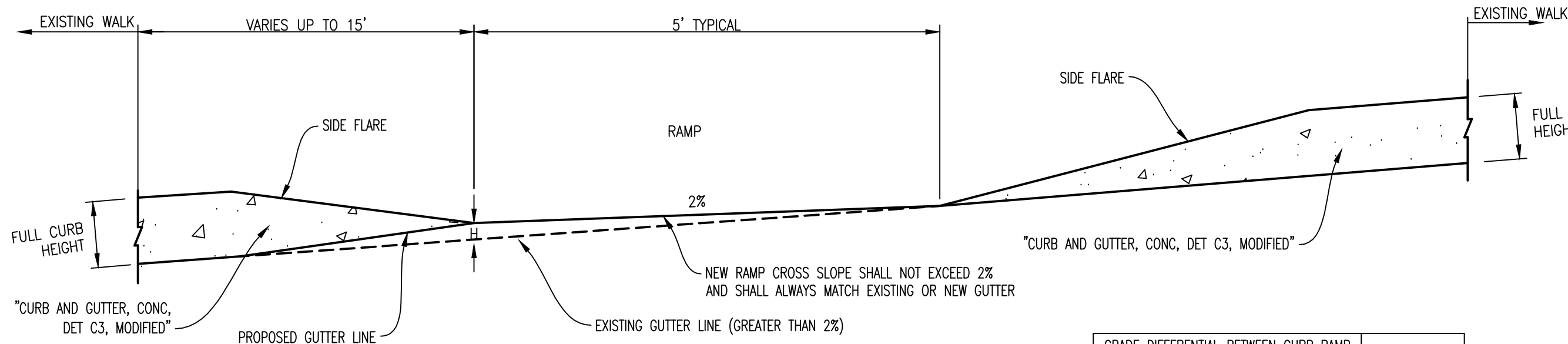


GUTTER PAN TRANSITION DETAIL
NO SCALE

TRANSITION NOTES

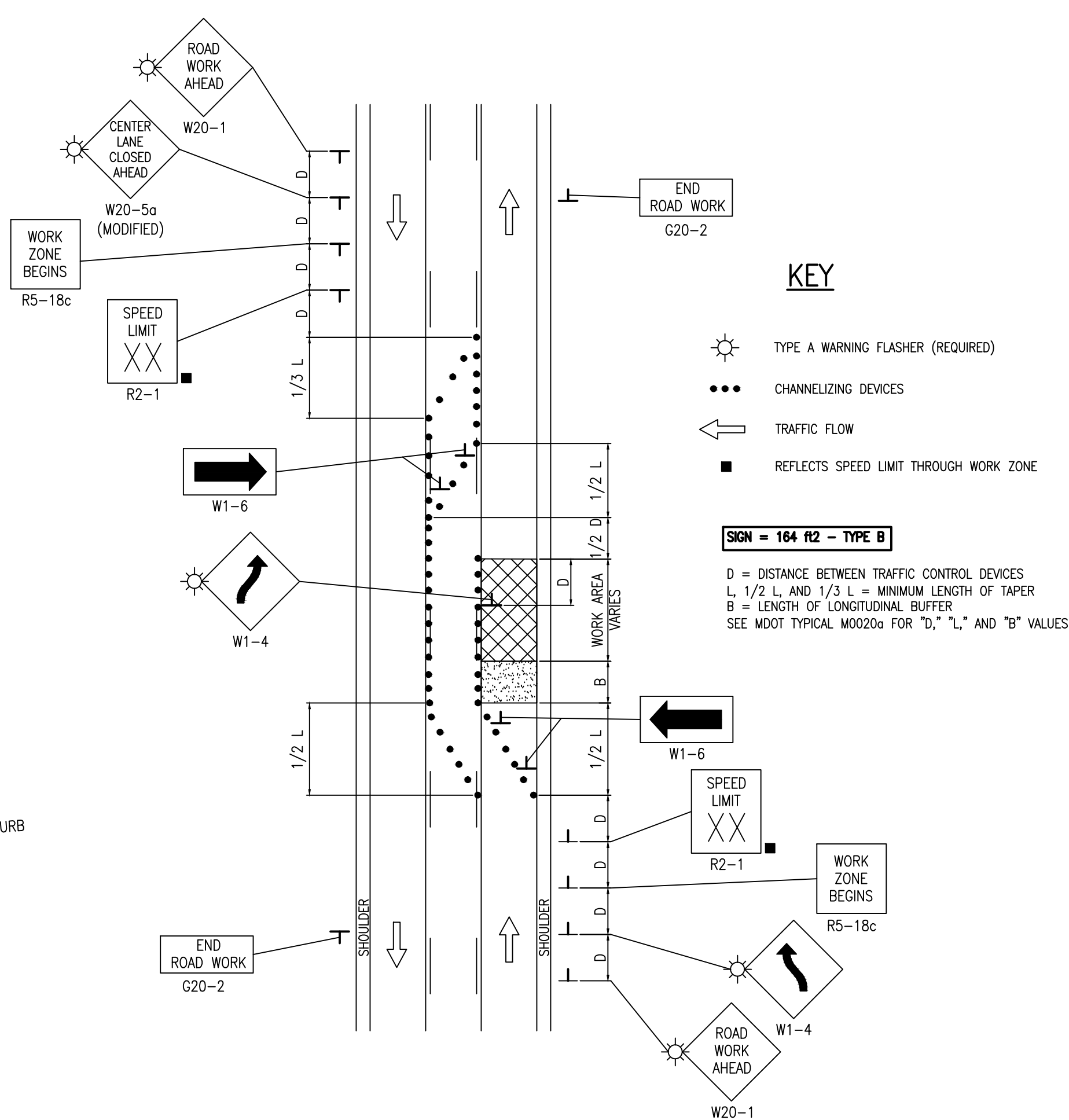
- CONTRACTOR SHALL INSTALL A GUTTER PAN TRANSITION ONLY WHEN THE EXISTING GUTTER GRADES EXCEED 2% IN FRONT OF THE PROPOSED RAMP FACE.
- A GUTTER PAN TRANSITION TIES THE CROSS SLOPE OF THE NEW CURB RAMP TO THE EXISTING GUTTER GRADES. ROADWAY TRANSITIONS SHALL NOT EXTEND MORE THAN 18" (WIDTH OF GUTTER PAN) INTO THE EXISTING ROAD.



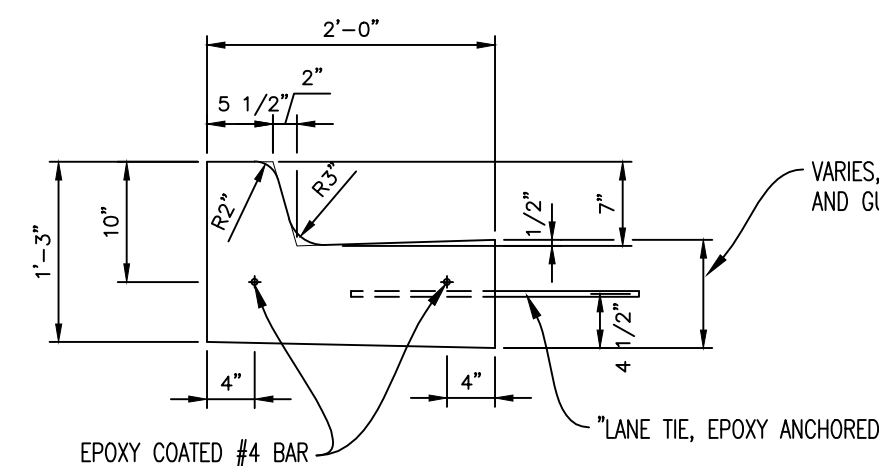
GUTTER PAN TRANSITION PROFILE
NO SCALE

GRADE DIFFERENTIAL BETWEEN CURB RAMP CROSS SLOPE AND ROADWAY GRADE	"H"
1%	0.04' (1/2")
2%	0.08' (1")
3%	0.12' (1-1/2")
4%	0.16' (2")
5%	0.20' (2-1/2")

HMA APPLICATION CHART				
IDENT NO.	ITEM	RATE OF APPLICATION	PERFORMANCE GRADE	COMMENTS
HP	HAND PATCHING	330 LB/SYD	64-22	HAND PATCHING NEEDED TO PROPERLY INSTALL CURB SECTIONS AND NEW RAMP CONFIGURATIONS



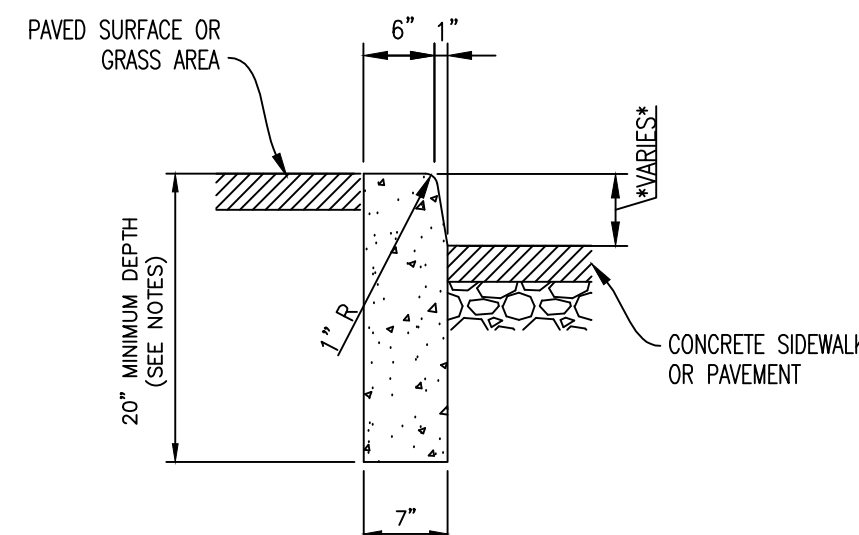
SPECIAL TRAFFIC DETAIL A
NO SCALE



CURB AND GUTTER NOTES

- LANE TIES SHALL BE INSTALLED WHETHER THE CURB AND GUTTER IS PLACED ADJACENT TO CONCRETE PAVEMENT OR CONCRETE BASE PAVEMENT WITH AN ASPHALT OVERLAY. IN EITHER CASE, THE LANE TIES SHALL BE ANCHORED IN THE CONCRETE PORTION OF THE PAVEMENT.
- THE BOTTOM OF THE PROPOSED CURB AND GUTTER SHALL, AT LEAST, MEET THE BOTTOM OF THE ADJACENT EXISTING PAVEMENT.

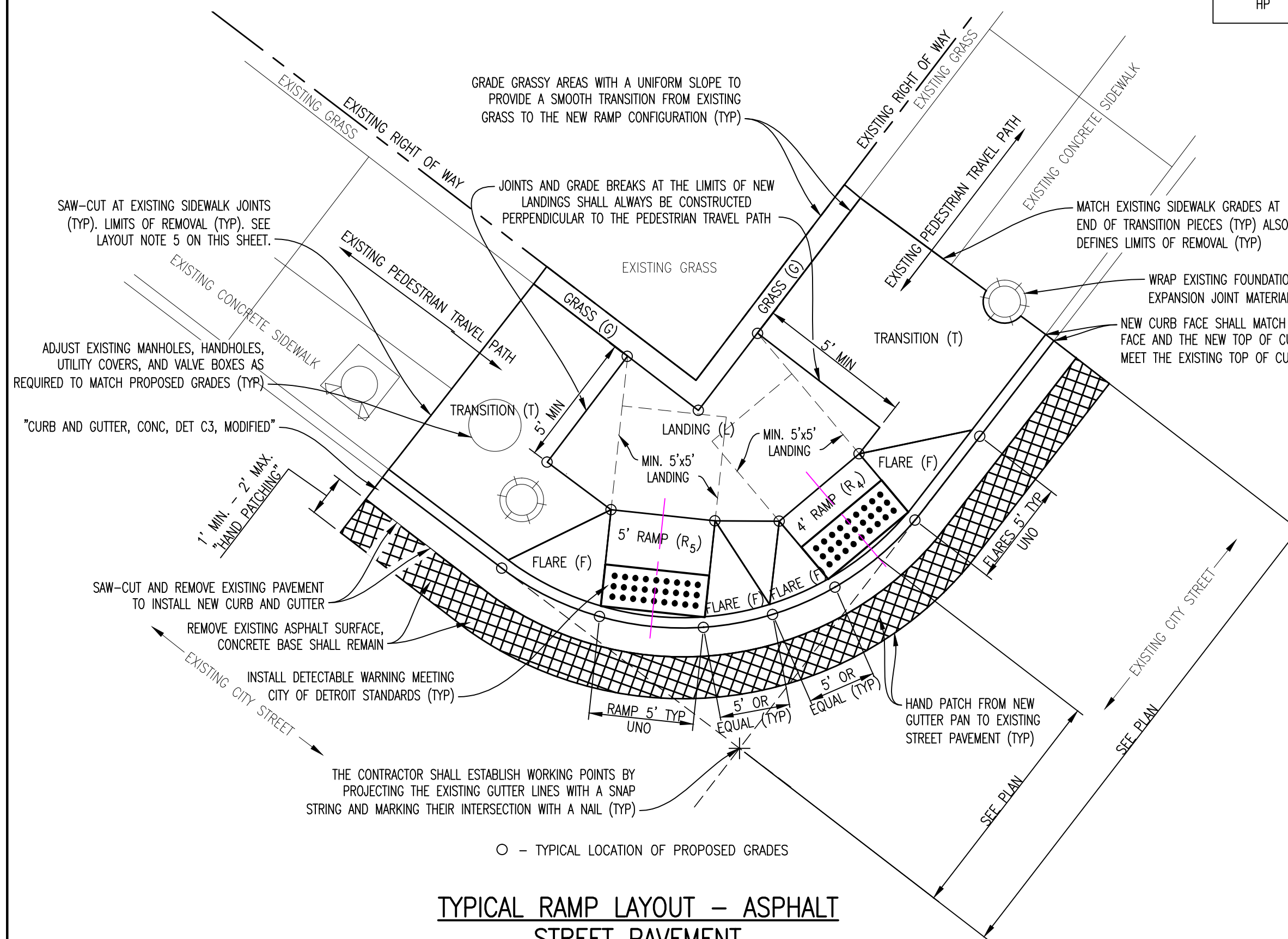
CURB AND GUTTER, CONC, DET C3, MODIFIED
NO SCALE



NOTES:

- DEPTH OF CURB SHALL BE 20" UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- THIS CURB MAY BE USED WITH CONCRETE ON BOTH SIDES OF THE NEW CURB.
- THIS CURB SHALL NOT BE USED ADJACENT TO STREET PAVEMENT UNLESS SPECIFICALLY CALLED FOR ON THE PLANS.

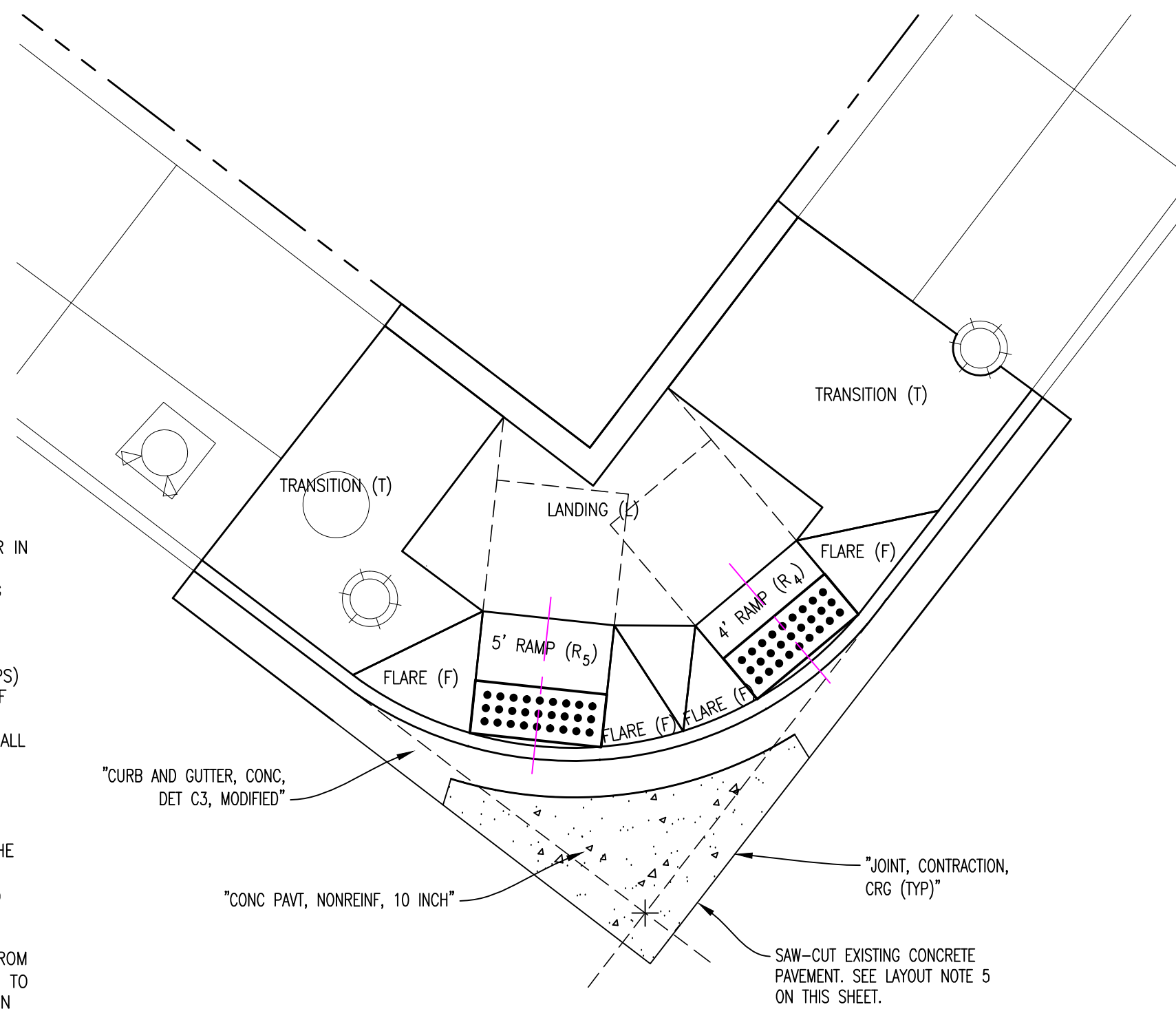
CURB, CONCRETE, DETAIL CD, MODIFIED
NO SCALE



TYPICAL RAMP LAYOUT - ASPHALT STREET PAVEMENT
NO SCALE

LAYOUT NOTES

- CONTRACTOR SHALL INSTALL THE NEW CURB AND GUTTER IN THE SAME ALIGNMENT AS THE EXISTING CURB/GUTTER UNLESS OTHERWISE NOTED ON THE PLANS. THE EXISTING CURB RADIUS SHALL BE MAINTAINED UNLESS SHOWN OTHERWISE ON THE PLAN.
- R₅ - DENOTES LENGTH OF NEW RAMP - 5 FEET MEASURED FROM THE BACK OF THE PROPOSED CURB (RADIAL RAMPS) OR MEASURED FROM THE GRADE BREAK AT THE FOOT OF THE RAMP (DIRECTIONAL RAMPS).
- EXISTING POLE, PEDESTAL OR BUILDING FOUNDATIONS SHALL HAVE 1" ISOLATION (EXPANSION) JOINT MATERIAL PLACED AROUND THE EXISTING FOUNDATION WHEN POURING NEW CONCRETE. INCIDENTAL TO "SIDEWALK, CONC, 4 INCH, MODIFIED"
- BOLD LINES OUTLINING THE VARIOUS COMPONENTS OF THE RAMP LAYOUT (LANDING (L), RAMP (R), FLARE (F), TRANSITION (T), SIDEWALK (S/W)) ARE NOT INTENDED TO INDICATE THE LOCATION OF NEW SIDEWALK JOINTS. SEE GENERAL NOTE 12 ON SHEET 3.
- WHEN SAW-CUTTING FOR REMOVALS, IF THE DISTANCE FROM THE NEAREST EXISTING PAVEMENT OR SIDEWALK JOINT(S) TO THE EDGE OF THE SAW-CUT IS LESS THAN 3 FEET, THEN THE PAVEMENT OR SIDEWALK SHALL BE REMOVED AND REPLACED TO THAT JOINT.



TYPICAL RAMP LAYOUT - CONCRETE STREET PAVEMENT
NO SCALE